

MANDELA STATION

@ West Oakland BART

PRELIMINARY DEVELOPMENT PLAN

July 24, 2020
Amended September 16, 2020



HENSEL PHELPS



GENERAL

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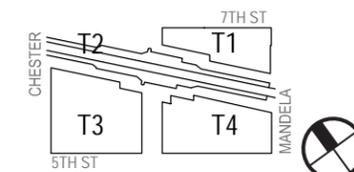
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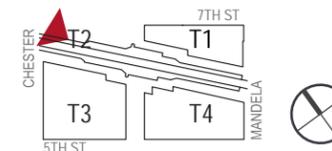


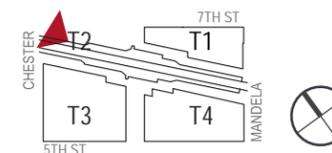
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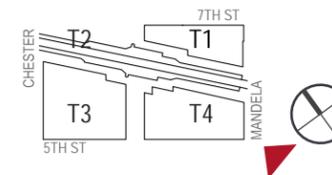
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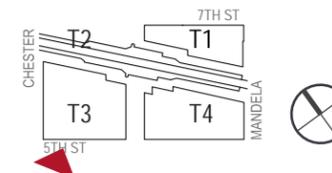
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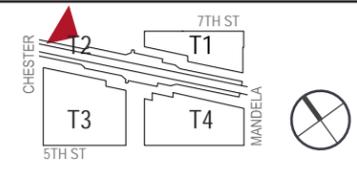
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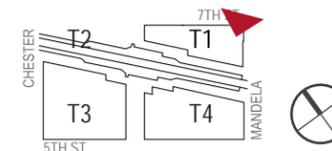


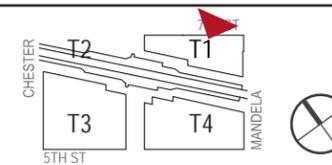


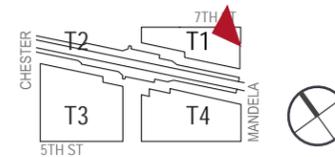


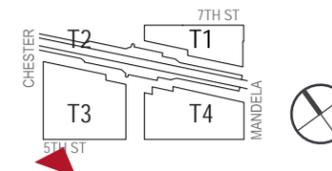


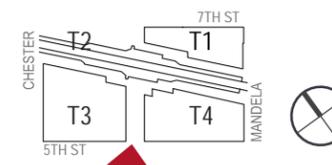


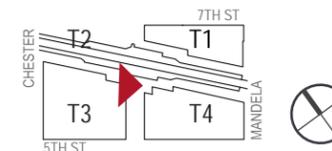


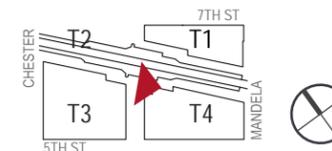


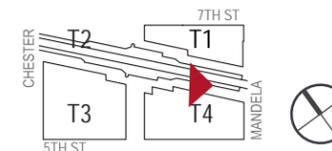


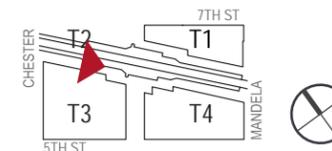


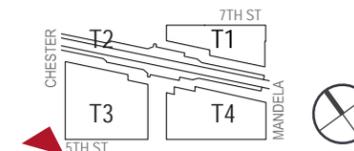


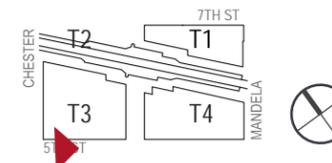


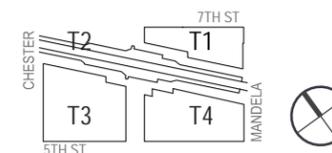


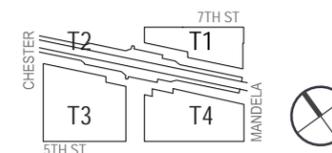


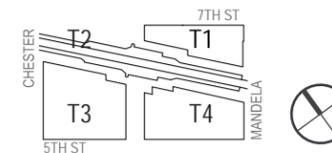


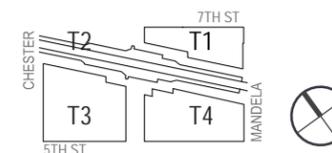


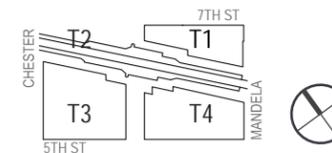












SUMMARY OF PROPOSED ENTITLEMENT CHANGES

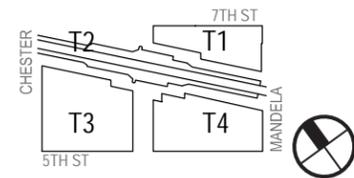
Development Program

- 
 T1 – eliminated the 82,460sf of office, and added 22 market rate units that have been transferred from T3; reduced retail from 17,185sf to 14,350sf; increased to 31 stories
- 
 T2 – eliminated the under-track retail kiosks
- 
 T3 – moved the 22 market rate units to T1
- 
 T4 – reduced the retail from 30,800sf to 23,184sf
- 
 Parking – changed the shared 400 car parking garage to 3 separate garages: T1-125 spaces; T3-50 spaces; T4- 210spaces
- 
 Requested State Affordable Housing Bonus waivers for parking and open space.

Revision Cloud Key



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PROJ. # 168-153 WO BART
DATE: July 24, 2020

SUMMARY OF PROPOSED ENTITLEMENT REVISIONS

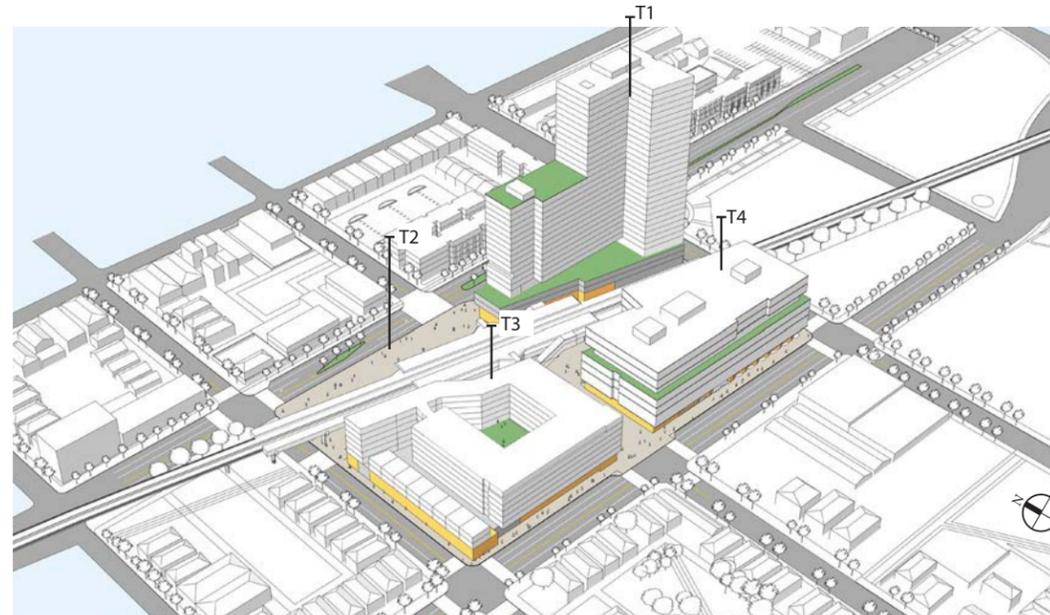
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Proposed Program

| USES | T1 | T2 | T3 | T4 | Program Total |
|-----------------------|-----------|------------------------|------------|------------|------------------------------------|
| OFFICE | | | | 300,000 sf | 300,000 sf |
| RETAIL | 14,350 sf | Flexible Kiosk Program | 15,944 sf | 23,184 sf | 53,478 sf |
| OTHER NON-RESIDENTIAL | | | 2,057 sf | 8,540 sf | 10,597 sf |
| RESIDENTIAL | 522 units | | 240 units* | | 762 units (20% min. affordable) |
| PARKING | 125 | | 50 | 210 | 385 spaces |

*79 units at 30% AMI, 11 units at 50% AMI, 148 units at 60% AMI, 2 units at 120% AMI

Note: Program is based on the previously approved Conditions of Approval, As approved by the Planning Commission February 6, 2019.



Open Space Calculation

| | | |
|-----------------------------------|-----|-------------------|
| Residential Open Space Req | | |
| 240 Units | 200 | 48,000 SF |
| 522 Units | 200 | 104,400 SF |
| Required Open Space | | 152,400 SF |
| Open Space Provided | | |
| T1 Open Space | | 17,648 SF |
| T3 Open Space | | 24,815 SF |
| | | 42,463* SF |

Approximate Public Open Space Provided

| | |
|-------------------|-----------|
| Mandela Plaza | 30,032 SF |
| Under Track Plaza | 8,464 SF |
| Art Alley | 20,923 SF |
| Center Square | 15,949 SF |
| 75,368 SF | |

Note:

The Applicant is requesting a waiver/reduction of the group usable open space requirements for T-1 and for T-3 pursuant to Government Code section 65915 (e); and

The Applicant is requesting a reduction in the amount of parking required for T-1 and T-3 pursuant to Government Code section 65915 (p)

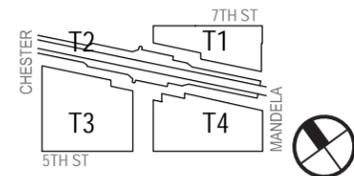
The Hall of Fame Plaques will be removed from the right of way to be relocated back into the 7th Street right of way at an appropriate realigned location by the completion of T1.

The dancing lights will be removed from the right of way and there is no plan for their relocation as part of the FDP at this time.



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**ZONING
INFO**

SHEET: A-01.01

| GENERAL ZONING INFORMATION | REFERENCE | NOTES |
|---|----------------|---|
| ASSESORS BLOCK | Survey | See Survey; Tract 8046, Blocks 494 and 493 |
| ZONING USE DISTRICT | Oak GIS | S-15W |
| PERMITTED AND/OR CONDITIONAL USES | 17.97.010-.020 | Residential, Community Assembly, General Retail, Full +Limited Service Retail, Parking, Commercial Office permitted |
| HEIGHT & BULK DISTRICT | 17.97 | S-15W 60' and S-15W 100'; master plan consistent with zoning requirements |
| GENERAL PLAN / POLICY PLAN | OAK-GIS | General Commercial; West Oakland Specific Plan |
| HISTORIC OR LANDMARK STATUS | OAK-GIS | None |
| LIQUEFACTION HAZARD ZONE | OAK-GIS | Severity 4 |
| CONDO CONVERSION IMPACT AREA | OAK-GIS | None |
| HEIGHT AND BULK CONTROLS | | |
| SITE AREA | Survey | 241,282 SF |
| FLOOR AREA RATIO (FAR) | 17.97.130 | S-15W 60' - 3.0; S-15W 100' - 5.0; Master Plan within zoning density requirements |
| HEIGHT LIMIT | 17.97 | 60' and 100' (Height limit modified to allow 80' and 320' tall building pursuant to State Affordable Housing Exemption) |
| REQUIRED SETBACKS | 17.97.060 | No Front Yard Setbacks Required; Interior Lot subject to PUD |
| REAR YARDS / COURTS | | None Required |
| ADJACENCIES | | None Significant |
| UNIT SEPARATION / EXPOSURE REQUIREMENTS | 17.108.080 | 8' minimum at living room window +2' for each floor above = maximum 10% of It width |
| DETAILED CONTROLS & REQUIREMENTS | | |
| RESIDENTIAL DENSITY LIMITS | 17.97.130 | S-15W 60' - 375sf/unit; S-15W100' - 225sf/unit; Density increase per State Affordable Housing and PUD density bonus |
| OPEN SPACE REQUIREMENTS | 17.97.130 | S-15W 60' - 150sf/unit; and S-15W 100' - 75sf/unit; Overall master plan within zoning limites |
| SCREENING & SETBACK OF PARKING & LOADING | 17.116.290 | All parking garages are screened per zoning requirements |
| OFF-STREET PARKING - RESIDENTIAL | 17.116.060 | 0.5 parking space per dwelling unit required; Parking meets zoning requirements with approved reductions |
| OFF-STREET PARKING - RETAIL | 17.116.080 | 1 space/600 Sf of ground floor; Parking meets zoning requirements with approved reductions |
| OFF-STREET PARKING - COMMERCIAL | 17.116.080 | None required |
| OFF-STREET PARKING DIMENSIONS | 17.116.200 | 50-50 compact / standard; or 75% intermediate + 12.5% compact |
| OFF-STREET DRIVE AISLE DIMENSIONS | 17.116.210 | 21'-24' two way aisle widths |
| OFF-STREET LOADING - RESIDENTIAL | 17.116.120 | Loading per zoning requirements |
| OFF-STREET LOADING - RETAIL | 17.116.150 | Loading per zoning requirements |
| LOADING BERTH DIMENSIONS | 17.116.220 | 10'x23', 12' high for residential activities, 10'x25', 8' high for undertaking service commercial activities |
| BICYCLE REQUIREMENTS - RESIDENTIAL+COMMERCIAL | 17.117.090 | LT: total 221 spaces; ST: total 54 spaces LT: 1 per 10,000 sf of commercial; ST: 1 per 20,000 sf of commercial LT: 0.25 spaces per dwelling unit; ST: 0.05 per dwelling unit; bicycle parking provide per zoning code |
| BICYCLE REQUIREMENTS - RETAIL | 17.117.110 | LT: total 6 spaces; ST: total 28 spaces LT: 1 per 12k; ST: 1 per 2k; bicycle parking provided per zoning code |

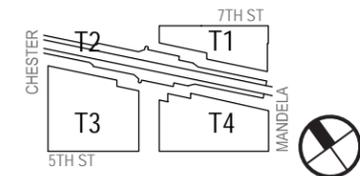
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*The dancing lights will be removed from the right of way and there is no plan for their relocation as part of the FDP at this time.



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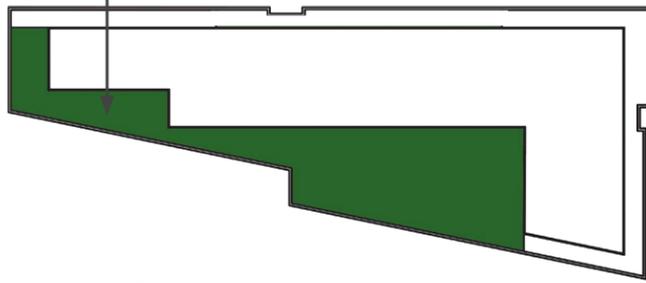
**ZONING
SUMMARY**

SHEET:

A-01.02

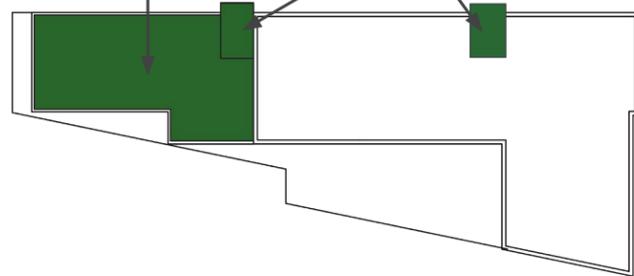
BUILDING T1

Landscaped Terrace
Common Open Space



BUILDING T1 - LEVEL 4 - AMENITY

Common Use Terrace
Level 5-31 - Common Use Decks

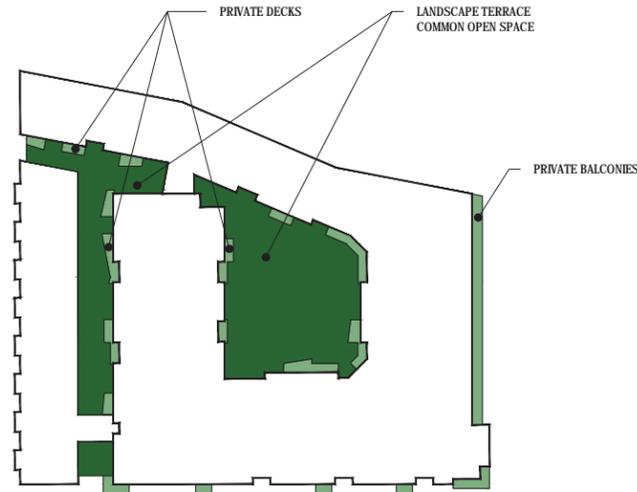


BUILDING T1 - LEVEL 19 - AMENITY

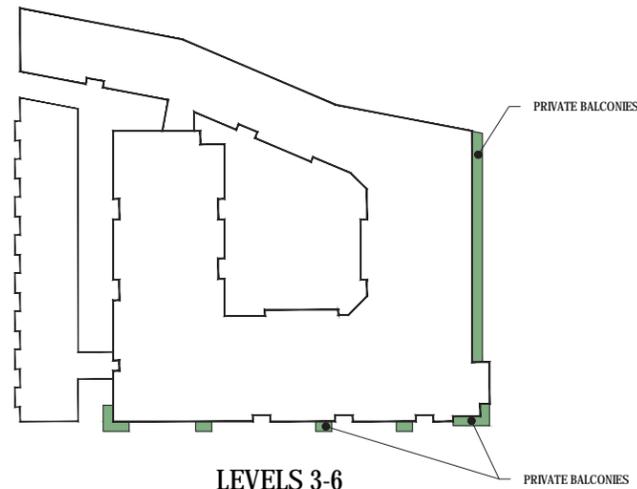
| | |
|------------------|-------------------|
| Common Use Decks | 2,334 SF |
| Roof Terraces | 15,314 SF |
| TOTAL | 17,648* SF |

*Open space waiver of 86,752 sf requested for T1 per Density Bonus Letter.

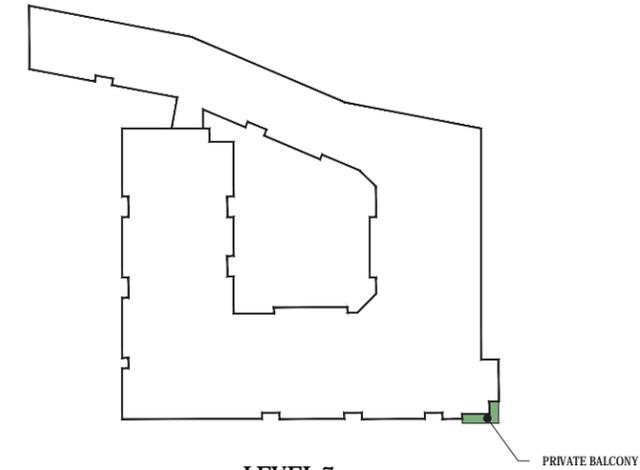
BUILDING T3



LEVEL 2



LEVELS 3-6



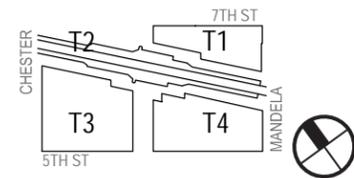
LEVEL 7

| LEVEL | COMMON | PRIVATE |
|----------------------------|--------------------|-----------------------|
| LV2 | 10,187 SF | 2,528 SF |
| LV3 | | 1,161 SF |
| LV4 | | 1,161 SF |
| LV5 | | 1,161 SF |
| LV6 | | 1,161 SF |
| LV7 | | 142 SF |
| SUBTOTAL | 10,187 SF | 14,628 SF (X2) |
| TOTAL T3 OPEN SPACE | 24,815 SF * | |

*Open space waiver of 23,185 sf requested for T3 per Density Bonus Letter



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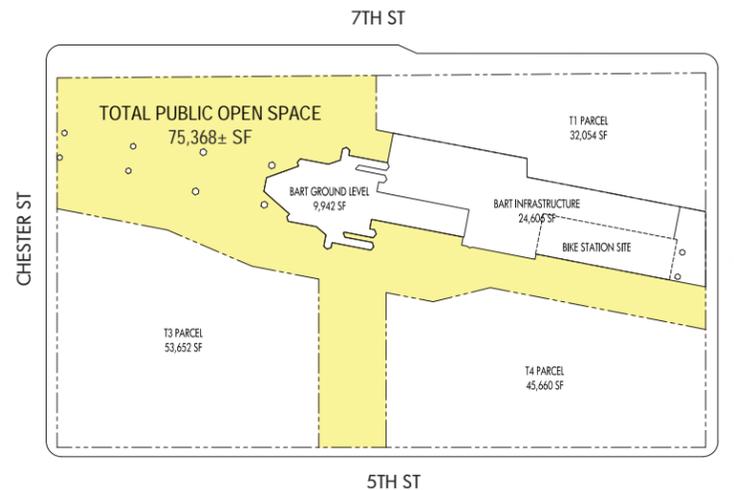
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SHEET: OPEN SPACE ANALYSIS
A-01.03

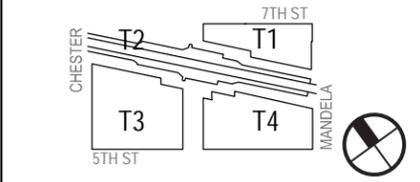
Note: The Applicant is requesting a waiver/reduction of the group usable open space requirements for T-1 and for T-3 pursuant to Government Code section 65915 (e);

PUBLIC OPEN SPACE

GROUND LEVEL - PUBLIC OPEN SPACE 75,368± SF

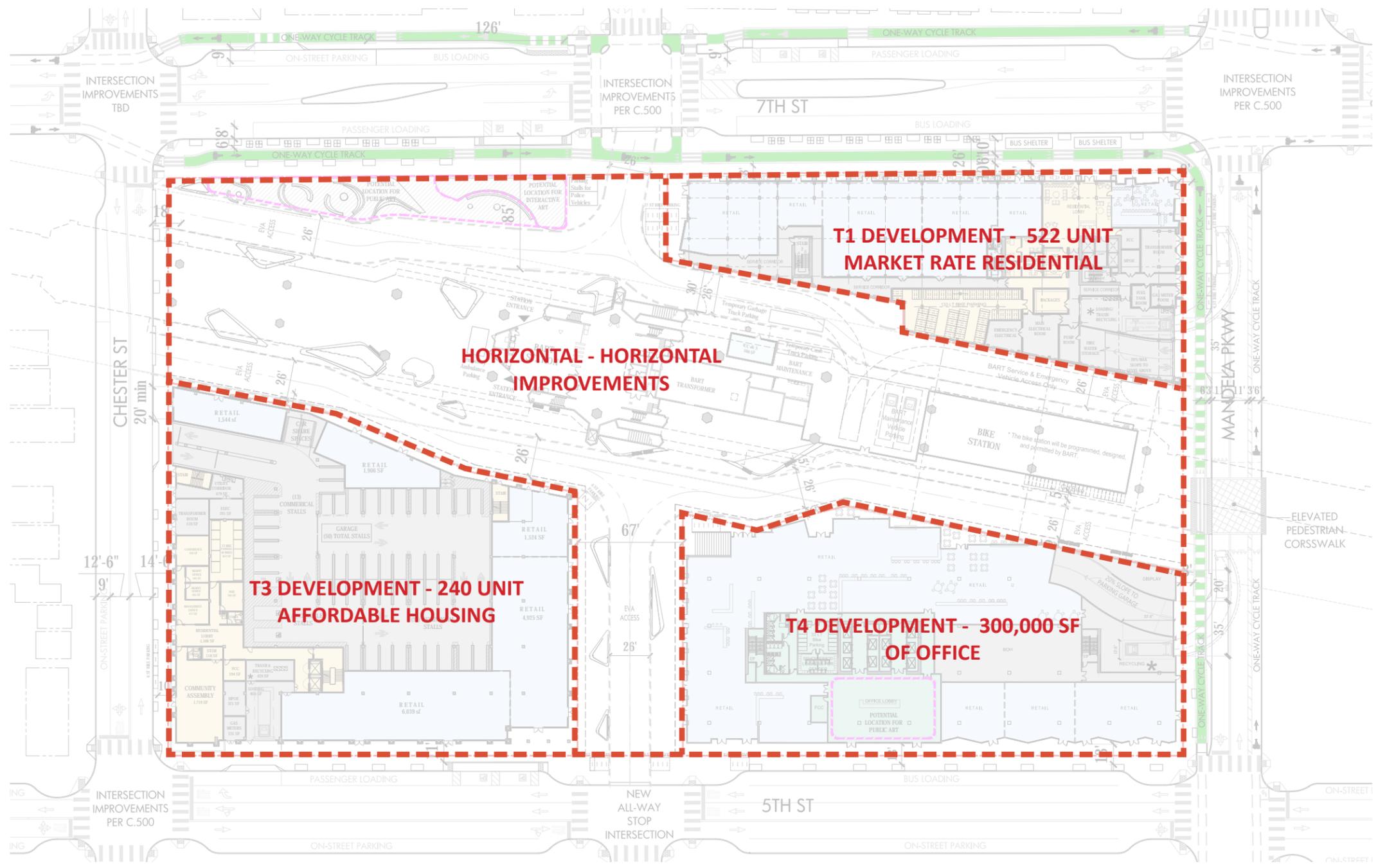


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PROJECT SCHEDULE

| | | | | | | | |
|---|---|---|--|---|---|---|--|
| 1. Horizontal | | | | 3. Phase II – T1 Development & T2 Development (plus PZ/PX for that phase) | | | |
| i. | Submit application for Final Development Plan | 1 year following PDP approval | | i. | Submit application for Final Development Plan | 3 years following PDP approval | |
| ii. | Anticipated FDP approval date by Agency | 1 year following submittal of Horizontal FDP application | | ii. | Anticipated FDP approval date by Agency | 1 year following submittal of Phase II FDP application | |
| iii. | Commence construction – Initial PX and/or PZ job (additional PX and PZ jobs will be tied to each later phase) | 2 years following FDP approval | | iii. | Commence construction | The latter of 2 years following FDP approval or 2 years following Phase I commencement of construction | |
| 2. Phase I – T3 Development (plus PZ/PX for that phase) | | | | 4. Phase III – T4 Development (plus PZ/PX for that phase) | | | |
| i. | Submit application for Final Development Plan | 1 year following PDP approval | | i. | Submit application for Final Development Plan | 5 years following PDP approval | |
| ii. | Anticipated FDP approval date by Agency | 1 year following submittal of Phase I FDP application | | ii. | Anticipated FDP approval date by Agency | 1 year following submittal of Phase III FDP application | |
| iii. | Commence construction | 2 years following FDP approval (allowing time to secure affordable financing) | | iii. | Commence construction | The latter of 2 years following FDP approval or 2 years following Phase II commencement of construction | |

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**PHASING
 PLAN**

SHEET: A-01.04

LEED CS v4 SCORECARD

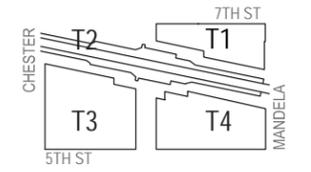
West Oakland Station T4

| | Confirmed | | | | Phase | Credit Number | Credit Name | Points Available |
|---------------------------|-------------------|-------|----|-----------|--------|--|--|--|
| | Likely | Maybe | No | Confirmed | | | | |
| IP | 1 | | | | D | Credit | Integrative Process - In design phases, achieve synergies between building, energy AND water related systems | 1 |
| | 1 | | | | | | | 1 |
| LOCATION & TRANSPORTATION | | | | 20 | D | Credit | LEED for Neighborhood Development Location - Locate within LEED ND certified development site boundary | 20 |
| | 2 | | | | D | Credit | Sensitive Land Protection - Develop on previously developed land or follow criteria for non-sensitive | 2 |
| | 2 | | | 1 | D | Credit | High Priority Site - Locate project on infill location in historic district, priority designation or brownfield | 3 |
| | 2 | 2 | | 2 | D | Credit | Surrounding Density & Diverse Uses - Site within 1/4 mile of surrounding density criteria and/or a 1/2 mile of diverse uses | 6 |
| | 6 | | | | D | Credit | Access to Quality Transit - Locate functional entries within 1/4 mile of existing transit or 1/2 mile of planned transit services | 6 |
| | 1 | | | | D | Credit | Bicycle Facilities - Provide a bike network and storage areas | 1 |
| | 1 | | | | D | Credit | Reduced Parking Footprint - Don't exceed minimum local code requirements for parking capacity | 1 |
| | 1 | | | | D | Credit | Green Vehicles - 5% of spaces or 20% discount for parking and electric car charging OR liquid, gas or battery facilities | 1 |
| | 12 | 5 | | 20 | | | Totals | 20 |
| | SUSTAINABLE SITES | Yes | | | | C | Prereq | Construction Activity Pollution Prevention - Implement an erosion control plan, per the EPA CGP v2012 |
| | | | | 1 | D | Credit | Site Assessment - Complete site survey including: topography, hydrology, climate, vegetation, soils, human use, human health | 1 |
| | | | | 2 | D | Credit | Site Development - Protect or Restore Habitat - Preserve 40% of greenfield AND on-site restoration OR financial support | 2 |
| 1 | | | | | D | Credit | Open Space - Provide outdoor space greater than or equal to 30% of total site area, 25% of which is vegetated | 1 |
| | | | | 3 | D | Credit | Rainwater Management - Manage runoff for at least the 85th percentile of local rainfall events | 3 |
| | | 1 | | 1 | D | Credit | Heat Island Reduction - Meet nonroof and roof criteria OR place a minimum of 75% parking spaces under cover | 2 |
| 1 | | | | | D | Credit | Light Pollution Reduction - Backlight-uplight-glare method or calculation method, exterior luminaires and signage requirements | 1 |
| 1 | | | | D | Credit | Tenant Design and Construction Guidelines - Provide Manual or automative glare control devices | 1 | |
| 3 | 1 | | 7 | | | Totals | 11 | |
| WATER | Yes | | | | D | Prereq 1 | Outdoor Water Use Reduction - Permanent non-irrigated landscape OR reduce landscape water use 30% for peak watering month | NA |
| | Yes | | | | D | Prereq 2 | Indoor Water Use Reduction - Reduce aggregate water use by 20% for fixtures and fittings | NA |
| | Yes | | | | D | Prereq 3 | Building-Level Water Metering - Install permanent water meters that measure potable water use, share data with USGBC | NA |
| | 1 | | | 1 | D | Credit | Outdoor Water Use Reduction - Reduce water use no irrigation or reduced irrigation 50% - 100% | 2 |
| | 3 | | | 3 | D | Credit | Indoor Water Use Reduction - Reduce fixture and fitting water use by 25% - 50% | 6 |
| | | | 2 | D | Credit | Cooling Tower Water Use - Conduct a one-time potable water analysis, measure control parameters in Table 1 | 2 | |
| | | | 1 | D | Credit | Water Metering - Meters for 2 or more water subsystems: irrigation, indoor plumbing, hot water, boiler, reclaimed water, or other | 1 | |
| 4 | | | 7 | | | Totals | 11 | |
| ENERGY & ATMOSPHERE | Yes | | | | C | Prereq 1 | Fundamental Commissioning and Verification - Commissioning for ASHRAE 0-2005 and 1.1-2007 | NA |
| | Yes | | | | D | Prereq 2 | Minimum Energy Performance - Whole building energy simulation OR ASHRAE 50% Design Guide OR ABCPG | NA |
| | Yes | | | | D | Prereq 3 | Building-Level Energy Metering - Use building-level energy meters or submeters that can aggregate building-level data | NA |
| | Yes | | | | D | Prereq 4 | Fundamental Refrigerant Management - Do not use CFC-based refrigerants in HVAC&R systems, or have a phase out plan | NA |
| | 3 | 1 | | 2 | C | Credit | Enhanced Commissioning - Implement systems commissioning or monitor-based commissioning | 6 |
| | 5 | 2 | | 11 | D | Credit | Optimize Energy Performance - Whole building energy simulation or follow ASHRAE Advanced Energy Design Guide | 18 |
| | | | | 1 | D | Credit | Advanced Energy Metering - Install advanced energy metering for whole building and individual energy sources | 1 |
| | | | | 2 | C | Credit | Demand Response - Participate in existing demand response program or provide infrastructure for demand response programs | 2 |
| | | | | 3 | D | Credit | Renewable Energy Production - Use renewable energy system to meet 1-10% of usage | 3 |
| | | 1 | | | D | Credit | Enhanced Refrigerant Management - Refrigerants with ODP of 0 and GWP of less than 50 OR calculate refrigerant impact | 1 |
| | | | 2 | C | Credit | Green Power and Carbon Offsets - Use 50-100% green power or carbon offsets | 2 | |
| 8 | 6 | | 19 | | | Totals | 33 | |

| | Confirmed | | | | Phase | Credit Number | Credit Name | Points Available |
|---|-----------|-------|----|-----------|-------|---------------|---|-------------------|
| | Likely | Maybe | No | Confirmed | | | | |
| MATERIALS & RESOURCES | Yes | | | | D | Prereq | Storage and Collection of Recyclables - Dedicated areas for waste collection, collection and storage | NA |
| | Yes | | | | D | Prereq | Construction and Demolition Waste Management Planning - Establish C&D waste diversion goals | NA |
| | | 3 | | 3 | C | Credit | Building Life-Cycle Impact Reduction - Historic building reuse, renovate blighted buildings OR whole building LCA | 6 |
| | | 1 | | 1 | C | Credit | Building Product Disclosure and Optimization - Environmental Product Declarations | 2 |
| | | 1 | | 1 | C | Credit | Building Product Disclosure and Optimization - Sourcing of Raw Materials | 2 |
| | | 1 | | 1 | C | Credit | Building Product Disclosure and Optimization - Material Ingredients | 2 |
| | 1 | 6 | | 7 | | | Totals | 14 |
| INDOOR ENVIRONMENTAL | Yes | | | | D | Prereq | Minimum Indoor Air Quality Performance - Meet ASHRAE 62.1-2010 | NA |
| | Yes | | | | D | Prereq | Environmental Tobacco Smoke Control - Prohibit smoking indoors, restrict outdoor smoking within 25 feet | NA |
| | 1 | 1 | | | D | Credit | Enhanced Indoor Air Quality Strategies - Comply with enhanced IAQ strategies | 2 |
| | 1 | 1 | | 1 | C | Credit | Low-Emitting Materials - Achieve level of compliance for product categories or use budget calculation method | 3 |
| | | | | 1 | C | Credit | Construction IAQM Plan - Implement IAQMP & protect materials and equipment during construction | 1 |
| | | | | 3 | D | Credit | Daylight - Install glare control devices, spatial daylight autonomy, illuminance calculations OR daylight floor area measurement | 3 |
| | 1 | | | | D | Credit | Quality Views - Vision glazing for 75% of regularly occupied floor area, with at least two kinds of view types | 1 |
| 2 | 3 | | 5 | | | Totals | 10 | |
| INNOVATION* | | 1 | | | D | Credit | Innovation: Low Mercury Lighting | 1 |
| | | 1 | | | D | Credit | Innovation: Greenbuilding Education | 1 |
| | | | | 1 | D | Credit | TBD | 1 |
| | | | | 1 | D | Credit | TBD | 1 |
| | | | | 1 | D | Credit | TBD | 1 |
| | 1 | 2 | | 3 | | | Totals | 6 |
| <i>*Innovation in Design includes Exemplary Performance credits</i> | | | | | | | | |
| REGIONAL** | | | | 1 | D | Credit | Optimize Energy Performance (10 Points) | 1 |
| | 1 | | | | D | Credit | Access to Quality Transit (5 Points) | 1 |
| | | 1 | | | D | Credit | BPDO - Raw Materials (1 point) | 1 |
| | | | | 1 | D | Credit | Rainwater Management (3 Points) | 1 |
| | | | | 1 | D | Credit | Outdoor Water Use Reduction (2 points) | 1 |
| 1 | 2 | | 3 | | | Totals | 4 | |
| <i>**only 4 Regional Credits are Applicable</i> | | | | | | | | |
| Confirmed Certification Level: | | | | | | | | Not SILVER |
| Confirmed + Likely Certification Level: | | | | | | | | Silver |
| Confirmed + Likely + Maybe Certification Level: | | | | | | | | |
| Total Confirmed Points | | | | | | | | 32 |
| Total Confirmed + Likely Points | | | | | | | | 58 |
| Total Confirmed + Likely + Maybe Points | | | | | | | | 58 |



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PROJ. # 168-153 WO BART
DATE: June 6, 2020

LEED CHECK LIST

SHEET: A-01.06



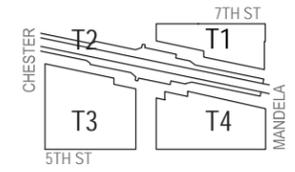
NEW HOME RATING SYSTEM, VERSION 7.0

Blueprint Scoresheet

| Mandela Station T1 | | Points Targeted | Community | Energy | IAQ/Health | Resources | Water |
|---|---|-----------------|-----------|-------------|------------|-----------|-------------|
| | | Possible Points | | | | | |
| CALGreen | | | | | | | |
| Yes | CALGreen Res (REQUIRED) | 4 | | 1 | 1 | 1 | 1 |
| A. SITE | | | | | | | |
| Yes | A6. Stormwater Control: Prescriptive Path | | | | | | |
| | A6.3 Non-Leaching Roofing Materials | 1 | | | | | 1 |
| C. LANDSCAPE | | | | | | | |
| C3. Resource Efficient Landscapes | | | | | | | |
| Yes | C3.3 Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species | 3 | | | | | 3 |
| E. EXTERIOR | | | | | | | |
| Yes | E4. Durable and Non-Combustible Cladding Materials | 1 | | | | 1 | |
| Yes | E5. Durable Roofing Materials | Y | R | R | R | R | R |
| | E5.2 Roofing Warranty for Shingle Roofing | | | | | | |
| F. INSULATION | | | | | | | |
| F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content | | | | | | | |
| Yes | F1.1 Walls and Floors | 1 | | | | 1 | |
| Yes | F1.2 Ceilings | 1 | | | | 1 | |
| F2. Insulation that Meets the CDPH Standard Method—Residential for Low Emissions | | | | | | | |
| Yes | F2.1 Walls and Floors | 1 | | | 1 | | |
| Yes | F2.2 Ceilings | 1 | | | 1 | | |
| G. PLUMBING | | | | | | | |
| G2. Install Water-Efficient Fixtures | | | | | | | |
| Yes | G2.1 WaterSense Showerheads 1.8 gpm with Matching Compensation Valve | 2 | | | | | 2 |
| Yes | G6. Submeter Water for Tenants | 2 | | | | | 2 |
| H. HEATING, VENTILATION, AND AIR CONDITIONING | | | | | | | |
| H6. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality | | | | | | | |
| Yes | H6.1 Meet ASHRAE Standard 62.2-2010 Ventilation Residential Standards | Y | R | R | R | R | R |
| Yes | H8. High Efficiency HVAC Filter (MERV 13+) | 1 | | | 1 | | |
| J. BUILDING PERFORMANCE AND TESTING | | | | | | | |
| J5. Building Performance Exceeds Title 24 Part 6 | | | | | | | |
| Option 1: Compliance Over Title 24 | J5.1 Home Outperforms Title 24 | 25 | | 25+ | | | |
| K. FINISHES | | | | | | | |
| K1. Entryways Designed to Reduce Tracked-In Contaminants | | | | | | | |
| Yes | K1.2 Entryways to Buildings | 1 | | | 1 | | |
| L. FLOORING | | | | | | | |
| ≥75% | L2. Low-Emitting Flooring Meets CDPH 2010 Standard Method—Residential | 3 | | | 3 | | |
| M. APPLIANCES AND LIGHTING | | | | | | | |
| Yes | M1. ENERGY STAR® Dishwasher | 1 | | | | | 1 |
| Yes | M2. Efficient Clothes Washing and Drying | 1 | | 1 | | | |
| <20 cubic feet | M3. Size-Efficient ENERGY STAR Refrigerator | 2 | | 2 | | | |
| Full Circuit | M6. Electric Vehicle Charging Stations and Infrastructure | 2 | | 2 | | | |
| Yes | M8. Gearless Elevator | 1 | | 1 | | | |
| N. COMMUNITY | | | | | | | |
| N1. Smart Development | | | | | | | |
| Yes | N1.1 Infill Site | 2 | 1 | | | 1 | |
| >35 | N1.3 Conserve Resources by Increasing Density | 4 | | 2 | | 2 | |
| | N1.5 Home Size Efficiency | 8 | | | | 10 | |
| 740 | Enter the area of the home, in square feet | | | | | | |
| 2 | Enter the number of bedrooms | | | | | | |
| N2. Home(s)/Development Located Near Transit | | | | | | | |
| Yes | N2.2. Within 1/2 mile of a Major Transit Stop | 2 | 2 | | | | |
| N3. Pedestrian and Bicycle Access | | | | | | | |
| Yes | N3.2 Connection to Pedestrian Pathways | 1 | 1 | | | | |
| Yes | N3.3 Traffic Calming Strategies | 2 | 2 | | | | |
| Yes | N3.5 Bicycle Storage for Residents | 1 | 1 | | | | |
| 1 space per unit | N3.7 Reduced Parking Capacity | 2 | 2 | | | | |
| N4. Outdoor Gathering Places | | | | | | | |
| Yes | N4.1 Public or Semi-Public Outdoor Gathering Places for Residents | 1 | 1 | | | | |
| Yes | N4.2 Public Outdoor Gathering Places with Direct Access to Tier 1 Community Services | 1 | 1 | | | | |
| Yes | N9.2 Community Location | 2 | 1 | | 1 | | |
| N11. Mixed-Use Developments | | | | | | | |
| Yes | N11.2 At Least 2% of Development Floor Space Supports Mixed Use | 1 | 1 | | | | |
| O. OTHER | | | | | | | |
| Yes | O1. GreenPoint Rated Checklist in Blueprints | Y | R | R | R | R | R |
| Yes | O2. Pre-Construction Kickoff Meeting with Rater and Subcontractors | 2 | | 0.5 | | 1 | 0.5 |
| Yes | O7. Green Appraisal Addendum | Y | R | R | R | R | R |
| Yes | O11. Tobacco Free Buildings | 2 | | | 2 | | |
| Summary | | | | | | | |
| Total Available Points in Specific Categories | | 375.5 | 46 | 110.5 | 70 | 95 | 54 |
| Minimum Points Required in Specific Categories | | 50 | 2 | 25 | 6 | 6 | 6 |
| Total Points Targeted | | 85 | 15 | 32.5 | 11 | 16 | 10.5 |



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PROJ. # 168-153 WO BART
DATE: June 6, 2020

GREENPOINTRATED

SHEET: A-01.07

WEST OAKLAND STATION T3



NEW HOME RATING SYSTEM, VERSION 7.0

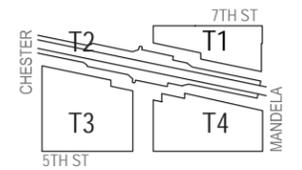
Blueprint Scoresheet



| | | Points Targeted | Community | Energy | IAQ/Health | Resources | Water | Responsible Party | Blueprint Page No. | |
|---|---|-----------------|-----------|-------------|------------|-----------|------------|-------------------|--------------------|--|
| | | Possible Points | | | | | | | | |
| CALGreen | | | | | | | | | | |
| Yes | CALGreen Res (REQUIRED) | 4 | | 1 | 1 | 1 | 1 | | | |
| C. LANDSCAPE | | | | | | | | | | |
| Yes | C1. Plants Grouped by Water Needs (Hydrozoning) | 1 | | | | | 1 | | | |
| C3. Resource Efficient Landscapes | | | | | | | | | | |
| Yes | C3.1 No Invasive Species Listed by Cal-IPC | 1 | | | | 1 | | | | |
| Yes | C3.3 Drought Tolerant, California Native, Mediterranean Species, or Other Appropriate Species | 1 | | | | | 3 | | | |
| E. EXTERIOR | | | | | | | | | | |
| Yes | E4. Durable and Non-Combustible Cladding Materials | 1 | | | | 1 | | | | |
| E5. Durable Roofing Materials | | | | | | | | | | |
| Yes | E5.2 Roofing Warranty for Shingle Roofing | Y | R | R | R | R | R | | | |
| F. INSULATION | | | | | | | | | | |
| F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content | | | | | | | | | | |
| Yes | F1.1 Walls and Floors | 1 | | | | 1 | | | | |
| F2. Insulation that Meets the CDPH Standard Method—Residential for Low Emissions | | | | | | | | | | |
| Yes | F2.1 Walls and Floors | 1 | | | 1 | | | | | |
| Yes | F2.2 Ceilings | 1 | | | 1 | | | | | |
| G. PLUMBING | | | | | | | | | | |
| G2. Install Water-Efficient Fixtures | | | | | | | | | | |
| Yes | G2.1 WaterSense Showerheads 1.8 gpm with Matching Compensation Valve | 2 | | | | | 2 | | | |
| Yes | G2.2 WaterSense Bathroom Faucets with 1.0gpm or less | 1 | | | | | 1 | | | |
| 1.28 gpf | G2.3 WaterSense Toilets with a Maximum Performance (MaP) Threshold of No Less Than 500 Grams 1.28gpf OR 1.1 gpf | 1 | | | | | 2 | | | |
| H. HEATING, VENTILATION, AND AIR CONDITIONING | | | | | | | | | | |
| H6. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality | | | | | | | | | | |
| Yes | H6.1 Meet ASHRAE Standard 62.2-2010 Ventilation Residential Standards | Y | R | R | R | R | R | | | |
| J. BUILDING PERFORMANCE AND TESTING | | | | | | | | | | |
| J5. Building Performance Exceeds Title 24 Part 6 | | | | | | | | | | |
| Option 1: Compliance Over Title 24 | J5.1 Home Outperforms Title 24 | 25 | | 25+ | | | | | | |
| N. COMMUNITY | | | | | | | | | | |
| N1. Smart Development | | | | | | | | | | |
| Yes | N1.1 Infill Site | 2 | 1 | | | 1 | | | | |
| N2. Home(s)/Development Located Near Transit | | | | | | | | | | |
| Yes | N2.2. Within 1/2 mile of a Major Transit Stop | 2 | 2 | | | | | | | |
| N3. Pedestrian and Bicycle Access | | | | | | | | | | |
| | N3.1 Pedestrian Access to Services Within 1/2 Mile of Community Services | 2 | 2 | | | | | | | |
| 10 | Enter the number of Tier 1 services | | | | | | | | | |
| 10 | Enter the number of Tier 2 services | | | | | | | | | |
| Yes | N9.2 Community Location | 2 | 1 | | 1 | | | | | |
| O. OTHER | | | | | | | | | | |
| Yes | O1. GreenPoint Rated Checklist in Blueprints | Y | R | R | R | R | R | | | |
| Yes | O2. Pre-Construction Kickoff Meeting with Rater and Subcontractors | 2 | | 0.5 | | 1 | 0.5 | | | |
| Yes | O7. Green Appraisal Addendum | Y | R | R | R | R | R | | | |
| P. DESIGN CONSIDERATIONS | | | | | | | | | | |
| P3. Commissioning | | | | | | | | | | |
| Yes | P3.1 Design Phase | 2 | | 1 | 1 | | | | | |
| Yes | P3.2 Construction Phase | 3 | | 2 | 1 | | | | | |
| Yes | P3.3 Post-Construction Phase | 3 | | 2 | 1 | | | | | |
| Summary | | | | | | | | | | |
| Total Available Points in Specific Categories | | 375.5 | 46 | 110.5 | 70 | 95 | 54 | | | |
| Minimum Points Required in Specific Categories | | 50 | 2 | 25 | 6 | 6 | 6 | | | |
| Total Points Targeted | | 58 | 6 | 31.5 | 7 | 6 | 7.5 | | | |



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PROJ. # 168-153 WO BART
DATE: June 6, 2020

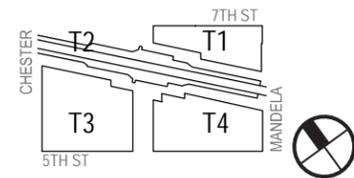
GREENPOINT RATED

SHEET: A-01.08



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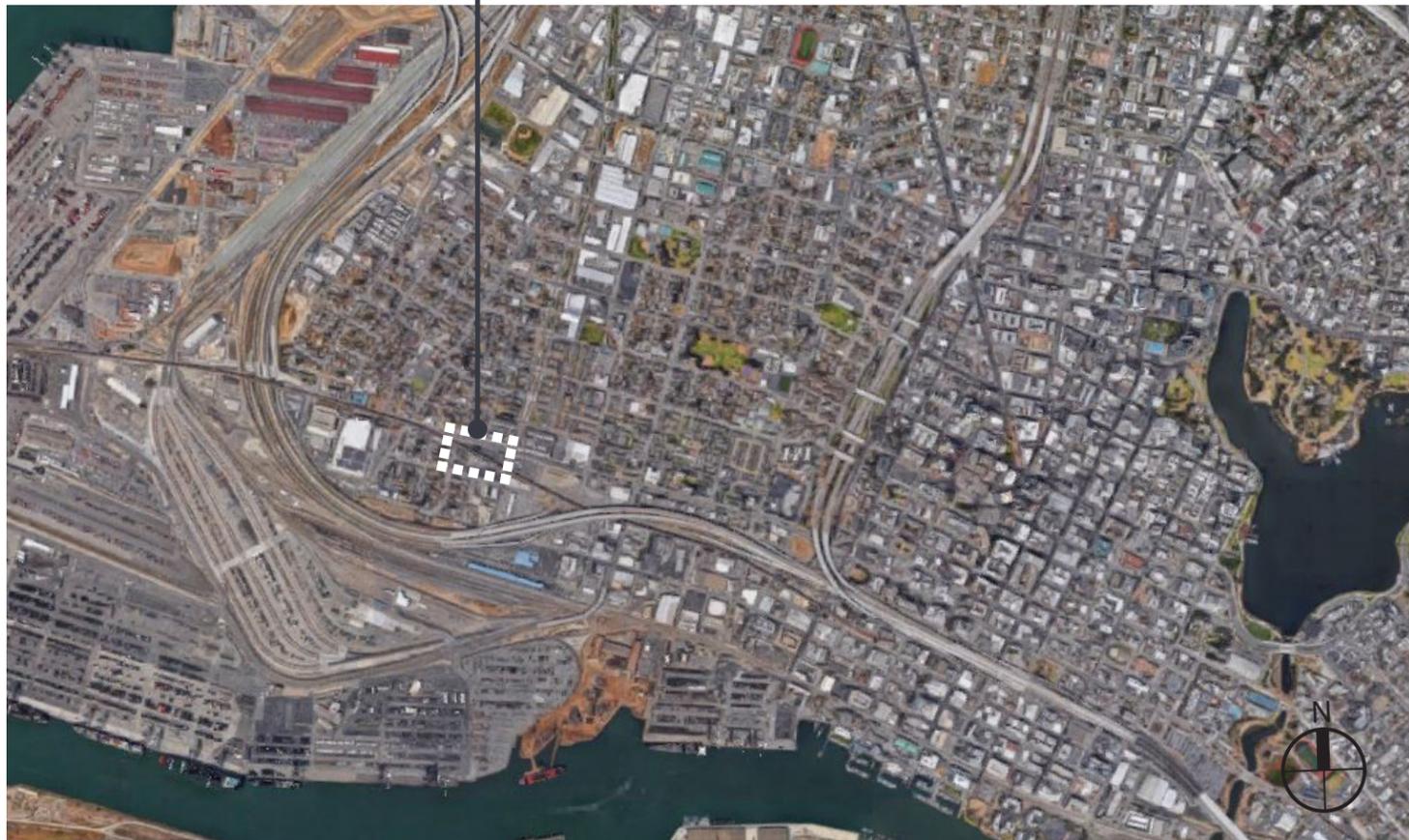


PROJ. # 168-153 WO BART
DATE: July 24, 2020

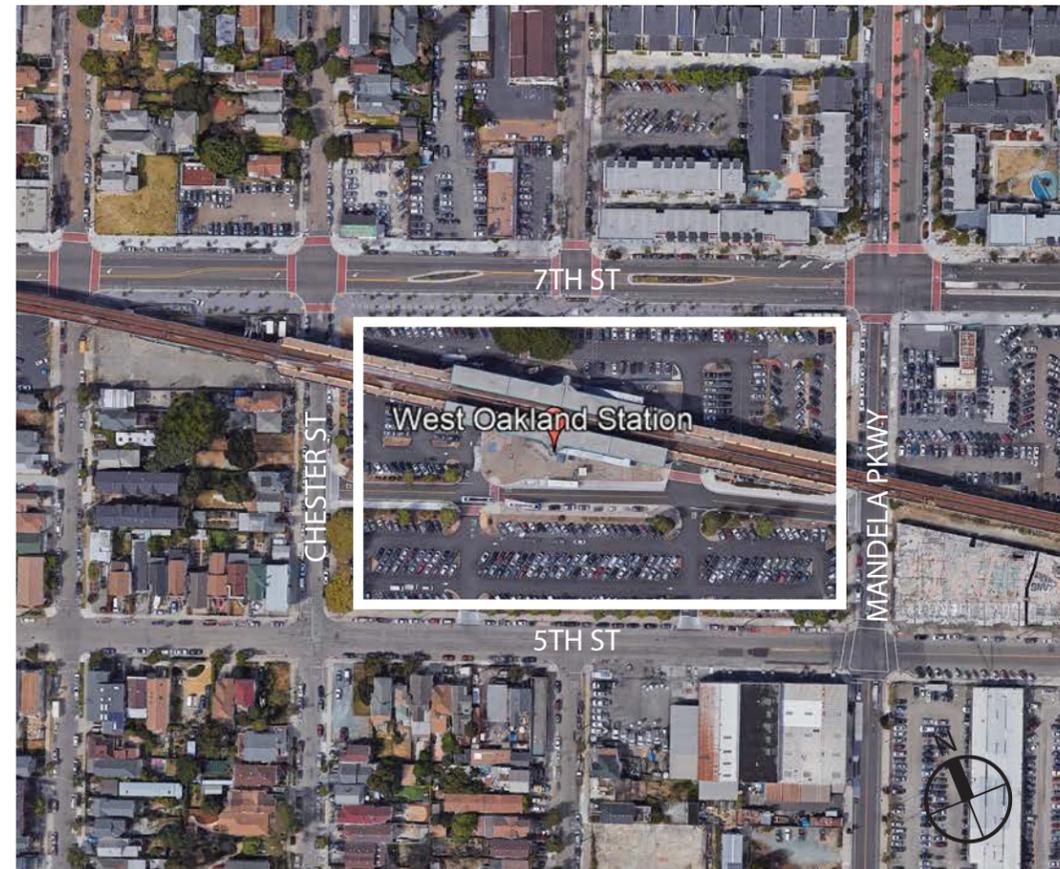
**CONTEXT
MAP**

SHEET: A-01.09

SITE LOCATION



LOCATION OF PROJECT SITE WITHIN WEST OAKLAND NEIGHBORHOOD



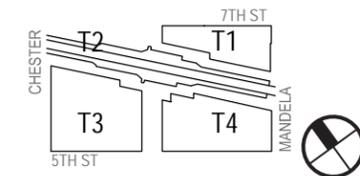
5 ACRE PROJECT SITE



SITE



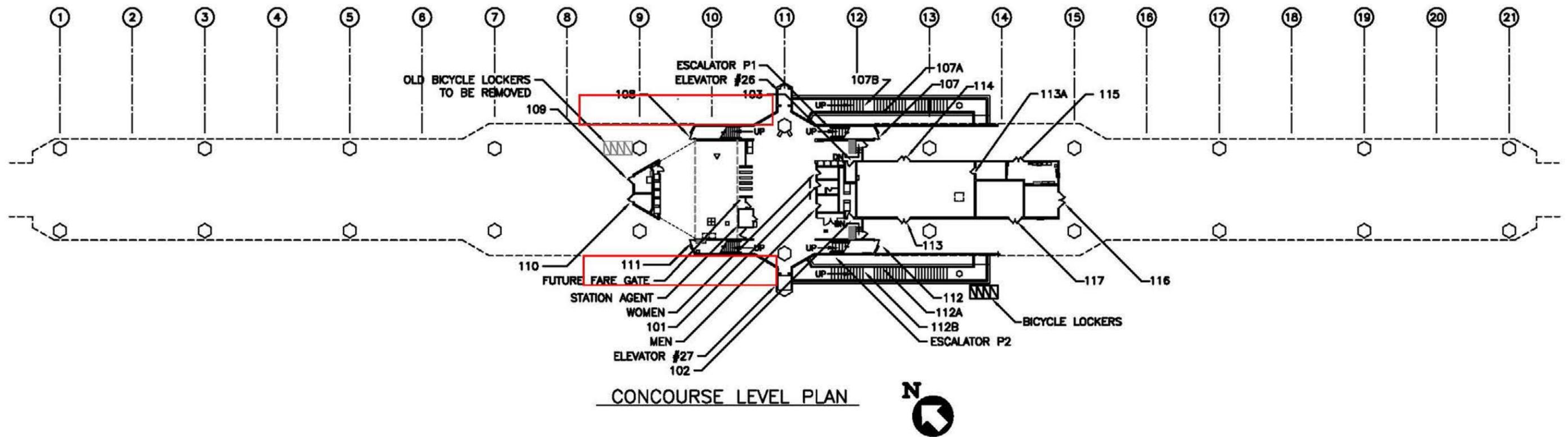
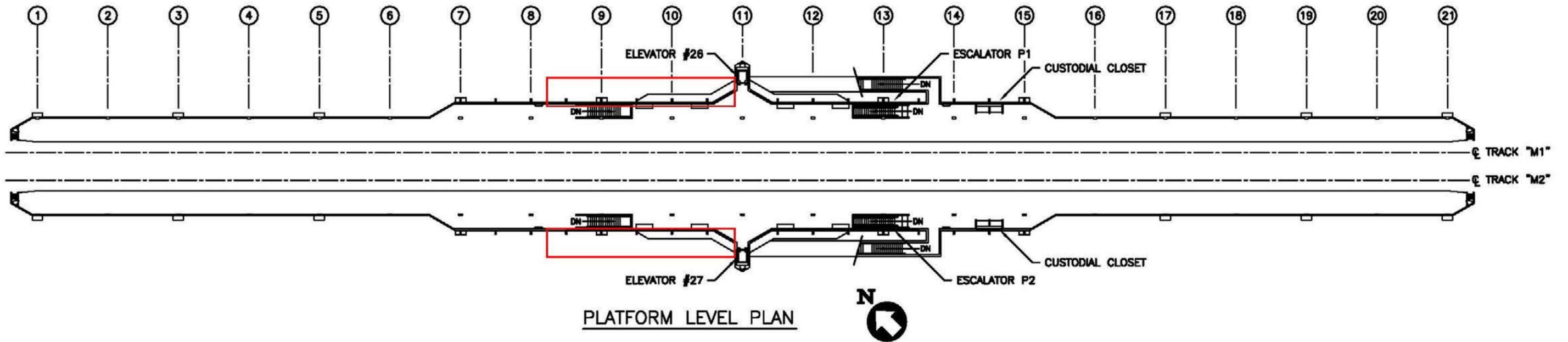
**MANDELA STATION
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1451 7th St, Oakland,
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PROJ. # 168-153 WO BART
DATE: July 24, 2020

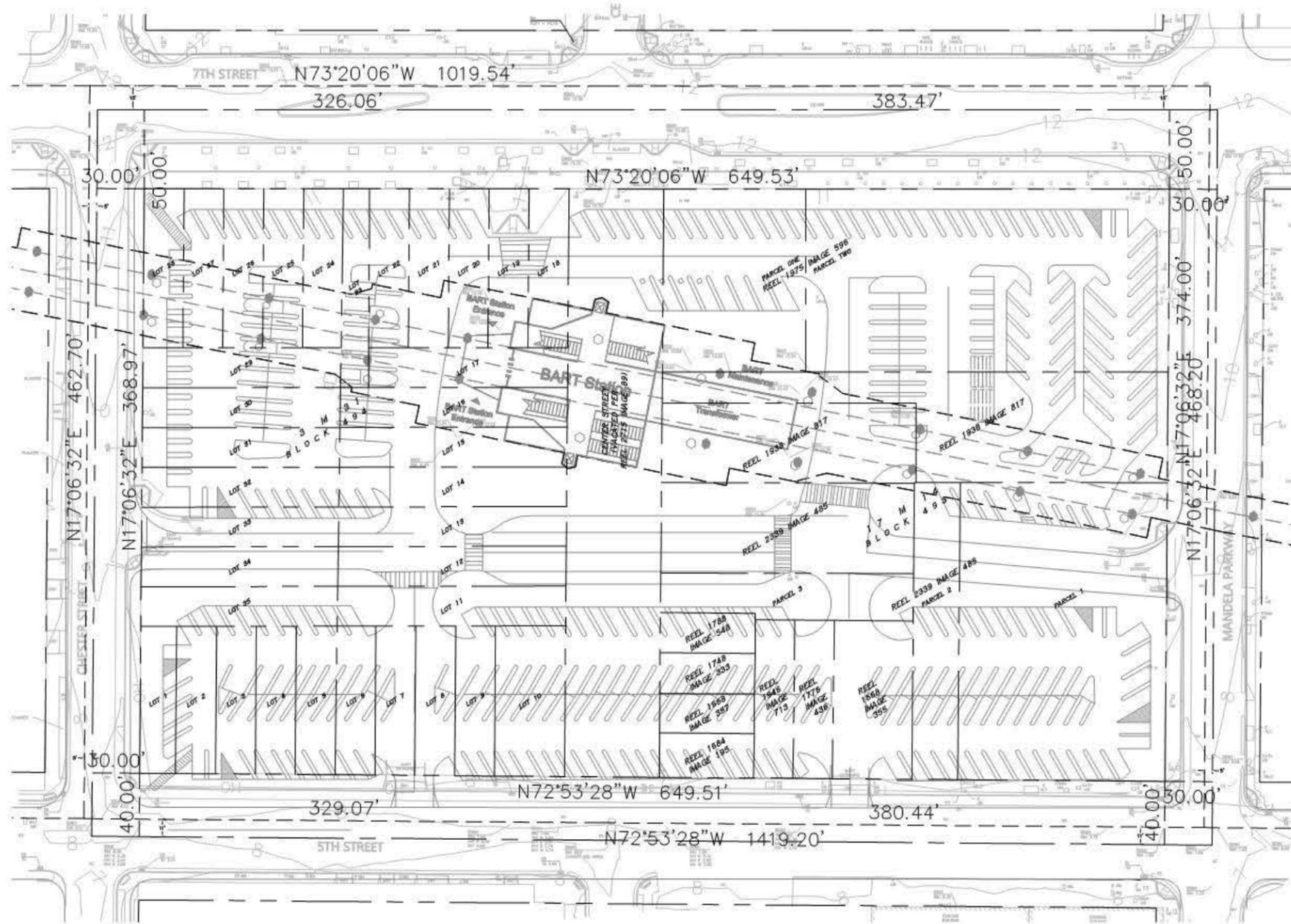
**CONTEXT
PHOTOS**

SHEET: A-01.10

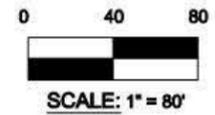


| DOOR/ROOM LEGEND | | | | | |
|------------------|------------------|--------|-----------------|--------|---------------|
| DR.NO. | USE | DR.NO. | USE | DR.NO. | USE |
| 101 | STAFF RESTROOM | 109 | CUSTODIAN | 113A | BATTERY RM. |
| 102 | ELEV. EQUIP. RM. | 110 | STAFF BREAKROOM | 114 | TRAIN CONTROL |
| 103 | STORAGE | 111 | ELECT. EQUIP. | 115 | ELECT. EQUIP. |
| 107 | STORAGE | 112 | STORAGE | 118 | TRANSFORMER |
| 107A | GATE | 112A | GATE | 117 | FAN ROOM |
| 107B | STORAGE | 112B | STORAGE | | |
| 108 | STORAGE | 113 | TRAIN CONTROL | | |

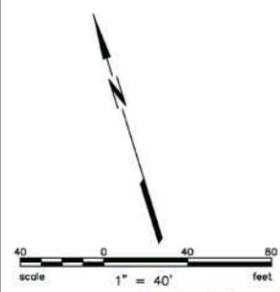
**WEST OAKLAND STATION - OWS
"M" LINE - STATION M-10
FLOOR PLANS**



NOTE:
 EXISTING BOUNDARY FROM RECORD INFORMATION FOR
 PLANNING PURPOSES ONLY



| | | | |
|-----------------|------------|-----------|-----|
| Date: | 12/06/2018 | No.: | |
| Scale: | AS SHOWN | Design: | N/A |
| Drawn: | ADD | Approved: | KS |
| Job No.: | 180384 | | |
| Revisions: | | | |
| Drawing Number: | 180384 | | |
| | 1 of 5 | | |



BASIS OF BEARINGS

THE BEARING OF NORTH 72°53'28" WEST BETWEEN FOUND MONUMENTS ON 5TH AVENUE BETWEEN HENRY STREET AND KIRKHAM STREET AS SHOWN ON THAT RECORD OF SURVEY, R/S 1687, FILED AUGUST 7, 2000 IN BOOK 25 OF RECORDS OF SURVEY AT PAGES 58-69, INCLUSIVE, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.

BENCHMARK

THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H," ELEVATION 10.784 FEET (DATUM = MEAN SEA LEVEL).

NOTES

1. THE SURVEY WAS CONDUCTED IN NOVEMBER OF 2018.
2. ALL UNITS ARE IN US SURVEY FEET AND DECIMALS THEREOF.
3. THE TOPOGRAPHIC MAPPING SHOWN HEREON IS A WORKING DOCUMENT. THE TOPOGRAPHIC SURVEY AND MAPPING IS IN PROGRESS AND HAS NOT BEEN COMPLETED OR FINALIZED.

SURVEYOR'S STATEMENT:

THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE PROFESSIONAL LAND SURVEYOR'S ACT.

KEVIN STEIN, P.L.S. NO. 9028

DATE:



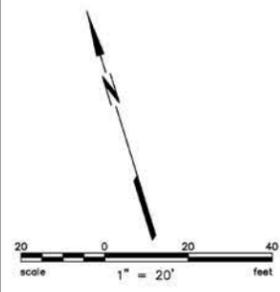
DRAWING NAME: K:\2018\180384\180384_001.dwg
DRAWING NUMBER: 180384
DATE: 12/06/2018

SYMBOLS & LEGEND

- EXISTING**
- FOUND CITY OF OAKLAND MONUMENT
 - VALVE
 - FIRE HYDRANT
 - BACKFLOW PREVENTION DEVICE
 - RISER
 - SIGN
 - STREET LIGHT (UNLESS NOTED OTHERWISE)
 - LIGHT POLE (UNLESS NOTED OTHERWISE)
 - GUY ANCHOR
 - UTILITY POLE
 - TELEPHONE MANHOLE
 - MANHOLE
- PROPERTY**
- SUBJECT PROPERTY
 - ADJOINING LOT
 - CENTERLINE
 - SURVEY TIE
 - STORM DRAIN
 - SANITARY SEWER
 - OVERHEAD UTILITY LINE
 - CONCRETE
 - DETECTABLE WARNING

ABBREVIATIONS

- AC ASPHALT CONCRETE
- CA CABLE
- CB CATCH BASIN
- CO CLEAN OUT
- COMM COMMUNICATION
- CONC CONCRETE
- DI DROP INLET
- DWY DRIVEWAY
- E ELECTRIC
- EP EDGE OF PAVEMENT
- FL SURFACE FLOWLINE
- G GAS
- GYP GUY POLE
- INV BOTTOM INSIDE OF PIPE
- LG LIP OF GUTTER
- M METER
- MH MANHOLE
- OH OVERHEAD UTILITY LINE
- SD STORM DRAIN
- SL STREETLIGHT
- SS SANITARY SEWER
- TC TOP FACE OF CURB
- TEL TELECOMMUNICATION LINE
- TG TOP OF GRATE
- TS TRAFFIC SIGNAL
- TV TELEVISION
- UB UTILITY BOX
- V VALVE
- VLT VAULT
- W WATER METER



1646 N. CALIFORNIA BLVD.,
SUITE 400
WALNUT CREEK, CA 94596
(925) 940-2200
www.bkf.com

CALIFORNIA

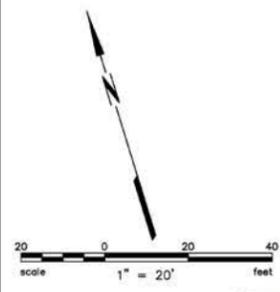
TOPOGRAPHIC AND BOUNDARY SURVEY
WEST OAKLAND BART

OAKLAND COUNTY OF ALAMEDA

| Revisions | |
|-----------|-------------|
| No. | Description |
| | |
| | |
| | |
| | |

Date: 12/06/2018
Scale: AS SHOWN
Design: N/A
Drawn: ADD
Approved: KS
Job No: 180384

DRAWING NAME: K:\2018\180384\West_Oakland_Bart_Topo_Survey\ASAP\Working\Topo\180384.dwg
DATE: 12/06/2018 10:00:00 AM
DRAWN BY: ADD



- SYMBOLS & LEGEND**
- EXISTING**
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 - VALVE
 - FIRE HYDRANT
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 - VL VULT
 - W WATER
 - WM WATER METER



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TOPOGRAPHIC AND BOUNDARY SURVEY
WEST OAKLAND BART

OAKLAND

COUNTY OF ALAMEDA

Revisions

| | | | |
|---------|------------|-----------|--------|
| Date: | 12/06/2018 | No.: | |
| Scale: | AS SHOWN | Drawn: | N/A |
| Design: | N/A | Approved: | KS |
| Drawn: | ADD | Job No.: | 180384 |

Drawing Number:
180384

DRAWING NAME: K:\2018\180384\180384_001\180384_001.dwg; DATE: 12/06/2018 10:00:00 AM; USER: bkf; PLOT DATE: 12/06/2018 10:00:00 AM; PLOT USER: bkf; PLOT DEVICE: HP DesignJet 5000; PLOT SCALE: 1:1; PLOT SHEET: 1 OF 1

VESTING TENTATIVE PARCEL MAP NO. 10940

FOR CONDOMINIUM PURPOSES

SHEET INDEX

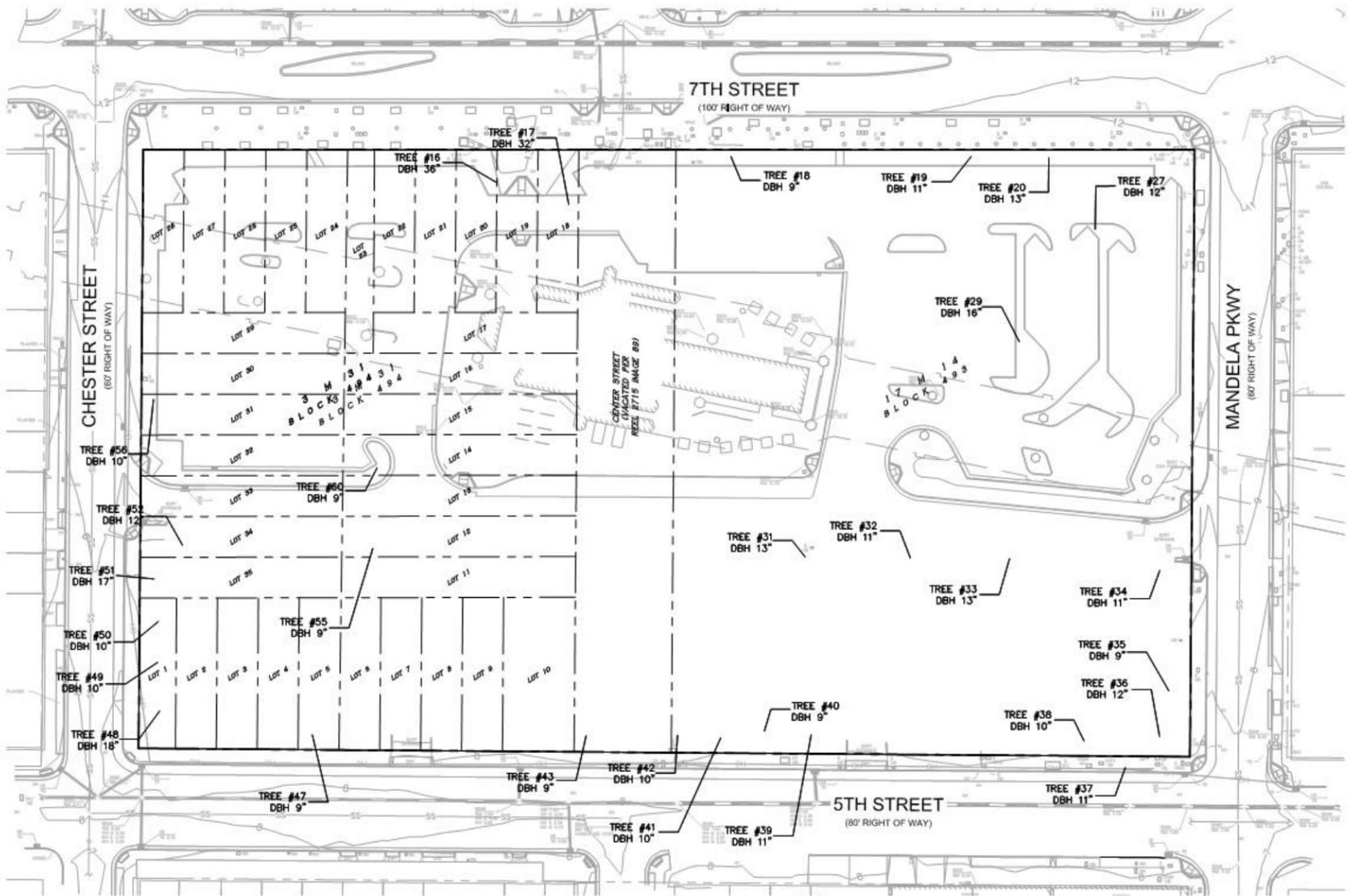
| SHEET # | SHEET TITLE |
|---------|-----------------------------------|
| 1 | TITLE SHEET/EXISTING SITE PLAN |
| 2 | PROPOSED SITE PLAN |
| 3 | EMERGENCY VEHICLE ACCESS EASEMENT |

SYMBOLS & LEGEND

| EXISTING | SYMBOL | DESCRIPTION |
|----------------------------|--------|----------------------------|
| VALVE | | VALVE |
| FIRE HYDRANT | | FIRE HYDRANT |
| BACKFLOW PREVENTION DEVICE | | BACKFLOW PREVENTION DEVICE |
| RISER | | RISER |
| SIGN | | SIGN |
| STREET LIGHT | | STREET LIGHT |
| LIGHT POLE | | LIGHT POLE |
| GUY ANCHOR | | GUY ANCHOR |
| UTILITY POLE | | UTILITY POLE |
| TELEPHONE MANHOLE | | TELEPHONE MANHOLE |
| MANHOLE | | MANHOLE |
| EXISTING PROPERTY LINE | | EXISTING PROPERTY LINE |
| ADJOINING LOT CENTERLINE | | ADJOINING LOT CENTERLINE |
| SURVEY TRIE | | SURVEY TRIE |
| STORM DRAIN | | STORM DRAIN |
| SANITARY SEWER | | SANITARY SEWER |
| OVERHEAD UTILITY LINE | | OVERHEAD UTILITY LINE |
| CONCRETE | | CONCRETE |
| DETECTABLE WARNING | | DETECTABLE WARNING |
| BOUNDARY LINE | | BOUNDARY LINE |

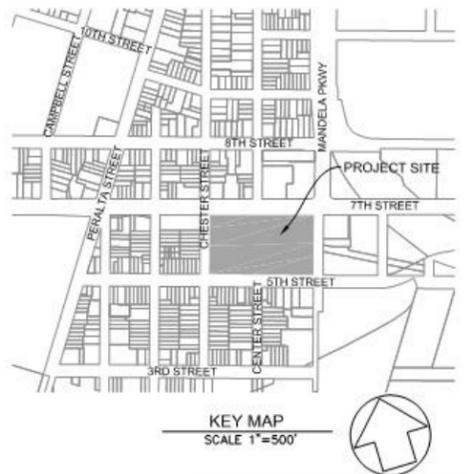
ABBREVIATIONS

| | |
|------|-----------------------------------|
| AC | ASPHALT CONCRETE |
| CA | CABLE |
| CB | CATCH BASIN |
| CO | CLEAN OUT |
| COMM | COMMUNICATION |
| CONC | CONCRETE |
| DBH | DIAMETER AT BREAST HEIGHT |
| DI | DROP INLET |
| DWY | DRIVEWAY |
| E | ELECTRIC |
| EP | EDGE OF PAVEMENT |
| EVAE | EMERGENCY VEHICLE ACCESS EASEMENT |
| FL | SURFACE FLOWLINE |
| G | GAS |
| GYP | GUY POLE |
| INV | BOTTOM INSIDE OF PIPE |
| LG | LIP OF GUTTER |
| M | METER |
| MH | MANHOLE |
| MIN | MINIMUM |
| OH | OVERHEAD UTILITY LINE |
| PL | PROPERTY LINE |
| R | RADIUS |
| SD | STORM DRAIN |
| SF | SQUARE FEET |
| SL | STREETLIGHT |
| SS | SANITARY SEWER |
| (T) | TOTAL |
| TC | TOP FACE OF CURB |
| TEL | TELECOMMUNICATION LINE |
| TG | TOP OF GRATE |
| TV | TRAFFIC SIGNAL |
| TS | TELEVISION |
| UB | UTILITY BOX |
| V | VALVE |
| VT | VAULT |
| W | WATER |
| WM | WATER METER |



EXISTING CONDITIONS
SCALE 1"=40'

| Tree number | DBH (in) | Species Name |
|-------------|----------|-------------------------|
| 16 | 36 | Pinus Pinea |
| 17 | 32 | Pinus Pinea |
| 20 | 13 | Liriodendron Tulipifera |
| 29 | 12 | Maytenus Boaria |
| 29 | 16 | Maytenus Boaria |
| 31 | 13 | Liriodendron Tulipifera |
| 33 | 13 | Liriodendron Tulipifera |
| 36 | 12 | Liriodendron Tulipifera |
| 48 | 18 | Platanus X Acerifolia |
| 51 | 17 | Platanus X Acerifolia |
| 52 | 12 | Liriodendron Tulipifera |



KEY MAP
SCALE 1"=500'

PROPERTY ADDRESS:
1451 7TH STREET
OAKLAND, CA 94607

OWNER AND SUBDIVIDER:
SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT
300 LAKESIDE DRIVE, 22ND FLOOR
OAKLAND, CA 94612
PHONE: 510-597-6300

BASIS OF BEARINGS

THE BEARING OF NORTH 72°53'28" WEST BETWEEN FOUND MONUMENTS ON 5TH STREET BETWEEN HENRY STREET AND KIRKHAM STREET AS SHOWN ON THAT RECORD OF SURVEY, R/S 1687, FILED AUGUST 7, 2000 IN BOOK 25 OF RECORDS OF SURVEY AT PAGES 58-59, INCLUSIVE, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.

BENCHMARK

THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H," ELEVATION 10.784 FEET (DATUM = MEAN SEA LEVEL).

NOTES

- BASED ON INFORMATION CONTAINED IN THE PRELIMINARY TITLE REPORT, ORDER NUMBER NCS-891862-SC, DATED JANUARY 31, 2018, PROVIDED BY FIRST AMERICAN TITLE COMPANY, THE SUBJECT PROPERTY IS OWNED BY SFBART BEING ASSESSOR PARCEL NUMBERS 004-0077-003, 004-0071-003, AND THE VACATION OF CENTER STREET; THERE ARE 36 PARCELS, NOT INCLUDING THE VACATION OF CENTER STREET. THE EXISTING PARCELS WILL BE RECONFIGURED TO CREATE 3 LOTS AND ONE REMAINDER PARCEL VIA PARCEL MAP TO BE PROCESSED.
- THE INTENT OF THE MAP IS TO MERGE THE 36 PARCELS AND THE VACATED CENTER STREET INTO THE FOUR CONSOLIDATE PARCELS DEFINING THE KEY COMPONENTS OF THE NEW DEVELOPMENT: LOT 1 MARKET RATE RESIDENTIAL TOWER, LOT 2 OFFICE BUILDING, LOT 3 AFFORDABLE HOUSING, AND THE DESIGNATED REMAINDER PARCEL FOR THE EXISTING WEST OAKLAND BART STATION AND NEW ENTRY PLAZAS.
- THE CONTROL SURVEY WAS CONDUCTED IN NOVEMBER OF 2018.
- ALL UNITS ARE IN US SURVEY FEET AND DECIMALS THEREOF.
- THE TOPOGRAPHIC SURVEY IS BASED ON A FIELD SURVEY COMPLETED IN DECEMBER 2018.
- ALL BUILDINGS TO THE NORTH OF THE BART STATION SHALL BE 30' FROM THE BART PLATFORM, AND ALL BUILDINGS TO THE SOUTH OF THE BART STATION SHALL BE 33' FROM THE BART PLATFORM.
- ALL EMERGENCY VEHICLE ACCESS EASEMENTS SHALL BE 26' WIDE.
- LOTS 1, 2 AND 3 ARE ALLOWED UP TO FOUR COMMERCIAL CONDOMINIUMS ON EACH LOT.
- MULTIPLE MAPS MAY BE FILED FOR THIS PROJECT.

SURVEYOR'S STATEMENT:

THIS MAP CORRECTLY REPRESENTS A SURVEY MADE BY ME OR UNDER MY DIRECTION IN CONFORMANCE WITH THE REQUIREMENTS OF THE PROFESSIONAL LAND SURVEYOR'S ACT.

PAUL KITTRIDGE
PROJECT MANAGER
P.L.S. # 5790



DATE:

1646 N. CALIFORNIA BLVD., SUITE 400
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(925) 940-2200
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BKF100
YEARS
ENGINEERS • SURVEYORS • PLANNERS

VESTING TENTATIVE PARCEL MAP NO. 10940
TITLE SHEET

CALIFORNIA
COUNTY OF ALAMEDA
CITY OF OAKLAND

DRAWING NAME: K:\2018\190384_WestOakland_BART_TOD_Survey\ENG\TENTATIVE PARCEL MAP\SHEETS\01\WBTMTS.dwg
PLOT DATE: 04-06-20 PLOTTED BY: mitr

| Revisions | No. | DATE | BY | DESCRIPTION |
|-----------|-----|------------|----|-------------|
| AS SHOWN | | 04/06/2020 | | |
| CC | | | | |
| ES | | | | |

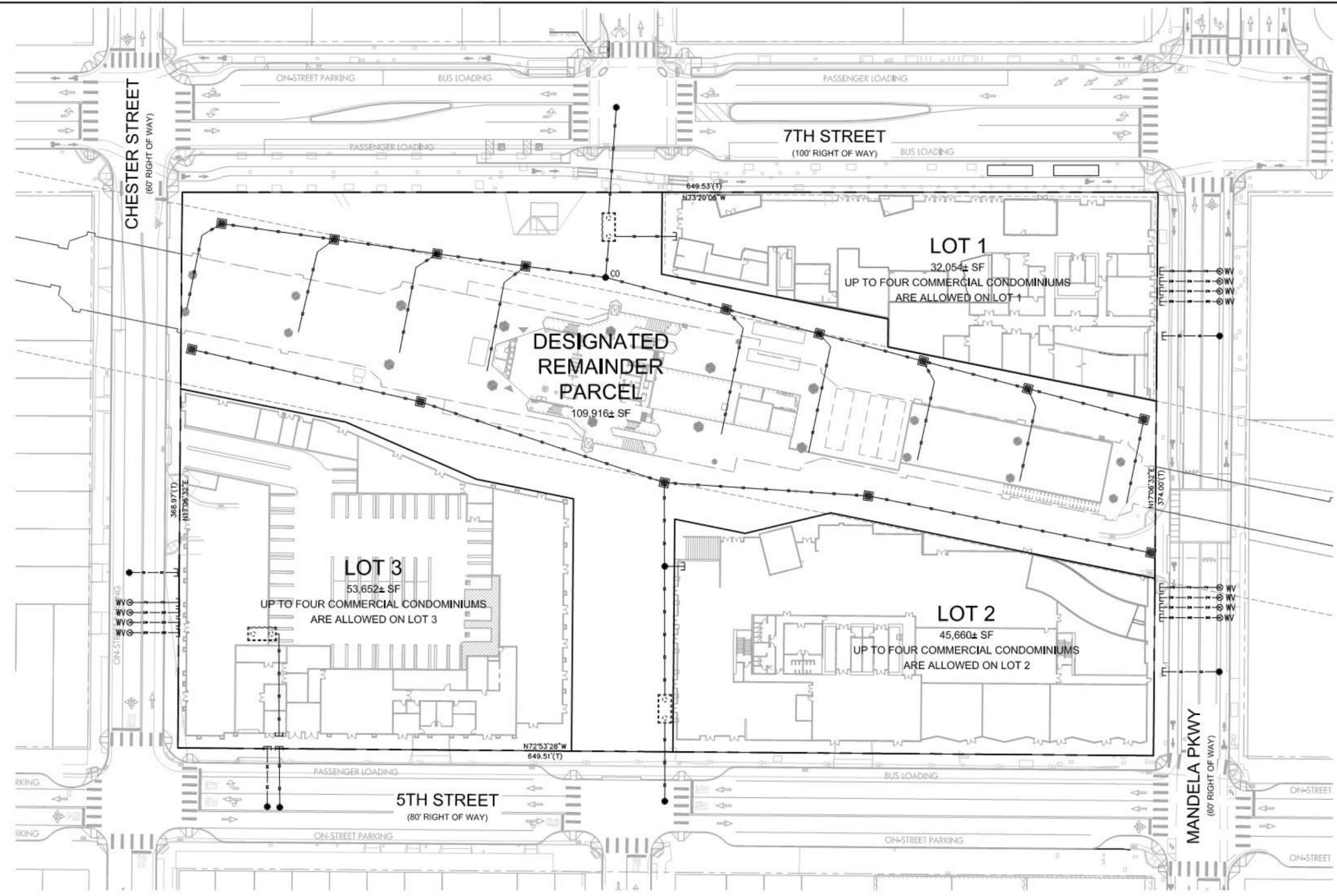
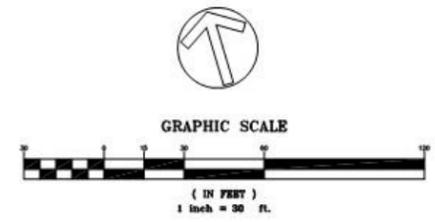
TENTATIVE
PARCEL MAP
C-1.01A

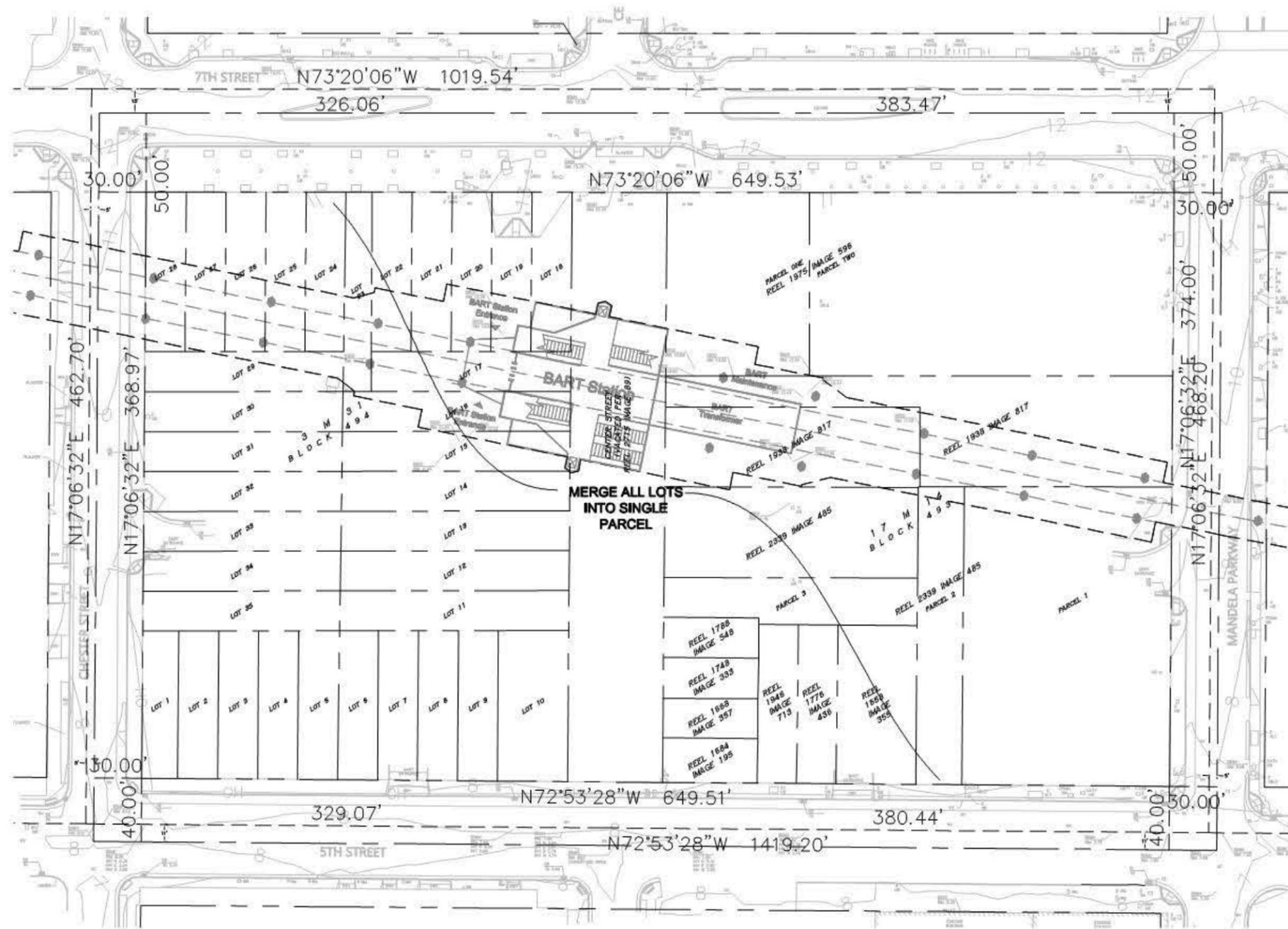
| Revisions | No. | Date |
|--------------|-----|------------|
| AS SHOWN | | 04/06/2020 |
| BY CC | | |
| BY EC | | |
| BY ES | | |
| No. 20180384 | | |

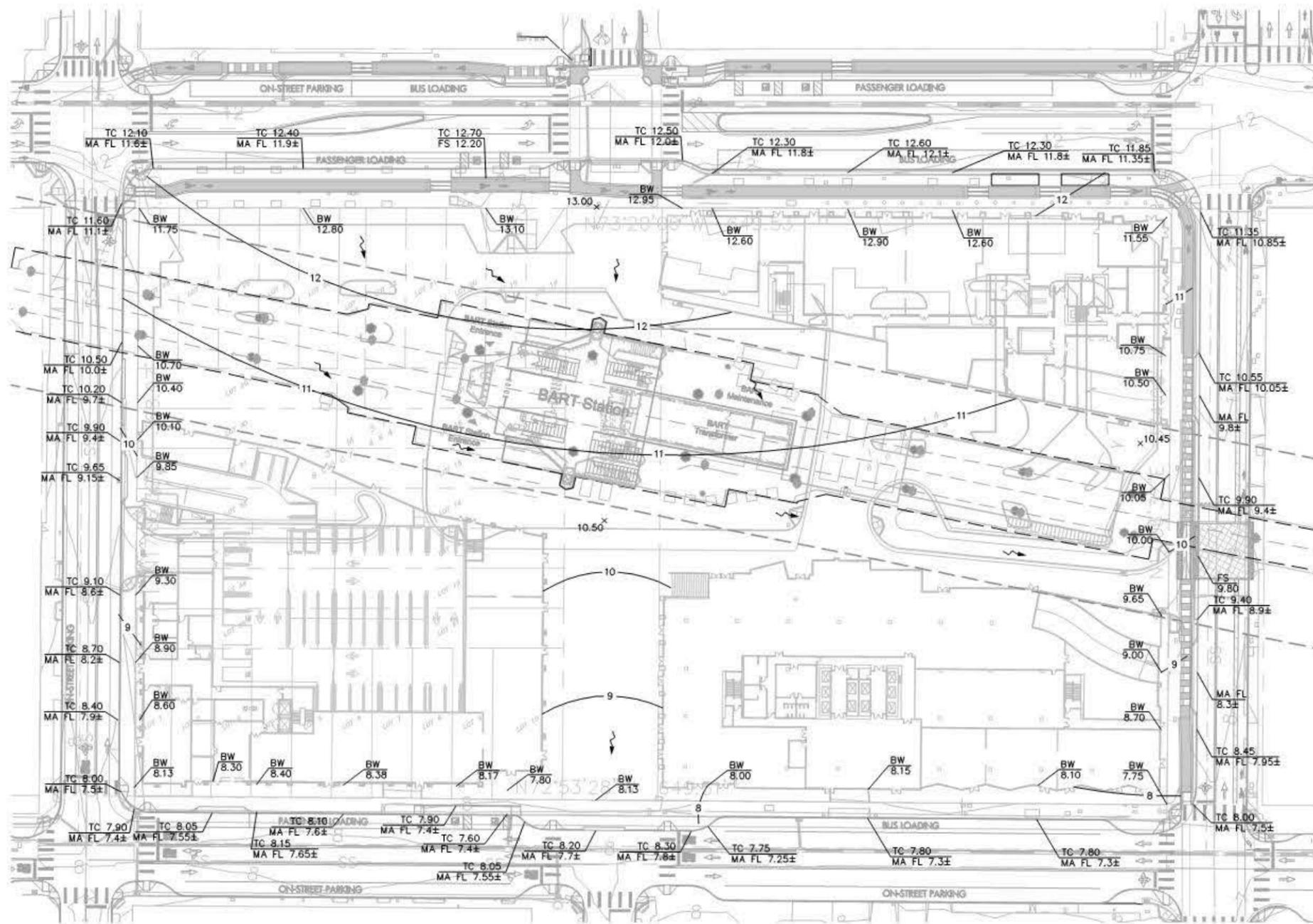
TENTATIVE
PARCEL MAP
C-1.01B

DRAWING NAME: K:\2018\190384_WestOakland_BART_TOD_Survey\ENG\TENTATIVE PARCEL MAP\SHEETS\02\0201MPL.dwg
PLOT DATE: 04-06-20 PLOTTED BY: mitr

LEGEND
BOUNDARY LINE
PROPOSED PARCEL LINE







ABBREVIATIONS:

| | |
|----|------------------|
| FF | FINISHED FLOOR |
| FG | FINISHED GROUND |
| FS | FINISHED SURFACE |
| FL | FLOW LINE |
| MA | MATCH |
| TC | TOP OF CURB |
| TS | TOP OF STEP |
| TW | TOP OF WALL |

LEGEND:

- DIRECTION OF PROPOSED DRAINAGE
- GRADE BREAK
- CONTOUR

GRADING NOTES:

- TC ELEVATIONS ARE 6" ABOVE ADJACENT PAVEMENT FL ELEVATIONS, UNLESS OTHERWISE NOTED ON PLAN.

EARTHWORK SUMMARY:

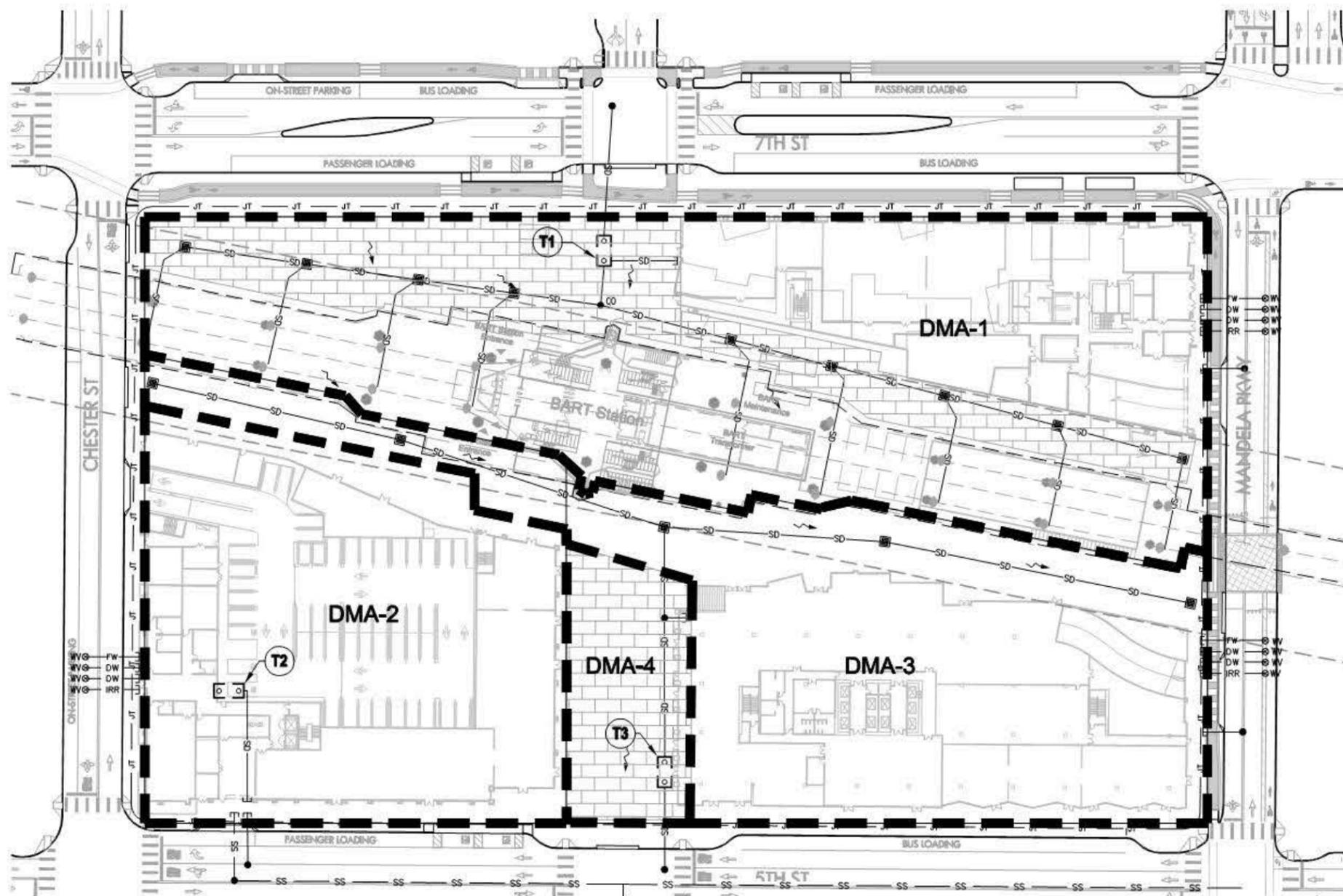
TOTAL EXCAVATION: 51,303 CUBIC YARDS
 TOTAL FILL: 0 CUBIC YARDS
 TOTAL OFFHAUL: 51,303 CUBIC YARDS



0 40 80



SCALE: 1" = 80'

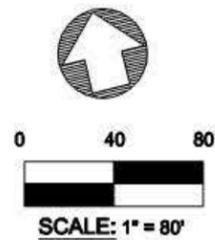


LEGEND:

- DRAINAGE MANAGEMENT AREA (DMA)
- PERMEABLE PAVING
- FILTER VAULT
SEE SHEET C3.01
- TREATMENT AREA LABEL

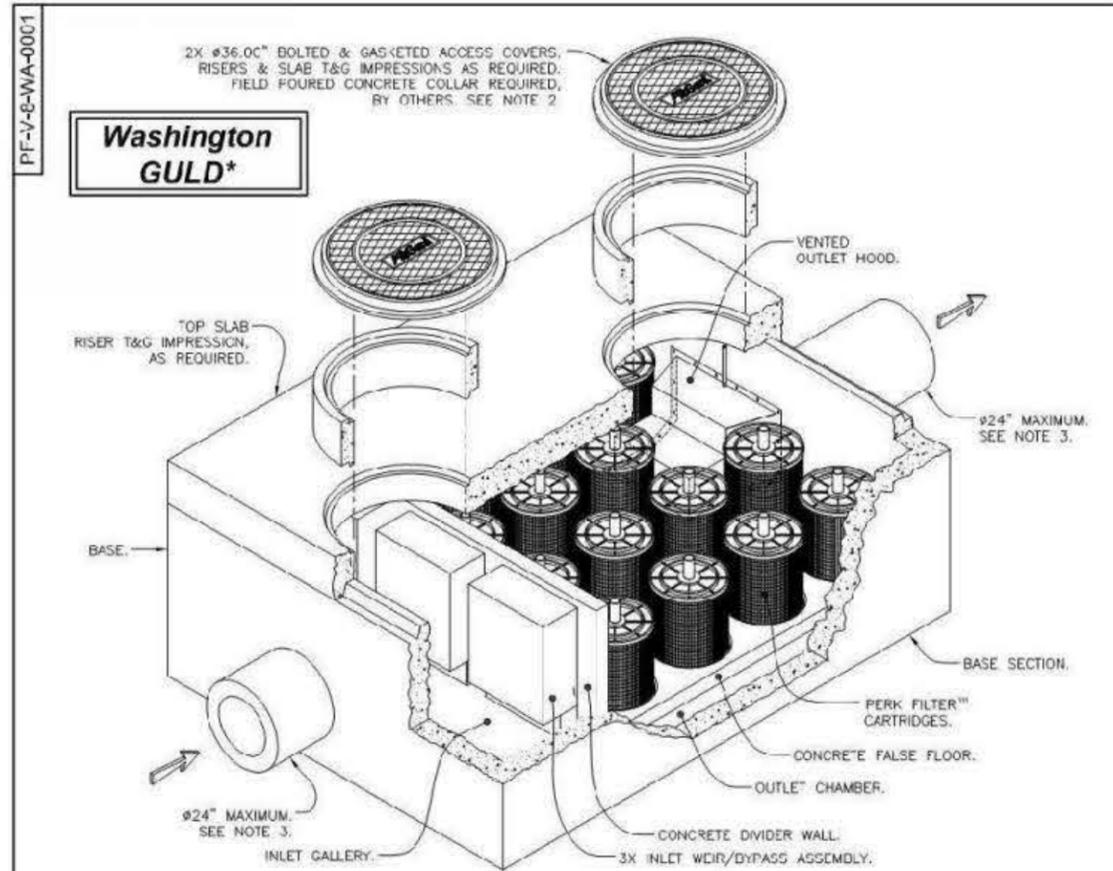
STORMWATER MANAGEMENT NOTES:

1. THE TREATMENT VAULTS WERE SELECTED BASED ON 243,131 SF OF IMPERVIOUS SURFACE AND A TREATMENT INTENSITY OF 0.2 IN/HR. SEE OLD CASTLE WASHINGTON CULD PF-V-8-WA-0001 PERK FILTER DETAIL ON SHEET C3.01. ALL FILTER VAULTS SHOWN ARE COMPRISED OF (31) - 18" CARTRIDGES.



| TOTAL SITE AREA (SF) | TOTAL LAND AREA DISTURBED (SF) | TOTAL EXISTING/PRE-PROJECT IMPERVIOUS SURFACE (SF) | REPLACED IMPERVIOUS SURFACE (SF) | NEW IMPERVIOUS SURFACE (SF) | TOTAL POST-PROJECT IMPERVIOUS SURFACE (SF) |
|----------------------|--------------------------------|--|----------------------------------|-----------------------------|--|
| 243,130 | 243,130 | 212,865 | 177,410 | 25,135 | 202,545 |

| DMA | Impervious Area (SF) | Pervious Area (SF) | Treatment Type | Designated Treatment Area |
|-----|----------------------|--------------------|------------------|---------------------------|
| 1 | 74,600 | 28,445 | Filter Vault | T1 |
| 2 | 58,278 | 0 | Filter Vault | T2 |
| 3 | 69,667 | 0 | Filter Vault | T3 |
| 4 | 0 | 12,140 | Permeable Paving | T4 |

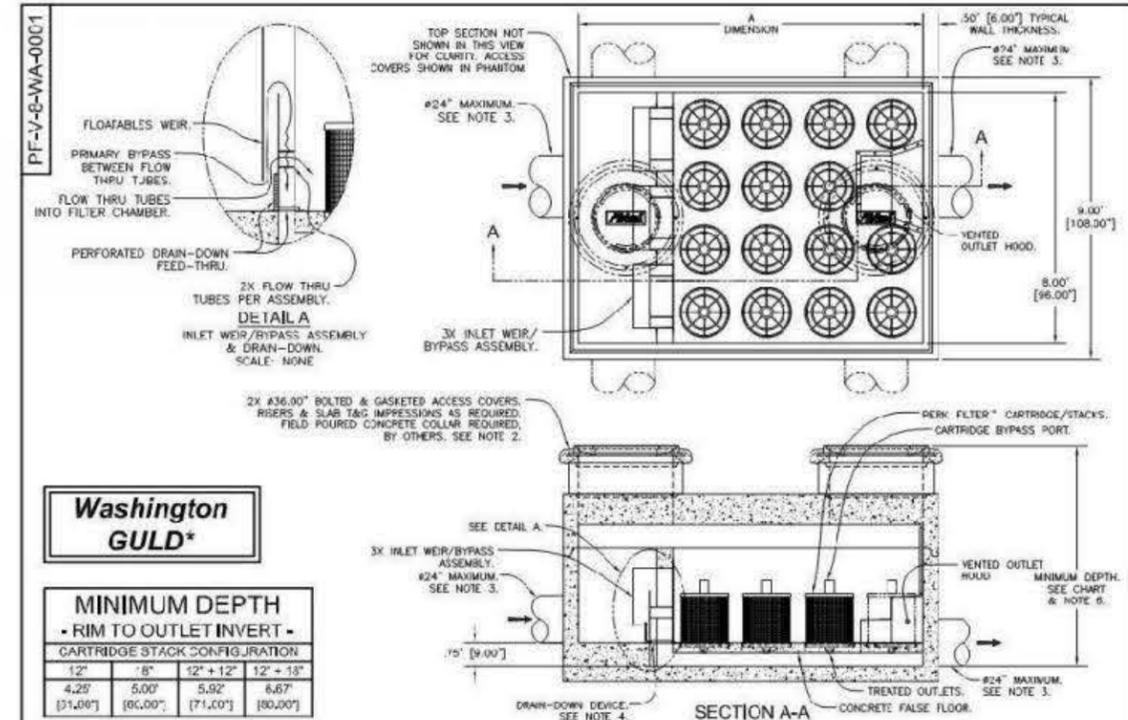


Notes:

1. Precast concrete structure shall be manufactured in accordance with ASTM Designation C857 and C858.
 2. Filter system shall be supplied with traffic rated (H20) bolted & gasketed Ø36" circular access covers with risers as required. Shallow applications may require configurations with (H20) bolted & gasketed square/rectangular access hatches. Field poured concrete collars required, by others.
 3. Inlet & outlet pipe(s) (Ø 24" maximum) may enter device on all three sides of the inlet & outlet chambers respectively.
 4. Inlet chamber shall be supplied with a drain-down device designed to remove standing water between storm events.
 5. For depths less than specified minimums contact Oldcastle® Stormwater Solutions for engineering assistance.
- ★ Treatment Flow Rates shown conform to Washington State GULD Specifications

Perk Filter™
8' Wide Concrete Vault
Washington State GULD
Six to Thirty One Cartridges / Stacks

Oldcastle®
Stormwater Solutions
7921 Southpark Plaza, Suite 200 | Littleton, CO | 80120 | P: 303.779.0119 | dcorse@stormwater.com
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DRAWING NO. PF-V-8-WA-0001 REV E ECO ECO-0122 DATE JPR 10/3/14 JPR 3/2/11 SHEET 1 OF 2



MINIMUM DEPTH - RIM TO OUTLET INVERT -

CARTRIDGE STACK CONFIGURATION

| 12" | 18" | 12" + 12" | 12" + 18" |
|----------------|----------------|----------------|----------------|
| 4.25' [31.00"] | 5.00' [60.00"] | 5.92' [71.00"] | 6.67' [80.00"] |

8' VAULT TREATMENT FLOW RATES, TOTAL FLOW CAPACITIES & MAXIMUM HEAD LOSS

CARTRIDGE STACK CONFIGURATION

| CARTRIDGE STACK QUANTITY | A DIMENSION - LENGTH - (FEET) | 12" | | 18" | | 12" & 12" | | 12" & 18" | |
|--------------------------|-------------------------------|---------------------------------|---------------------------|---------------------------------|---------------------------|---------------------------------|---------------------------|---------------------------------|---------------------------|
| | | TREATMENT FLOW RATE (GPM) (CFS) | TOTAL FLOW CAPACITY (CFS) | TREATMENT FLOW RATE (GPM) (CFS) | TOTAL FLOW CAPACITY (CFS) | TREATMENT FLOW RATE (GPM) (CFS) | TOTAL FLOW CAPACITY (CFS) | TREATMENT FLOW RATE (GPM) (CFS) | TOTAL FLOW CAPACITY (CFS) |
| 6 | 7 | 43.8 / 0.691 | 8.6 | 61.2 / 0.138 | 12.7 | 81.6 / 0.162 | 14.5 | 102.3 / 0.277 | 15.3 |
| 7 | 7 | 47.8 / 0.106 | 9.6 | 71.4 / 0.159 | 12.6 | 95.2 / 0.212 | 14.5 | 119.3 / 0.265 | 16.4 |
| 8 | 9 | 54.4 / 0.121 | 8.6 | 81.6 / 0.182 | 12.6 | 108.8 / 0.213 | 14.6 | 136.3 / 0.303 | 16.5 |
| 9 | 9 | 61.2 / 0.136 | 8.6 | 91.8 / 0.205 | 12.6 | 122.4 / 0.273 | 14.6 | 153.0 / 0.341 | 16.5 |
| 10 | 9 | 63.0 / 0.132 | 8.7 | 102.0 / 0.227 | 12.6 | 136.0 / 0.303 | 14.2 | 170.3 / 0.379 | 16.0 |
| 11 | 9 | 74.8 / 0.167 | 8.7 | 112.2 / 0.253 | 12.6 | 146.4 / 0.314 | 14.2 | 187.3 / 0.417 | 16.7 |
| 12 | 11 | 81.6 / 0.182 | 8.7 | 122.4 / 0.273 | 12.6 | 163.2 / 0.364 | 14.2 | 204.3 / 0.455 | 16.7 |
| 13 | 11 | 88.4 / 0.197 | 8.8 | 132.6 / 0.291 | 13.0 | 176.4 / 0.394 | 14.6 | 221.3 / 0.493 | 16.6 |
| 14 | 11 | 95.2 / 0.212 | 8.8 | 142.8 / 0.311 | 13.0 | 190.4 / 0.425 | 14.3 | 238.0 / 0.531 | 16.9 |
| 15 | 11 | 102.0 / 0.227 | 8.8 | 153.0 / 0.341 | 13.1 | 204.2 / 0.439 | 15.0 | 255.0 / 0.669 | 16.9 |
| 16 | 13 | 106.8 / 0.243 | 8.8 | 163.2 / 0.364 | 13.1 | 217.6 / 0.495 | 15.0 | 272.3 / 0.607 | 20.0 |
| 17 | 13 | 115.6 / 0.258 | 8.9 | 173.4 / 0.387 | 13.2 | 231.2 / 0.516 | 15.1 | 289.0 / 0.644 | 20.1 |
| 18 | 13 | 122.4 / 0.273 | 8.9 | 183.6 / 0.409 | 13.2 | 244.8 / 0.548 | 15.1 | 306.3 / 0.682 | 20.1 |
| 19 | 13 | 128.2 / 0.288 | 8.9 | 193.8 / 0.432 | 13.2 | 258.4 / 0.576 | 15.2 | 323.3 / 0.720 | 20.2 |
| 20 | 15 | 136.0 / 0.303 | 8.9 | 204.0 / 0.455 | 13.3 | 272.0 / 0.607 | 15.2 | 340.3 / 0.758 | 20.3 |
| 21 | 15 | 142.8 / 0.318 | 9.0 | 214.2 / 0.478 | 13.3 | 285.6 / 0.637 | 15.3 | 357.3 / 0.796 | 20.3 |
| 22 | 15 | 148.6 / 0.334 | 9.0 | 224.4 / 0.500 | 13.4 | 299.2 / 0.667 | 15.3 | 374.3 / 0.834 | 20.4 |
| 23 | 15 | 156.4 / 0.349 | 9.0 | 234.6 / 0.523 | 13.4 | 312.8 / 0.698 | 15.4 | 391.3 / 0.873 | 20.5 |
| 24 | 18 | 163.2 / 0.364 | 9.0 | 244.8 / 0.548 | 13.4 | 326.4 / 0.728 | 15.4 | 408.0 / 0.910 | 20.5 |
| 25 | 18 | 170.0 / 0.379 | 9.1 | 255.0 / 0.569 | 13.5 | 340.0 / 0.759 | 15.4 | 425.0 / 0.948 | 20.6 |
| 26 | 18 | 176.8 / 0.394 | 9.1 | 265.2 / 0.591 | 13.5 | 353.6 / 0.791 | 15.5 | 442.0 / 0.986 | 20.7 |
| 27 | 18 | 183.6 / 0.409 | 9.1 | 275.4 / 0.614 | 13.6 | 367.2 / 0.819 | 15.5 | 459.0 / 1.024 | 20.7 |
| 28 | 18 | 190.4 / 0.425 | 9.2 | 285.6 / 0.637 | 13.6 | 380.8 / 0.849 | 15.5 | 476.0 / 1.061 | 20.8 |
| 29 | 18 | 197.2 / 0.440 | 9.2 | 295.8 / 0.660 | 13.6 | 394.4 / 0.879 | 15.7 | 493.0 / 1.099 | 20.9 |
| 30 | 18 | 204.0 / 0.455 | 9.2 | 306.0 / 0.682 | 13.7 | 408.0 / 0.910 | 15.8 | 510.0 / 1.137 | 20.9 |
| 31 | 18 | 210.8 / 0.471 | 9.2 | 316.2 / 0.705 | 13.7 | 421.6 / 0.940 | 15.8 | 527.0 / 1.175 | 21.0 |
| MAXIMUM HEAD LOSS | | 1.7 FEET | 2.3 FEET | 2.9 FEET | 3.5 FEET | 4.1 FEET | 4.7 FEET | 5.3 FEET | 5.9 FEET |

★ Treatment Flow Rates shown conform to Washington State GULD Specifications.

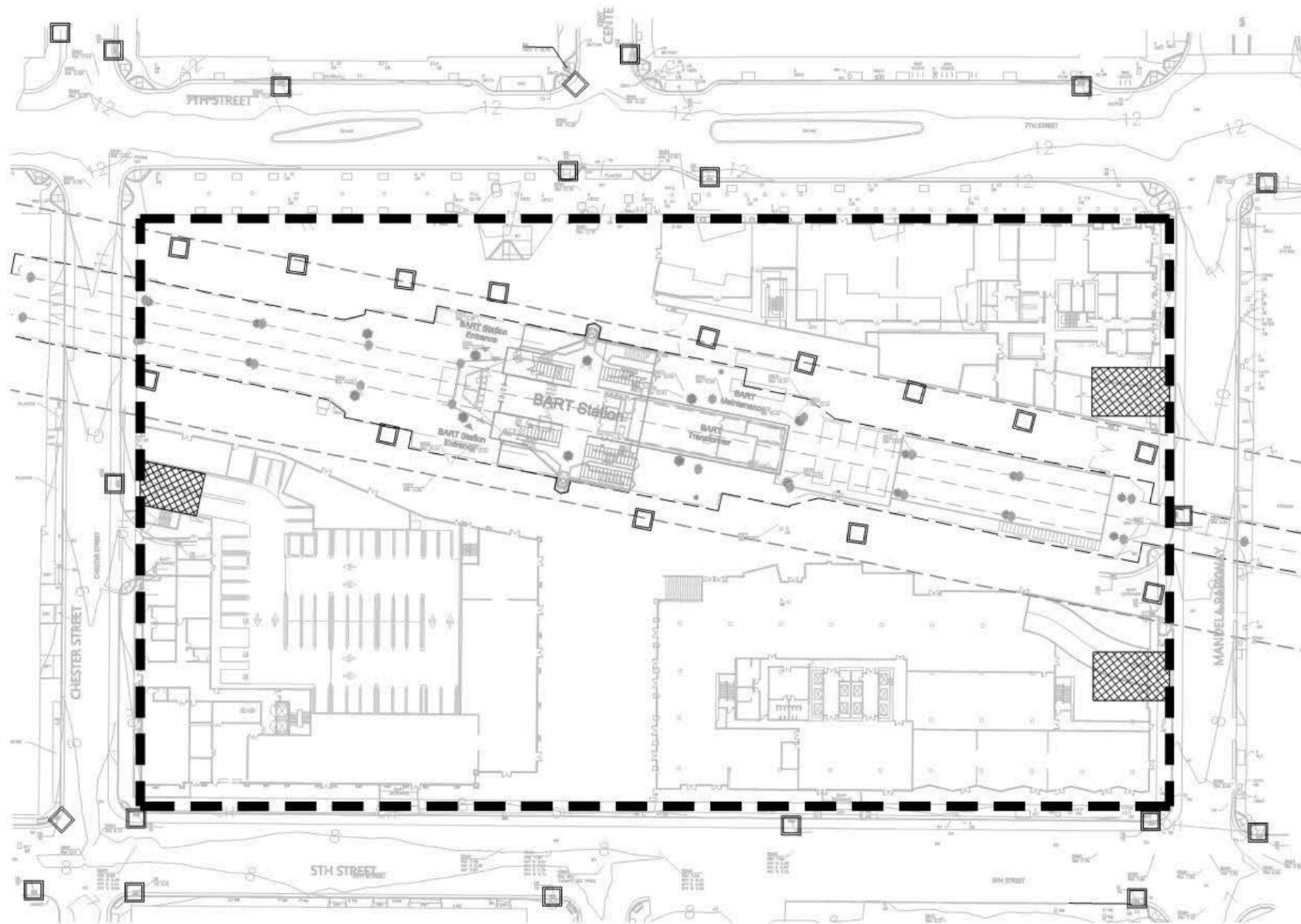
Perk Filter™
8' Wide Concrete Vault
Washington State GULD
Six to Thirty One Cartridges / Stacks

Oldcastle®
Stormwater Solutions
7921 Southpark Plaza, Suite 200 | Littleton, CO | 80120 | P: 303.779.0119 | dcorse@stormwater.com
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DRAWING NO. PF-V-8-WA-0001 REV E ECO ECO-0122 DATE JPR 10/3/14 JPR 3/2/11 SHEET 2 OF 2

Special Projects

Non-LID Treatment Summary

| Category | Impervious Area Created/Replaced (acres) | Site Coverage (%) | Project Density or FAR | Density/Criteria | Allowable Credit (%) | Applied Credit (%) |
|---------------------------|--|-------------------|------------------------|--|----------------------|--------------------|
| A | | | N.A. | N.A. | 100% | |
| B | | | | Res ≥ 50 DU/ac or FAR ≥ 2:1 | 50% | |
| | | | | Res ≥ 75 DU/ac or FAR ≥ 3:1 | 75% | |
| | | | | Res ≥ 100 DU/ac or FAR ≥ 4:1 | 100% | |
| C | | | | Location credit (select one)⁵: | | |
| | | | | Within ¼ mile of transit hub | 50% | 50% |
| | | | | Within ½ mile of transit hub | 25% | |
| | | | | Within a planned PDA | 25% | |
| | | | | Density credit (select one): | | |
| | | | | Res ≥ 30 DU/ac or FAR ≥ 2:1 | 10% | |
| | | | | Res ≥ 60 DU/ac or FAR ≥ 4:1 | 20% | |
| | | | | Res ≥ 100 DU/ac or FAR ≥ 6:1 | 30% | 30% |
| | | | | Parking credit (select one): | | |
| | | | | ≥ 10% at-grade surface parking ⁶ | 10% | |
| No surface parking | 20% | 20% | | | | |
| TOTAL TOD CREDIT = | | | | | 100% | |



EROSION CONTROL LEGEND:

-  STABILIZED CONSTRUCTION ENTRANCE (TC-1)** WITH ENTRANCE/OUTLET TIRE WASH (TC-3)**
-  STORM DRAIN INLET PROTECTION (SC-10)**
-  FIBER ROLL (SC-05)**

**REFER TO SHEET C4.01 FOR DETAILS

EROSION CONTROL NOTES:

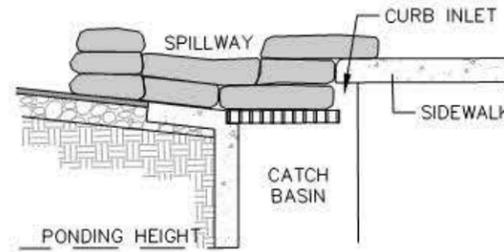
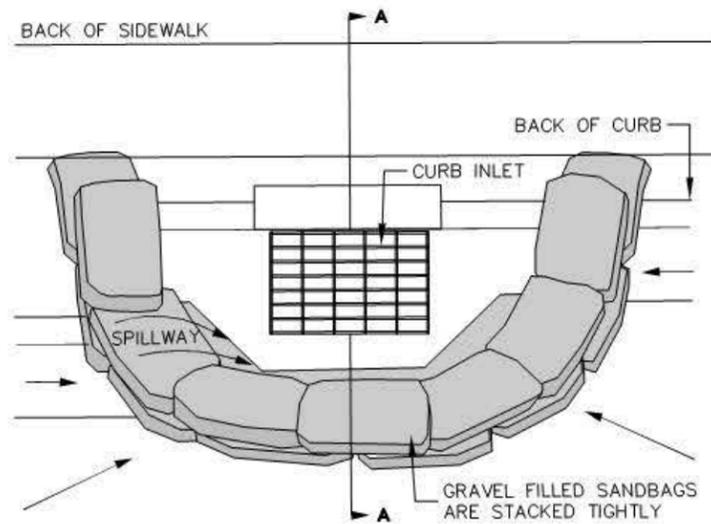
1. SITE ACCESS SHOWN ON THIS PLAN IS PROVIDED FOR INFORMATION PURPOSES ONLY. CONTRACTOR SHALL LOCATE CONSTRUCTION ACCESS DRIVEWAYS AS NECESSARY.
2. EROSION AND SEDIMENT CONTROL MEASURES SHALL BE IN EFFECT AND MAINTAINED BY THE CONTRACTOR ON A YEAR-ROUND BASIS UNTIL ALL DISTURBED AREAS ARE STABILIZED UNLESS OTHERWISE PERMITTED BY THE COUNTY INSPECTOR.
3. ALL INLETS RECEIVING STORM WATER RUNOFF FROM THE PROJECT AREA MUST BE EQUIPPED WITH REQUIRED INLET PROTECTION.
4. ALL PAVED AREAS SHALL BE KEPT CLEAR OF EARTH MATERIALS AND DEBRIS. THE SITE SHALL BE MAINTAINED SO AS TO MINIMIZE SEDIMENT LADEN RUNOFF ENTERING THE STORM DRAIN SYSTEM.
5. STOCKPILED EARTHEN MATERIAL SHALL BE EITHER COVERED WITH A TARP OR WATERED SUFFICIENTLY TO ELIMINATE DUST.
6. REFERENCE: "CALIFORNIA STORM WATER BEST MANAGEMENT PRACTICE (BMP) HANDBOOK", JANUARY 2015.



0 40 80



SCALE: 1" = 80'

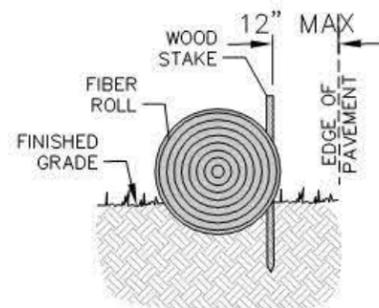


SECTION A-A

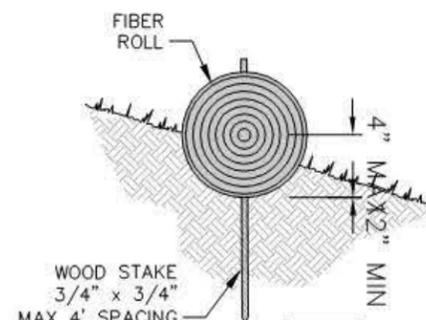
NOTES:

1. PLACE CURB TYPE SEDIMENT BARRIERS ON GENTLY SLOPING STREET SEGMENTS, WHERE WATER CAN POND AND ALLOW SEDIMENT TO SEPARATE FROM RUNOFF.
2. SANDBAGS OF EITHER BURLAP OR WOVEN 'GEOTEXTILE' FABRIC, ARE FILLED WITH GRAVEL LAYERED AND PACKED TIGHTLY.
3. LEAVE A ONE SANDBAG GAP IN THE TOP ROW TO PROVIDE A SPILLWAY FOR OVERFLOW.
4. INSPECT BARRIERS AND REMOVE SEDIMENT AFTER EACH STORM EVENT. SEDIMENT AND GRAVEL MUST BE REMOVED FROM THE TRAVELED WAY IMMEDIATELY.

① CURB INLET SEDIMENT BARRIER
NOT TO SCALE



ENTRENCHMENT DETAIL
IN FLAT AREA

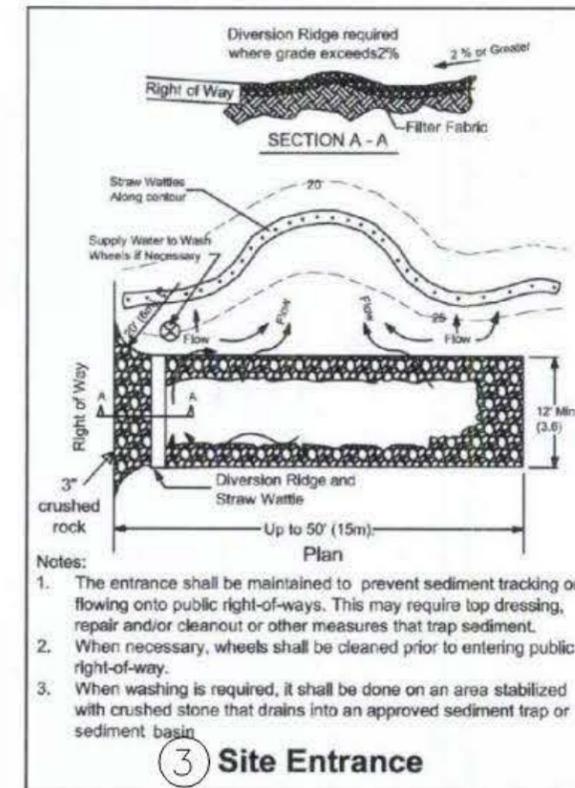


ENTRENCHMENT DETAIL
IN SLOPED AREA

NOTES

1. FIBER ROLLS ARE TUBES MADE FROM POROUS BIODEGRADABLE FIBER STUFFED IN A PHOTO-DEGRADABLE OPEN WEAVE NETTING. THEY ARE APPROXIMATELY 8" DIAMETER.
2. FIBER ROLL INSTALLATION REQUIRES THE PLACEMENT AND SECURE STAKING OF THE ROLL IN A TRENCH, 2"-4" DEEP, DUG ON CONTOUR. RUNOFF MUST NOT BE ALLOWED TO RUN UNDER OR AROUND ROLL. ROLLS SHOULD BE ABUTTED SECURELY TO PROVIDE A TIGHT JOINT, NOT OVERLAPPED.

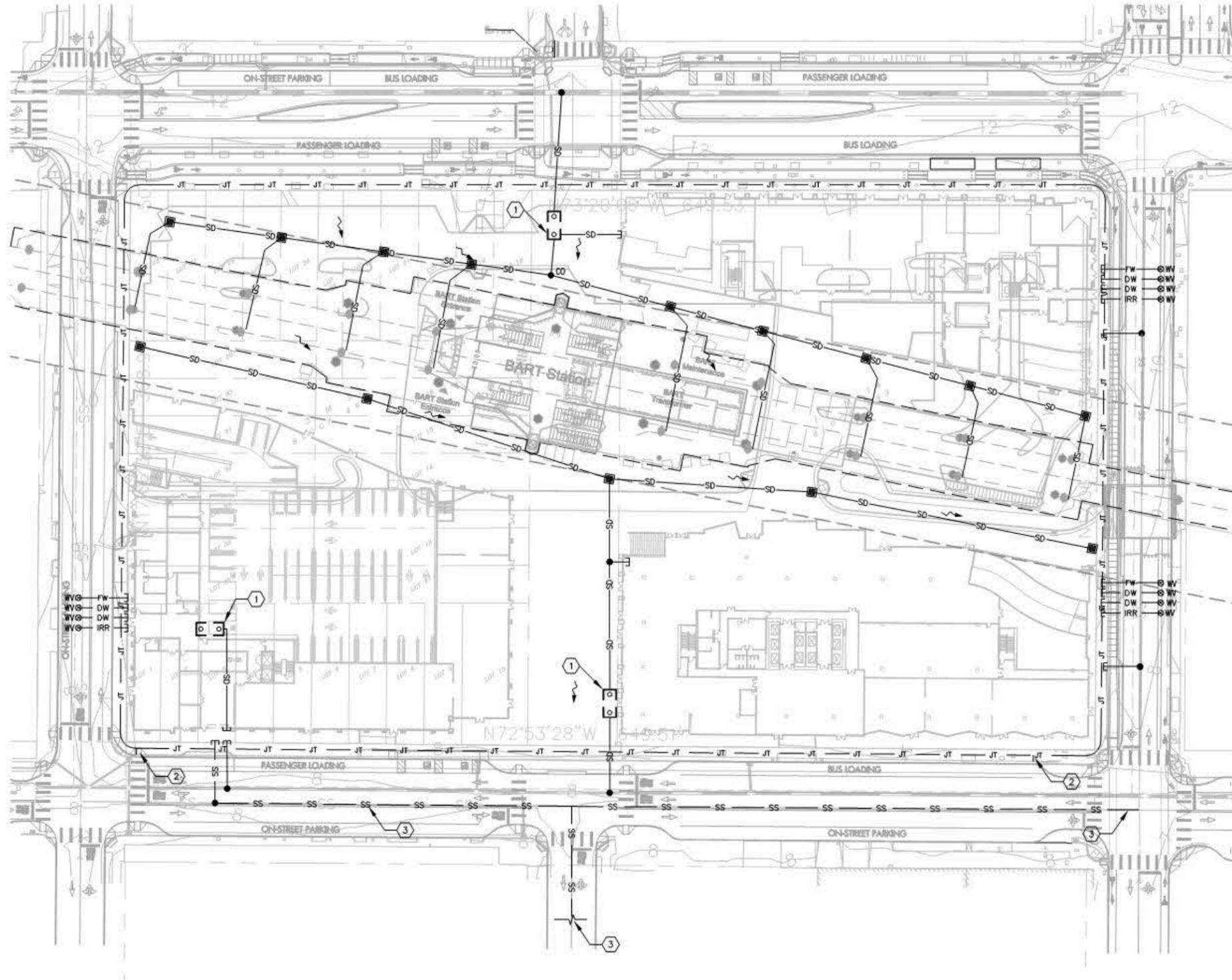
② FIBER ROLL
NOT TO SCALE



Notes:

1. The entrance shall be maintained to prevent sediment tracking or flowing onto public right-of-ways. This may require top dressing, repair and/or cleanout or other measures that trap sediment.
2. When necessary, wheels shall be cleaned prior to entering public right-of-way.
3. When washing is required, it shall be done on an area stabilized with crushed stone that drains into an approved sediment trap or sediment basin.

③ Site Entrance

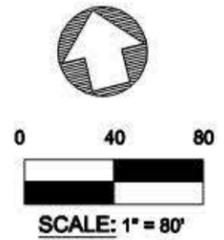


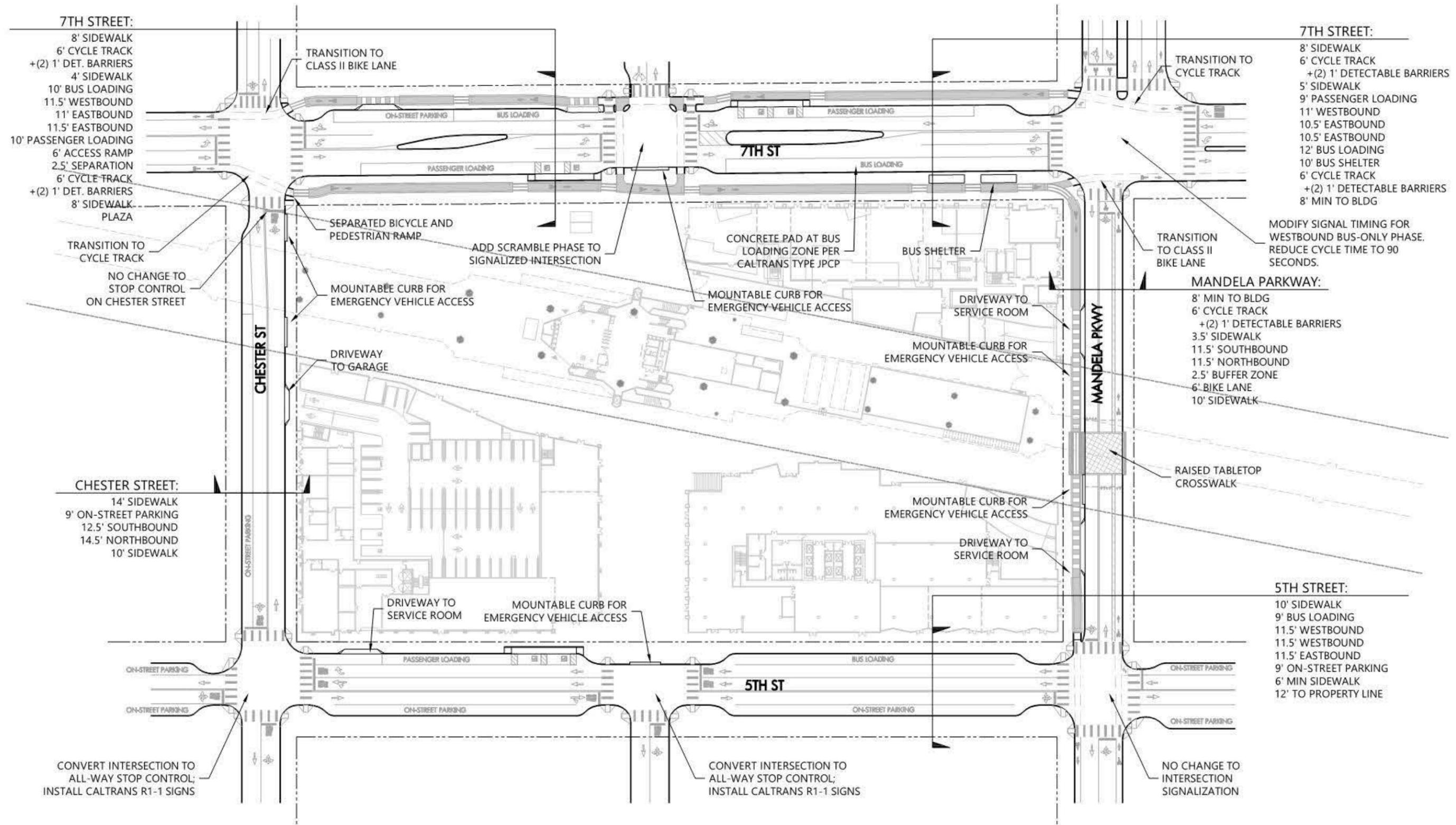
UTILITY KEYNOTES:

- ① NEW OLD CASTLE WASHINGTON GULD PV-V-8-WA-0001 PERK FILTER OR APPROVED EQUAL SEE DETAIL ON SHEET C3.01
- ② POTENTIAL CONNECTION LOCATION FOR ELECTRICAL AND TELECOM
- ③ REBUILD/UPSIZE SANITARY SEWER LINE TO CONNECTION AT CENTER ST x 3RD ST

UTILITY LEGEND:

- SD— STORM DRAIN LINE
- IRR— IRRIGATION LINE
- FW— FIRE WATER LINE
- DW— DOMESTIC WATER LINE
- SS— SANITARY SEWER LINE
- - - JT - - - JOINT TRENCH: ELEC + TELECOM
- DOMESTIC WATER MANHOLE PER CITY OF OAKLAND STANDARD DETAIL D-11
- STORM DRAIN MANHOLE
- WV⊙ WATER VALVE
- DRAIN INLET





7TH STREET:
 8' SIDEWALK
 6' CYCLE TRACK
 +(2) 1' DET. BARRIERS
 4' SIDEWALK
 10' BUS LOADING
 11.5' WESTBOUND
 11' EASTBOUND
 11.5' EASTBOUND
 10' PASSENGER LOADING
 6' ACCESS RAMP
 2.5' SEPARATION
 6' CYCLE TRACK
 +(2) 1' DET. BARRIERS
 8' SIDEWALK
 PLAZA

7TH STREET:
 8' SIDEWALK
 6' CYCLE TRACK
 +(2) 1' DETECTABLE BARRIERS
 5' SIDEWALK
 9' PASSENGER LOADING
 11' WESTBOUND
 10.5' EASTBOUND
 10.5' EASTBOUND
 12' BUS LOADING
 10' BUS SHELTER
 6' CYCLE TRACK
 +(2) 1' DETECTABLE BARRIERS
 8' MIN TO BLDG

CHESTER STREET:
 14' SIDEWALK
 9' ON-STREET PARKING
 12.5' SOUTHBOUND
 14.5' NORTHBOUND
 10' SIDEWALK

MANDELA PARKWAY:
 8' MIN TO BLDG
 6' CYCLE TRACK
 +(2) 1' DETECTABLE BARRIERS
 3.5' SIDEWALK
 11.5' SOUTHBOUND
 11.5' NORTHBOUND
 2.5' BUFFER ZONE
 6' BIKE LANE
 10' SIDEWALK

5TH STREET:
 10' SIDEWALK
 9' BUS LOADING
 11.5' WESTBOUND
 11.5' WESTBOUND
 11.5' EASTBOUND
 9' ON-STREET PARKING
 6' MIN SIDEWALK
 12' TO PROPERTY LINE



CYCLE TRACK CASE STUDY



CYCLE TRACK CASE STUDY

Rendering of Better Market Street with trapezoidal delineator between pedestrians and bicycles

Source:
San Francisco
Public Works
Better Market
Street Project

Trapezoidal delineator

Enhancing Mobility, Access and Safety for Pedestrians (Part II)

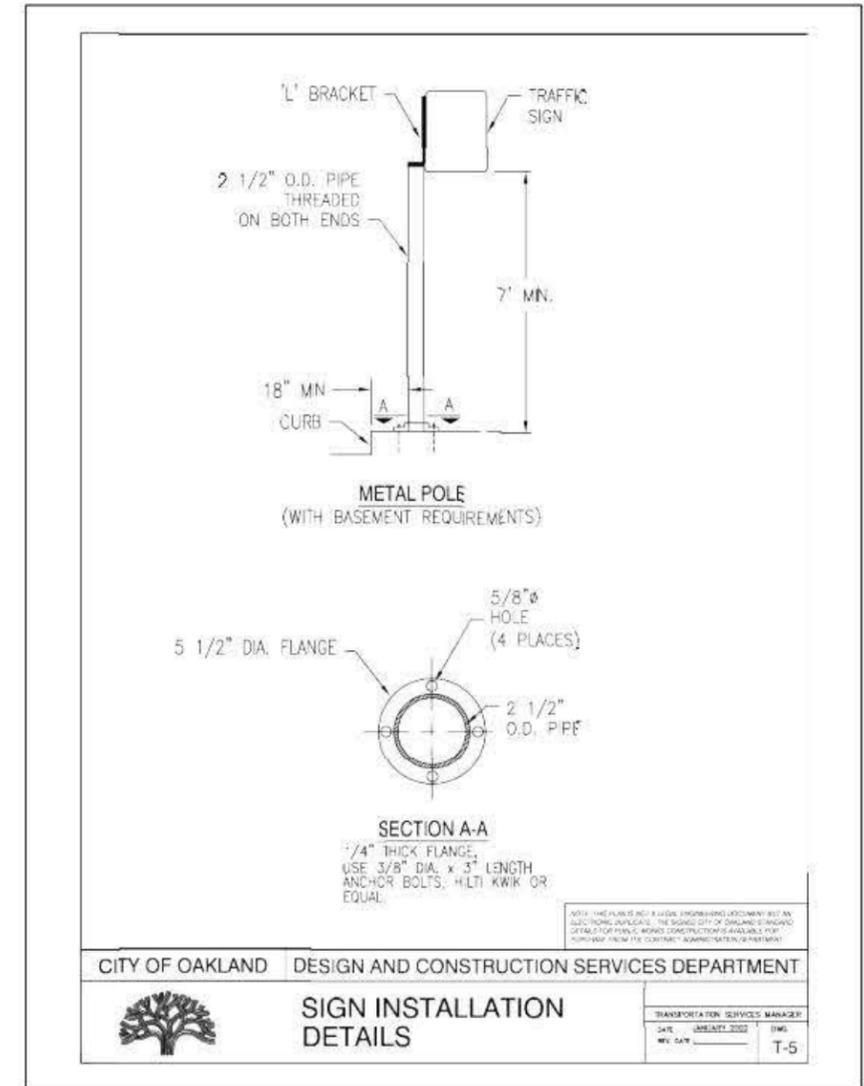
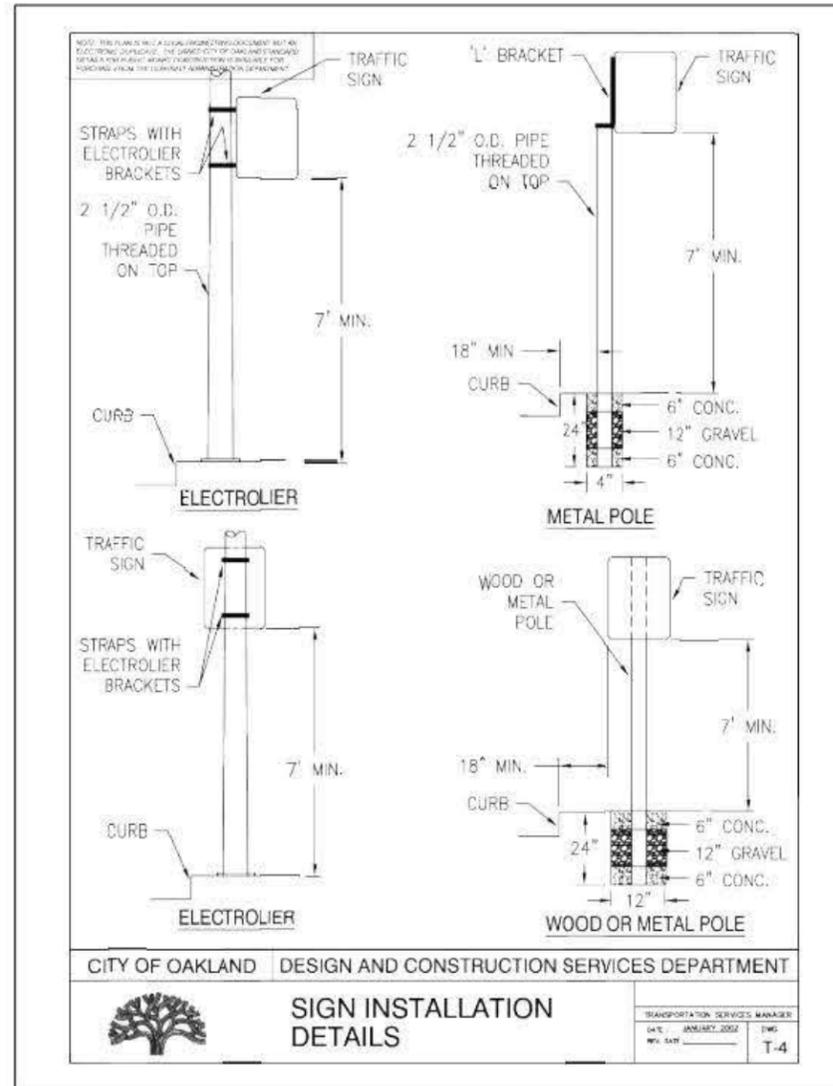
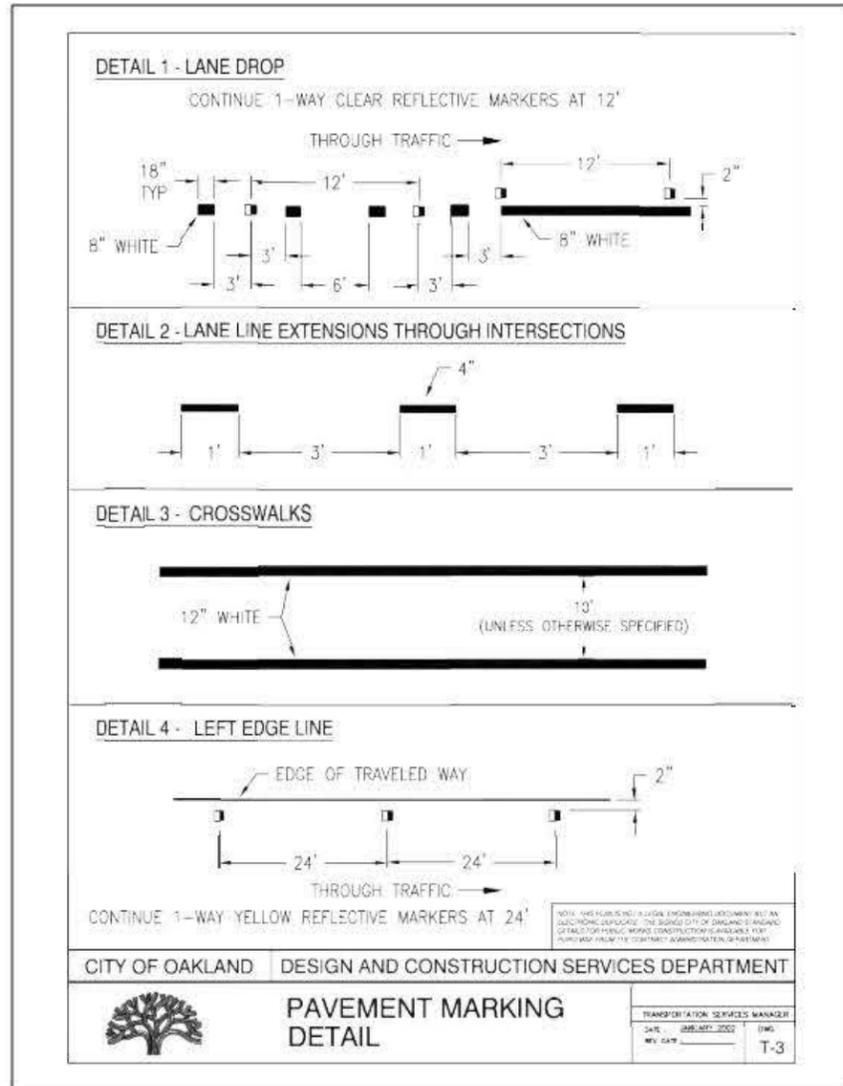
Recommendation
Use a trapezoidal indicator between bicycle and pedestrian sides of a separated bikeway at sidewalk level

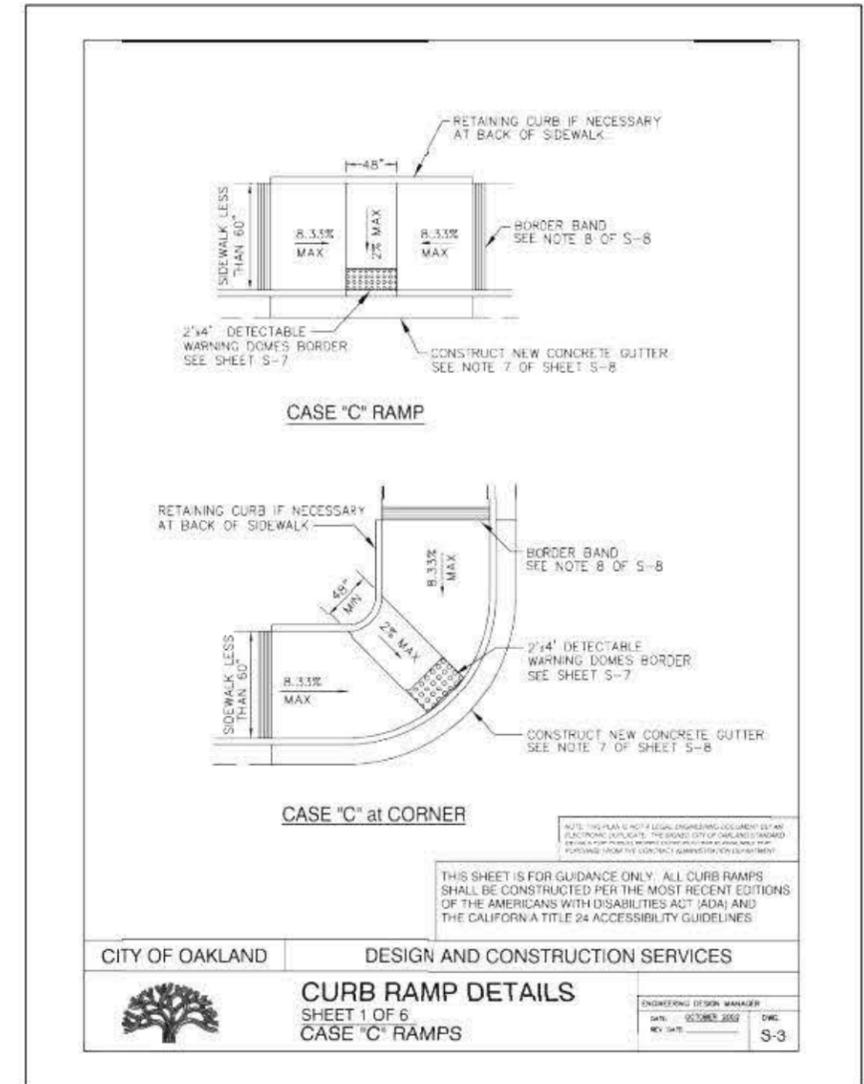
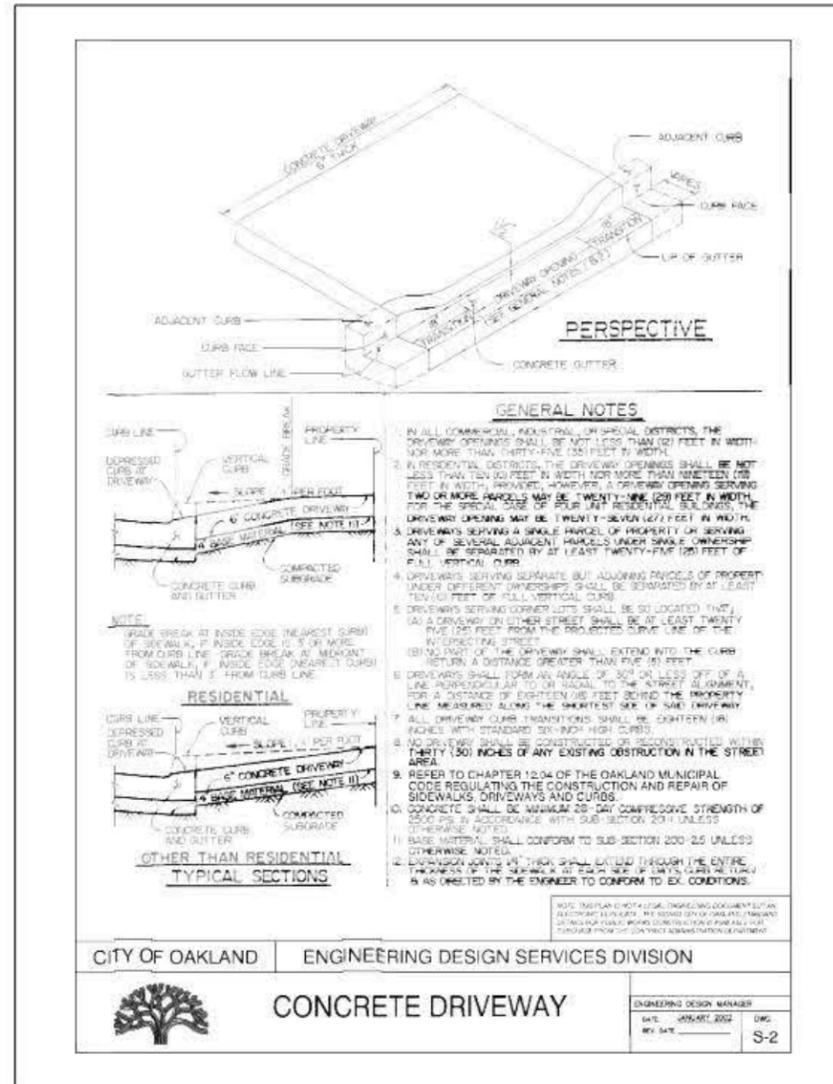
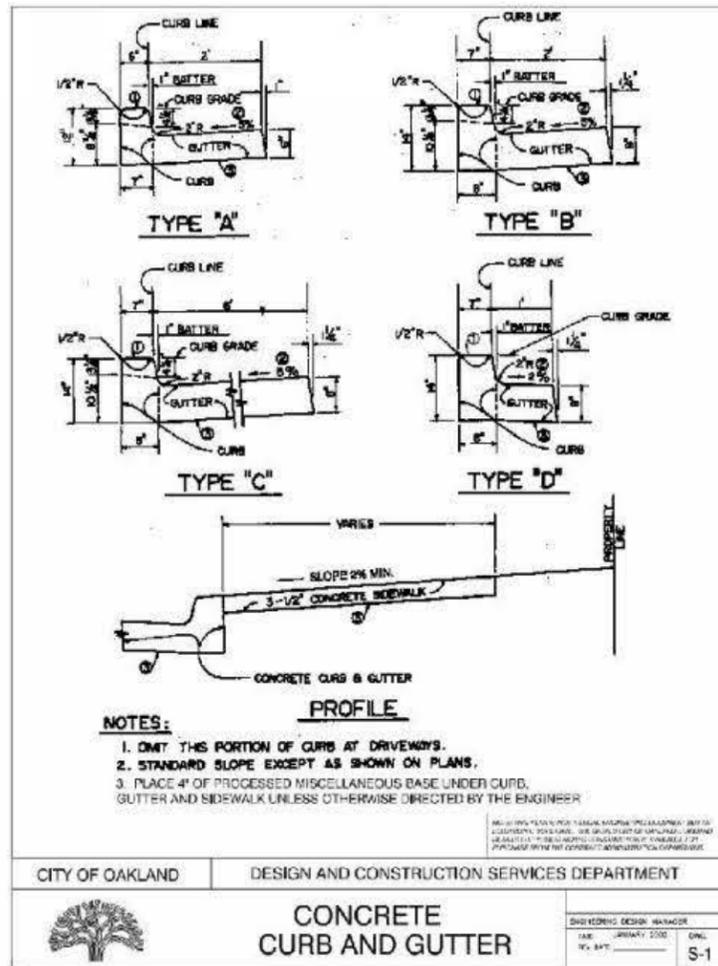
PLAN, Regular Tile: 12.00" (300 mm) width, 12.00" (300 mm) height

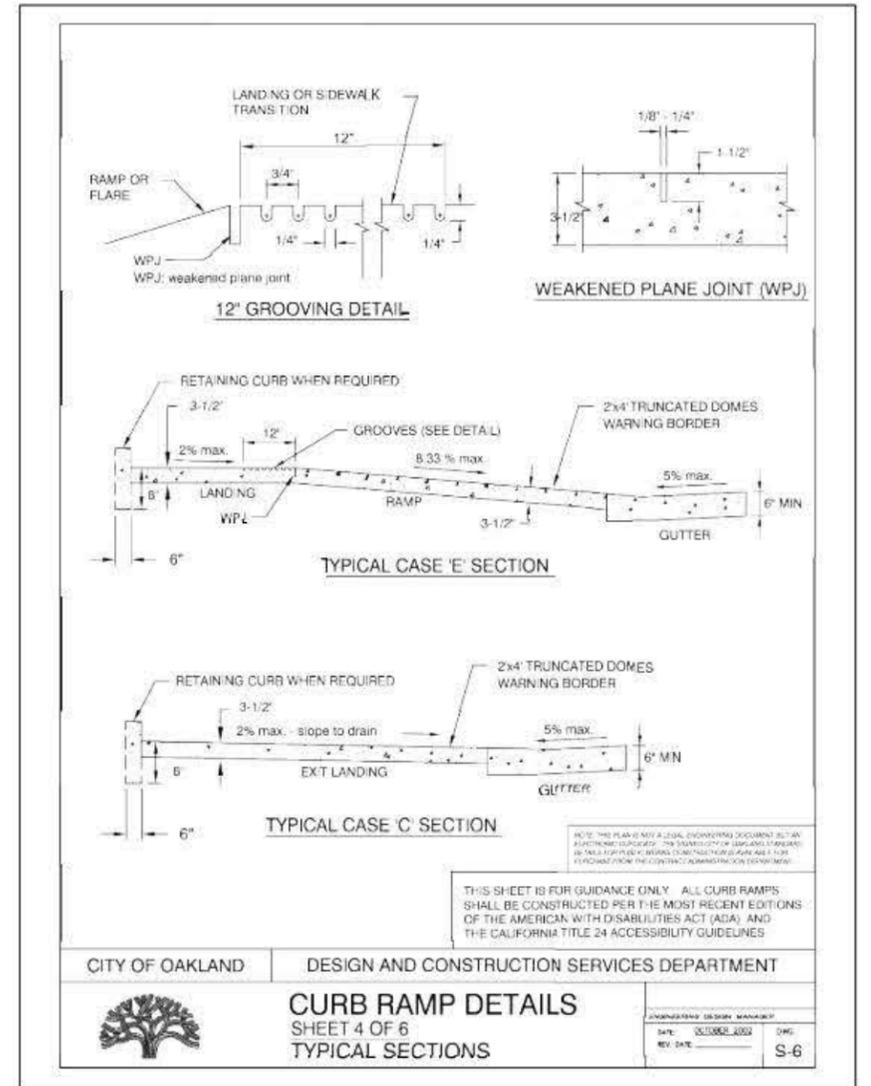
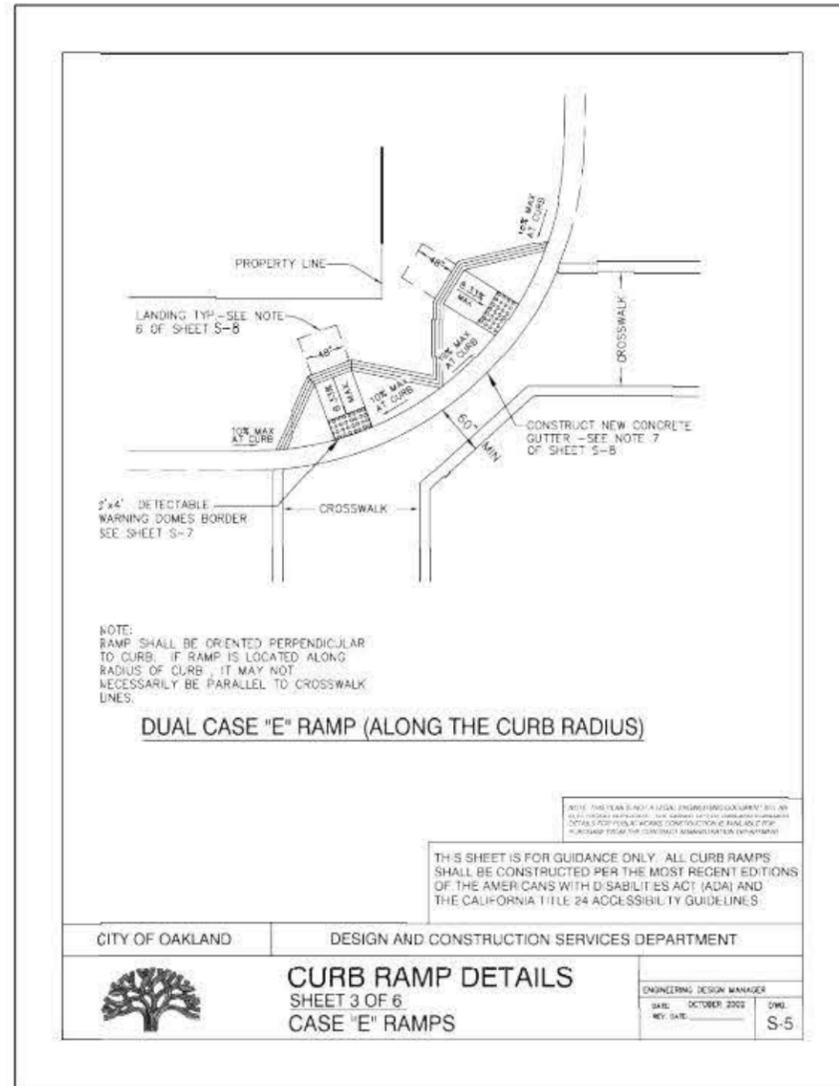
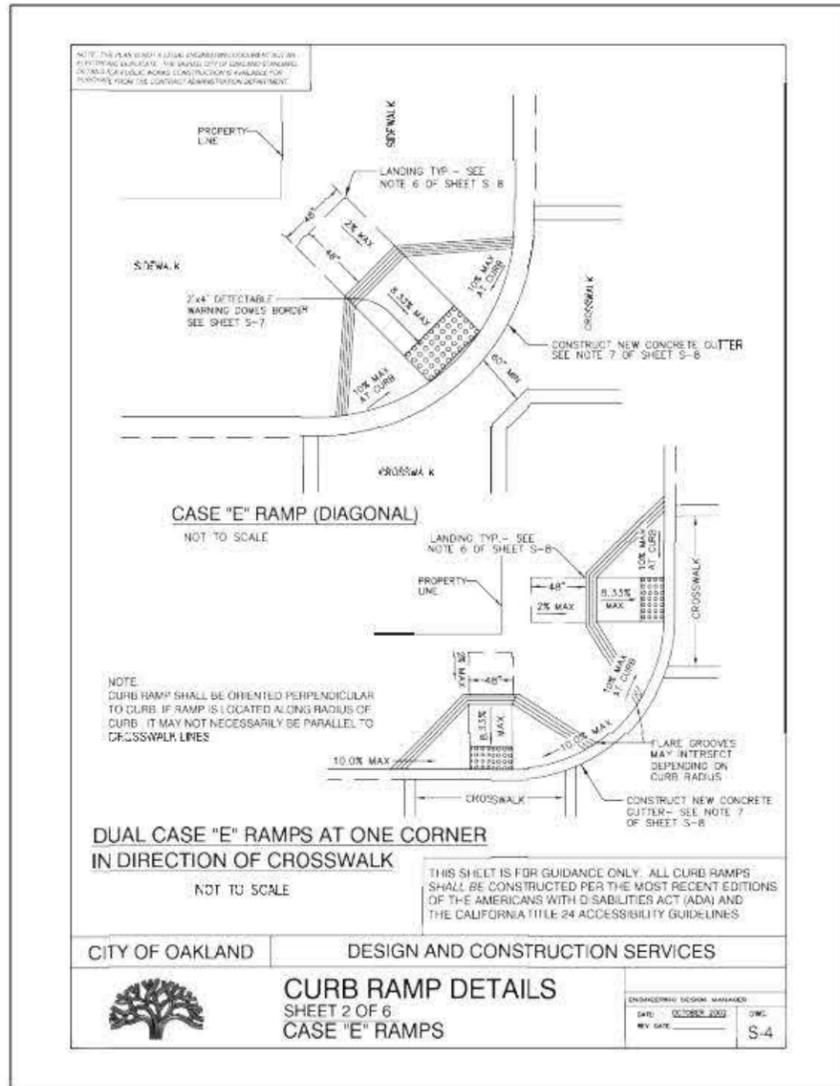
PLAN, End Tile: 12.00" (300 mm) width, 12.00" (300 mm) height

SECTION: 10.00" (250 mm) top width, 6.25" (160 mm) bottom width, 1.00" (25 mm) height

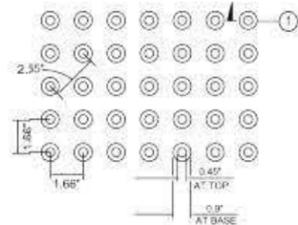
DETAIL: 1.875" (50 mm) top width, 1.00" (25 mm) bottom width, 0.75" (20 mm) height, 22° angle





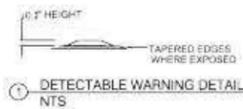


1. Curb ramps shall have 4 ft by 2 ft long detectable warning border centered and squared at the ramp bottom.
2. The detectable warning border shall contrast visually with the adjoining surfaces, either light or dark. The material used to provide contrast shall be an integral part of the walking surface and shall contrast by at least 70 percent. A fully cured 2 ft x 2 ft sample of both the cast-in-place detectable warning border and the adjacent curb ramp shall be submitted to the Engineer for review and approval prior to installation.
3. Dome orientation shall conform to the latest ADA / Title 24 regulations.
4. The closest corner of the truncated dome tile to the street shall be set 6" from the flowline.
5. Detectable dome warning tiles or strips made of materials other than concrete shall only be used with the written approval of the Director of Public Works.
6. Precast concrete detectable warning dome pavers shall be installed on top of a 4 inch thick concrete pad. Mortared joints shall be flush with top surface and finished to provide a smooth surface. Pavers shall be laid such that joints are level with adjoining joints to provide a smooth transition from paver to paver and from paver to concrete surface.



Bottom Diameter = 0.9 inches
 Top Diameter = 0.4 inches
 Height = 0.2 inches
 Center-to-Center Spacing = 2.35 inches

PLAN VIEW OF DETECTABLE WARNING DOMES
 NTS



THIS SHEET IS FOR GUIDANCE ONLY. ALL CURB RAMP SHALL BE CONSTRUCTED PER THE MOST RECENT EDITIONS OF THE AMERICAN WITH DISABILITIES ACT (ADA) AND THE CALIFORNIA TITLE 24 ACCESSIBILITY GUIDELINES.

| | |
|---|----------------------------------|
| CITY OF OAKLAND | DESIGN AND CONSTRUCTION SERVICES |
| CURB RAMP DETAILS | |
| SHEET 5 OF 6 | |
| DETECTABLE WARNINGS | |
| ENGINEERING DESIGN MANAGER DATE: OCTOBER 2012 REV DATE: | CWG S-7 |

CASE "C" CURB RAMP NOTES

1. If site conditions do not accommodate a Case E ramp, a Case "C" Curb Ramp shall be constructed.
2. If the sidewalk is less than 5-feet wide, a Case C ramp shall be installed.
3. The maximum slope of the lower landing of a Case C ramp shall be 2%. In all cases the lower landing shall be sloped to drain.
4. No vertical obstructions shall be allowed within a Case C curb ramp.

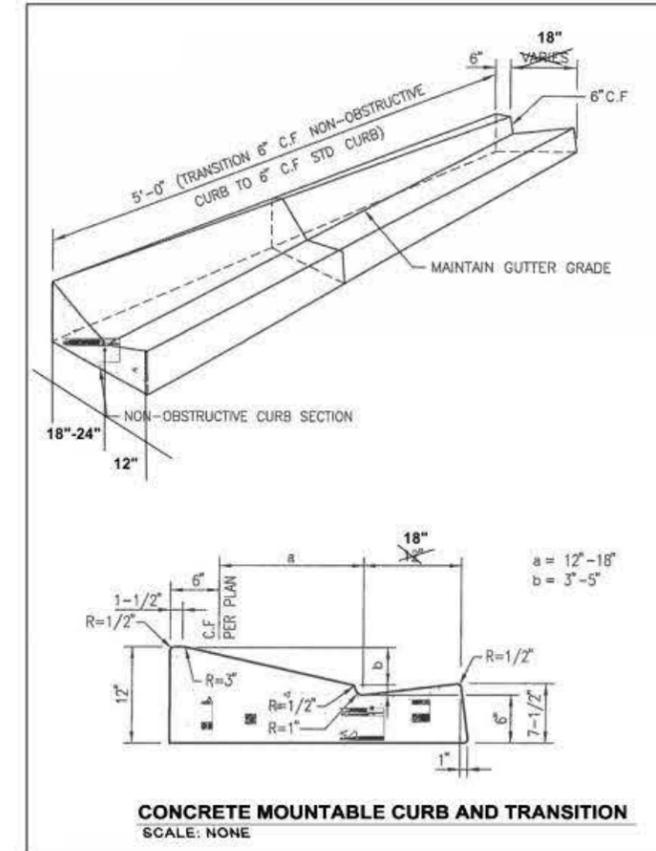
CASE "E" CURB RAMP NOTES:

5. Two ramps shall be provided at each corner of an intersection, unless otherwise allowed by the plans or by the Engineer. The centerline and path of travel of both curb ramps must be perpendicular to the gutter.
6. A level landing (4-ft deep with 2% max. slope) shall be provided at the upper end of each Case E curb ramp over its full width to permit safe exit from the ramp surface. If a level landing cannot be installed, then the side flares of the ramp shall slope less than or equal to 8.33%.

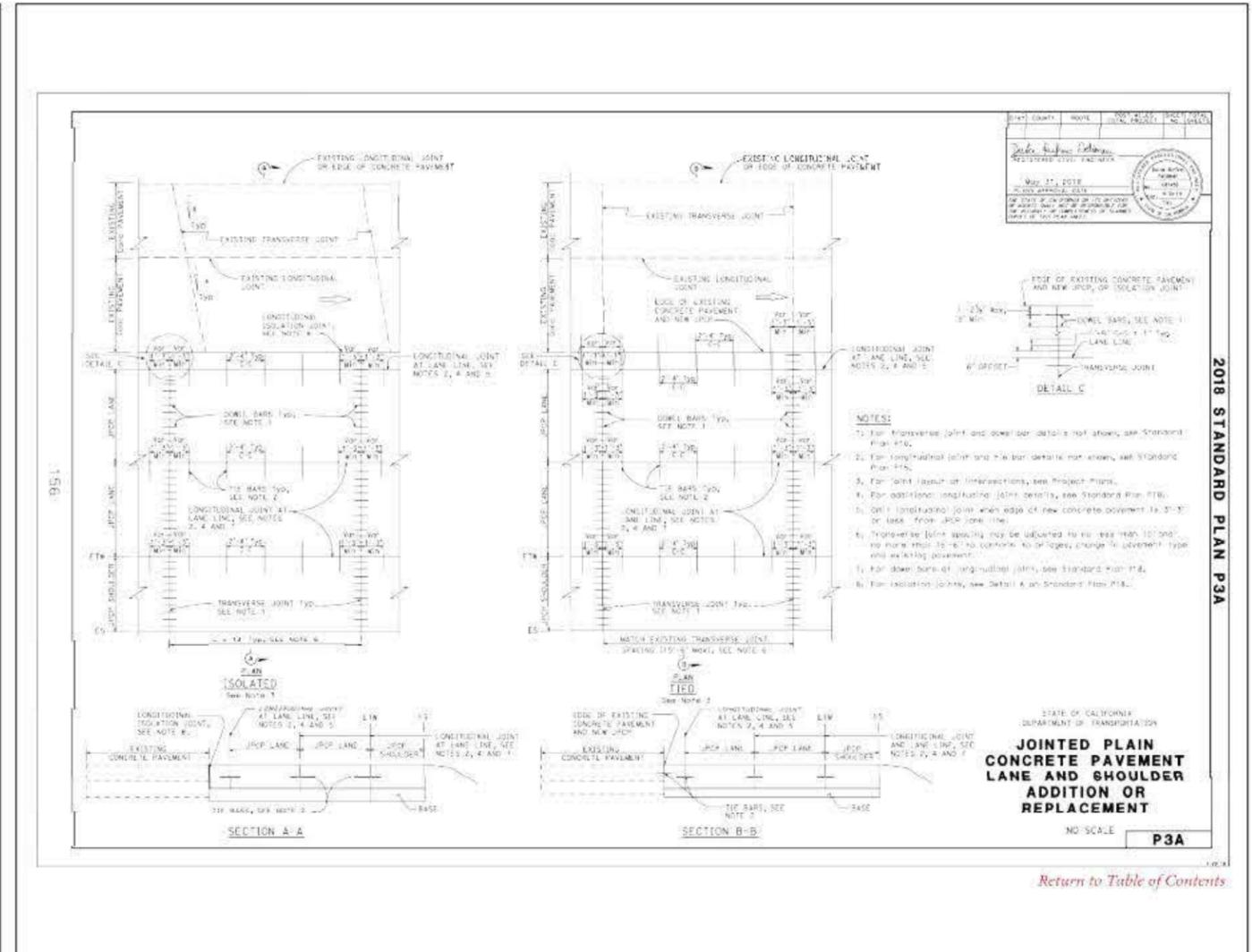
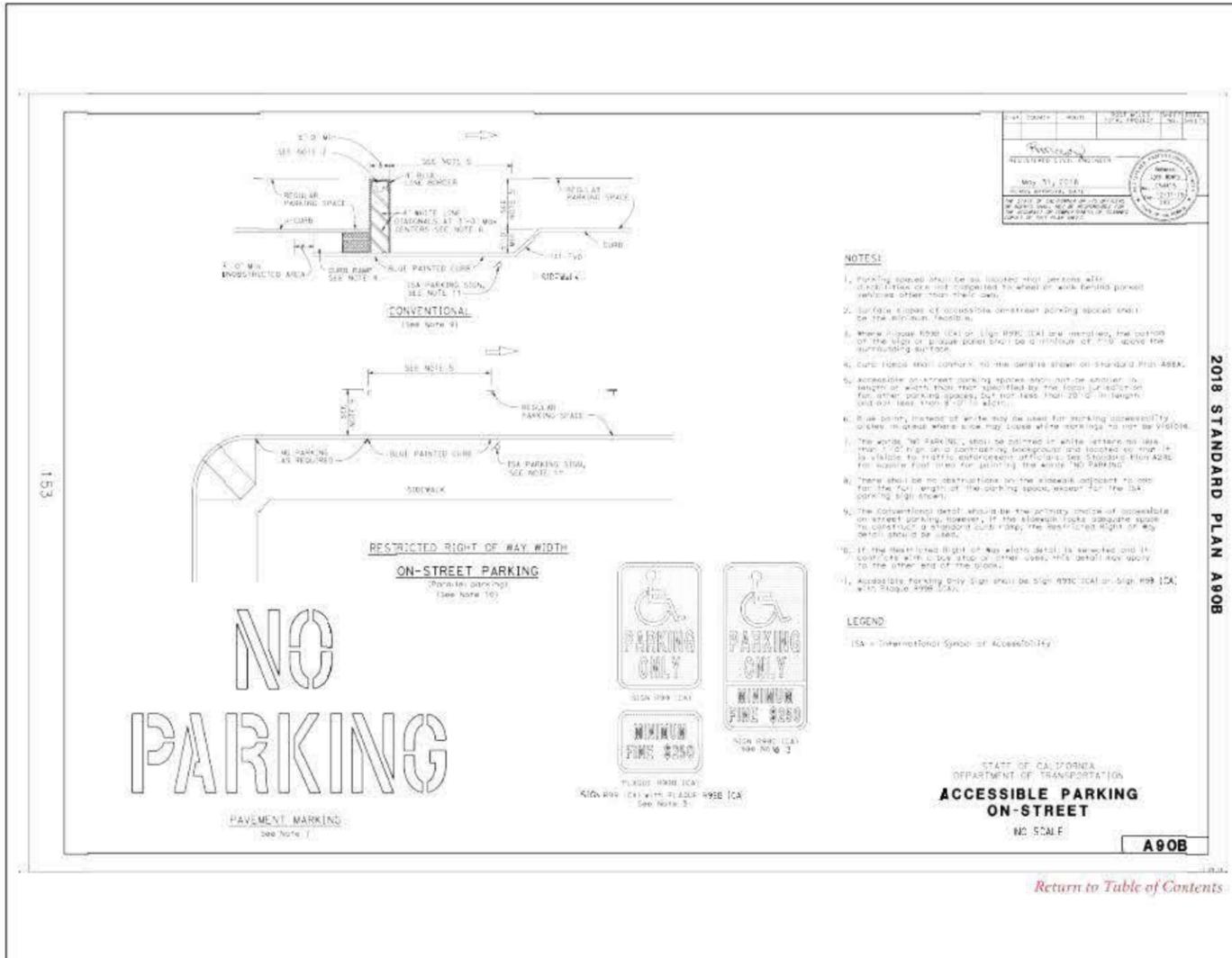
GENERAL CURB RAMP NOTES:

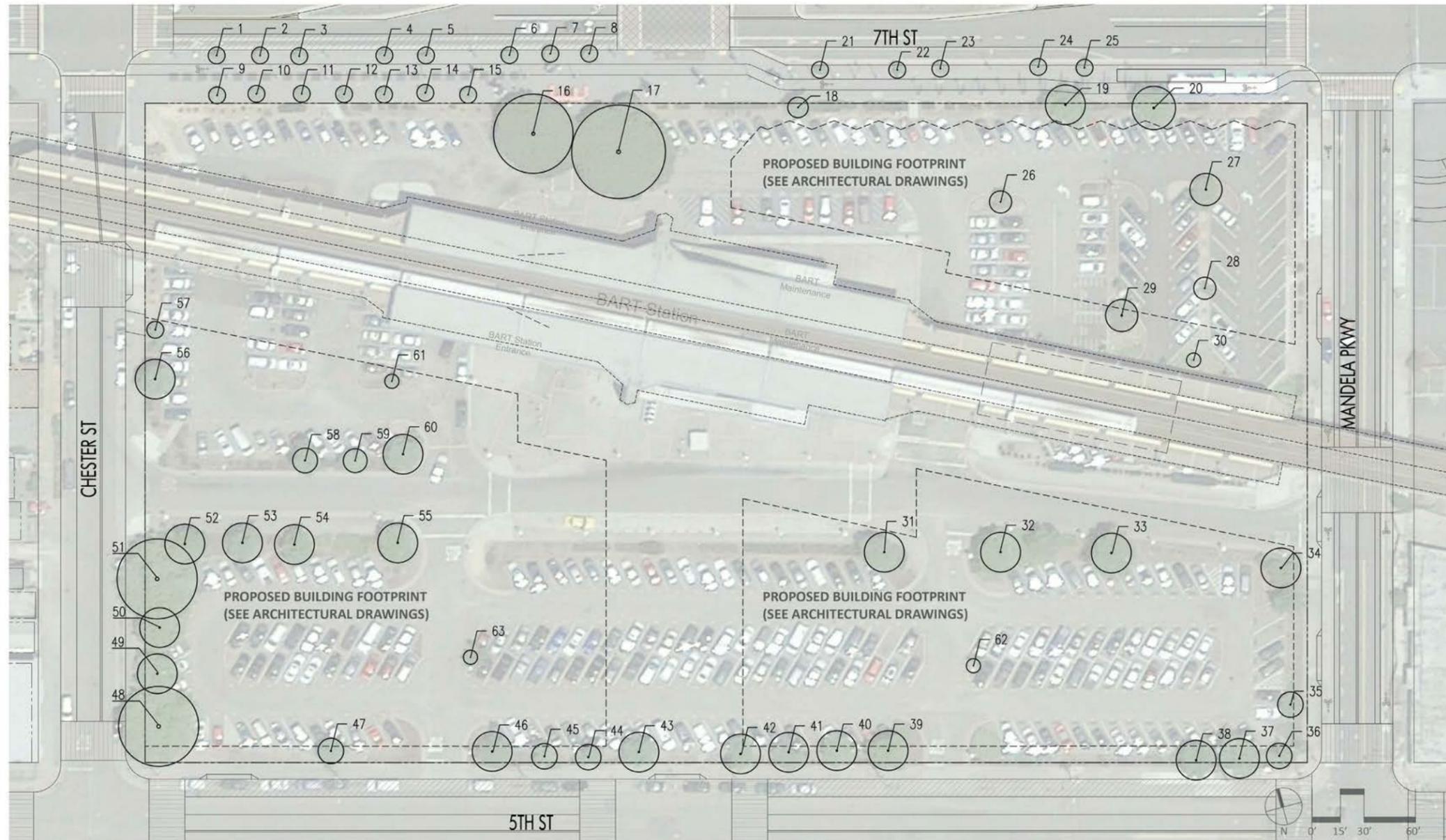
7. When constructing curb ramps, construct concrete gutter to match existing gutter widths. The new gutter shall be flush with the bottom of the curb ramp and the new gutter shall be considered part of the curb ramp construction.
8. Each curb ramp shall be bounded by a 12- inch border band with 1/4 inch deep grooves scored 3/4 inches apart except in the curb and gutter area.
9. Vertical obstructions, such as utility poles and traffic signals, shall only be allowed with the approval of the Engineer.
10. Existing utility boxes and covers shall be adjusted to conform flush with the curb ramp surface. All utility boxes and covers located within the detectable warning border area shall be relocated.

| | |
|---|-----------------------------|
| CITY OF OAKLAND | ENGINEERING DESIGN DIVISION |
| CURB RAMP DETAILS | |
| SHEET 6 OF 6 | |
| GENERAL NOTES | |
| ENGINEERING DESIGN MANAGER DATE: OCTOBER 2012 REV DATE: | CWG S-8 |



CONCRETE MOUNTABLE CURB AND TRANSITION
 SCALE: NONE





Trees Proposed for Removal

| Key | Species | DBH (in) |
|-----|--------------------|----------|
| 1 | Pistacia chinensis | 4 |
| 2 | Pistacia chinensis | 4 |
| 3 | Pistacia chinensis | 4 |
| 4 | Pistacia chinensis | 3 |
| 5 | Pistacia chinensis | 5 |
| 6 | Pistacia chinensis | 4 |
| 7 | Pistacia chinensis | 4 |
| 8 | Pistacia chinensis | 4 |
| 9 | Pistacia chinensis | 4 |
| 10 | Pistacia chinensis | 4 |
| 11 | Pistacia chinensis | 4 |
| 12 | Pistacia chinensis | 4 |
| 13 | Pistacia chinensis | 4 |

| Key | Species | DBH (in) |
|-----|-------------------------|----------|
| 14 | Pistacia chinensis | 4 |
| 15 | Pistacia chinensis | 4 |
| 16 | Pinus pinea | 36 |
| 17 | Pinus pinea | 32 |
| 18 | Liriodendron tulipifera | 9 |
| 19 | Liriodendron tulipifera | 11 |
| 20 | Liriodendron tulipifera | 13 |
| 21 | Pistacia chinensis | 4 |
| 22 | Pistacia chinensis | 4 |
| 23 | Pistacia chinensis | 4 |
| 24 | Pistacia chinensis | 4 |
| 25 | Pistacia chinensis | 5 |
| 26 | Maytenus boaria | 7 |

| Key | Species | DBH (in) |
|-----|-------------------------|----------|
| 27 | Maytenus boaria | 12 |
| 28 | Maytenus boaria | 5 |
| 29 | Maytenus boaria | 16 |
| 30 | Maytenus boaria | 2 |
| 31 | Liriodendron tulipifera | 13 |
| 32 | Liriodendron tulipifera | 11 |
| 33 | Liriodendron tulipifera | 13 |
| 34 | Liriodendron tulipifera | 11 |
| 35 | Liriodendron tulipifera | 9 |
| 36 | Liriodendron tulipifera | 12 |
| 37 | Liriodendron tulipifera | 11 |
| 38 | Liriodendron tulipifera | 10 |
| 39 | Liriodendron tulipifera | 11 |

| Key | Species | DBH (in) |
|-----|-------------------------|----------|
| 40 | Liriodendron tulipifera | 9 |
| 41 | Liriodendron tulipifera | 10 |
| 42 | Liriodendron tulipifera | 10 |
| 43 | Liriodendron tulipifera | 9 |
| 44 | Liriodendron tulipifera | 7 |
| 45 | Liriodendron tulipifera | 8 |
| 46 | Liriodendron tulipifera | 8 |
| 47 | Liriodendron tulipifera | 9 |
| 48 | Platanus X acerifolia | 18 |
| 49 | Platanus X acerifolia | 10 |
| 50 | Platanus X acerifolia | 10 |
| 51 | Platanus X acerifolia | 17 |
| 52 | Liriodendron tulipifera | 12 |

| Key | Species | DBH (in) |
|-----|-------------------------|----------|
| 53 | Liriodendron tulipifera | 8 |
| 54 | Liriodendron tulipifera | 8 |
| 55 | Liriodendron tulipifera | 9 |
| 56 | Liriodendron tulipifera | 10 |
| 57 | Liriodendron tulipifera | 5 |
| 58 | Liriodendron tulipifera | 8 |
| 59 | Liriodendron tulipifera | 7 |
| 60 | Liriodendron tulipifera | 9 |
| 61 | Maytenus boaria | 5 |
| 62 | Maytenus boaria | 5 |
| 63 | Maytenus boaria | 7 |

Bolded trees are protected trees.



BUILDING T1
FDP PACKAGE

PROJECT

WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM

BKF ENGINEERS
1545 N. CALIFORNIA
BLVD, SUITE 400
WALNUT CREEK
CA 94596
(925) 940-2200

PGA DESIGN
444 17th Street
Oakland
CA 94612
(510) 455-1284

STOK
9458 Front Street
San Francisco
CA 94111
(415) 329-7100

STAMP

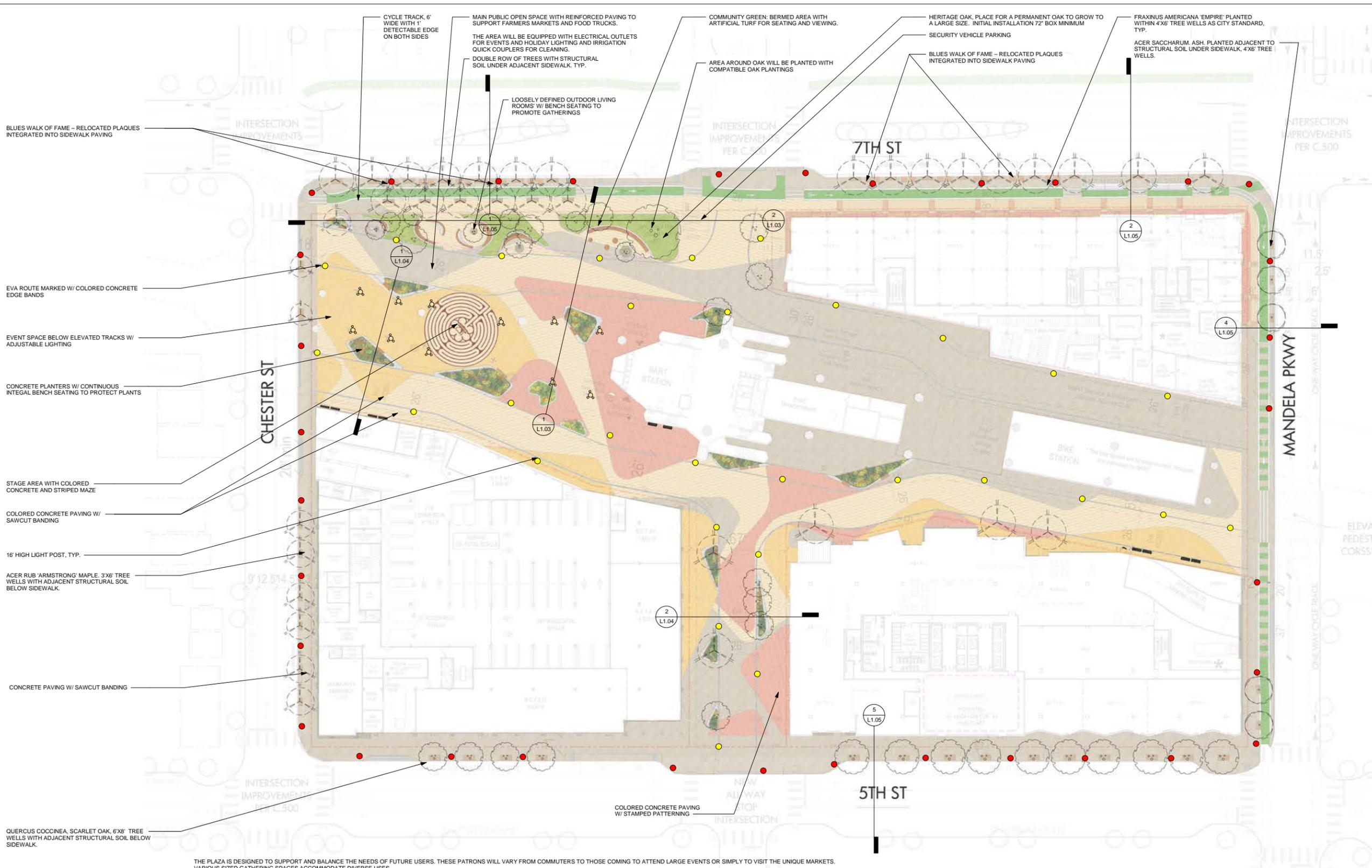
KEY

PROJ #: 168-153 WO BART
DATE: 06/04/20
SCALE: 1"=60' - 0"

TITLE:
**TREE
REMOVAL
PLAN**

SHEET:
L1.00

OWNER
HEC
HENSEL PHELPS
suda
 BUILDING T1 FDP PACKAGE
 PROJECT
WEST OAKLAND BART
 1451 7th St, Oakland, CA 94607
 ARCHITECT
jrdrv
 PROJECT TEAM
BKF ENGINEERS
BKF100
PGA DESIGN
stok
 STAMP
 KEY
 PROJ #: 168-153 WO BART
 DATE: 03/26/20
 SCALE: 1"=60' - 0"
 TITLE: **LAYOUT & MATERIALS PLAN**
 SHEET: **L1.01**



IRRIGATION METHOD & WATER EFFICIENT LANDSCAPE ORDINANCE

- IRRIGATION SYSTEM WILL BE DESIGNED TO PROVIDE THE MINIMUM AMOUNT OF WATER NECESSARY TO SUSTAIN GOOD PLANT HEALTH. ALL SELECTED COMPONENTS TO BE COMMERCIAL GRADE, SELECTED FOR DURABILITY, VANDAL RESISTANCE AND MINIMUM MAINTENANCE REQUIREMENT.
- THE SYSTEM WILL BE DRIPLINE IRRIGATION AS APPROPRIATE TO PLANT TYPE, EXPOSURE AND SLOPE CONDITIONS.
- CONTROL OF THE SYSTEM WILL BE VIA A WEATHER-ENABLED CONTROLLER CAPABLE OF DAILY SELF-ADJUSTMENT BASED ON REAL-TIME WEATHER CONDITIONS AS MEASURED BY AN ON-SITE WEATHER SENSOR.
- THE SYSTEM WILL INCLUDE A MASTER CONTROL VALVE AND FLOW SENSING CAPABILITY WHICH WILL SHUT DOWN ALL OR PART OF THE SYSTEM IF LEAKS ARE DETECTED.
- THE LANDSCAPE IS OVER 500 SQUARE FEET OF NEW LANDSCAPING AND WILL THEREFORE MEET THE REQUIREMENTS OF THE WATER EFFICIENCY LANDSCAPE ORDINANCE (WELO): [HTTPS://WATER.CA.GOV/LEGACYFILES/WATERUSEEFFICIENCY/LANDSCAPEORDINANCE/DOCS/TITLE%2023%20EXTRACT%20-%20OFFICIAL%20CCR%20PAGES.PDF](https://water.ca.gov/legacy/files/wateruseefficiency/landscapeordinance/docs/title%2023%20extract%20-%20official%20CCR%20PAGES.PDF)

WEST OAKLAND STATION CRIME PREVENTION THROUGH ENVIRONMENTAL DESIGN STRATEGIES

AREA LIGHTING:

- ALL LOADING, UNLOADING, KISS AND RIDE, DROP OFF, PICK UP AREAS WILL BE LIT TO 5 CANDLE FOOT. SIDEWALKS AROUND THE SITE WILL BE LIT TO 1.25 CANDLE FOOT (CITY STANDARD). BIKE LOCKER AREAS WILL BE LIT TO 10 CANDLE FOOT, AND EXTERIOR PATRON WAITING AREAS OUTSIDE THE BART FAIR GATES WILL BE LIT TO 10 CANDLE FOOT. SEE SHEET L1.07 FOR MORE DETAIL REGARDING THE LOCATION OF THESE AREAS.
- POLE LIGHTS ARE KEPT 20' CLEAR OF TREES AND LANDSCAPING. LIGHT POLES ARE LESS THAN 22' HIGH. OTHER LIGHTS WILL BE MOUNTED ON BUILDING, ON BOLLARDS OR ALONG OVERHEAD WIRES (<20' HIGH).
- THE LIGHTING WILL USE A FULL COLOR INDEX IN ORDER TO BETTER DISTINGUISH FORM AND MOVEMENT.

AREA PLANTING:

- PLANTINGS, INCLUDING THE RAISED PLANTERS AND BERMS THEY ARE IN, WILL NOT EXCEED 36" IN HEIGHT. TREES WILL BE LIMBED UP TO 7' HIGH. THE INTENTION IS TO AVOID BLIND SPOTS CAUSED BY HIGH PLANTS AND PLANTERS. TREES WILL BE THE TYPE THAT RETAIN A THIN TRUNK AS THEY GROW AND MATURE. THE TREE CANOPIES WILL NOT BE OVERLY DENSE AND WILL ALLOW LIGHT IN FROM ABOVE.

AREA SITE FURNISHINGS:

- SITE FURNISHINGS, EARTH FORMS, AND ARTWORK WILL BE DESIGNED NOT TO OBSCURE VISIBILITY. BENCHES HAVE SEPARATORS/ARMRESTS, SO THEY ARE USED FOR SITTING ONLY. PLANTER BOXES AND LANDSCAPE WALLS WILL HAVE GROOVES OR ELEMENTS TO DETER SKATEBOARDERS. FENCES ARE DESIGNED TO RESIST TEMPERING AND CLIMBING.

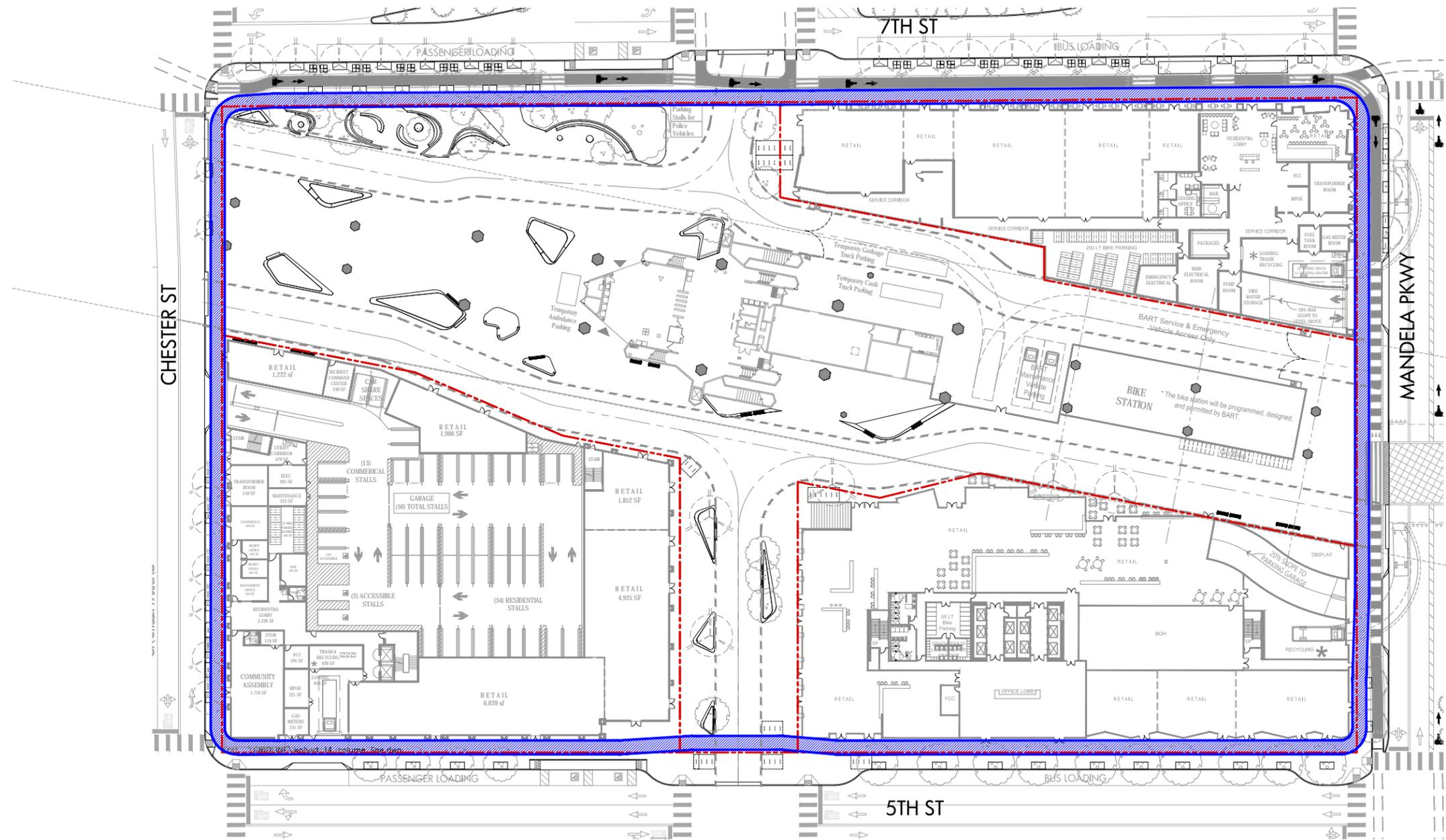
AREA SIGNAGE:

- DIRECTIONAL AND WAYFINDING SIGNAGE WILL BE PLACED AT APPROPRIATE LOCATIONS TO HELP DIRECT THE VISITOR TO THEIR DESTINATION.

SURVEILLANCE CAMERAS:

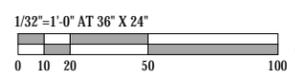
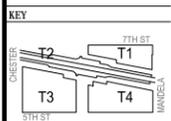
- A SERIES OF SURVEILLANCE CAMERAS WILL BE PLACED TO COVER THE ENTIRE SITE. THESE CAMERAS WILL COMPLY WITH BART FACILITY STANDARDS.

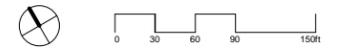
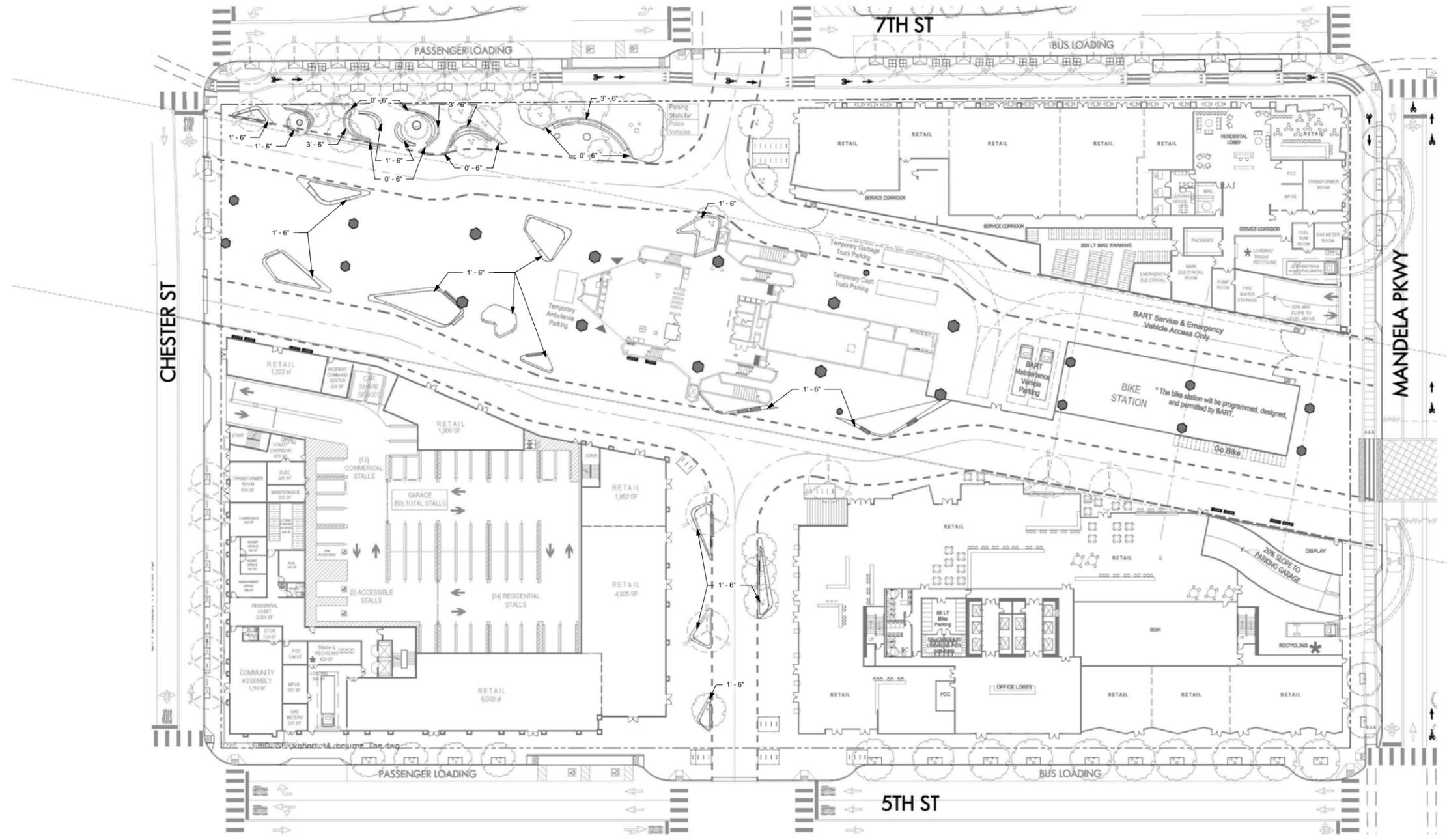
THE PLAZA IS DESIGNED TO SUPPORT AND BALANCE THE NEEDS OF FUTURE USERS. THESE PATRONS WILL VARY FROM COMMUTERS TO THOSE COMING TO ATTEND LARGE EVENTS OR SIMPLY TO VISIT THE UNIQUE MARKETS. VARIOUS SIZED GATHERING SPACES ACCOMMODATE DIVERSE USES.



LEGEND

-  8' CLEAR ZONE FOR PEDESTRIAN
-  PROPERTY LINE







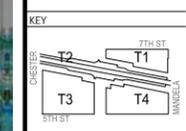
1 SECTION 1
 L1.03 1/16" = 1'-0"



2 SECTION 2
 L1.03 1/16" = 1'-0"



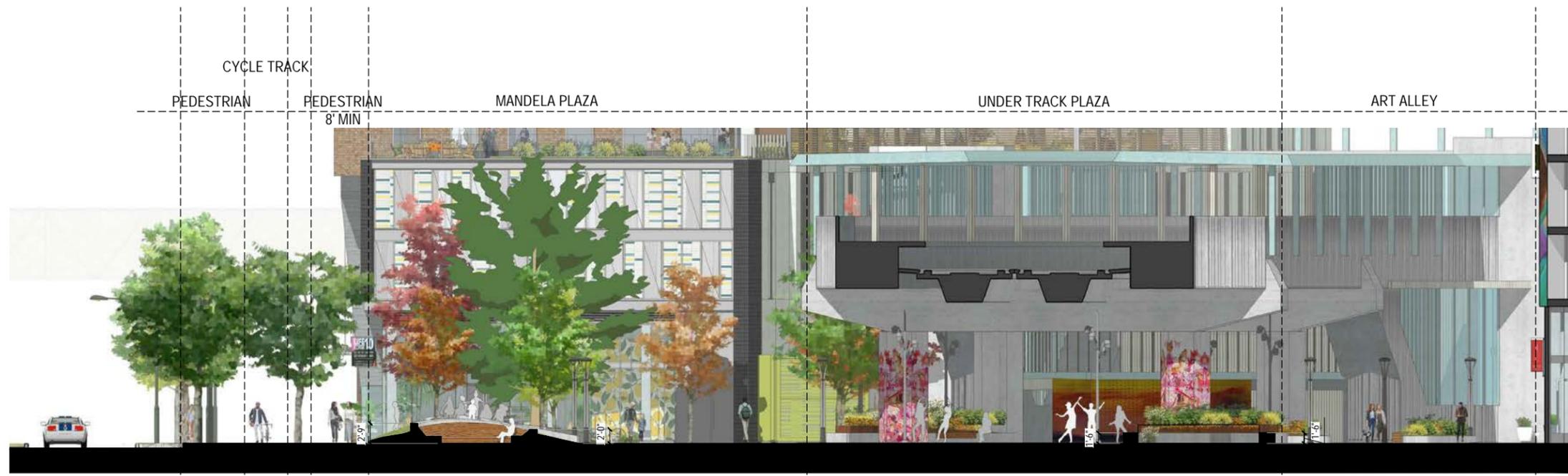
STAMP



PROJ #: 168-153 WO BART
 DATE: July 24, 2020
 SCALE: 1/16" = 1'-0"
 TITLE:

SECTIONS

SHEET: L1.03

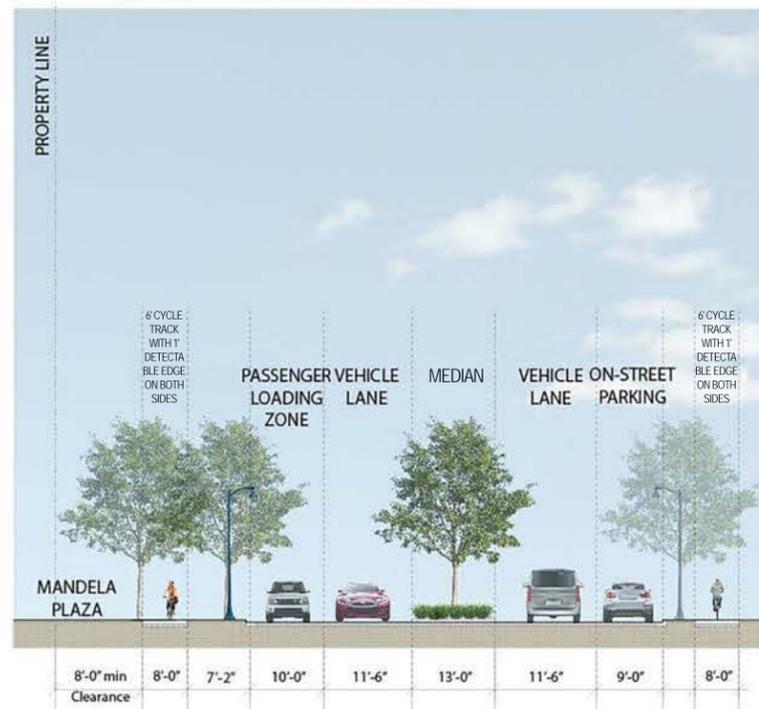


1 SECTION 3
L1.04 1/16" = 1'-0"

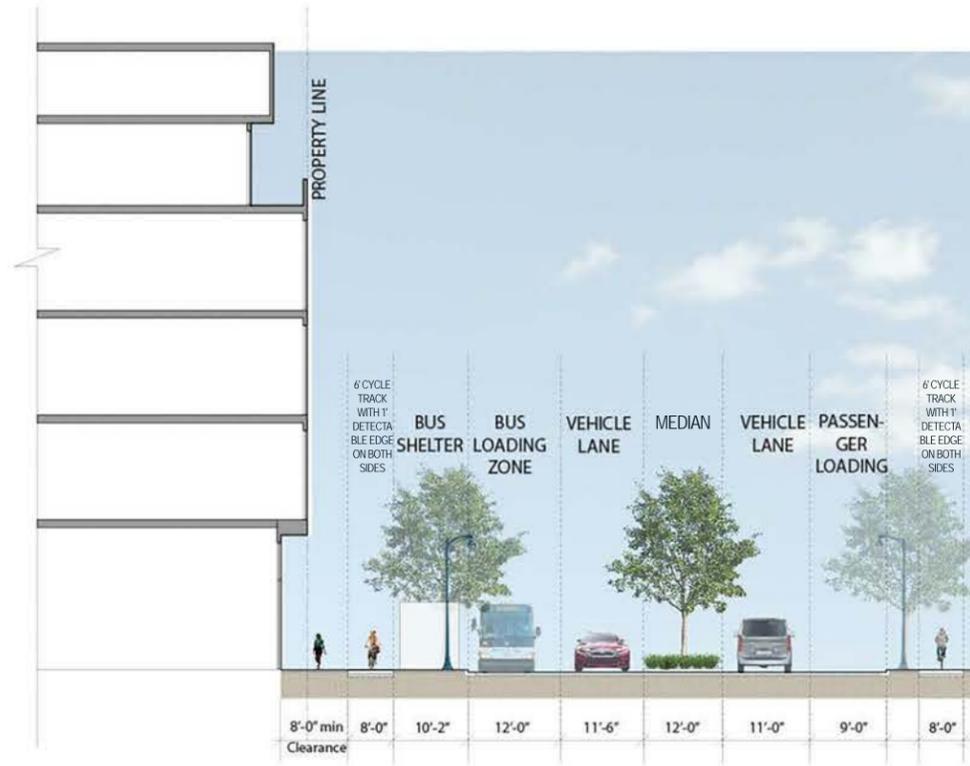


2 SECTION 4
L1.04 1/16" = 1'-0"

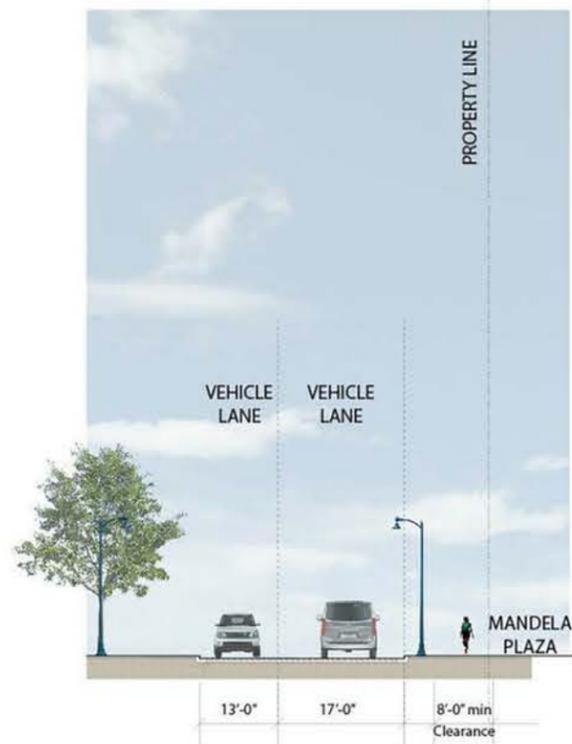




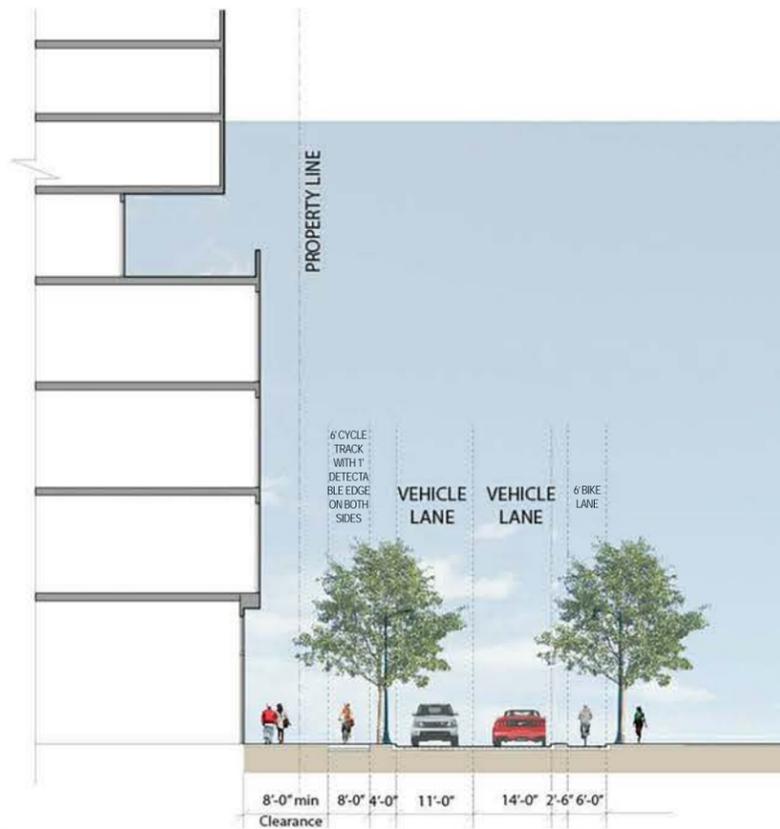
1 7TH STREET WEST SECTION
L1.05 1" = 20'-0"



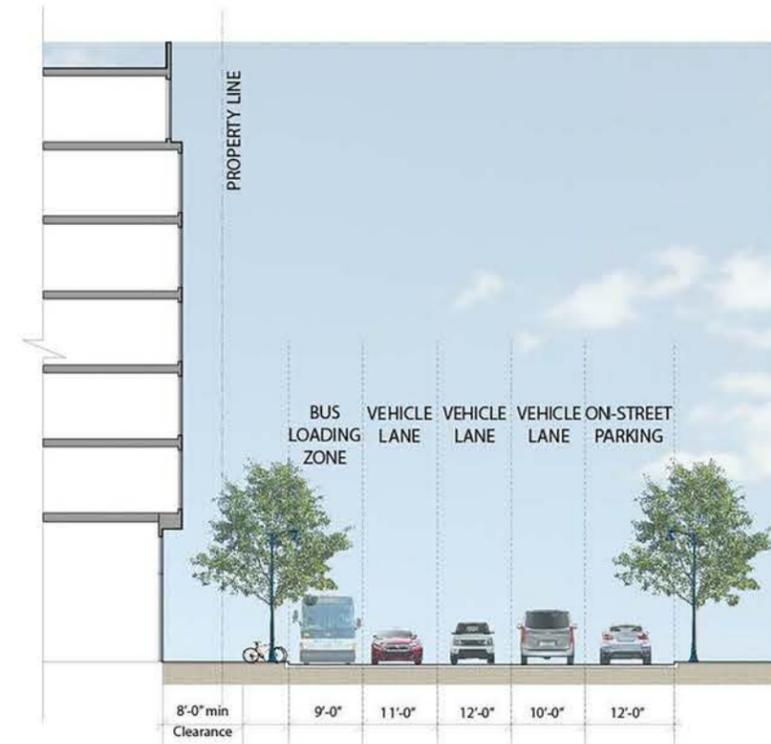
2 7TH STREET EAST SECTION
L1.05 1" = 20'-0"



3 CHESTER STREET SECTION
L1.05 1" = 20'-0"

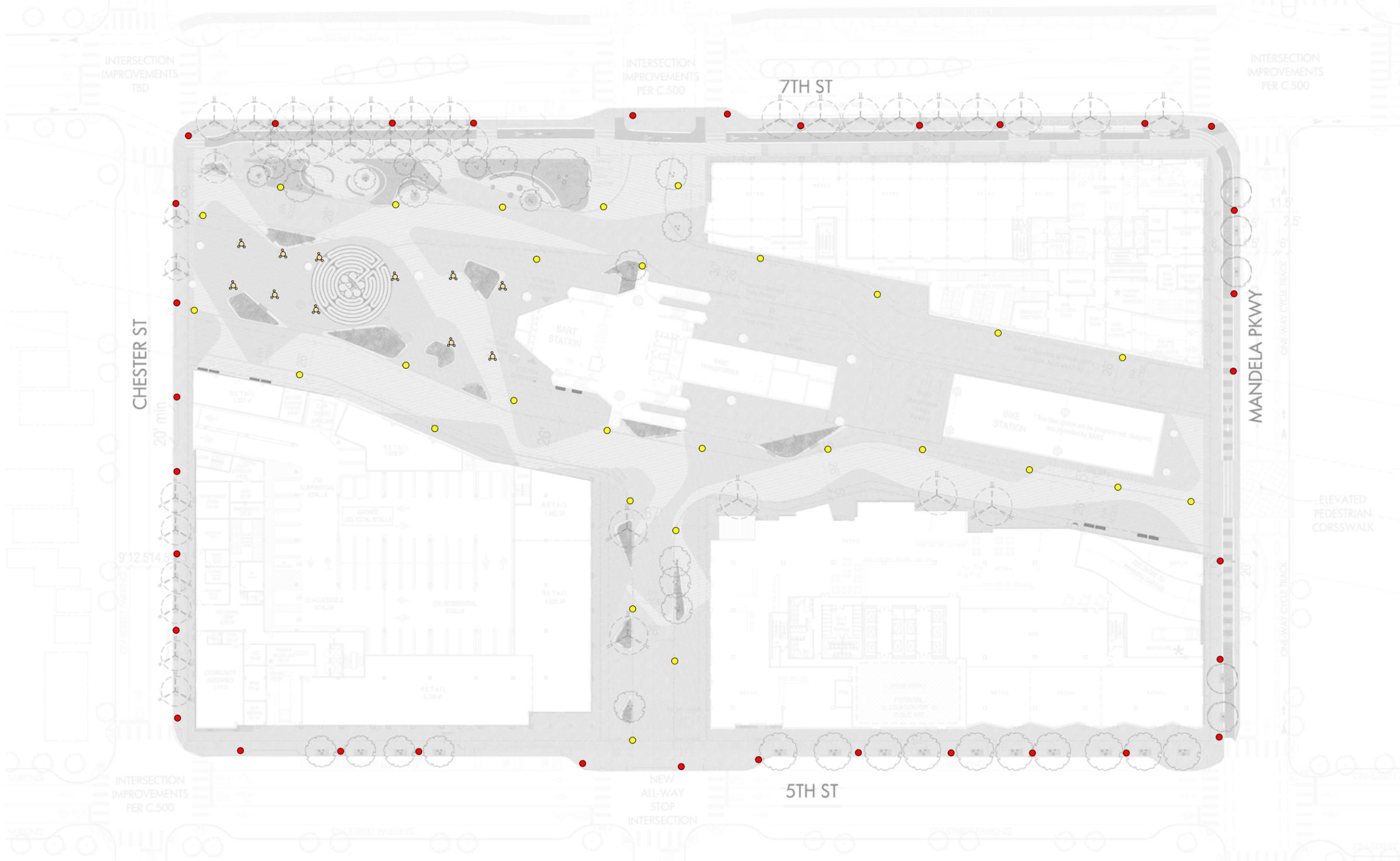


4 MANDELA PKWY SECTION
L1.05 1" = 20'-0"



5 5TH STREET SECTION
L1.05 1" = 20'-0"

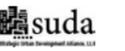
OWNER
 HENSEL PHELPS
 suda
 BUILDING T1 FDP PACKAGE
 PROJECT
WEST OAKLAND BART
 1451 7th St, Oakland, CA 94607
 ARCHITECT
 jrdv ARCHITECTS
 PROJECT TEAM
 B10 ARCHITECTS
 8015 SUITE 400
 10845 STONEMAN
 CA 94626
 (510) 420-2222
 PG+SIGN
 414 120 12001
 OAKLAND
 CA 94612
 (510) 420-0214
 STOK
 948 FORTYTHIRD
 SAN FRANCISCO
 CA 94111
 (415) 220-7100
 STRIP
 KEY
 PROJ # 100-153-000-BART
 DATE 06-04-20
 SCALE 1"=20'-0"
 TITLE **STREET SECTIONS**
 SHEET: **L1.05**



LEGEND - LIGHTING PLAN

- AREA LIGHTS FOR PLAZA
-  AREA LIGHTS WITH MULTIPLE HEADS FOR EVENT PLAZA
- STREET LIGHTS AS CITY REQUIRED

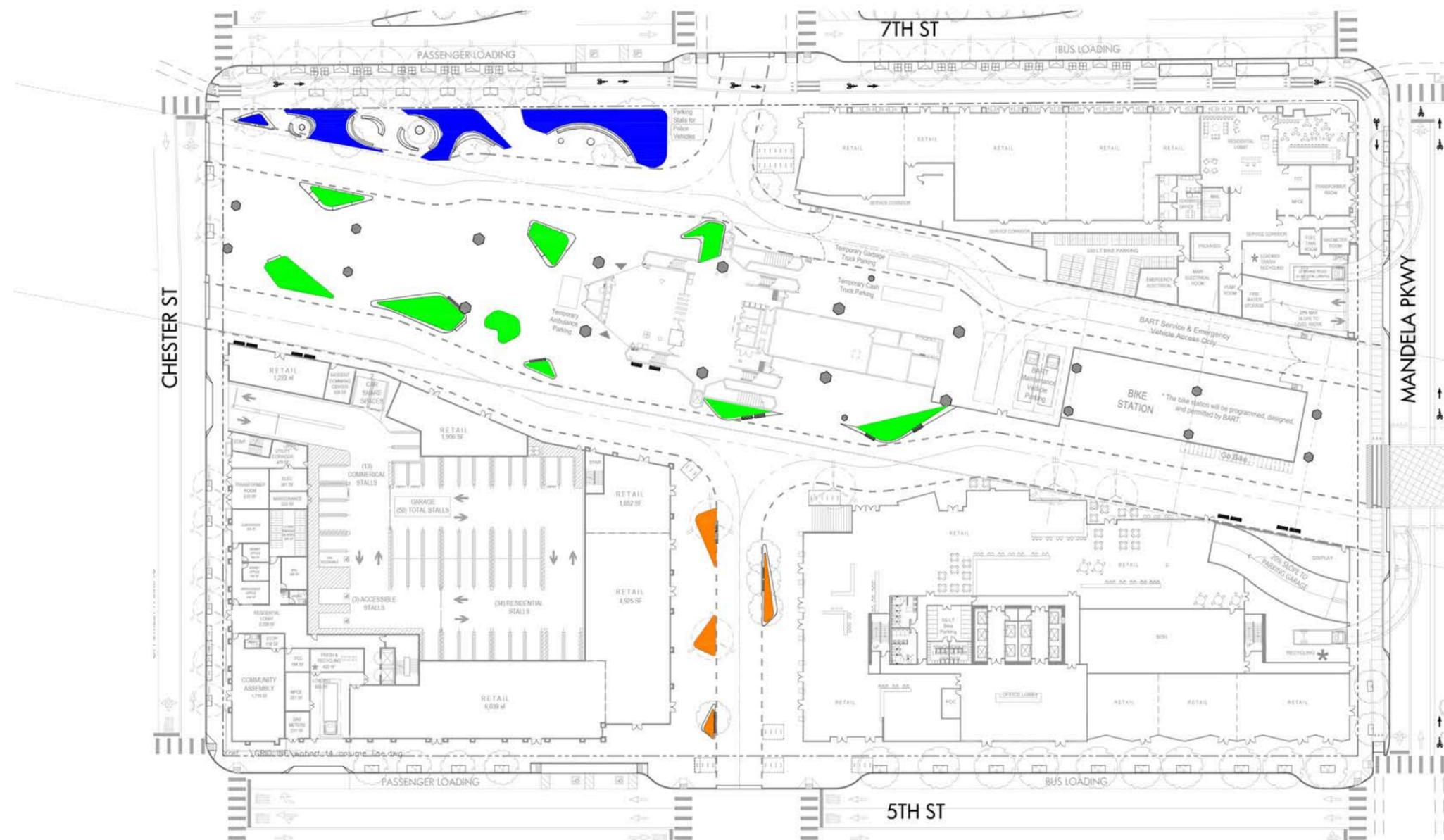
OWNER



 BUILDING T1
 FDP PACKAGE
 PROJECT
WEST OAKLAND BART
 1451 7th St, Oakland, CA 94607
 ARCHITECT

 PROJECT TEAM



 STAMP
 KEY
 PROJ #: 168-153 WO BART
 DATE: 03/26/20
 SCALE: 1"=60' - 0"
 TITLE:
LIGHTING PLAN
 SHEET:
L1.06



PLANTING LEGEND

- | | | |
|--|--|---|
| <p>PLANTING ZONE 1: MANDELA PLAZA ENTRY</p> <ul style="list-style-type: none"> CAREX DIVULSA NANDINA DOMESTICA ALBA 'LEMON-LIME' BERBERIS 'ORANGE ROCKET' IRIS DOUGLASIANA 'CANYON SNOW' MUHLENBERGIA DUBIA 'PINE MUHLY' PITTOSPORIUM TOBIRA 'MOJO' PHORMIUM 'YELLOW WAVE' CORDYLINA 'DESIGN-A-LINE' DIGIPLIXIS ILLUMINATION 'APRICOT' | <p>PLANTING ZONE 2: SHADE TOLERANT PLANTS BELOW BART TRACKS</p> <ul style="list-style-type: none"> MAHONIA 'SOFT CARESS' MUHLENBERGIA DUBIA 'PINE MUHLY' BLECHNUM SPICANT 'DEER FERN' HELLEBORUS ARGUTIFOLIUS WOODWARDIA FIMBRIATA 'GIANT CHAIN FERN' CORDYLINA 'DESIGN-A-LINE' EUPHORBIA CHARACIAS SSP. PHORMIUM 'YELLOW WAVE' | <p>PLANTING ZONE 3: SOUTH FACING ALLEY</p> <ul style="list-style-type: none"> CAREX DIVULSA MUHLENBERGIA DUBIA 'PINE MUHLY' IRIS DOUGLASIANA 'CANYON SNOW' MONARDELLA VILLOSA 'RUSSIAN RIVER' SALVIA SPATHACEA 'HUMMINGBIRD SAGE' SALVIA 'DARA'S CHOICE' |
|--|--|---|

PLEASE SEE RELATED IMAGES ON THE NEXT PAGE

OWNER
HEC
 HENSEL PHELPS
 suda
 BUILDING T1 FDP PACKAGE
 PROJECT

WEST OAKLAND BART
 1451 7th St, Oakland, CA 94607

ARCHITECT
jrdrv
 ARCHITECTS

PROJECT TEAM
 CIVIL ENGINEERS
 1146 N CALIFORNIA
 BLVD, SUITE 400
 WALNUT CREEK,
 CA 94598
 (925) 949-2209

PLANT DESIGN
 1411 7th Street
 Oakland
 CA 94612
 (925) 465-1284

STOK
 1449 First Street
 San Francisco
 CA 94111
 (415) 529-7100

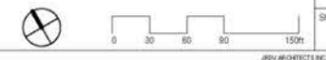
STAMP

KEY

PROJ.# 088-153 W/O BART
 DATE 09/24/20
 SCALE 1"=60' - 0"
 TITLE
PLANTING ZONE

SHEET **L1.07**

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TREES

STREET TREES



ACER RUBRUM 'ARMSTRONG'



QUERCUS COCCINEA



AFROCARPUS FALCATUS



PISTACIA CHINENSIS

PLAZA TREES



POPULUS DELTOIDES



CHITALPA TASHKENTENSIS



ALBIZIA JULIBRISSIN

SHRUBS & GROUNDCOVER

PLANTING ZONE 1 : ON THE PLAZA



CAREX DIVULSA



PHORMIUM 'YELLOW WAVE'



BERBERIS 'ORANGE ROCKET' 4'H X 2'W



NANDINA DOMESTICA ALBA 'LEMON-LIME' 4'H X 3'W



CORDYLIN 'DESIGN-A LINE' 3'X3'



PITTOSPORUM TOBIRA 'MOJO' 3'X3'



DIGIPLEXIS ILLUMINATION 'APRICOT' 2'H X 18"W



IRIS DOUGLASIANA 'CANYON SNOW'



MUHLENBERGIA DUBIA 'PINE MUHLY'

PLANTING ZONE 2 : UNDER THE TRUCK



MAHONIA 'SOFT CARESS' 4'X4'



PHORMIUM 'YELLOW WAVE'



CORDYLIN 'DESIGN-A LINE' 3'X3'



EUPHORBIA CHARACIAS SSP.



BLECHNUM SPICANT 'DEER FERN'



WOODWARDIA FIMBRIATA 'GIANT CHAIN FERN'



HELLEBORUS ARGUTIFOLIUS 3'X3'



MUHLENBERGIA DUBIA 'PINE MUHLY'

PLANTING ZONE 3 : IN THE ALLEY



MUHLENBERGIA DUBIA 'PINE MUHLY'



MONARDELLA VILLOSA 'RUSSIAN RIVER'



IRIS DOUGLASIANA 'CANYON SNOW'



SALVIA SPATHACEA 'HUMMINGBIRD SAGE'



SALVIA "DARA'S CHOICE"



CAREX DIVULSA



PHORMIUM TONY TIGER' 2.5'X2.5

OWNER
 HENSEL PHELPS

BUILDING T1 FDP PACKAGE PROJECT

WEST OAKLAND BART
 1451 7th St, Oakland, CA 94607

ARCHITECT
 ARCHITECTS

PROJECT TEAM
 BKF ENGINEERS
 1566 K CALIFORNIA BLVD, SUITE 400 WALKIT CREEK CA 94609 (925) 940-2200
 PGA DESIGN
 444 17th Street Oakland CA 94612 (510) 465-1284
 STOK
 945B Front Street San Francisco CA 94111 (415) 329-7100

STAMP

KEY

PROJ #: 168-153 WO BART
 DATE: 06/04/20
 SCALE:
 TITLE: LANDSCAPE PLANTS
 SHEET: L1.08

CAST-IN-PLACE CONCRETE PAVING



COLORED C.I.P w/ SAWCUT BANDING



COLORED C.I.P w/ SAWCUT BANDING



C.I.P w/ SAWCUT BANDING

NOTE: ALL PAVING SHALL BE ADA COMPLIANT AND MINIMIZE UNDULATING AFFECTS WHEN TRAVELING OVER SURFACE

PLANTER WALL



POUR-IN-PLACE CONCRETE WITH SACK FINISH

SITE FEATURES



SLOPED TURF



EVENT PLAZA

SITE FURNITURES



BENCH: MODEL: NEW STANDARD BENCH, MANUFACTURER: STREETLIFE, TEL: 215.247.0148, WEBSITE: <https://www.streetlife.nl/us>



BIKE RACK: MODEL: BOLA BIKE RACK, MANUFACTURER: LANDSCAPEFORMS, TEL: 800.430.6209, WEBSITE: <https://www.landscapeforms.com/en-US>



TRASH BIN: MODEL: CHASE PARK LITTER, MANUFACTURER: LANDSCAPEFORMS, WEBSITE: <https://www.landscapeforms.com/en-US>



PICNIC TABLE SET: MODEL: GRETCHEN PICNIC TABLE, ADA COMPLIANT STYLE, MANUFACTURER: LANDSCAPE FORMS, TELEPHONE: 800.430.6209, WEBSITE: WWW.LANDSCAPEFORMS.COM



DINING TABLE & CHAIR SET: 21 CHAIR WITH ARMS BY LANDSCAPE FORMS, 4 CHAIRS PER TABLE, CATENA TABLE, 48" DIAMETER, NO UMBRELLA HOLE, ADA COMPLIANT QUAD SUPPORT LEGS, ADJUSTABLE GLIDES. FINISH: LANDSCAPE FORMS PROPRIETARY PANGARD II® POLYESTER POWDERCOAT COLOR: TBD FROM MANUFACTURER'S STANDARD PALETTE MANUFACTURER: LANDSCAPE FORMS, TELEPHONE: 800.430.6209, WEBSITE: WWW.LANDSCAPEFORMS.COM



OWNER



BUILDING T1
FDP PACKAGE

PROJECT

WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM



STAMP

KEY

PROJ #: 168-153 WO BART

DATE: 06/04/20

SCALE:

TITLE:

LANDSCAPE MATERIALS

SHEET: **L1.09**

STREET LIGHT

Domus Luminaire



Manufacturer(s): Lumec

Catalog #: DM550-HPS-SC3M-SMB-SCITX (Large Domus) DOSHPS-SG3-SMB-SCITX-LMS (Small Domus)

Photometric File: 50105312.ies (Domus DM550, 200-250W) 50206072.ies (Domus DOS, 35-150W)

Application: Domus DM550 - Arterial and/or collector street lighting Domus DOS - Residential street lighting and/or sidewalk side pedestrian lighting

Lamp Type: 35W, 100W, 150W, 250W HPS

Optical System: Type III

Ballast Voltage: Multi-tap 120/240V

Color: Teal Green only

Special Requirements:

Luminaire Cost: \$\$\$



As shown: Domus DOS luminaire w/ 3' arm on 16' octagonal pole.

Color: Teal Green

Location: Coliseum Gardens



Page 19 of 71

PEDESTRIAN LIGHTING



PLAZA LIGHTS



landscape lighting

landscape lighting

ALCOTT AREA LIGHT



The Alcott area light is a contemporary interpretation of a traditional lamp post. Its gently curved joints are topped by a domed energy-efficient LED luminaire sealed with translucent lens. Programmable LEDs control color with multiple wiring and provide directional lighting, reducing light pollution and wasted energy.

General Description

- Offered in 3 standard mounting heights (12', 14', and 16')
- Asymmetrical and symmetrical distribution types available
- Mounting brackets and anchor hardware included
- Mounting steps complete installation and ready for installation
- Dim-to-light, International Dark-Sky approved
- UL-Listed, suitable for wet locations

Electrical

Single pole, 100V-277V, 60/60 Hz, Class 2 dimmable driver mounted within integrated cast aluminum luminaire. LED luminaire with weatherproof glass enclosure provides ease of accessibility. All cast pieces pre-welded and fully assembled.

Alcott Type 3

Type 3 Distribution: Structure housing LED luminaire and cabinet are cast aluminum, glass is aluminum anodized. Lamp: 48 Cree XP-E2 LEDs. CCT: 3000K, 3500K, 4000K. Drive Current: 350mA. Optic: Asymmetric Optic. Lens: Clear Lens. Power Supply: 100V-277V. LED Driver: TRIP-LED-T5W. Dimmable: 0-10V. UL5 Rating: 90 LED-01. IP Rating: IP66 for LED Cartridge.

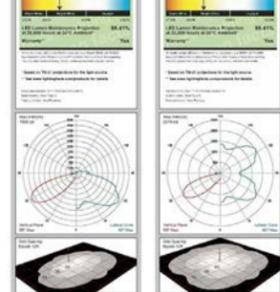
Alcott Type 5

Type 5 Distribution: Structure housing LED luminaire and cabinet are cast aluminum, glass is aluminum anodized. Lamp: 36 Cree XP-E2 LEDs. CCT: 3000K, 3500K, 4000K. Drive Current: 350mA. Optic: Asymmetric Optic. Lens: Clear Lens. Power Supply: 100V-277V. LED Driver: TRIP-LED-T5W. Dimmable: 0-10V. UL5 Rating: 90 LED-01. IP Rating: IP66 for LED Cartridge.

Lighting Facts

| Product | Lamp | Drive Current | Color Temp. | Input Voltage | Mounting |
|-----------|----------------|---------------|-------------|------------------|-----------|
| AC Alcott | 48 LED, Type 3 | 350mA | 40K (4000K) | LX1 (150-277VAC) | T2 (14ft) |
| | 36 LED, Type 5 | 350mA | 30K (3000K) | LX1 (150-277VAC) | T4 (18ft) |

EXAMPLE: AC-0483-030T-35K-LX1-T2 - Residential Clear



ALCOTT AREA LIGHT



Finish

Pre-painted, powder coated by Landscape Forms in a 10-step program of cleaning, priming, and powder coating that includes cutting, chipping, peening and baking to produce the best and most durable finish for both indoor and outdoor lighting. In addition, PowderCoat provides excellent resistance to UV radiation and pollutants.

To Order

Specify Product, Lamp, Drive Current, Color Temperature, Input Voltage, Mounting Height and Finishes/Color Code

| Product | Lamp | Drive Current | Color Temp. | Input Voltage | Mounting |
|-----------|----------------|---------------|-------------|------------------|-----------|
| AC Alcott | 48 LED, Type 3 | 350mA | 40K (4000K) | LX1 (150-277VAC) | T2 (14ft) |
| | 36 LED, Type 5 | 350mA | 30K (3000K) | LX1 (150-277VAC) | T4 (18ft) |

EXAMPLE: AC-0483-030T-35K-LX1-T2 - Residential Clear

Product Modifications

Don't see what you are looking for? Our goal is to partner with you as the designer to manufacture custom products for the space you are creating. We offer the option to modify our standard product to meet custom design specifications or needs. Contact your local Landscape Forms representative to learn more about these changes.

Warranty

LED lighting products are warranted for six years.

Other

UL-Listed, ENEC Certified, Dark Sky Approved, ICES, IP66, IK07, IK08

Alcott is designed by Robert A.M. Stern Architects



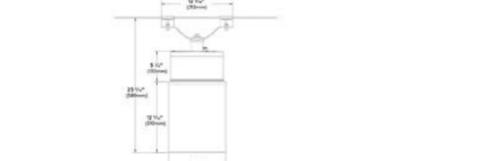
Visit landscapeforms.com for more information. Specifications are subject to change without notice. Landscape Forms supports the Landscape Architecture Foundation at the Second Century Fund. ©2015 Landscape Forms, Inc. Printed in U.S.A. Landscape Forms, Inc. | 950.271.2546 | 1700 3rd Street, Suite 100, San Francisco, CA 94103

PEDESTRIAN ACCENT LIGHT



OVERHEAD CABLE LIGHTS

Lumina LED Category



Specifications

Luminaire Housing/Hanger: One-piece aluminum housing made from low-temperature die-cast aluminum. Hanger bracket made from stainless steel with stainless steel hardware. Hanger Bracket provides 360° rotation and 15° tilt in either direction.

LED Driver: LEDs are driven by a fully compliant constant current programmable LED driver. Driver includes 0-10V dimming to 50%, meets the requirements of IESNA and includes a 5-year warranty.

Surge Protector: One shock-resistant surge protector device designed to protect luminaire from electrical surge up to 20A.

Power Cord: One shock-resistant surge protector device, UL listed, 100' maximum length. 50' power cord pre-installed at factory. Please specify power cord length outside of 7' increments from 5'-30'.

Standard exterior colors are White (RHS), Black (RHS), Bronze (RHS), and Silver (RHS). Select premium colors (RHS) are available, please specify from your Lumina LED luminaire options.

5 Year Limited LED Luminaire Warranty: Selux offers a 5 Year Limited Warranty to the original purchaser from the Lumina LED Luminaire. This limited warranty covers the luminaire and LED array when installed and operated according to Selux's instructions. For details, see "Selux Terms and Conditions of Sale."

LED Optics

Clear or frosted UV resistant acrylic outer cylinder creates the optical chamber. Technical Optics (RHS, 12, 14, 16, 18, 20, 22, 24, 26, 28, 30, 32, 34, 36, 38, 40, 42, 44, 46, 48, 50, 52, 54, 56, 58, 60, 62, 64, 66, 68, 70, 72, 74, 76, 78, 80, 82, 84, 86, 88, 90, 92, 94, 96, 98, 100, 102, 104, 106, 108, 110, 112, 114, 116, 118, 120, 122, 124, 126, 128, 130, 132, 134, 136, 138, 140, 142, 144, 146, 148, 150, 152, 154, 156, 158, 160, 162, 164, 166, 168, 170, 172, 174, 176, 178, 180, 182, 184, 186, 188, 190, 192, 194, 196, 198, 200, 202, 204, 206, 208, 210, 212, 214, 216, 218, 220, 222, 224, 226, 228, 230, 232, 234, 236, 238, 240, 242, 244, 246, 248, 250, 252, 254, 256, 258, 260, 262, 264, 266, 268, 270, 272, 274, 276, 278, 280, 282, 284, 286, 288, 290, 292, 294, 296, 298, 300, 302, 304, 306, 308, 310, 312, 314, 316, 318, 320, 322, 324, 326, 328, 330, 332, 334, 336, 338, 340, 342, 344, 346, 348, 350, 352, 354, 356, 358, 360, 362, 364, 366, 368, 370, 372, 374, 376, 378, 380, 382, 384, 386, 388, 390, 392, 394, 396, 398, 400, 402, 404, 406, 408, 410, 412, 414, 416, 418, 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1 - Mandela Plaza Center Street Entrance



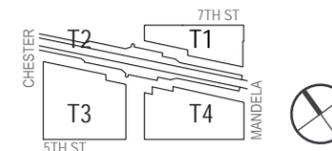
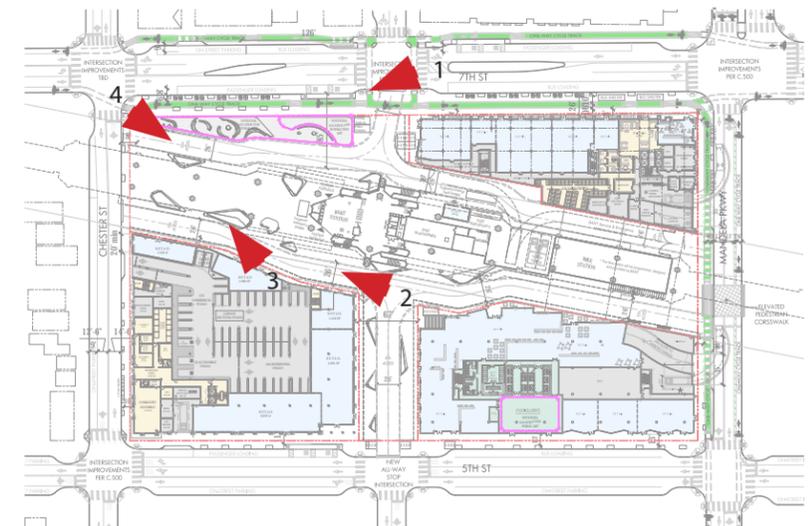
2 - Artwalk from Center Street

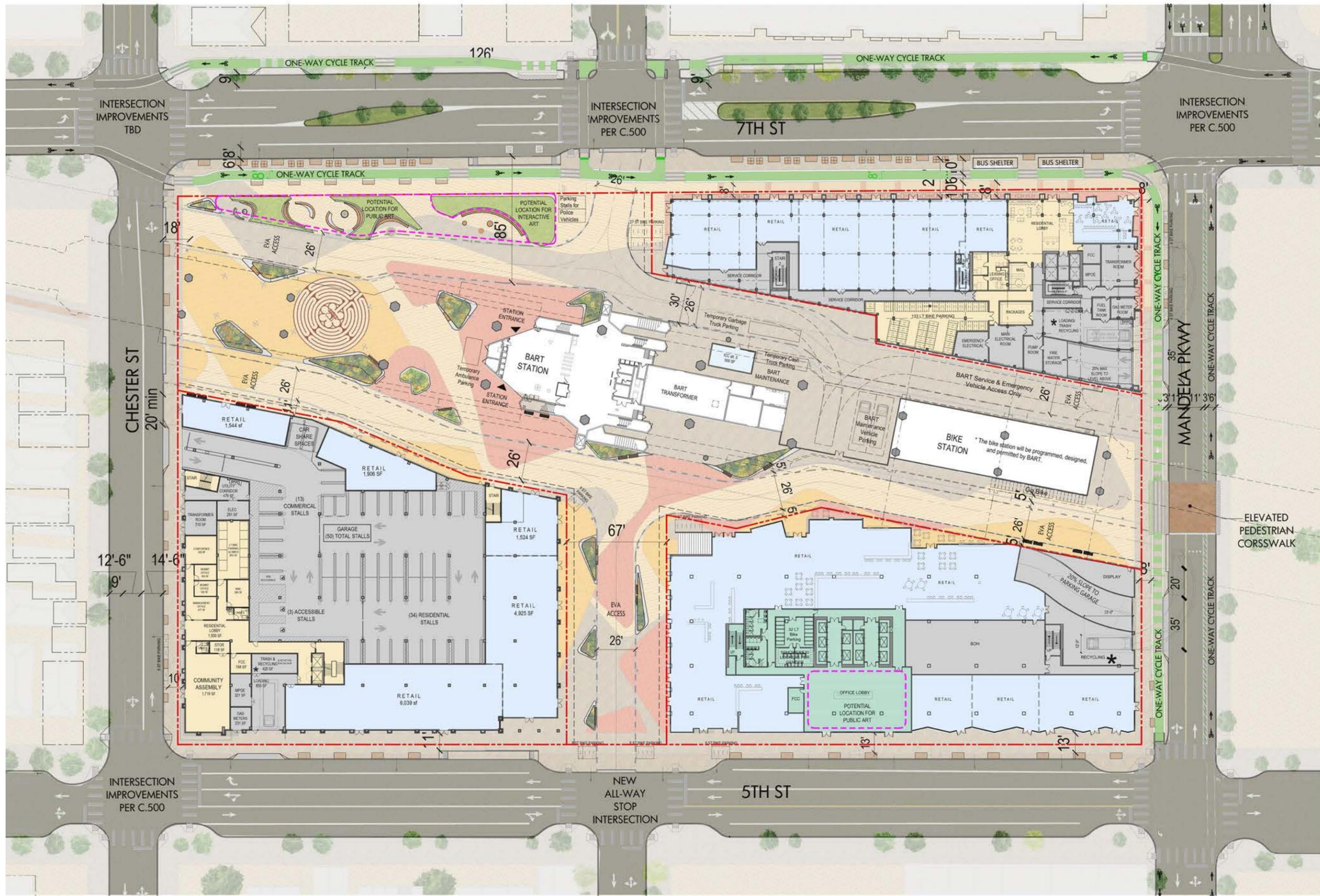


3 - Undertrack Plaza from Artwalk

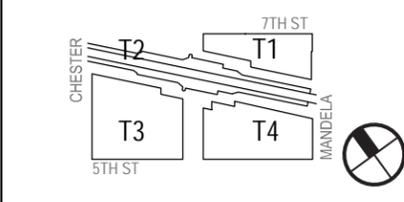


4 - Mandela Plaza Chester Street Entrance and Blues Walk of Fame





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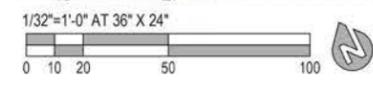
PROJ. # 168-153 WO BART
 DATE: July 24, 2020

SITE PLAN

SHEET:

A-9.00

The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.



Public Space Design

Public Space Improvements:

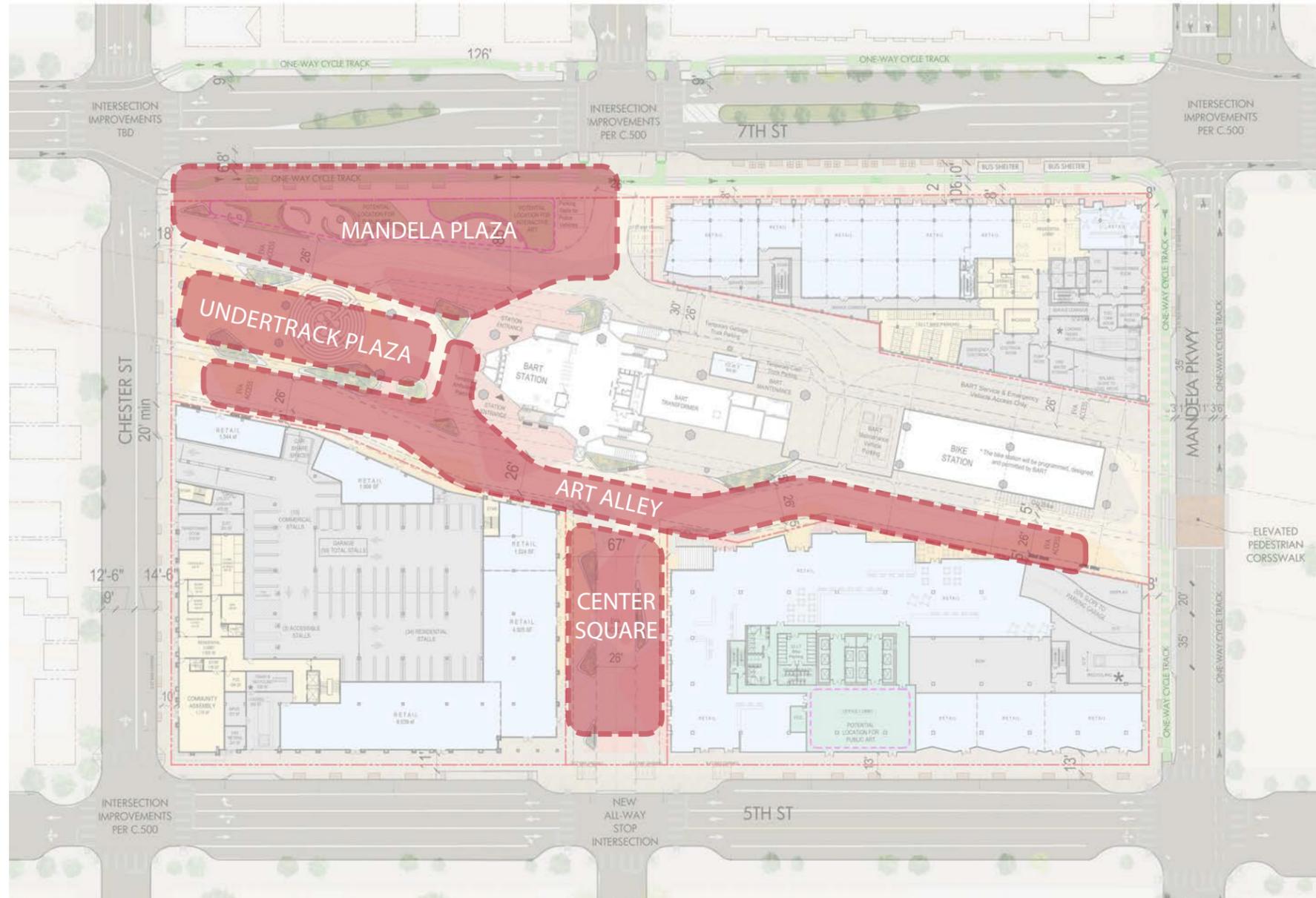
The public spaces for the Mandela Station site are designed to reinforce the vibrant mixed-use development that furthers Oakland's efforts to promote urban living at key transit sites, and provides an active and delightful center for the West Oakland community.

The site is designed with a series of important civic open spaces, including:

- Mandela Plaza, a major community civic plaza at 7th Street fronting BART Station entrance. This plaza is designed with varied paving and green spaces. It is designed with a delightful series of sculptural seating areas arranged to promote community interaction – a “Living Room” for the residents and surrounding neighborhood.
- Center Square, a pedestrian plaza replacing the vacated Center Street. This plaza is focused on providing food related tenants along T3 and T4, and will be designed with fixed and permanent seating to become a destination food hub for the neighborhood, BART patrons and the on-site residents.

- Art Alley, a pedestrian paseo that activates the area south of the BART tracks. This area is lined with small gallery spaces and will have a expansive “art wall” along the north façade of T3 framing the alley space.

- Undertrack Area, a flexible event space that will provide a venue for the programs that activate Mandela Plaza and Art Alley. The under track will feature movable food kiosks, pop-up events, farmers markets, art fairs, live music and other programmed events that will help make Mandela Station a destination location.



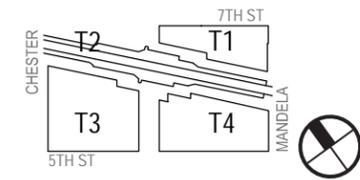
Hardscape and Green-scape Design: Landscape plan is designed to enhance the pedestrian public spaces, and to create a high quality of pedestrian experience and civic prominence. The existing trees will be replaced because of conflicts with the access plan. The new tree planting complements the overall landscape strategy of the 7th and 5th Street corridors to ensure a continuous, interesting and varied visual experience.

Landscape Materials: The landscape materials are designed with pavers or stamped concrete and other decorative materials to create a high quality public pedestrian experience and to maximize the types of uses that can occur on site. The landscape is designed to become a catalyst to a larger vision for the 7th Street corridor. The new tree planting complements the overall landscape strategy of the 7th Street corridor to ensure a continuous, interesting and varied visual experience.

Site Lighting: The Lighting plan is designed to create well lighted plazas and pedestrian pathways through the site. The visual security of all pedestrian spaces within the site is facilitated by locating retail and other public activities along all edges of the development. The landscape plan provides adequate lighting to provide a safe environment while conforming to current best practices to mitigate light pollution.



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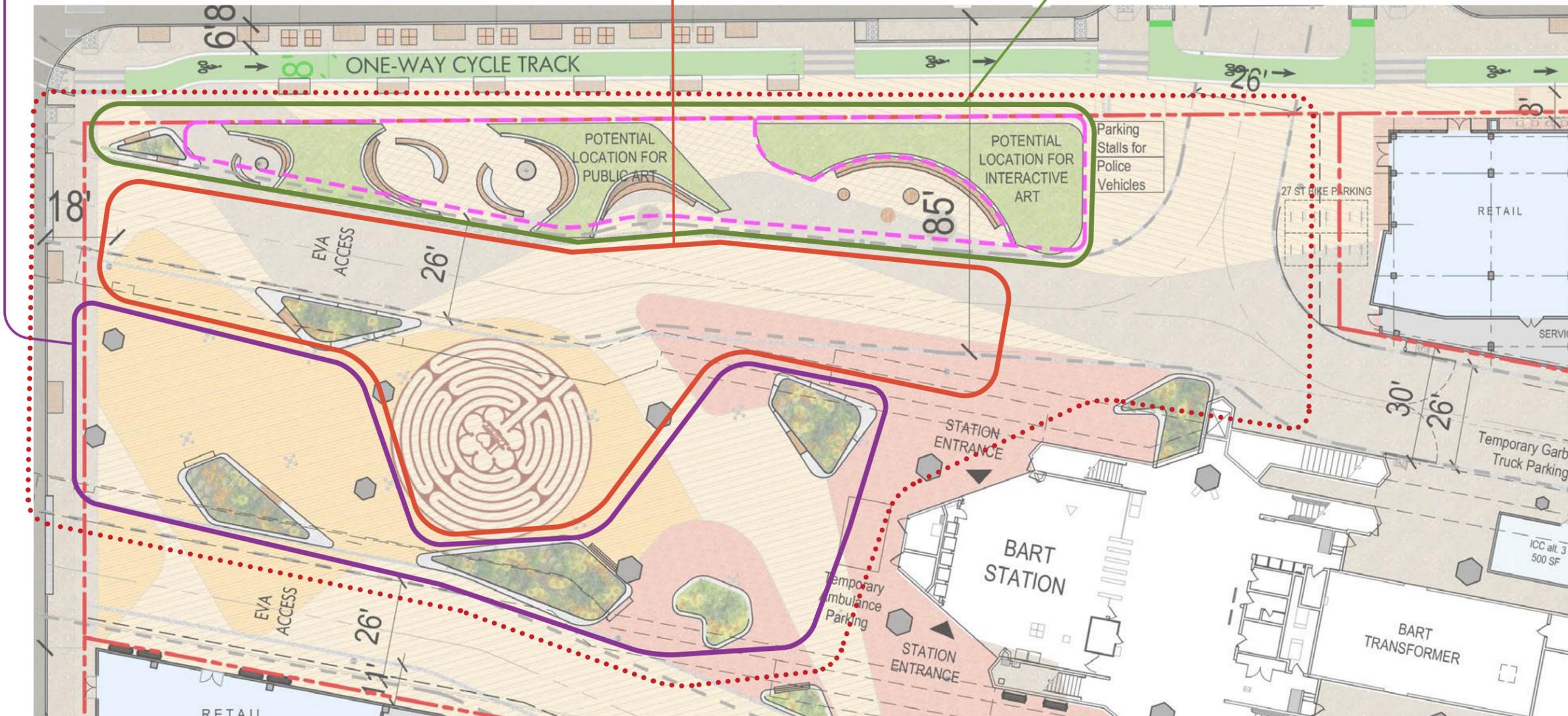
PROJ. # 168-153 WO BART
DATE: July 24, 2020

**PUBLIC SPACE
IMPROVEMENT:
PUBLIC SPACE
DESIGN**

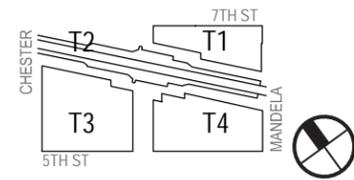
SHEET:

A-9.01

Mandela Plaza



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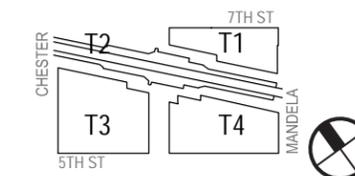
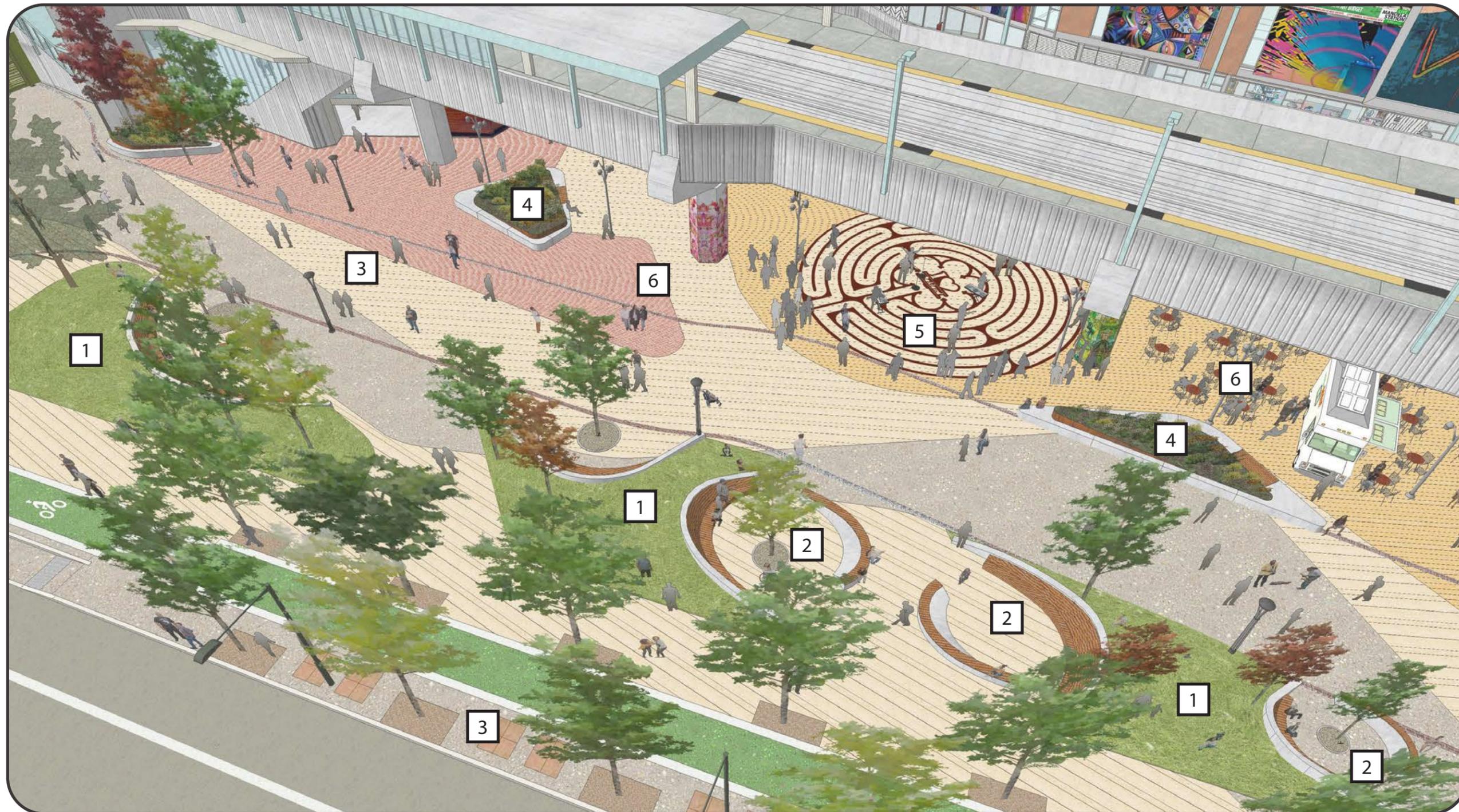
PROJ. # 168-153 WO BART
DATE: July 24, 2020

PUBLIC SPACE IMPROVEMENT: MANDELA PLAZA

SHEET:

A-9.02

Note: Undertrack painting is optional pending BART's approval.



Mandela Plaza Features

- 1 **SCULPTED BERMS** - Artificial turf berms to promote informal seating and interactive play options for a wide range of age groups.
- 2 **OUTDOOR 'LIVING ROOMS'** - Nodes framed with integral bench seating for small gatherings.
- 3 **BLUES WALK OF FAME** - Relocated plaques cast into 7th Street sidewalk paving north of Mandela Plaza and the T1 building.
- 4 **BENCH PLANTERS** - Concrete planters with consistent, bench-height edge to provide fixed seating option for plaza events.
- 5 **MAZE STRIPING** - Maze for public interaction. This open space is flexible for performances and other event activities.
- 6 **ADDITIONAL OPEN SPACE** - Framed open space for food trucks, outdoor dining, and other temporary programming.

Public Space Programming and Activation

The Developer will provide regular programming in the public plazas of the proposed Mandela Station at West Oakland BART Station as required in the Conditions of Approval (COA #66) approved on February 6, 2019 by the City of Oakland Planning Commission. The purpose of the programming is to provide future BART riders, project-site residents, shoppers and workers, and the larger community of West Oakland with excellent opportunities to relax and enjoy as well as providing local businesses and non-profits to financially support themselves and promote their services and products.

Programmed events may include:

1. Concert events will be held on a monthly schedule in co-operation with the Developer and local non-profit Zoo Labs among others. Zoo Labs empowers music artists as leaders and directs resources toward their ventures.
2. Farmers Market events will be held monthly in collaboration with the Mandela Grocery Cooperative, a worker-owned grocery store that provides nourishing foods and enhanced opportunities for food vendors, including the Freedom Farmers Market weekly from July to November.
3. Local vendor Pop-Ups events will be held monthly and co-sponsored by PopUp Village. Popupvillage.org features opportunities including:
 - Arts and Culture classes and exhibits
 - Food & Agriculture
 - Health & wellness
 - Youth Activities
 - Highlights local retail vendors
 - Performers of music, dance, spoken word
 - Education
 - These are a few of the collaborator of existing programs on 7th Street that as developers we intend to support and expand in collaboration with these organizations.

All program schedules and events will be advertised widely using: BART Station Public Information System, the networks of organizations that participated in the West Oakland Advisory Committee process and the local media outlets.



Community Art Fair



Local Farmers Market



Undertrack Program Events



Food Truck Events



Undertrack Program Events



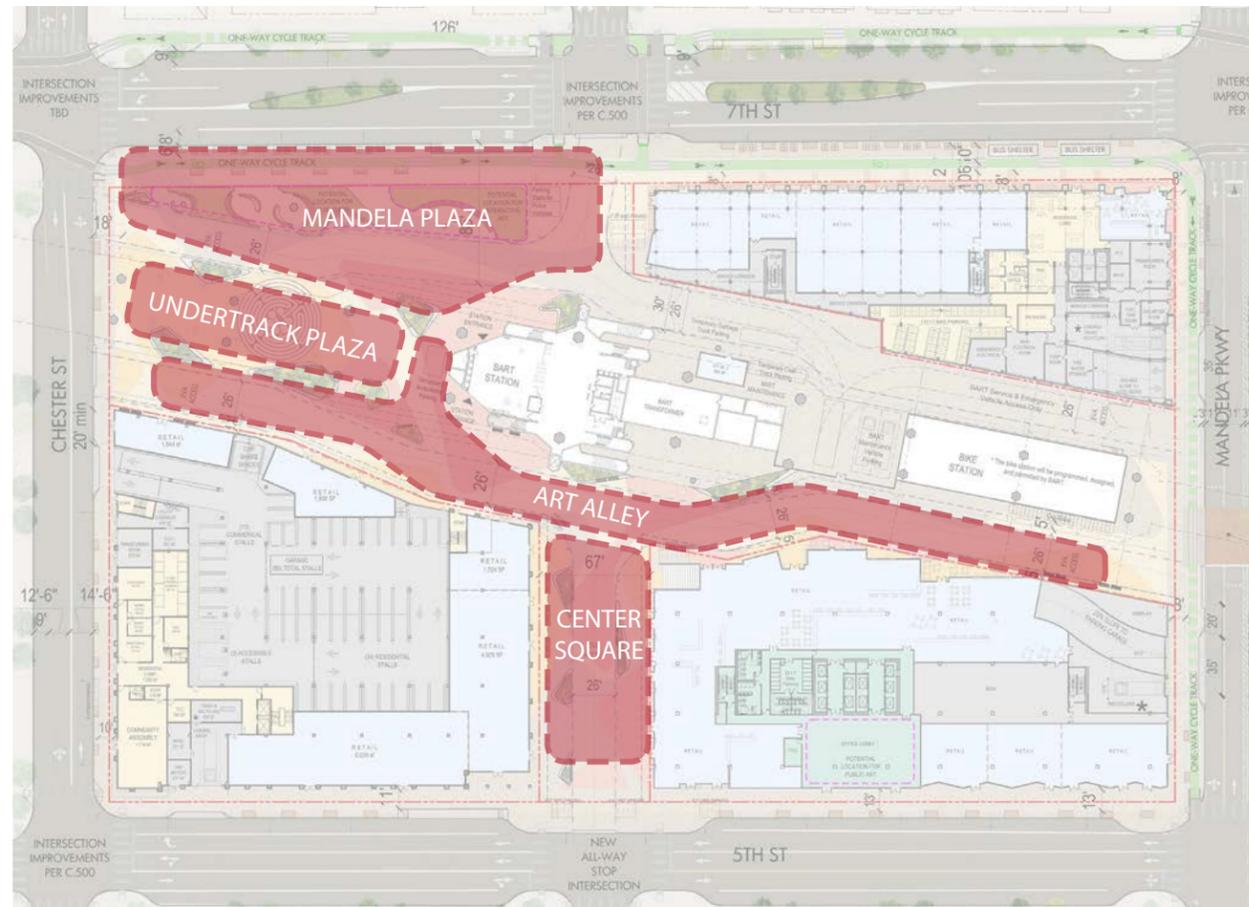
Undertrack Program Events



Food Truck Events



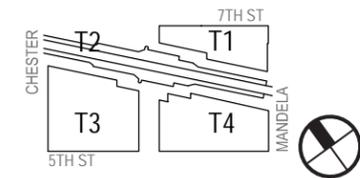
Sunday Music Show



Public Open Space Map



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PROJ. # 168-153 WO BART
DATE: July 24, 2020

EVENT SPACE PROGRAM

SHEET:

A-9.04

Under Track Strategy

The Under-Track area is designed to integrate with both Mandela Plaza on 7th Street and Art Alley to the south. These combination of these spaces are planned to be programmed and perceived both separately and as linked spaces.

Programmed Activation. Under-Track area is designed as a flexible and activated space that will support food kiosks, exhibits and a wide range of staged events. The physical space of the Under-Track area serves these programmed activities by creating a defined space with high “ceiling” and significant scale, that is visually connected to the surrounding Public Spaces. The space also functions as an interesting urban plaza when no events are staged. It becomes an delightful and safe passage through the site.

Landscape Paving. The paving patterns are designed to define the space and encourage pedestrians to flow through the space. The variety of paving materials is designed to reduce the scale of the space, by implying nodes and clusters that can contain smaller groups or events. The paving pattern and variety of materials will also create a visually interesting plaza surface that does not require any activation or events.

Seating. Seating is provided with a combination of fixed-seating that is integrated into the planters. And, movable café-seating and tables to be placed and managed by specific events. The goal is to promote this space as both safe pedestrian passage through the site, and as a place to rest or participate in a planned event. The number of movable seats will be managed by the client and will respond to the actual demand.

Lighting. The lighting of the Under-Track area is designed to provide a high level of ambient light and “decorative” feature lighting. The ambient light is provided by a double row of 10’high light standards with a fixture that provide directional up-lighting and ground-lighting. Feature lighting will be provided with either fixtures attached to the BART structure, or fixtures attached to the light standards. This lighting will provide changeable artistically colored lighting that creates a powerful night presence that extends to the plaza and 7th Street. Lighting levels are designed to ensure visual safety for this space at all times.

Decorative Surface Painting. The columns of the BART structure may be painted with a surface graphic that is colorful and visually animates the space. This painted surface will create a delightful backdrop that will enhance the space for events and for the enjoyment of the public.

Note: Undertrack painting is optional pending BART’s approval.



1 - From BART Station North Entrance



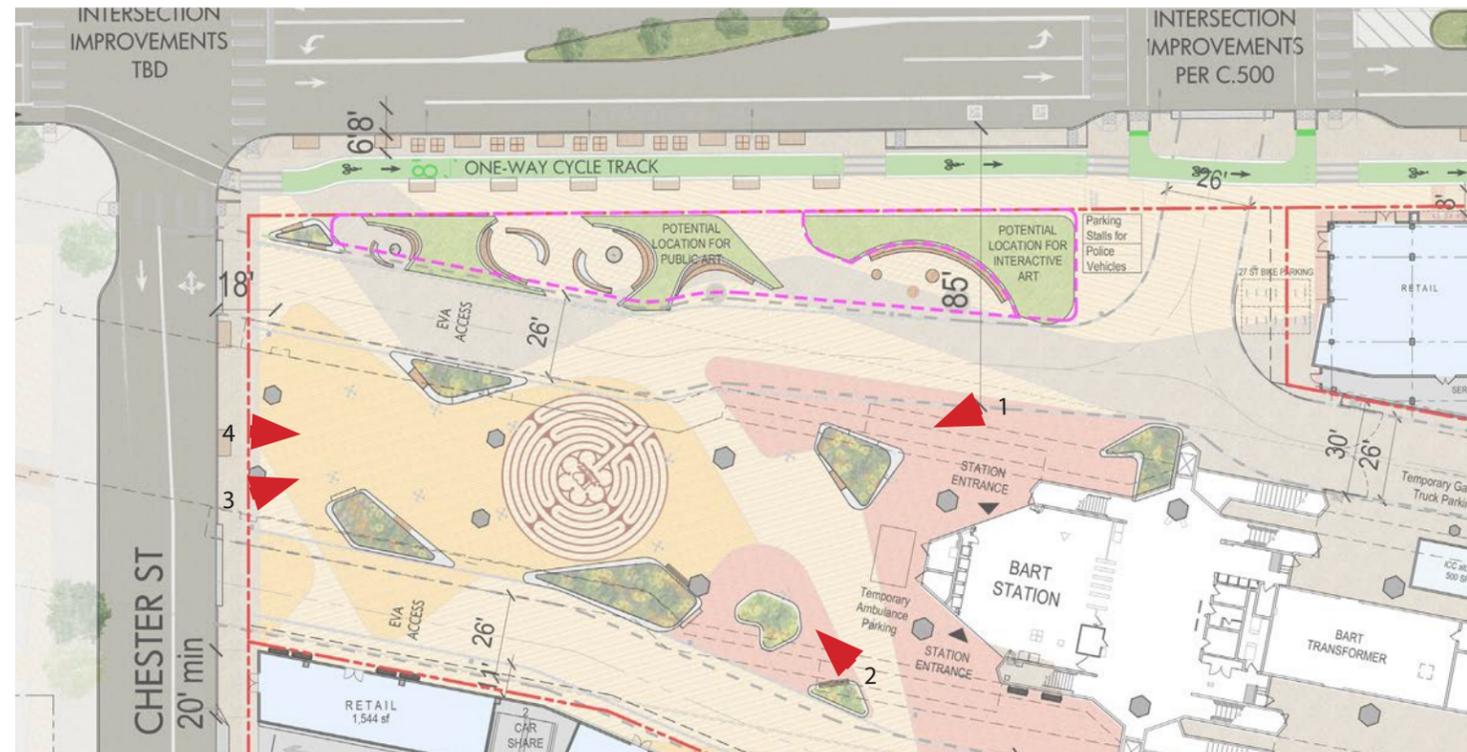
2 - From Art Walk



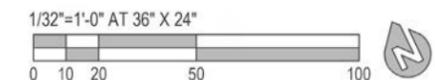
3 - From Mandela Plaza



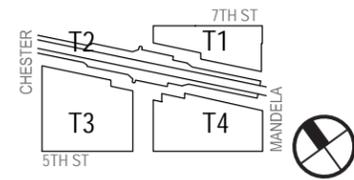
4 - From Under Track Plaza



Partial Site Plan



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**UNDER TRACK
SPACE**

SHEET:

A-9.05

Under-Track Activation



Farmers Markets



Food Trucks and Mobile Enterprise



Music and Staged Events



Off The Grid Events



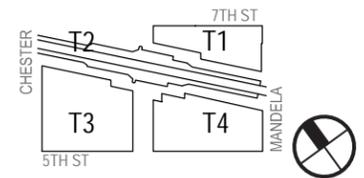
Night Markets



Art Fairs and Pop-up Events



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UNDER TRACK
SPACE

SHEET:

A-9.07

Mandela Plaza - Community Civic Space:

This urban plaza creates a major community civic plaza at 7th Street fronting BART Station entrance. It is designed with a delightful series of sculptural seating and green-berm areas arranged to promote community use – a “Living Room” for the residents and surrounding neighborhood. The plaza provides access to the BART station, and also celebrates the civic importance of this site in the community. This plaza is located centrally to the overall site in order to increase its public importance, public access, and public use for community, arts and cultural events. This central plaza is designed to enhance the 7th Street corridor activation and overall public experience.

This plaza is designed with varied paving and green spaces. The Landscape plan is designed to complement the importance and use of the space by the neighborhood and residents on site. The arced cluster seating and green berms along 7th Street create a welcoming pedestrian and community experience.

The new tree planting complements the overall landscape strategy of the 7th Street corridor to ensure a continuous, interesting and varied visual experience. Paving materials are quality pavers or stamped colored concrete and are aesthetically designed to differentiate unique spaces within the pedestrian plazas, promote visual access to the BART station entrance, and to create opportunities for cultural, community and arts events. The landscape plan is designed to create a delightful destination for the West Oakland community and users of the transit hub.



VIEW FROM 7TH STREET

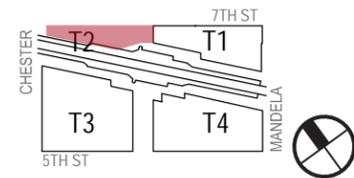


VIEW FROM 7TH STREET



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DATE: July 24, 2020

MANDELA PLAZA

SHEET:

A-9.08



VIEW OF BART ENTRANCE AT 7TH STREET

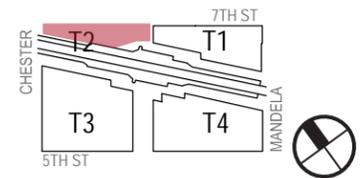


VIEW FROM CHESTER STREET



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PROJ. # 168-153 WO BART
DATE: July 24, 2020

MANDELA PLAZA

SHEET:

A-9.09



VIEW FROM 7TH STREET



VIEW FROM 7TH STREET

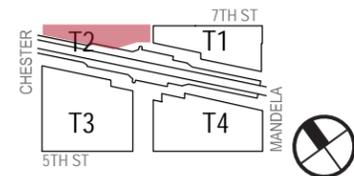


VIEW OF 7TH STREET SIDEWALK AND LANDSCAPED BERM



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MANDELA PLAZA

SHEET:

A-9.10



VIEW FROM 7TH STREET

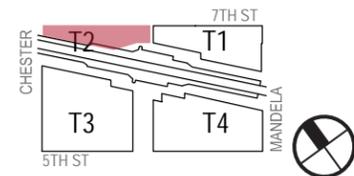


VIEW OF MANDELA PLAZA



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DATE: July 24, 2020

MANDELA PLAZA

SHEET:

A-9.11

Art Alley



Art Walk Events



Art Walk Activated Edges

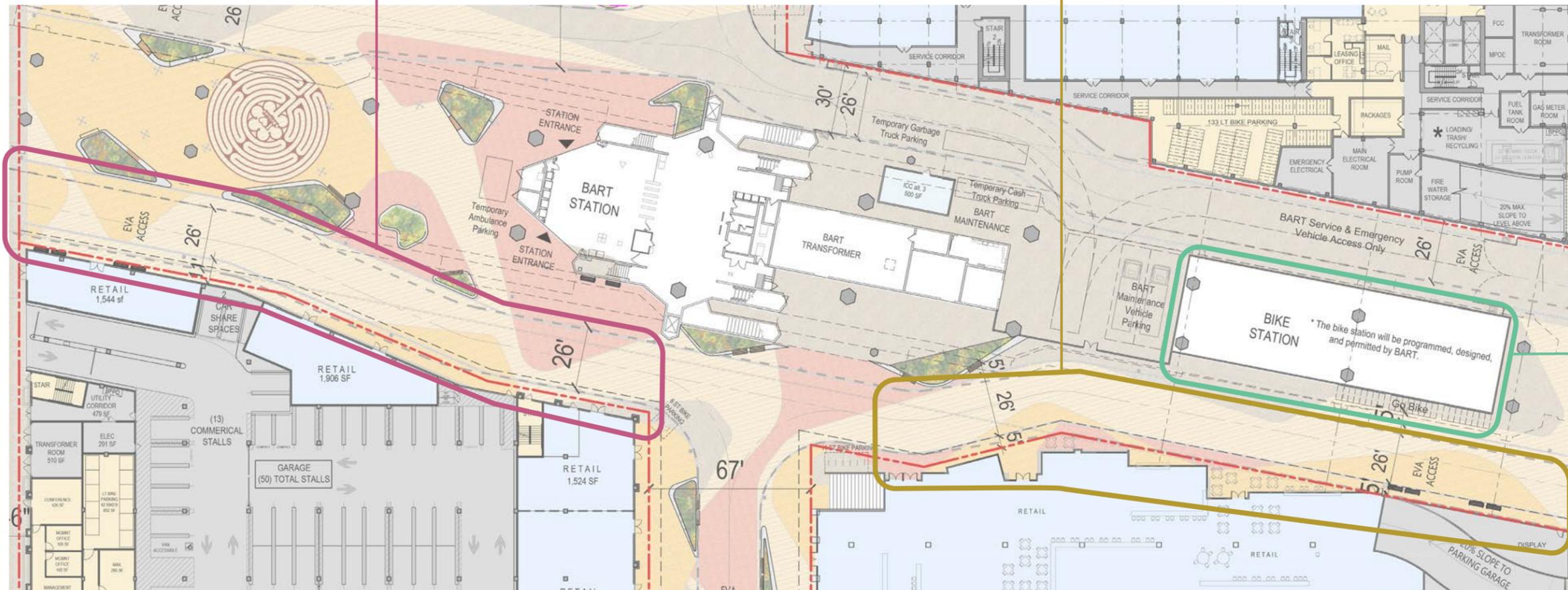
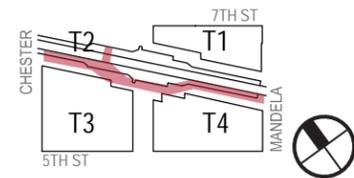


Places for Music and Poetry Events



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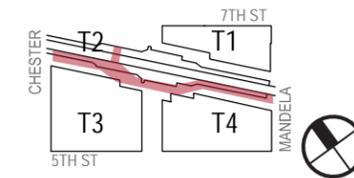
PROJ. # 168-153 WO BART
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PUBLIC SPACE
IMPROVEMENT:
ART ALLEY

SHEET: A-9.12



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DATE: July 24, 2020

ART ALLEY

SHEET: A-9.14

Center Square

Center Square: The vacated Center Street has been transformed into an active urban destination for the neighborhood. This space is both an important pedestrian connection to the BART station and a significant public plaza for the surrounding community. It is lined with a wide variety of local food offerings that showcase the cultural and ethnic diversity of West Oakland. The space creates an inviting place for the neighborhood to shop, dine, and relish a quality community experience.

The landscape materials are designed with high quality paver, brick, finished concrete and other materials to create a high quality public pedestrian experience and to maximize the types of uses that can occur on site.

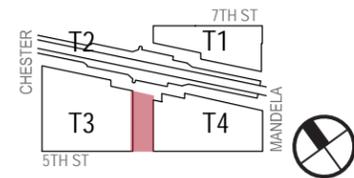
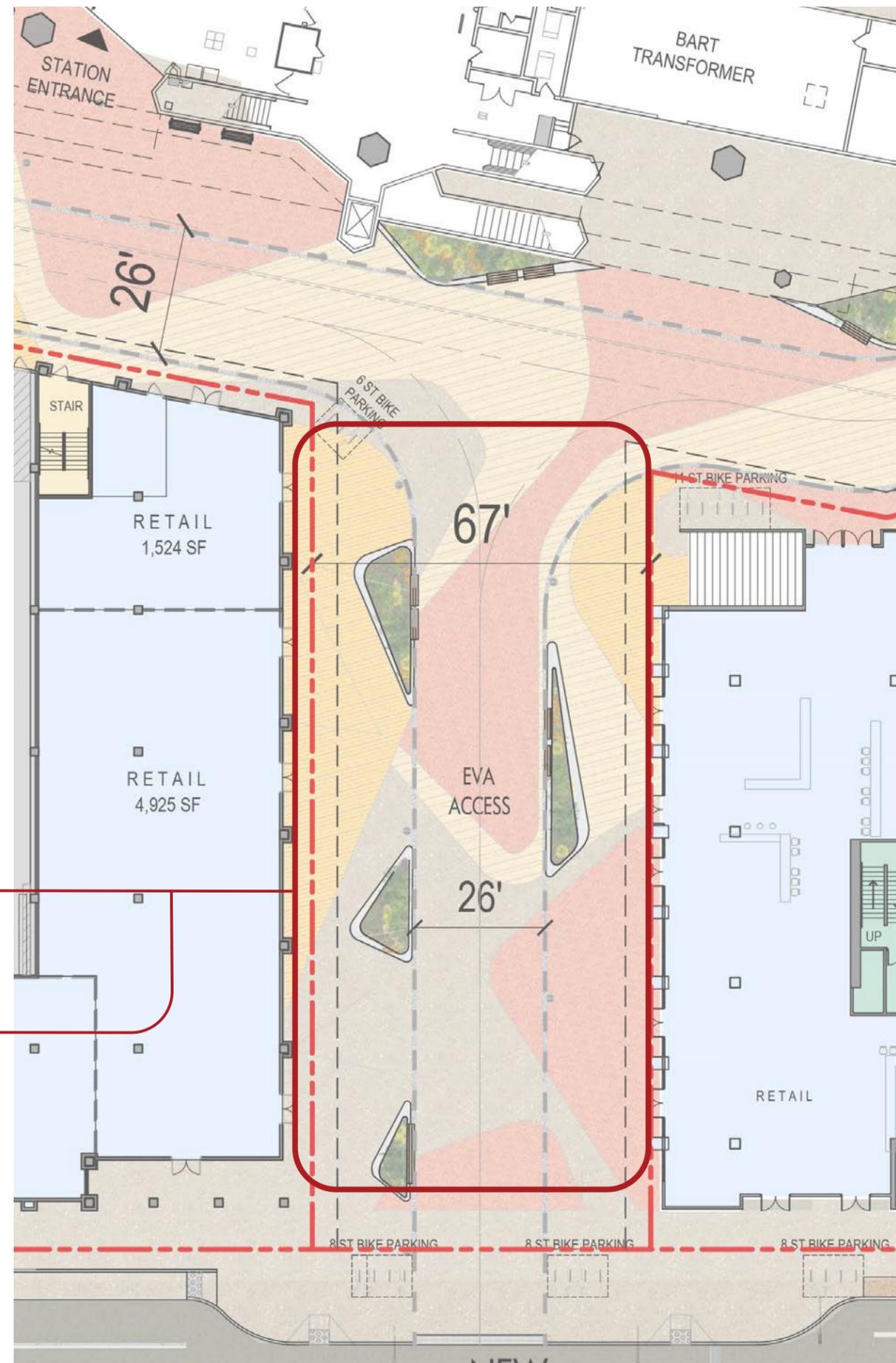
Site Lighting: The Lighting plan has been designed to create a well lit space that connects BART to 5th Street. The visual security of pedestrians within this space is facilitated by locating retail and other public activities along all edges of the development. The landscape plan is designed with adequate lighting to provide a safe environment while conforming to current best practices to mitigate light pollution.



Evening Food Festivals



Neighborhood Food Destination



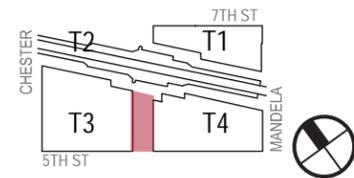


BIRDSEYE OF CENTER SQUARE



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DATE: July 24, 2020

**CENTER
SQUARE**

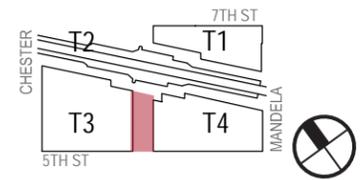
SHEET:

A-9.16



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VIEW OF CENTER SQUARE



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DATE: July 24, 2020

**CENTER
SQUARE**

SHEET:

A-9.17



VIEW FROM 5TH STREET

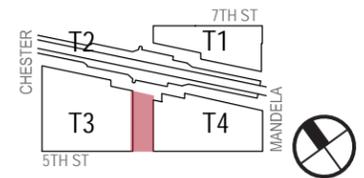


VIEW OF CENTER SQUARE



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PROJ. # 168-153 WO BART
DATE: July 24, 2020

**CENTER
SQUARE**

SHEET: **A-9.18**

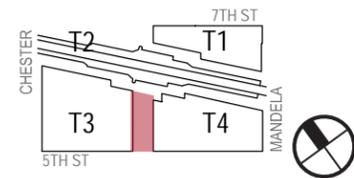


VIEW OF EXTERIOR STAIR



VIEW OF ART ALLEY FROM CENTER SQUARE

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SHEET: **CENTER SQUARE**
A-9.19

Crosswalks & Intersections

5th Street/Center Street and 5th Street/Chester Street Intersection: High-visibility crosswalks will be installed along with directional, ADA compliant curb ramps.

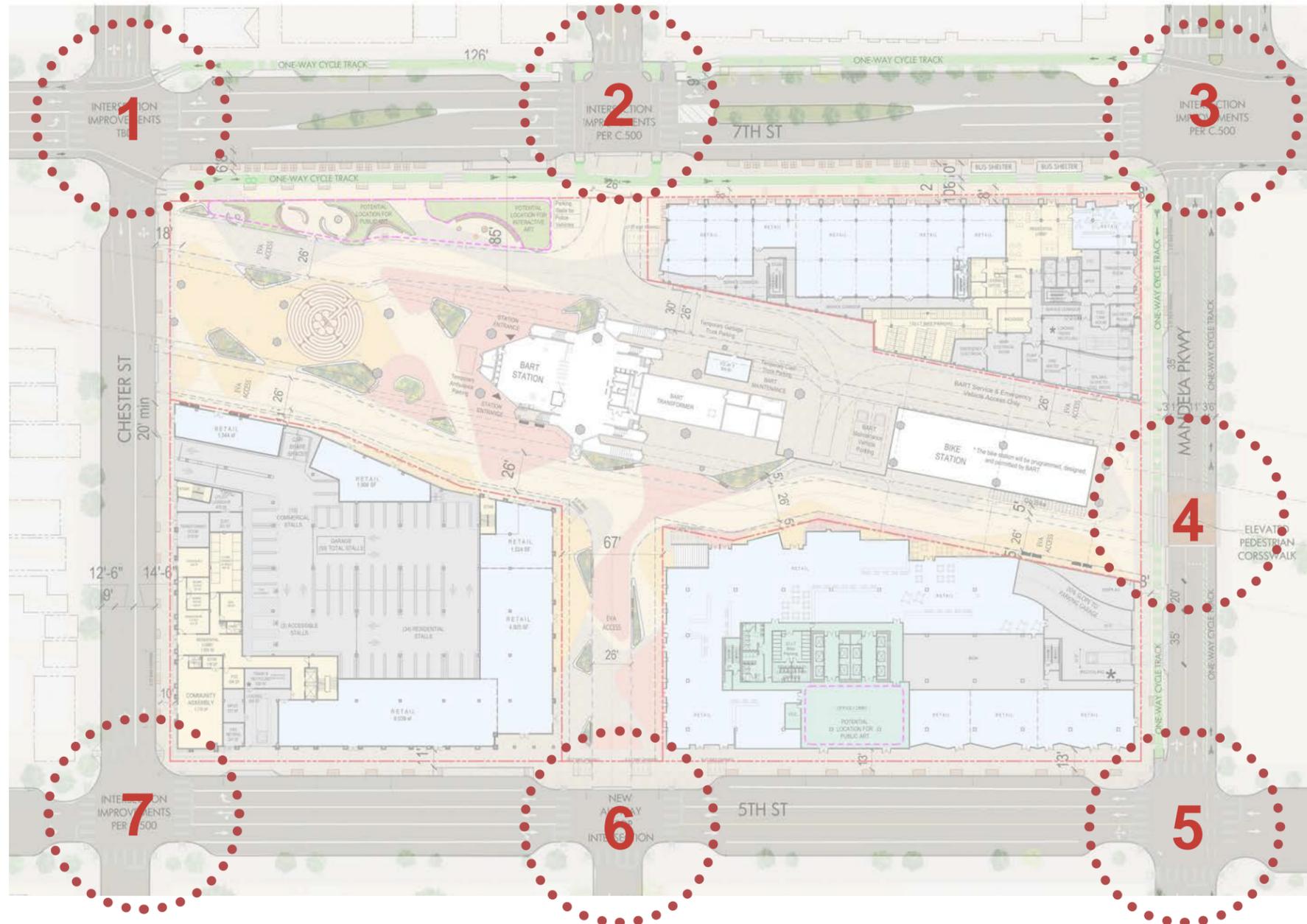
Chester Street/7th Street Intersection: To facilitate safe pedestrian, bicycle and vehicle movements, the curbs and crosswalks have been engineered to separate pedestrian and bicycle movements with clear non-conflicting pathways.

Bulb-outs have been provided where possible, including the NW, NE, and SW corners. Bus turning requirements prevent this on the SE corner.

Mandela Parkway/7th Street Intersection: This is a significant bike and pedestrian access point. To facilitate safe pedestrian, bicycle and vehicle movements, the curbs and crosswalks have been engineered to clearly separate pedestrian and bicycle movements with clear non-conflicting pathways. A dedicated bicycle signal phase will be evaluated for further consideration.

Mandela Parkway/5th Street Intersection: The NE, SE and SW crosswalks at this intersection have been bulbed out to minimize pedestrian crossing distances and ADA-compliant, directional curb ramps will be installed. Crosswalks will be reconfigured to create more directional connectivity.

*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.

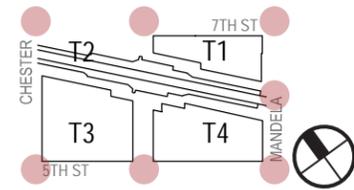


Mid-Block Crossing on Mandela: A high-visibility, mid-block pedestrian crossing will be added on Mandela Parkway between 7th and 5th Streets. The crossing design is raised and coordinated with the design of the two-way cycle track to facilitate safe pedestrian crossing of both the roadway and the cycle track.

7th Street/Center Street Intersection: The intersection crossing will be designed with a scramble pedestrian crossing to facilitate access. Bike and pedestrian access will be designed to prevent conflicts and promote ease of use. A protected intersection design will be provided with separated pedestrian crossing and bikeway crossing.



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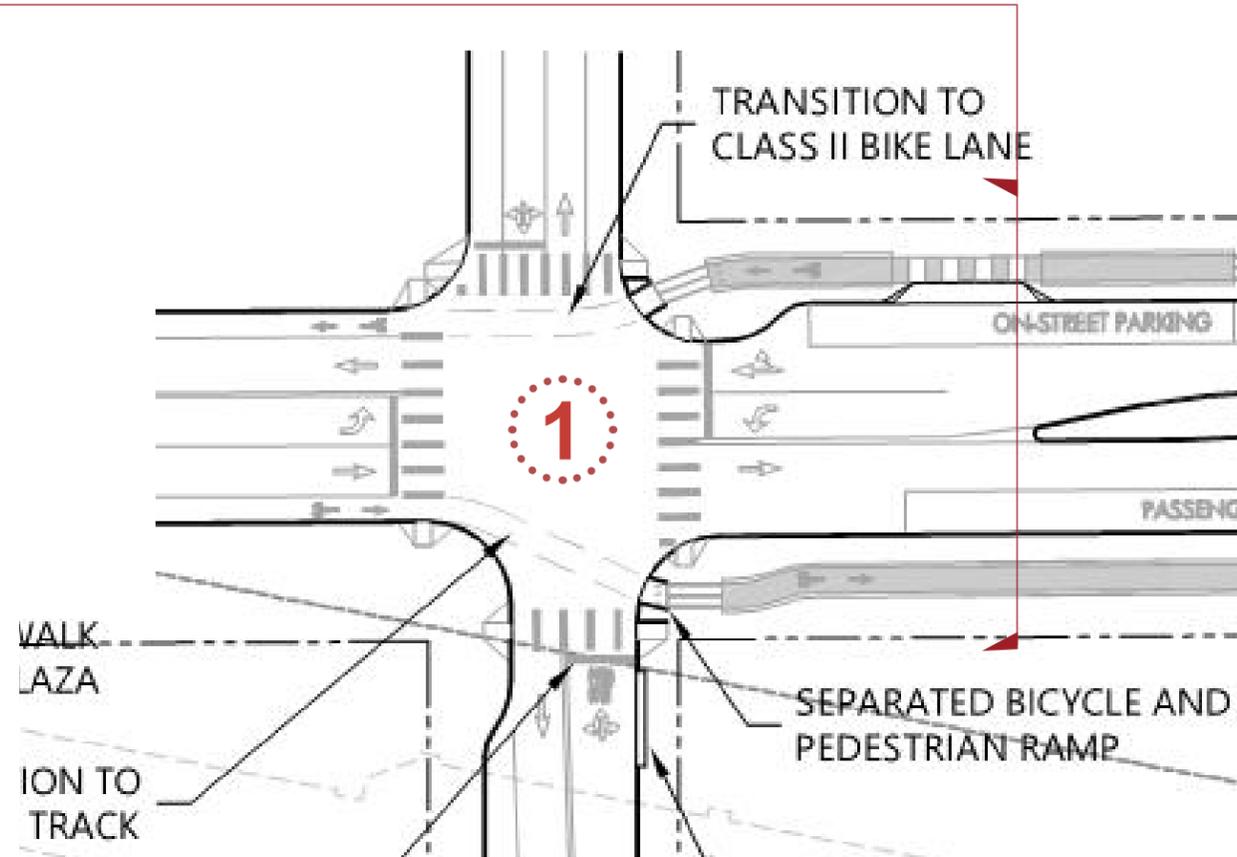
**PUBLIC SPACE
IMPROVEMENT:
CROSSWALKS &
INTERSECTIONS**

SHEET:

A-10.04

7TH STREET

8' SIDEWALK
 6' CYCLE TRACK
 +(2) 1' DET. BARRIERS
 4' SIDEWALK
 10' BUS LOADING
 11.5' WESTBOUND
 11' EASTBOUND
 11.5' EASTBOUND
 10' PASSENGER LOADING
 8.5' SEPARATION
 6' CYCLE TRACK
 +(2) 1' DET. BARRIERS
 8' SIDEWALK
 PLAZA



RAMP NOTES:

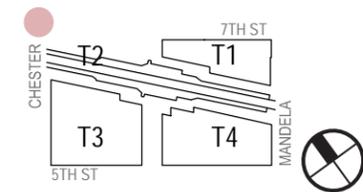
1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.

*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.



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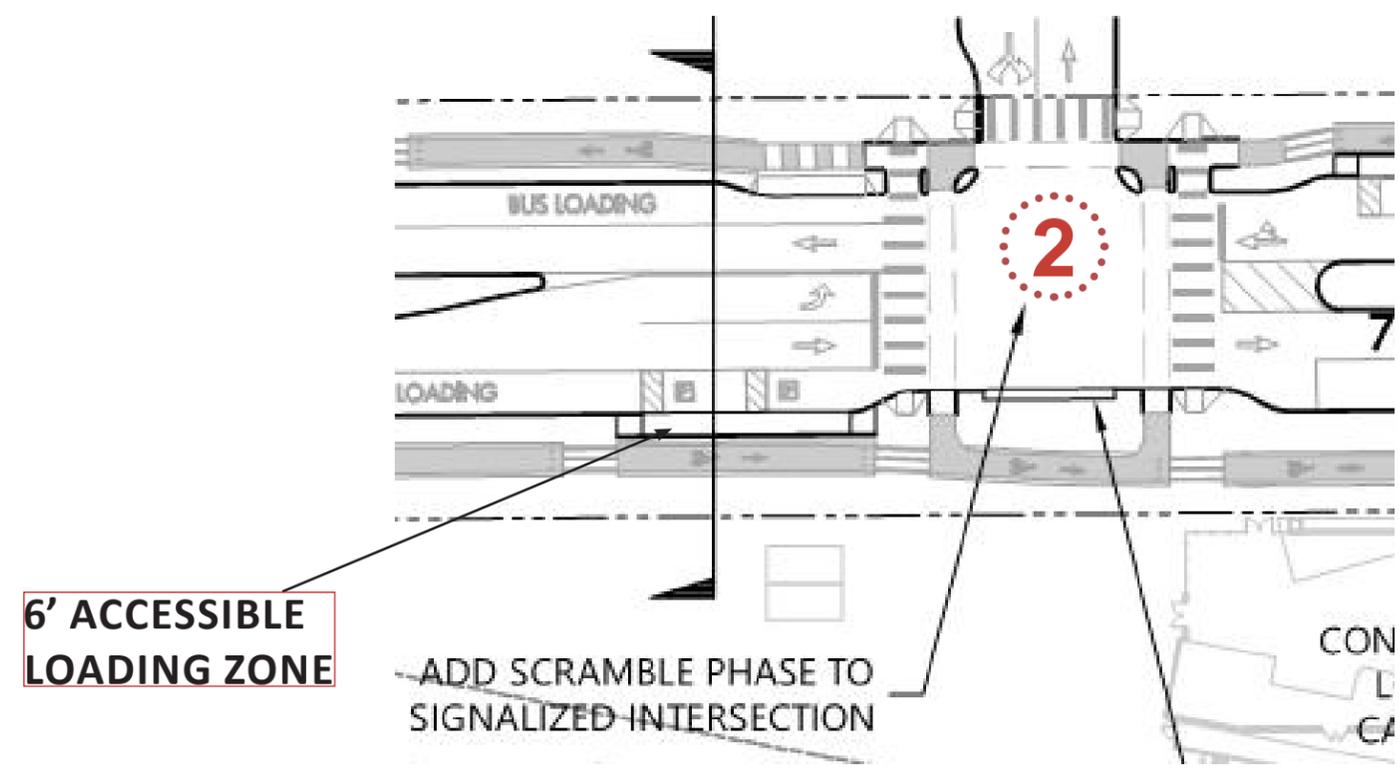
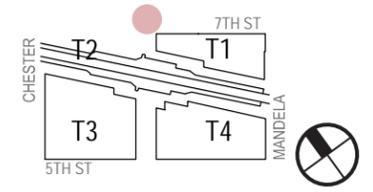


PROJ. # 168-153 WO BART
 DATE: July 24, 2020

CROSSWALKS &
 INTERSECTIONS

SHEET:

A-10.04.01



RAMP NOTES:

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 LANDSCAPE ARCHITECTS

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**CROSSWALKS &
 INTERSECTIONS**

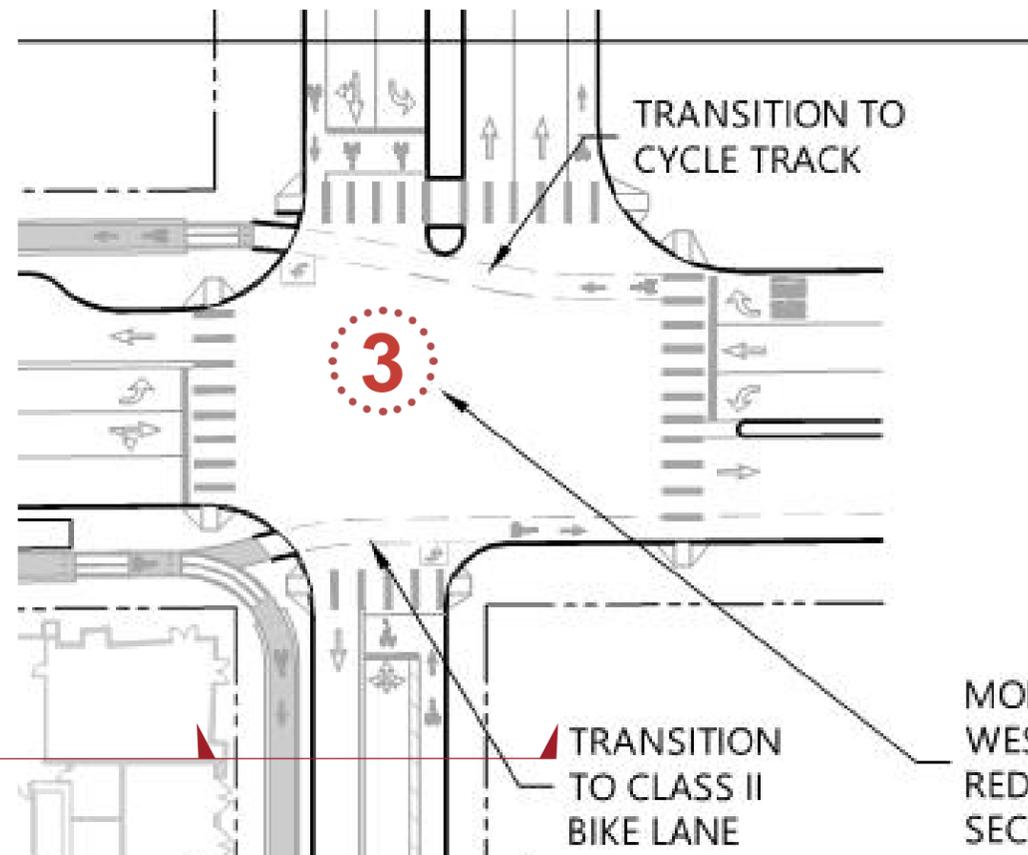
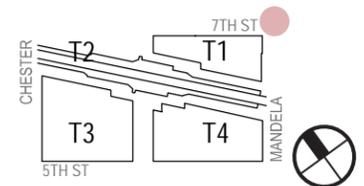
SHEET:

A-10.04.02



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MODIFY SIGNAL TIMING FOR
WESTBOUND BUS-ONLY PHASE.
REDUCE CYCLE TIME TO 90
SECONDS.

MANDELA PARKWAY

- 8' MIN. TO BUILDING
- 6' CYCLE TRACK
- +(2) 1' DET. BARRIERS
- 3.5' SIDEWALK
- 11.5' SOUTHBOUND
- 11.5' NORTHBOUND
- 2.5' BUFFER ZONE
- 6' BIKE LANE
- 10' SIDEWALK LANE

RAMP NOTES:

1. BICYCLE AND PEDESTRIAN RAMPS SHALL BE SEPERATED.
2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.



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**CROSSWALKS &
INTERSECTIONS**

SHEET:

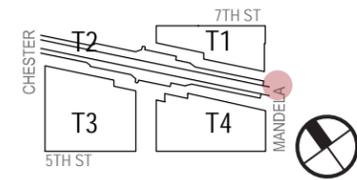
A-10.04.03

*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.

**DETECTABLE WARNING
DOMES BOTH SIDES**



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RAMP NOTES:

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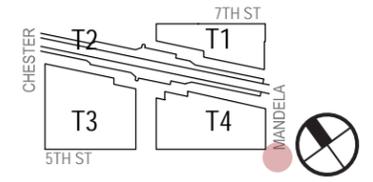
*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.
 *The Hall of Fame Plaques will be removed from the right of way to be relocated back into the 7th Street right of way at an appropriate realigned location by the completion of T1.
 *The dancing lights will be removed from the right of way and there is no plan for their relocation as part of the FDP at this time.

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**CROSSWALKS &
INTERSECTIONS**

SHEET:

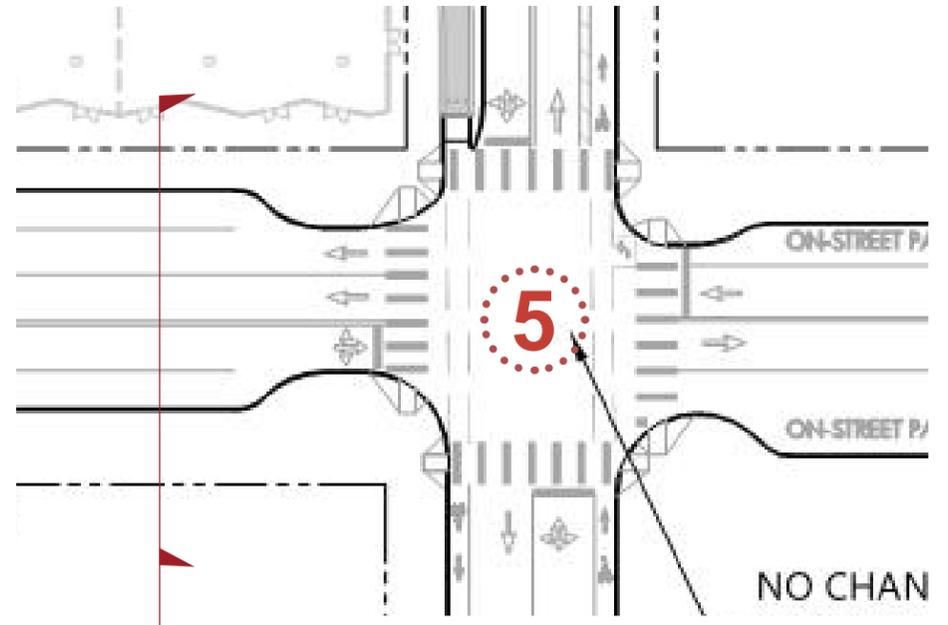
A-10.04.04



CROSSWALKS & INTERSECTIONS

SHEET:

A-10.04.05



5TH STREET

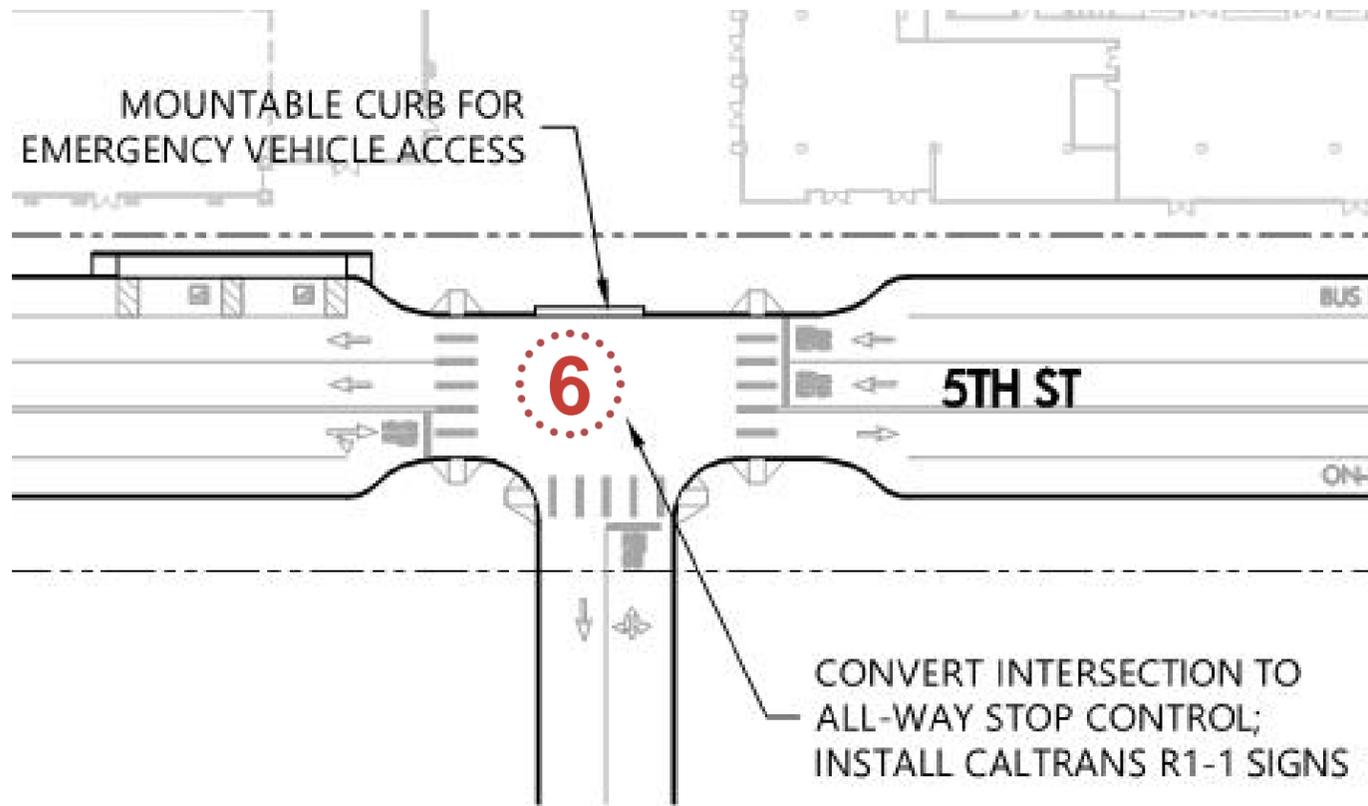
- 10' SIDEWALK
- 9' BUS LOADING
- 11.5' WESTBOUND
- 11.5' WESTBOUND
- 11.5' EASTBOUND
- 9' ON-STREET PARKING
- 6' MIN SIDEWALK
- 12' TO PROPERTY LINE



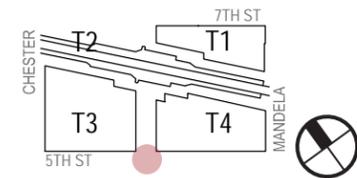
RAMP NOTES:

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**CROSSWALKS &
 INTERSECTIONS**

SHEET:

A-10.04.06

RAMP NOTES:

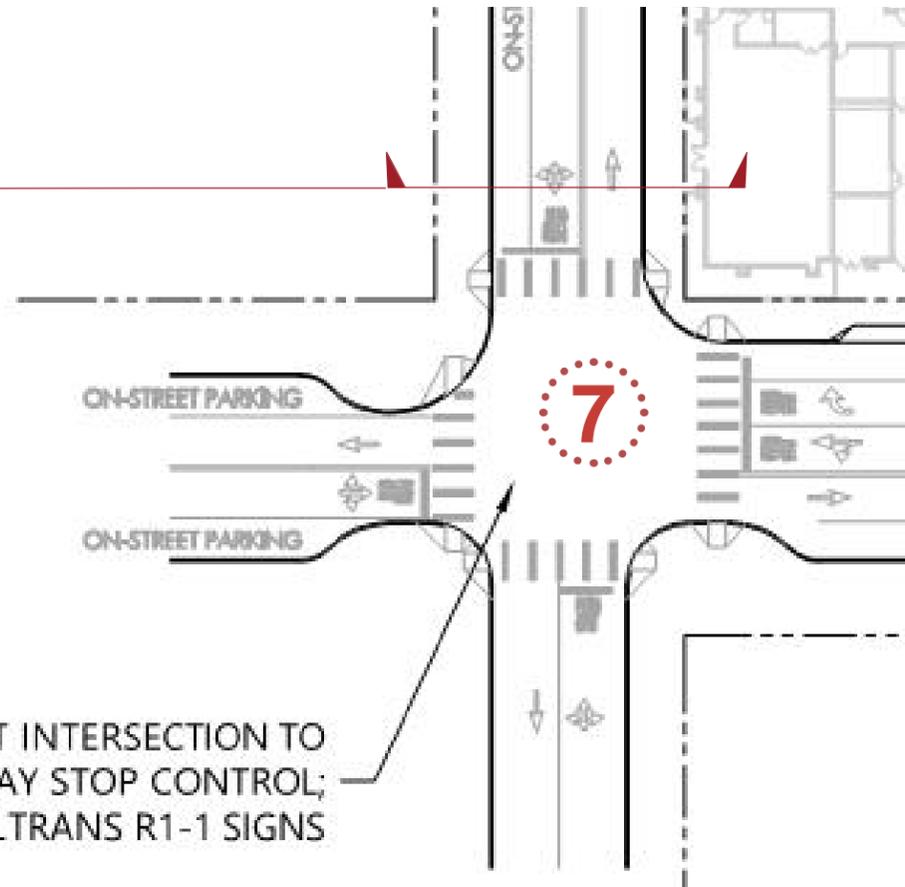
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2. DETECTABLE WARNING DOMES SHALL BE INSTALLED AT ALL RAMPS, SEE SHEET C5.03.



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CHESTER STREET

- 14' SIDEWALK
- 9' ON-STREET PARKING
- 12.5' SOUTHBOUND
- 14.5' NORTHBOUND
- 10' SIDEWALK



CONVERT INTERSECTION TO
ALL-WAY STOP CONTROL;
INSTALL CALTRANS R1-1 SIGNS

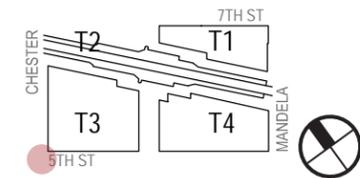


RAMP NOTES:

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**CROSSWALKS &
INTERSECTIONS**

SHEET:

A-10.04.07

Sidewalks

7th Street (between Mandela Parkway and Chester Street):

The sidewalk widths range from 10'-15', and are designed to allow adequate clearances in order to accommodate the needs of pedestrians, bus passengers, and curbside passenger loading. The sidewalk is clear space for passenger loading at the curb, and a protected class 4 cycle track with clearly marked pedestrian crossing indicators.

Street lighting and street trees are located to reinforce the visual experience of the sidewalk space.

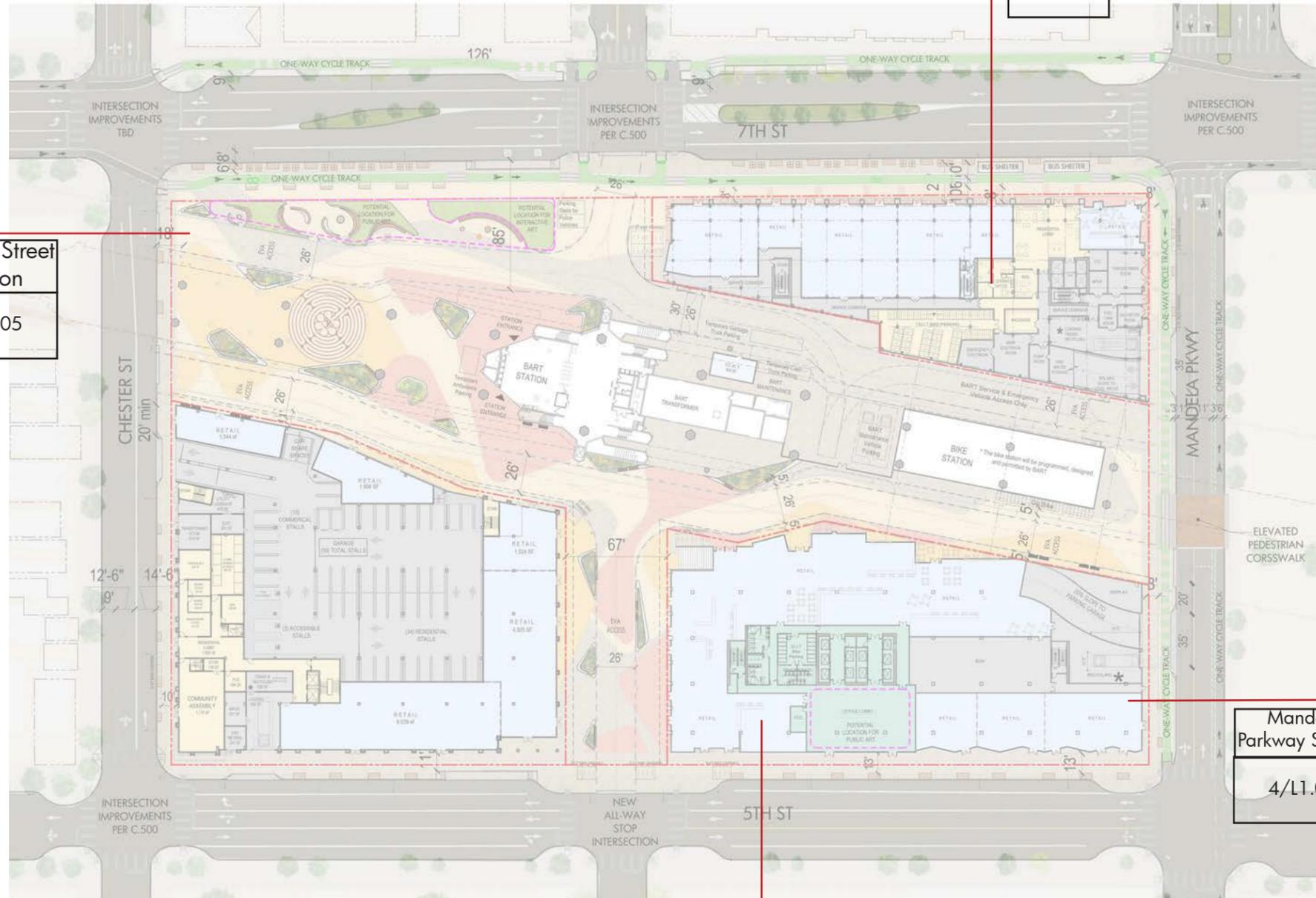
5th Street (between Mandela Parkway and Center Street):

The sidewalk width will be 10-15 feet min. wide in order to accommodate the needs of pedestrians, bus passengers, and curbside passenger loading.

5th Street (between Center and Chester Street):

Between 5th and 7th Streets, sidewalks adjacent to the project site are a minimum of 10 feet wide, and provide 8' clear path. The sidewalk is buffered from adjacent loading and street traffic by street pedestrian-scale lighting and street trees.

Chester Street Section
3/L1.05



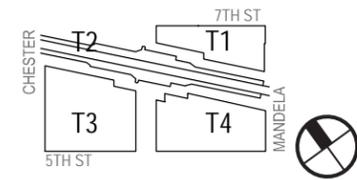
Parking and Truck Loading Entrances: for the project are designed to provide clear sight lines, and include ADA features such as tactile warning strips, and audible warnings for when vehicles cross the pedestrian path of travel.

*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.

*The developer will coordinate with the City of Oakland and the appropriate property owners, and document in the CMP prior to building permit, to determine the feasibility of and if deemed feasible, complete the sidewalk gap on the south side of 5th Street just east of Center Street.



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**PUBLIC SPACE
IMPROVEMENT:
SIDEWALKS**

SHEET:
A-10.05

Bikeways & Bike Parkings

Mandela Cycle Tracks:

To facilitate bicycle access from the bicycle lanes on Mandela Parkway south of 7th Street and north of 5th Street, two raised, one-way cycle tracks are located along the west and east sides of Mandela Parkway between 7th and 5th Streets.

These one-way cycle tracks are 6 feet wide, with an 8' and 10' separation from face of curb to the edge of the cycle track. In addition to the cycle track, sidewalks with a minimum width of 8' clear are provided.

7th St Cycle Tracks Eastbound:

To facilitate bicycle access in conjunction with passenger loading from vehicles and buses along eastbound 7th Street between Chester Street and Mandela Parkway, a raised, one-way cycle track is provided.

The cycle track travel surface is 6 feet, with an 8'-10' foot setback from sidewalk to the face of curb.

7th St Cycle Tracks Westbound:

On westbound 7th Street between Mandela Parkway and Chester Street, a raised, one-way cycle track is located with a width of 6 feet.

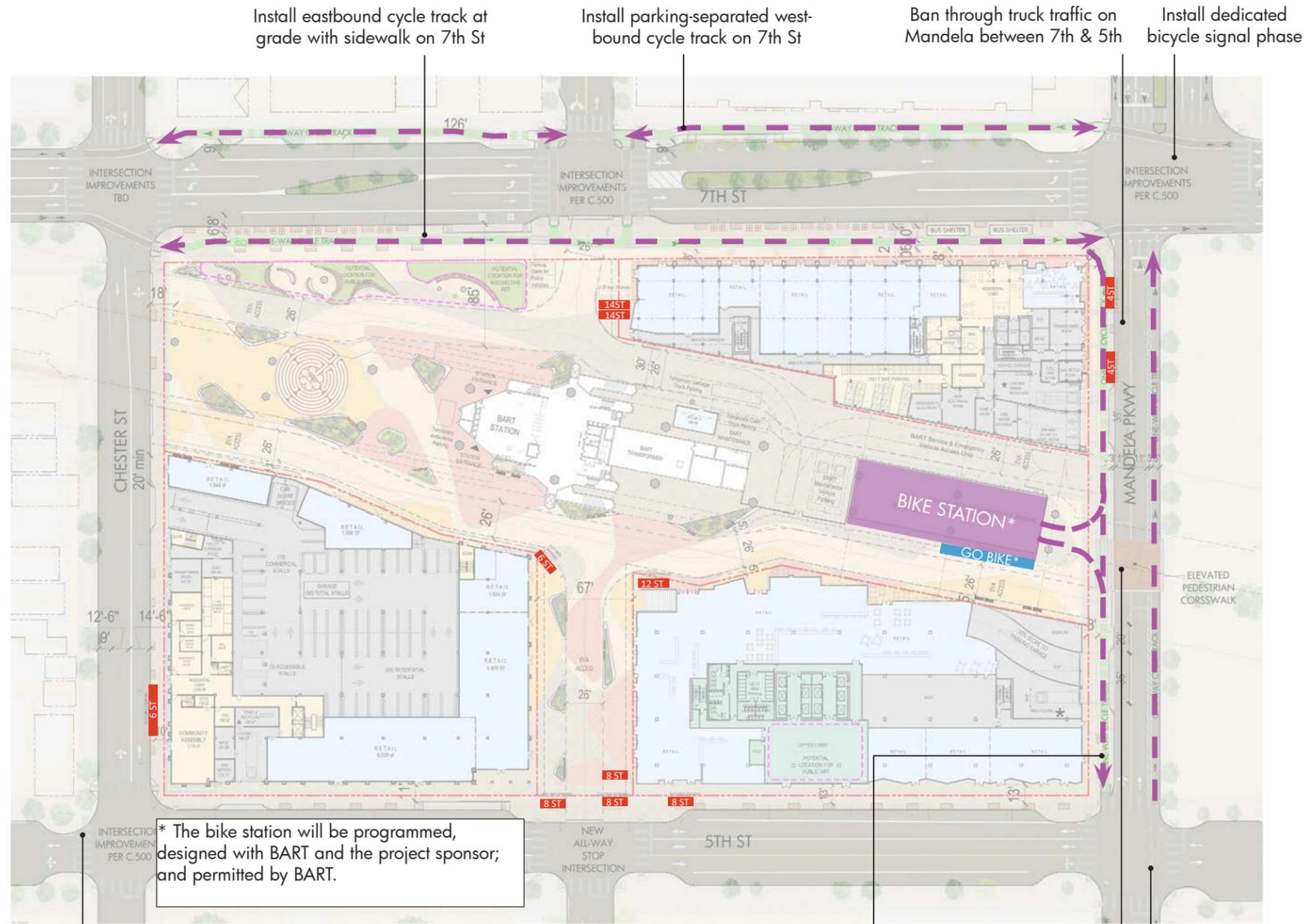
The cycle track is provided an 8' setback sidewalk from the face of curb to the edge of the cycle track to provide adequate space for pedestrian loading.

Ford Go-bike: has been co-relocated with the Bike Station, to provide ease of access, prevent plaza use conflicts and to reinforce the bike station as a central hub for bike users.

*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.

*Accommodating a pedestrian walkway and in-lane bicycle facility on 7th Street and Mandela Pkwy during the construction period of T1 may be accommodated, but it may require changes to the street lanes during construction which need to be studied by the developer and DOT and documented in the Construction Management Plan.

* The phasing of the bike station will be determined based on funding and construction phasing of the surrounding parcels; and will be documented within the Construction Management Plans. The phasing will likely follow completion of T1 and/or the completion of the adjacent EVA access routes; but, will be based on availability of grants and/or other funding.



Install high-visibility crosswalks, directional curb ramps & pedestrian refuges on 5th St

Install 2 one-way cycle tracks on Mandela between 7th & 5th on grade with sidewalk

Install high-visibility mid-block crosswalk

Install dedicated bicycle signal phase

Bike Station:

The project will include an enhanced bicycle station located on the east side of the station structure. The bike station will be programmed, designed with BART and the project sponsor; and permitted by BART. The city will be responsible for design review.

Additional bicycle parking for BART and commercial patrons is provided throughout the site in the form of bicycle racks. Bicycle parking for the TOD's commercial and residential tenants will also be provided in accordance with City of Oakland requirements.

Bike Parking Account:

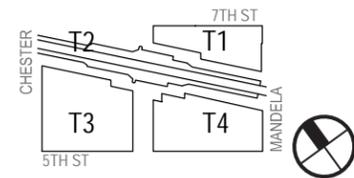
ON SITE SHORT TERM PARKING: 82

BIKE STATION: The bike station will be programmed, designed with BART and the project sponsor; and permitted by BART. The city will be responsible for design review.

GO BIKE PARKING NUMBER: 21



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PROJ. # 168-153 WO BART
DATE: July 24, 2020

**PUBLIC SPACE
IMPROVEMENT:
BIKES**

SHEET:

A-10.06

Passenger Loading

7th Street Passenger Loading:

Will be located on the south side of 7th Street west of Center Street. A portion (50 linear feet) of this loading area closest to the intersection with Center Street is designated as a loading space for passengers with disabilities. This area provides the most direct access to the station entrance.

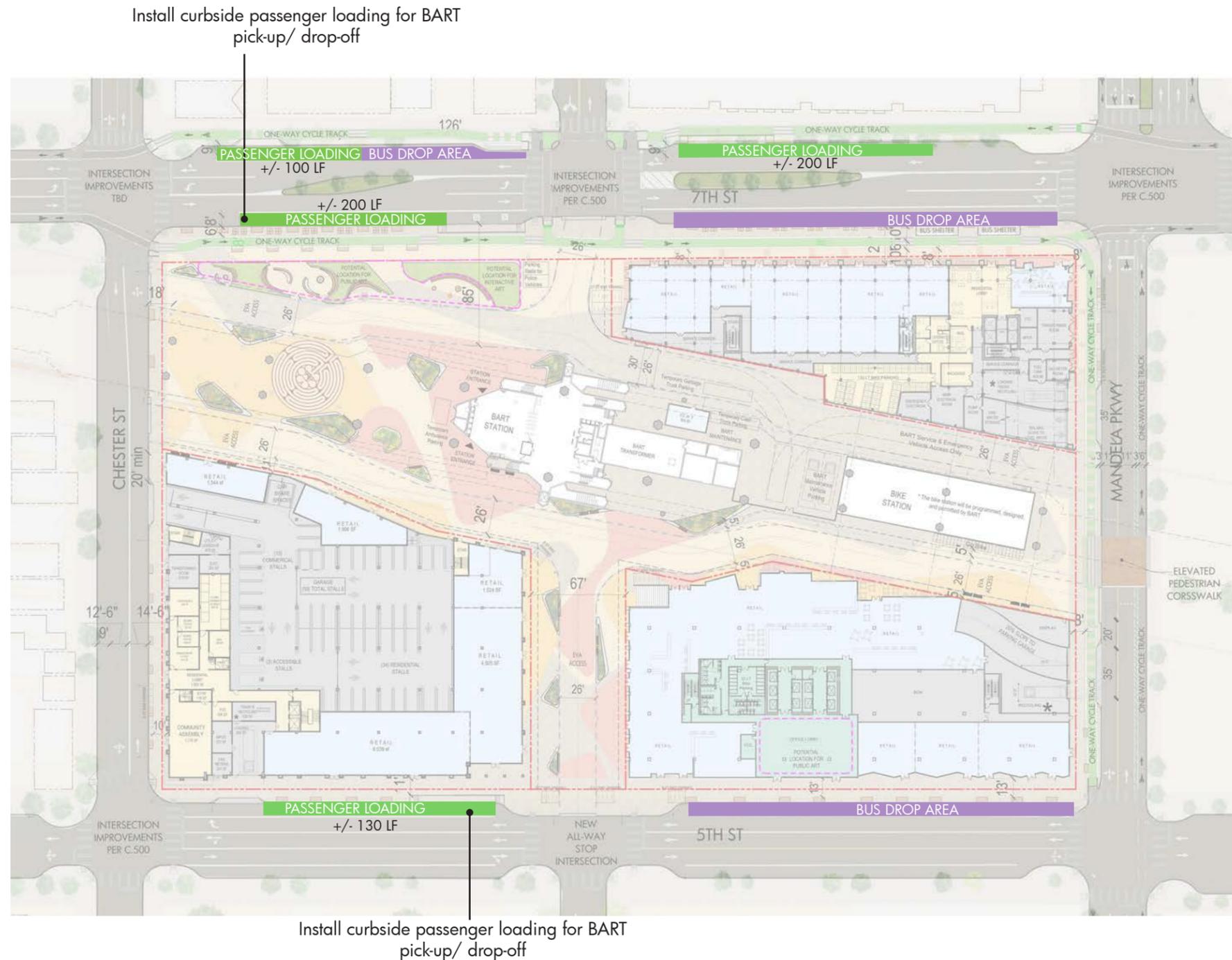
Another 200 feet of linear curb is on north side of 7th Street between Center Street and Mandela Parkway for passenger loading and unloading and 100 feet of linear curb near the Chester Street intersection.

5th Street Passenger Loading:

130 feet of linear curb is proposed along westbound 5th Street on the west side of the intersection with Center Street.

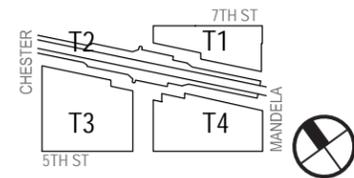
Wayfinding:

Signage directing vehicles to loading zones will be provided at key decision points like the Mandela Parkway and 7th Street intersection and the 5th Street and Kirkham or Union Street intersections. Loading zone locations will be incorporated into smartphone mapping and TNC apps to facilitate safe and efficient circulation and access.



**MANDELA STATION
@WEST OAKLAND
BART**

1451 7th St, Oakland,
CA 94607



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PROJ. # 168-153 WO BART

DATE: July 24, 2020

**PUBLIC SPACE
IMPROVEMENT:
PASSENGER
LOADING**

SHEET:

A-10.07

*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.

Bus Stops

7th Street Eastbound:

The existing bus stop on eastbound 7th Street will be retained and extended to the intersection with Center Street for an approximate total length of 260 linear feet. This stop would serve AC Transit Lines 29, 36, and 62 and will serve as both a stop and layover space for AC Transit Line 14. If Emery-Go-Round service is extended to the West Oakland Station, this stop could serve Emery-Go-Round vehicles as well.

7th Street Westbound: A new bus stop will be installed on westbound 7th Street to serve AC Transit Line 29 and Emery-Go-Round, if the service is extended to the West Oakland Station. 90 feet of linear curb is provided at this location to accommodate transit vehicles, as well as a concrete bus pad in the roadway.

5th Street Westbound: The bus stop and layover for lines 36 and 62 will be relocated to westbound 5th Street on the far side of the intersection with Mandela Parkway. 235 feet of curb length is provided to accommodate the bus stop and layovers, and a concrete bus pad will be installed in the roadway.

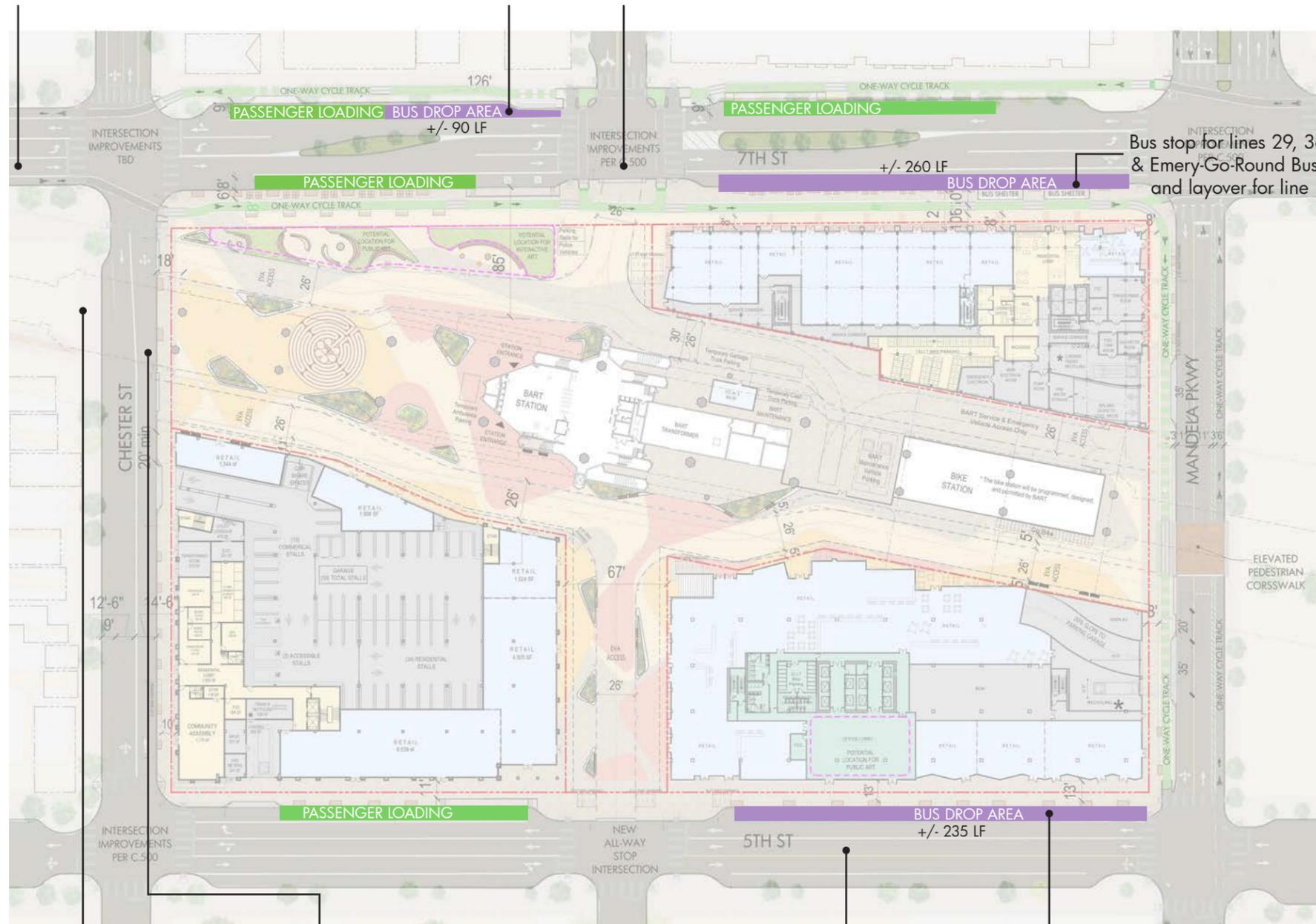
*The off-site phasing will be determined based on the Construction Management Plans. The off-site phasing will be complete prior to the Certificate of Occupancy of T1.

*The developer will work with the City to designate a bus stop for intercity coaches (e.g., Megabus and Bolt) and other shuttles on 7th Street between Henry and Chester Streets.

Install bus stop for intercity coaches (Bolt, Megabus) - optional location east of Mandela

Install bus stop for line 29 and Emery-Go-Round

Start sidewalk taper immediately after crosswalk to better accommodate bus access



Bus stop for lines 29, 36, 62 & Emery-Go-Round Bus stop and layover for line 14

Potential layover space for lines 36 & 62, requires further evaluation

Redesign Chester St cross-section to facilitate buses turning right

Install median barrier to prevent jaywalking to/from bus stop

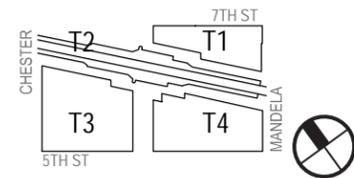
Bus stop and layover for lines 62 & 36

Intercity & Private Bus Stops: A bus stop for intercity coaches (e.g., Megabus and Bolt) could be installed on 7th Street between Henry and Chester Streets. Private employer shuttle could also utilize this stop. The existing BART surface parking immediately adjacent to this curb could be utilized for bus and shuttle transit passenger pick-up and drop-off (if not utilized as a layover space for AC Transit buses). A option to locate this bus stop east of Mandela Parkway is under consideration.

Passenger Amenities: All bus stops are provide with a high level of passenger amenities, including shelters with seating, maps and other information, and real-time bus arrival information; trash receptacles; and lighting.



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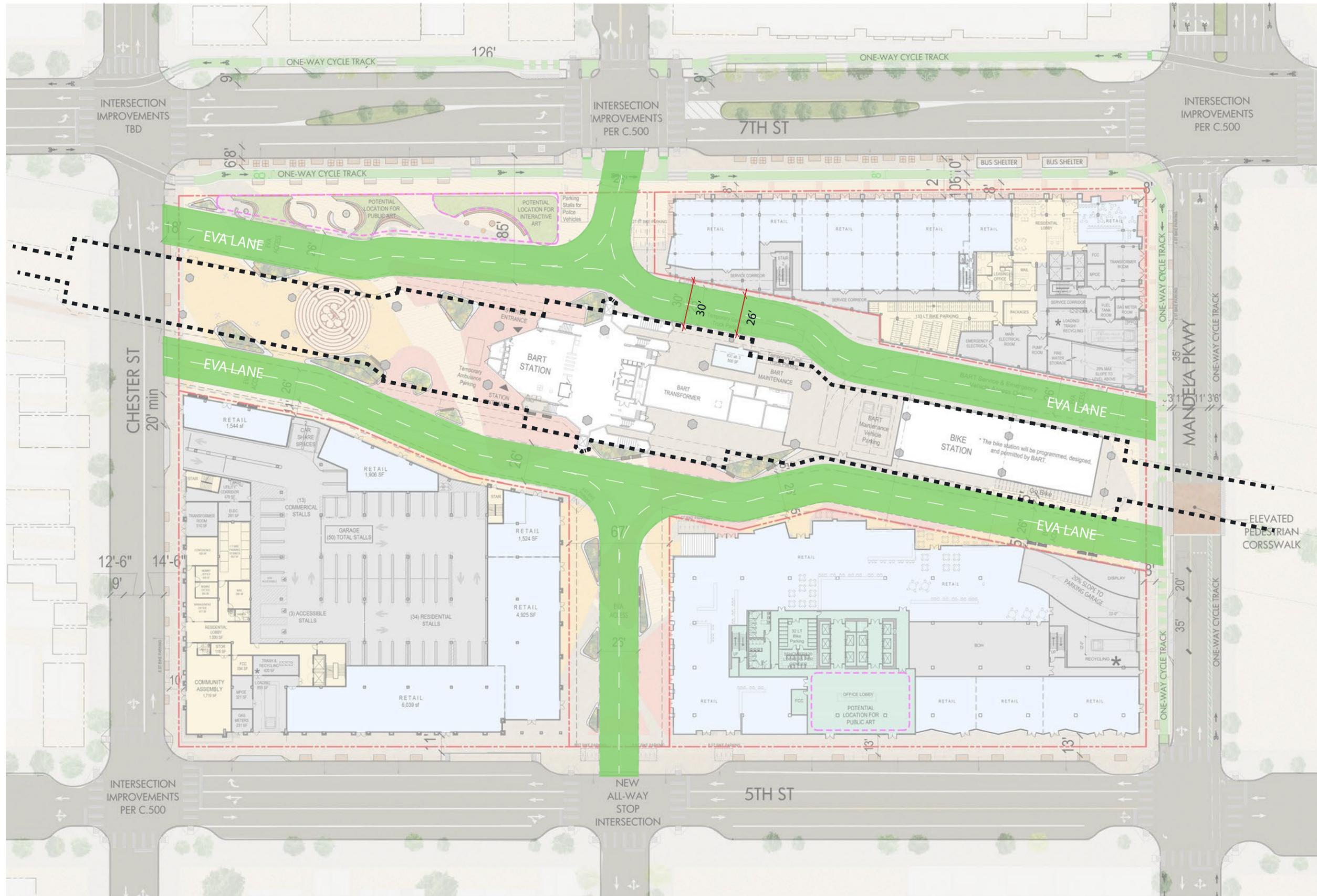


PROJ. # 168-153 WO BART
DATE: July 24, 2020

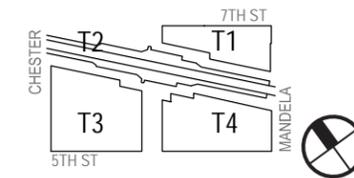
PUBLIC SPACE IMPROVEMENT: BUS STOPS

SHEET:

A-10.08



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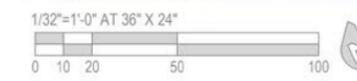
LANDSCAPE ARCHITECTS

PROJ. # 168-153 WO BART
 DATE: July 24, 2020

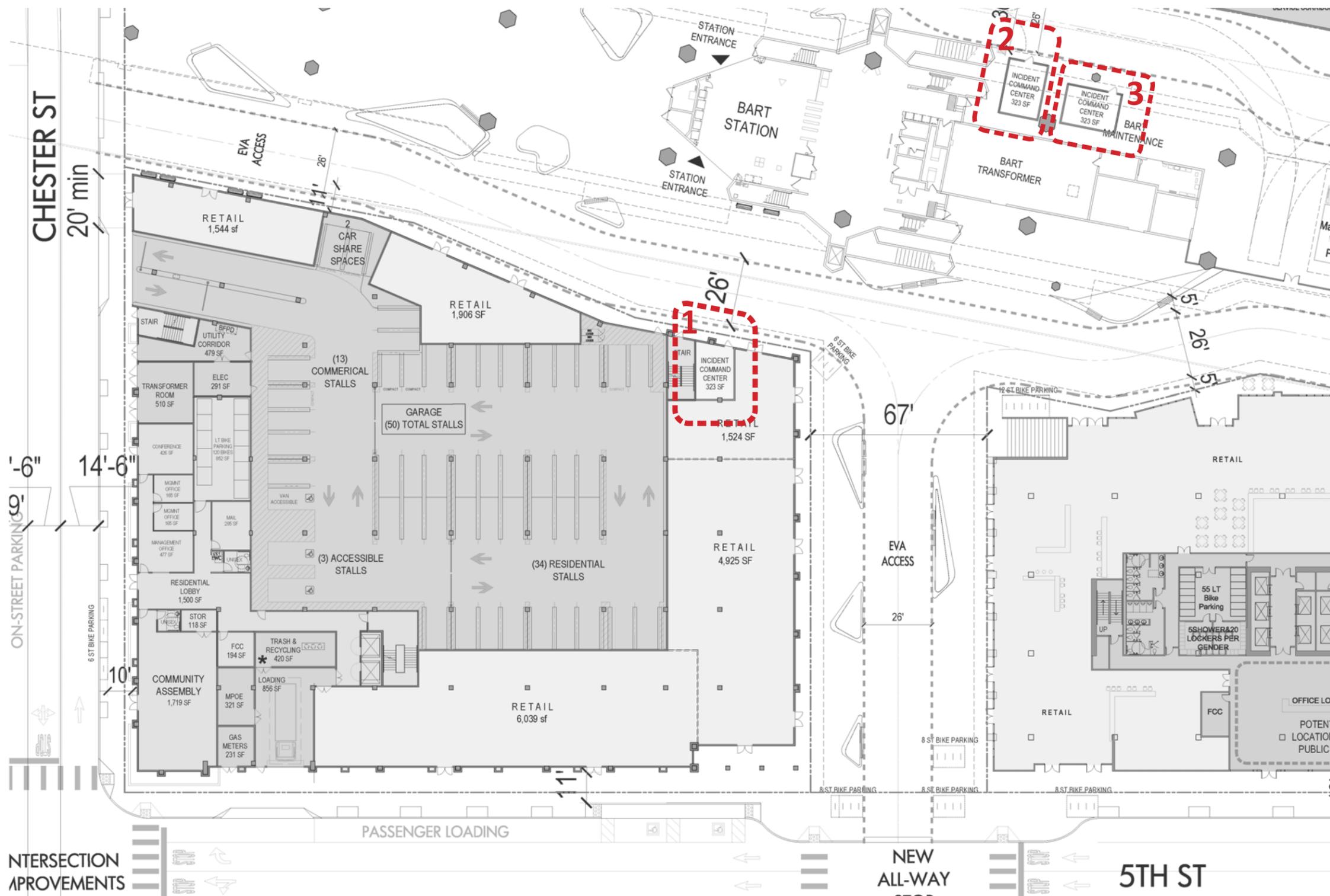
EMERGENCY
VEHICLE
ACCESS

SHEET:

The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.

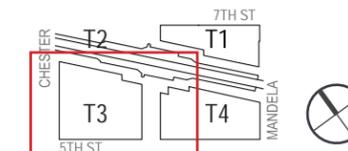


INCIDENT COMMAND CENTER LOCATION OPTIONS

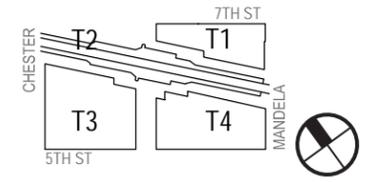


Note:
Options 2&3 are preferred locations by BART and Oakland Fire.

The final location for the ICC will be mutually determined by the City, BART, and developer.



**MANDELA STATION
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 BART**
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 CA 94607



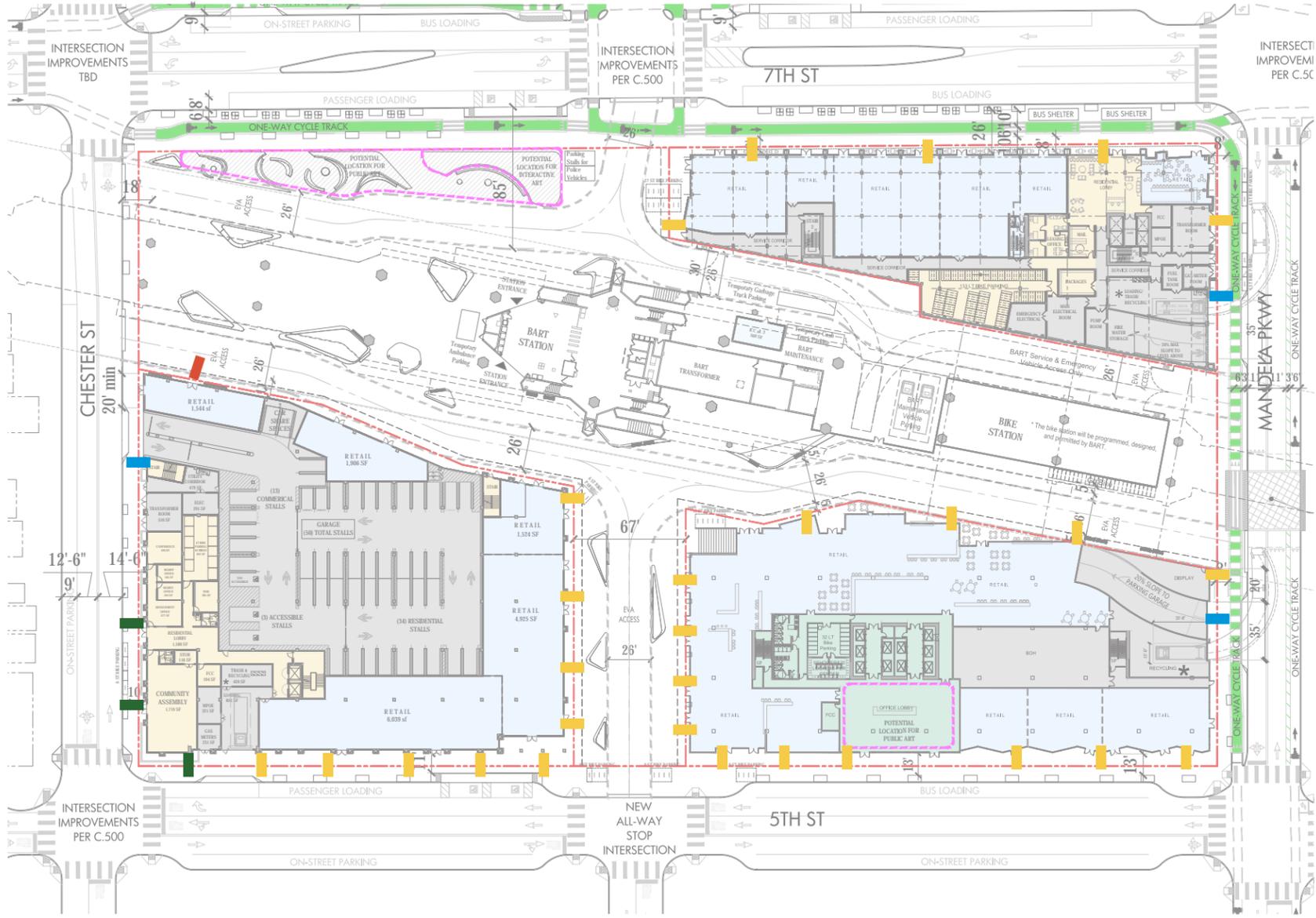
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 LANDSCAPE ARCHITECTS

PROJ. # 168-153 WO BART
 DATE: July 24, 2020

SIGN PLAN

SHEET:



- COMMERCIAL BLADE SIGN
- GARAGE ENTRANCE SIGN
- ART DISTRICT SIGN
- COMMUNITY BLADE SIGN



ART DISTRICT SIGN



COMMERCIAL BLADE SIGN

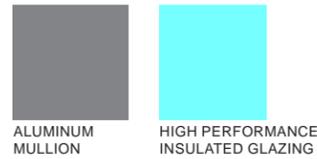


COMMERCIAL BLADE SIGN

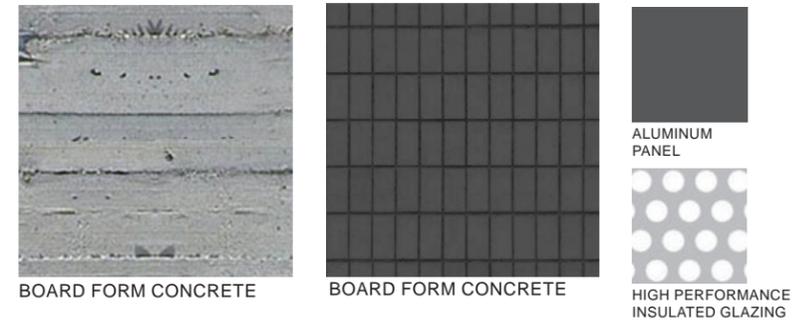
T1 NORTH / 7TH STREET ELEVATION - PODIUM



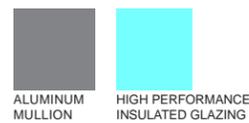
4-6TH FLOOR



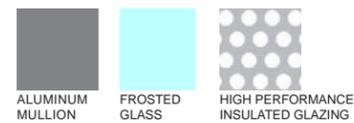
GROUND-3RD FLOOR



WINDOW SYSTEM



WINDOW SYSTEM FOR GARAGE

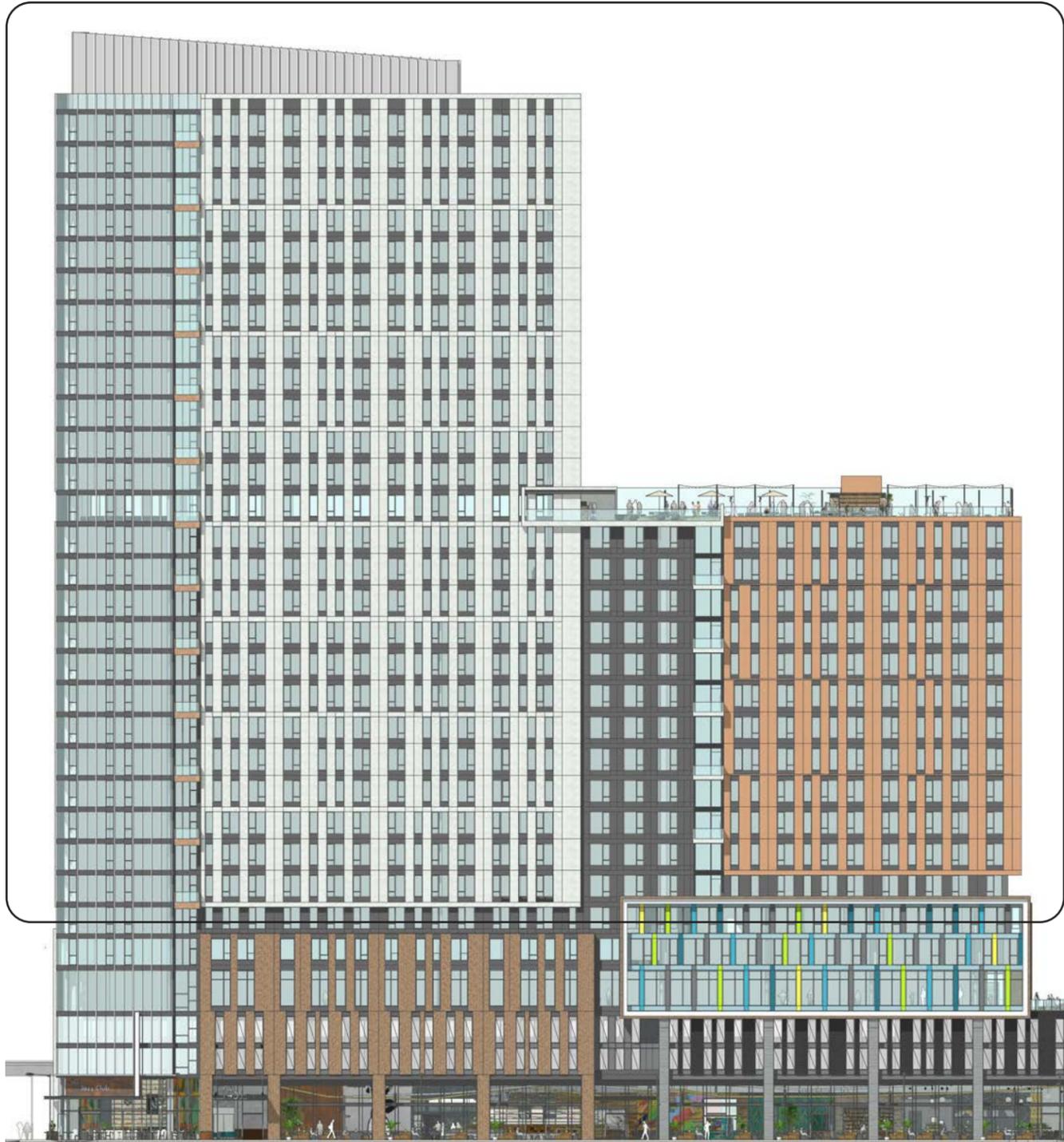


STOREFRONT SYSTEM



T1 NORTH / 7TH STREET ELEVATION - TOWER

7-31ST FLOOR



ROOF



CHANNEL METAL
MECHANICAL SCREEN

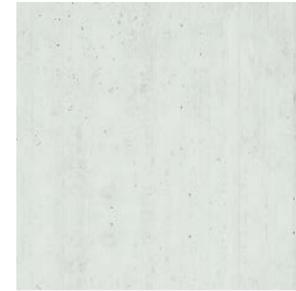
7-31ST FLOOR



ALUMINUM
MULLION



PRECAST
CONCRETE
WALL PANEL



PRECAST CONCRETE WALL
PANEL



PRECAST
CONCRETE
WALL PANEL



PRECAST
CONCRETE WALL
PANEL



PRECAST CONCRETE WALL
PANEL



PRECAST
CONCRETE
WALL PANEL



HIGH PER-
FORMANCE
INSULATED
GLAZING

WINDOW SYSTEM



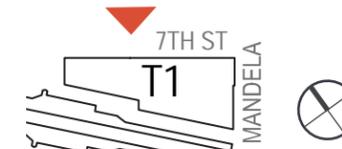
ALUMINUM
MULLION



HIGH PERFORMANCE INSULATED
GLAZING

Final Development Plan

Mandela Station @ West Oakland BART:
1451 7th St, Oakland, CA 94607
July 24, 2020

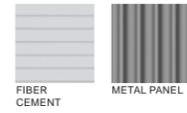
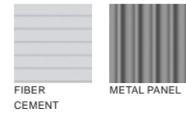
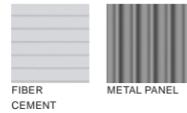


MATERIAL BOARD

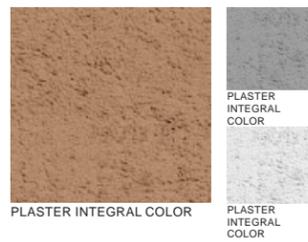
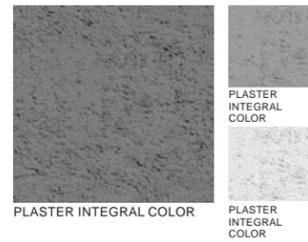
T3 SOUTH / 5TH STREET ELEVATION



7TH FLOOR



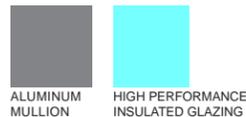
2-6TH FLOOR



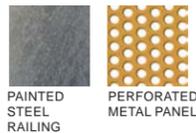
GROUND FLOOR



WINDOW SYSTEM



BALCONY



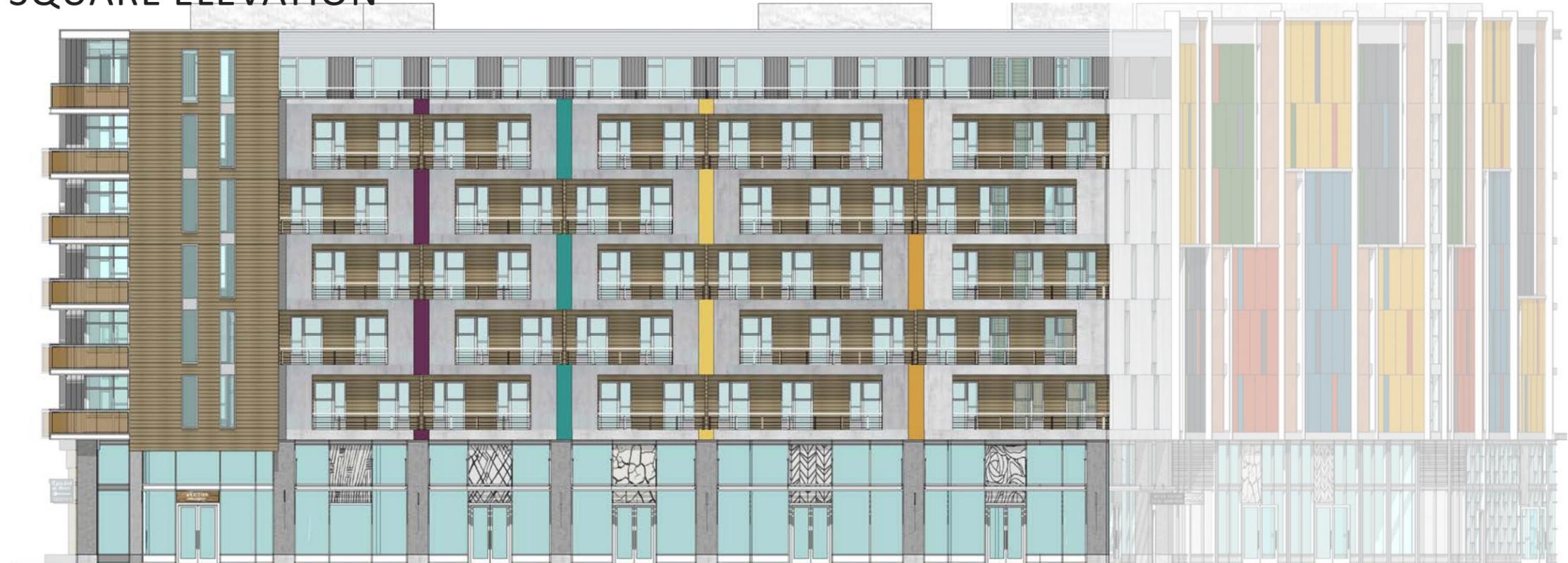
STOREFRONT SYSTEM



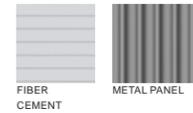
BASE



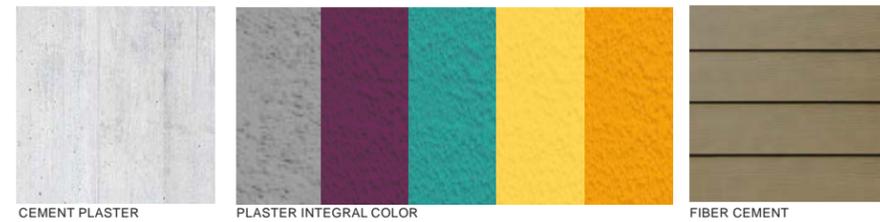
T3 EAST / CENTER SQUARE ELEVATION



7TH FLOOR



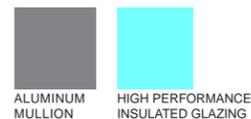
2-6TH FLOOR



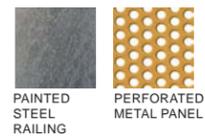
GROUND FLOOR



WINDOW SYSTEM



BALCONY

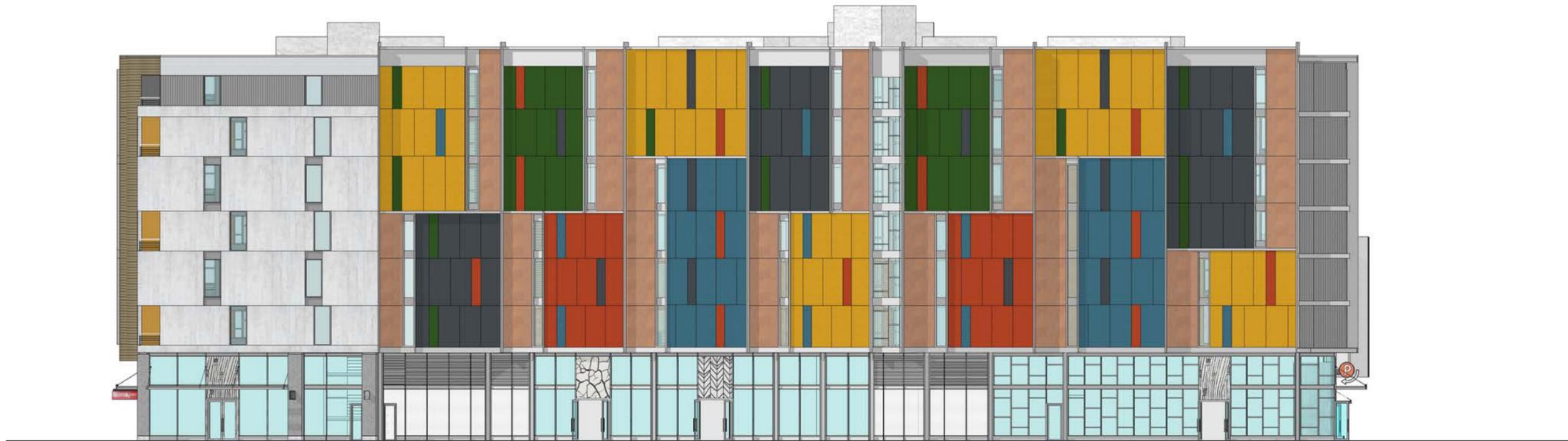


STOREFRONT SYSTEM

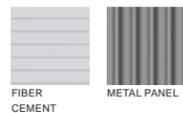


BASE





7TH FLOOR



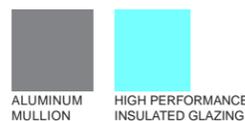
2-6TH FLOOR



GROUND FLOOR



WINDOW SYSTEM



STOREFRONT SYSTEM



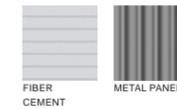
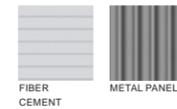
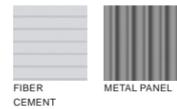
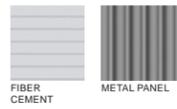
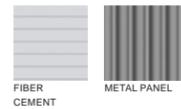
BASE



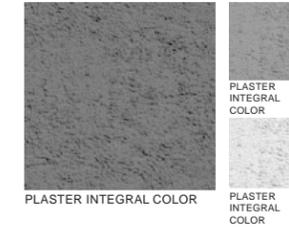
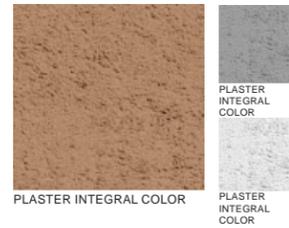
T3 WEST / CHESTER STREET ELEVATION



7TH FLOOR



2-6TH FLOOR



GROUND FLOOR



WINDOW SYSTEM



BALCONY

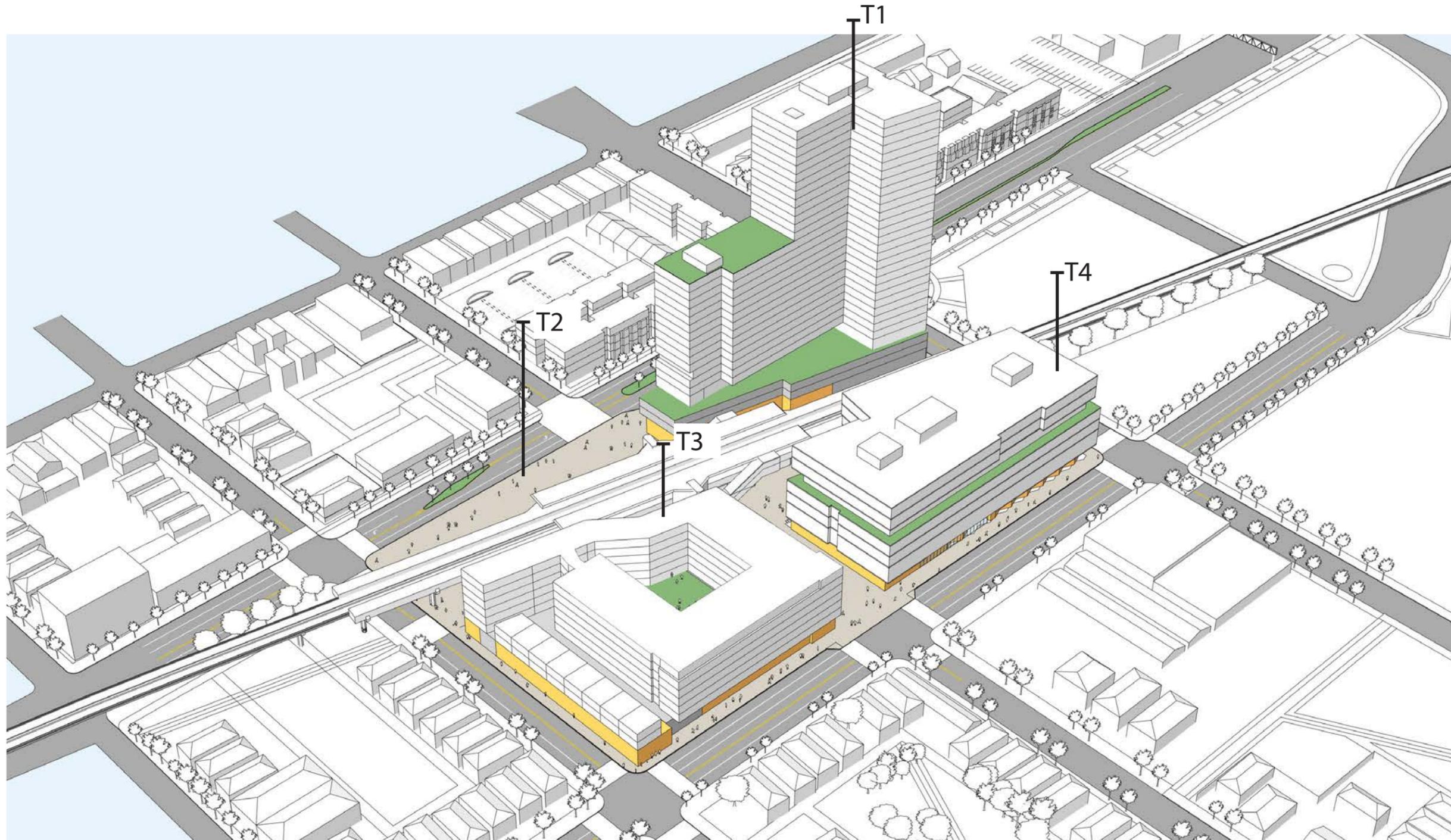


STOREFRONT SYSTEM



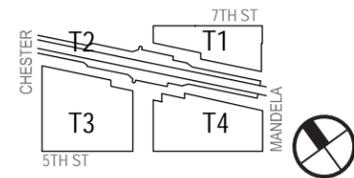
BASE





**MANDELA STATION
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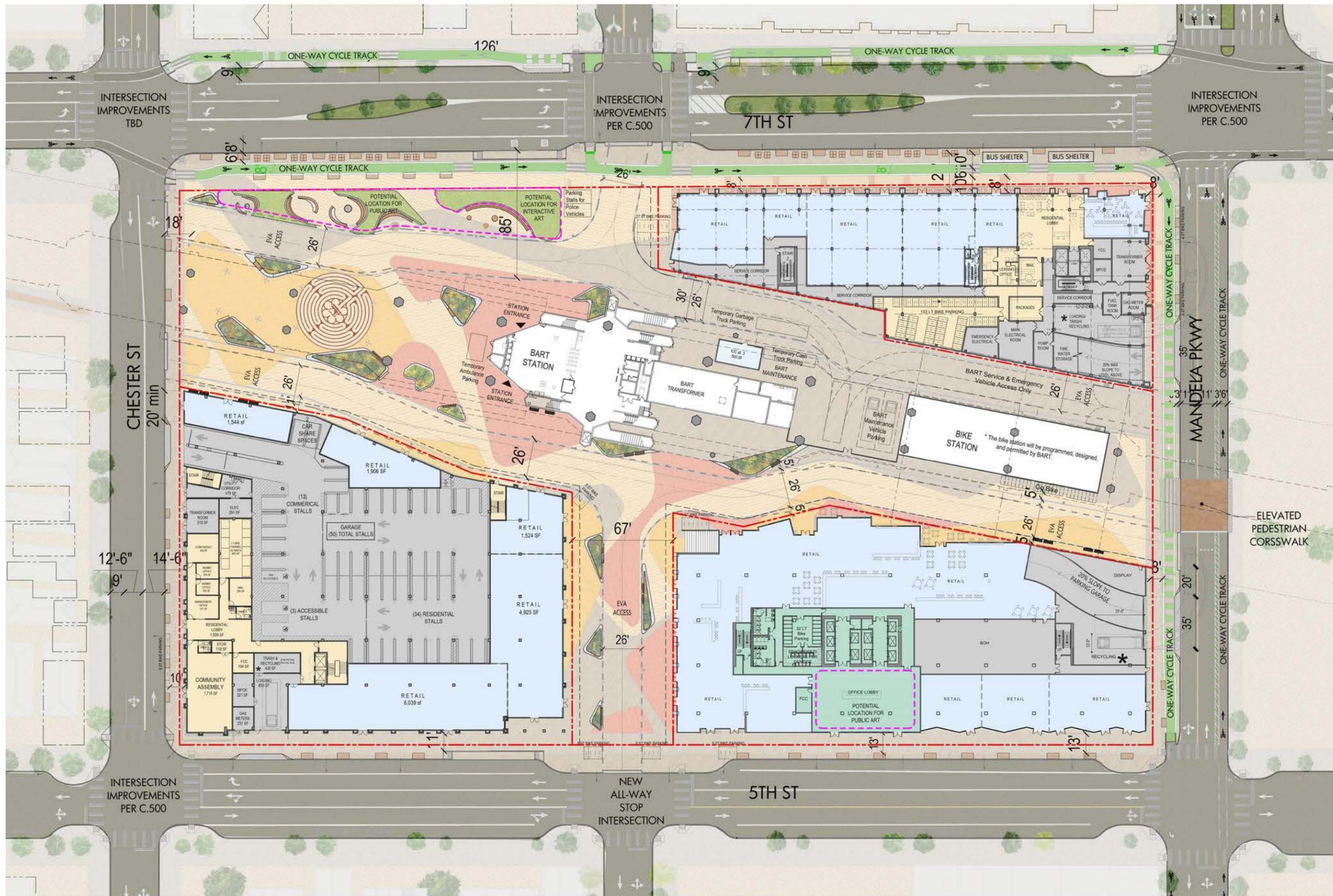


PROJ. # 168-153 WO BART
DATE: July 24, 2020

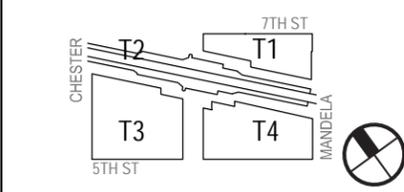
**MASSING
DIAGRAM**

SHEET:





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BART
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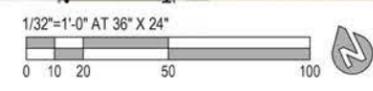
PROJ. # 168-153 WO BART
 DATE: July 24, 2020

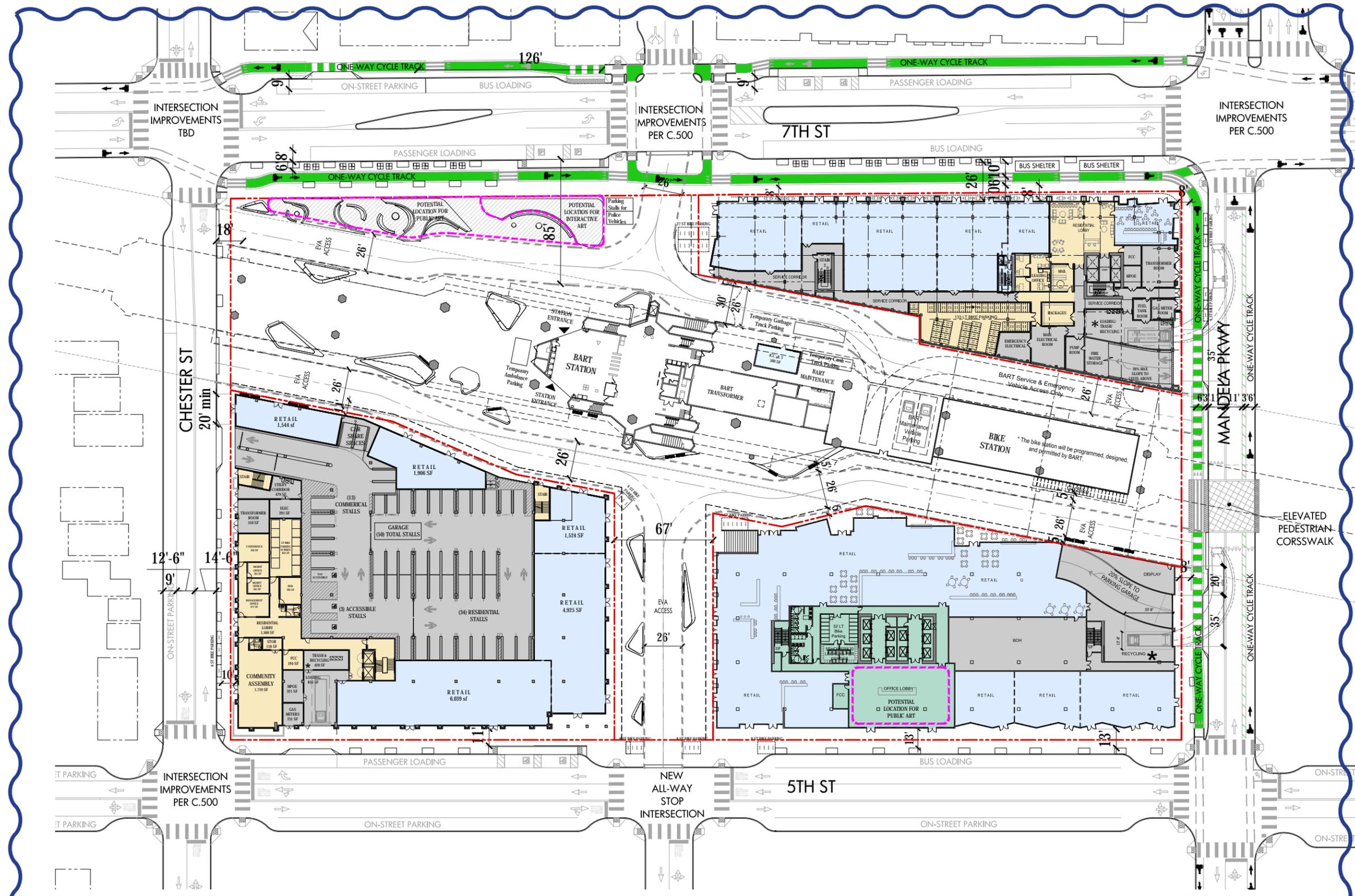
SITE PLAN

SHEET:

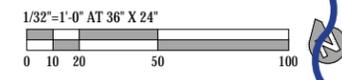
The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.

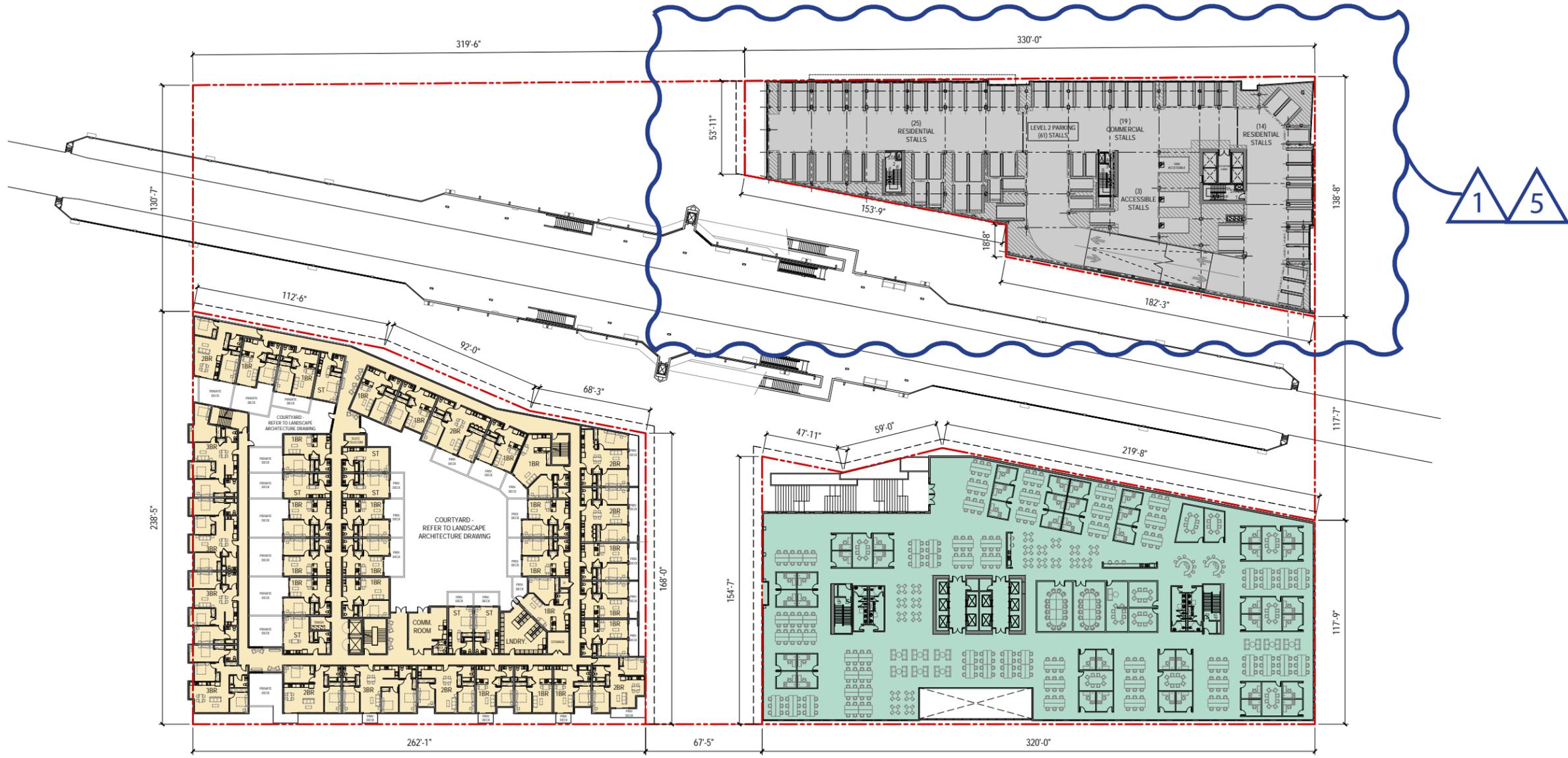
*The Hall of Fame Plaques will be removed from the right of way to be relocated back into the 7th Street right of way at an appropriate realigned location by the completion of T1.
 *The dancing lights will be removed from the right of way and there is no plan for their relocation as part of the FDP at this time.

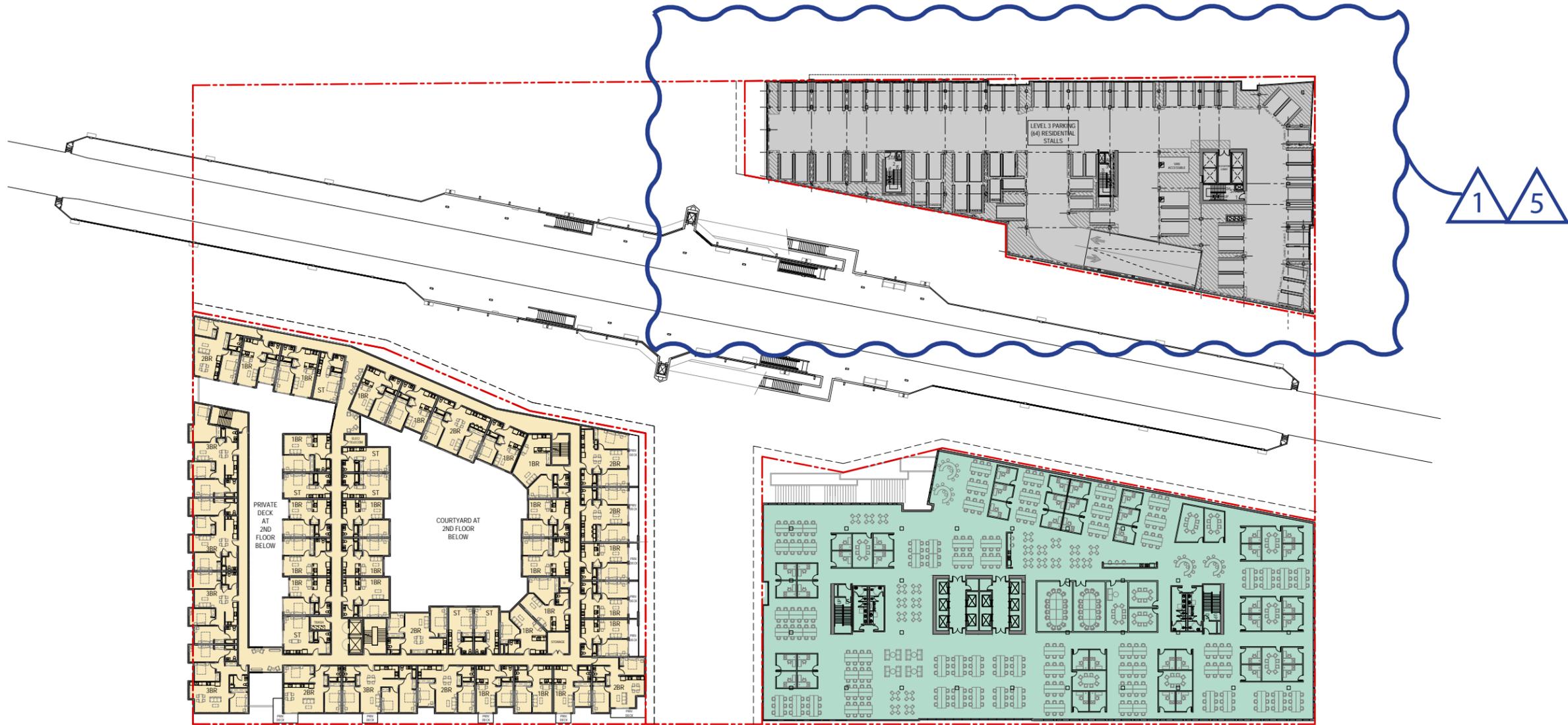


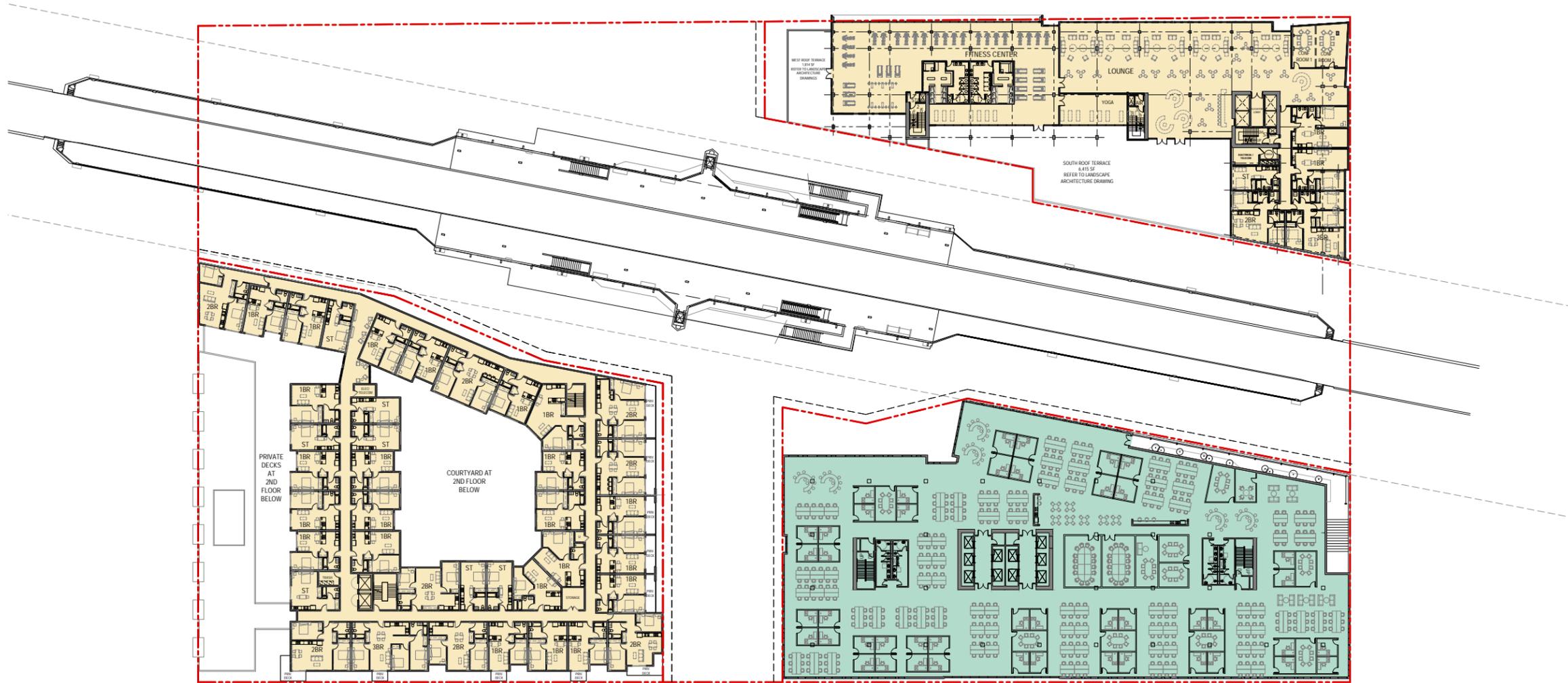


1. The recycling space on T1 site is 1,150 cubic feet minimum, on T3 site is 510 cubic feet minimum, and on T4 site is 647 cubic feet minimum. The total recycling space on site is 2,307 cubic feet minimum.









PDP PACKAGE

PROJECT

MANDELA STATION @ WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT

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JRDRV Architects, Inc.
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PO Box 70126
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510.266.4333 T
www.jrdrv.com

PROJECT TEAM

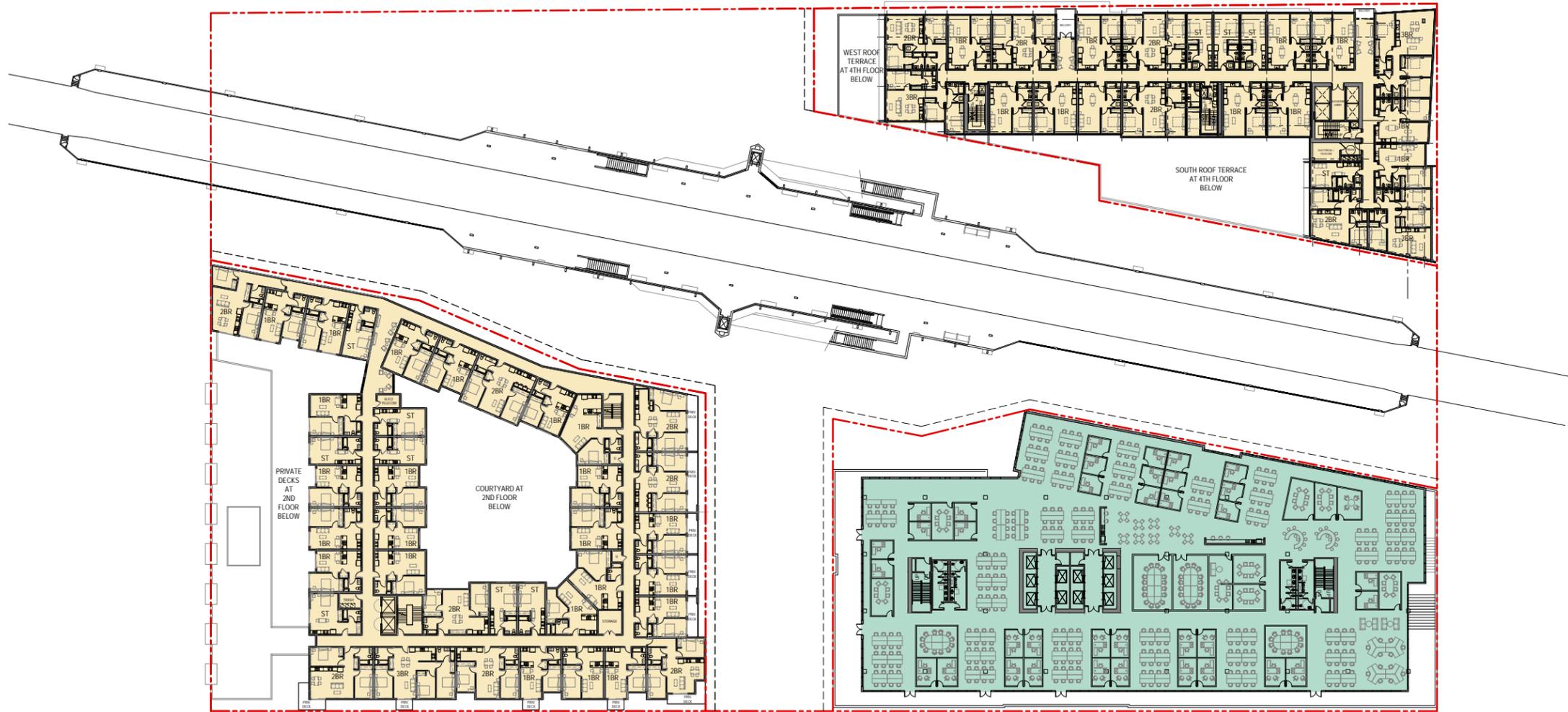
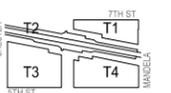
BKF ENGINEERS
1646 N CALIFORNIA
BLVD, SUITE 400
WALNUT CREEK
CA 94596
(925) 945-2200

PGA DESIGN
444 17th Street
Oakland
CA 94612
(510) 455-1284

STOK
845B Front Street
San Francisco
CA 94111
(415) 329-7100

STAMP

KEY



PROJ #: 168-153 WO BART
DATE: July 22, 20
SCALE: 1/32"=1'-0"
TITLE:

5TH
FLOOR
PLAN

SHEET:
A11.05

PDP PACKAGE

PROJECT

MANDELA STATION @ WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT

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The Cathedral Building
Broadway and Telegraph
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Oakland, CA 94612
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PROJECT TEAM

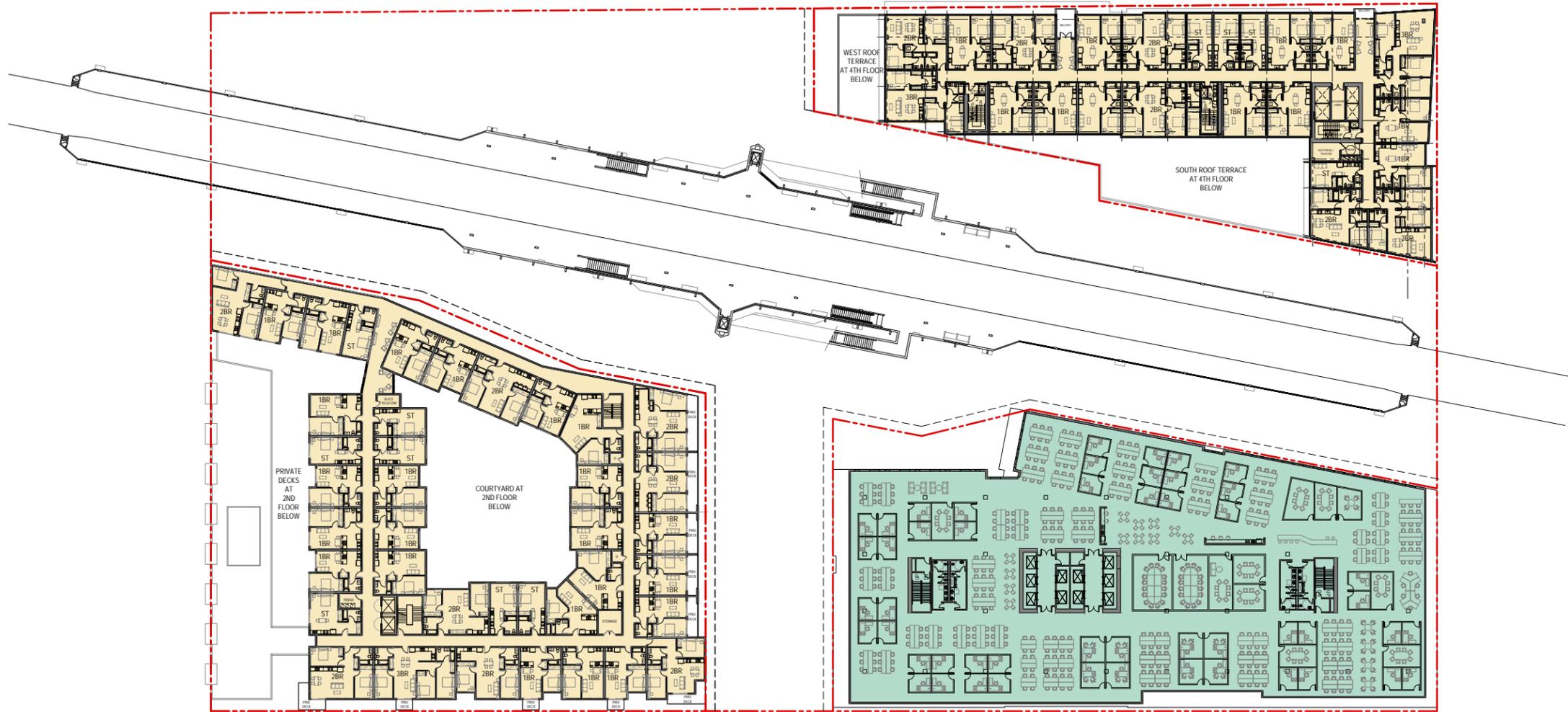
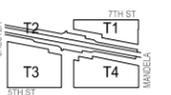
BKF ENGINEERS
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BLVD, SUITE 400
WALNUT CREEK
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STAMP

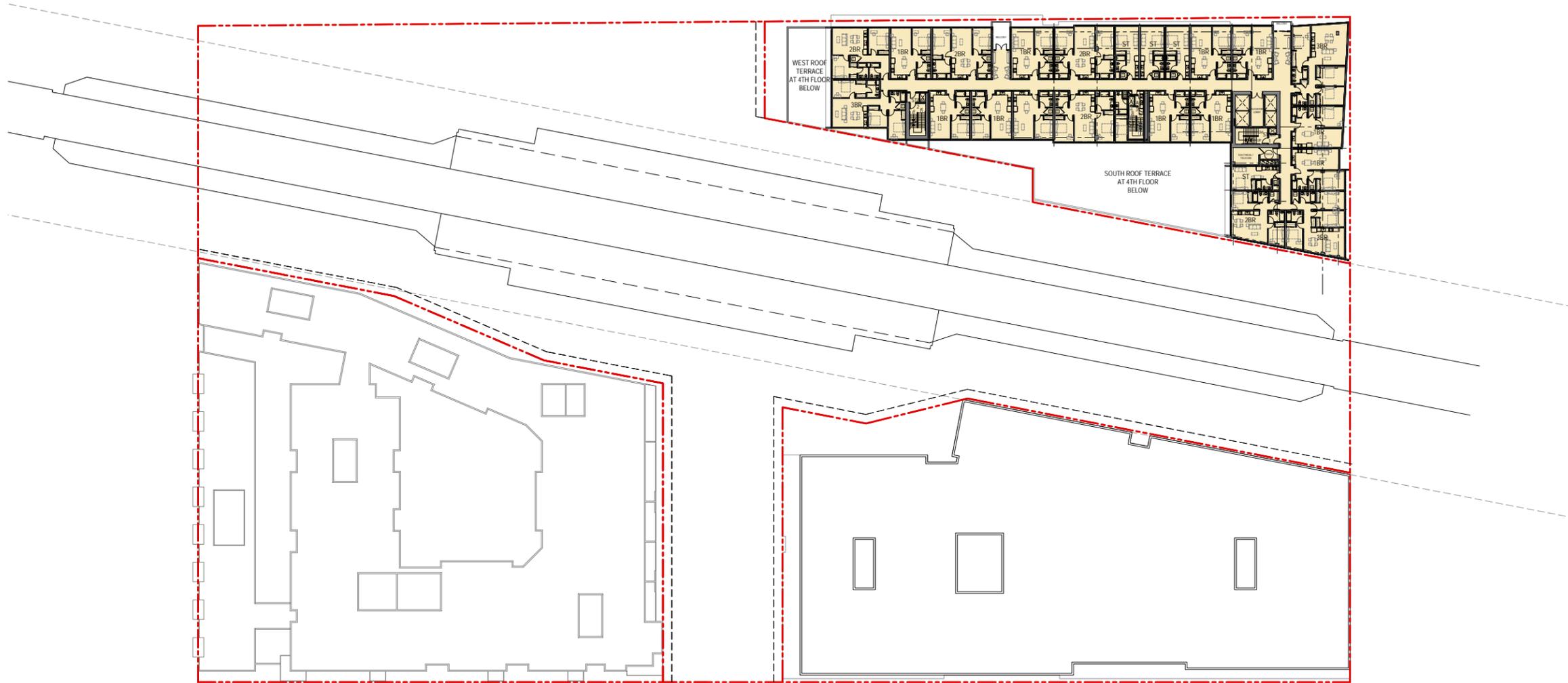
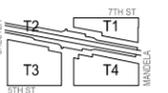
KEY



PROJ #: 168-153 WO BART
DATE: July 22, 20
SCALE: 1/32"=1'-0"
TITLE:

6TH-7TH
FLOOR
PLAN

SHEET: **A11.06**



PDP PACKAGE

PROJECT

MANDELA STATION @ WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT

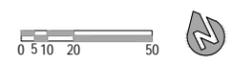
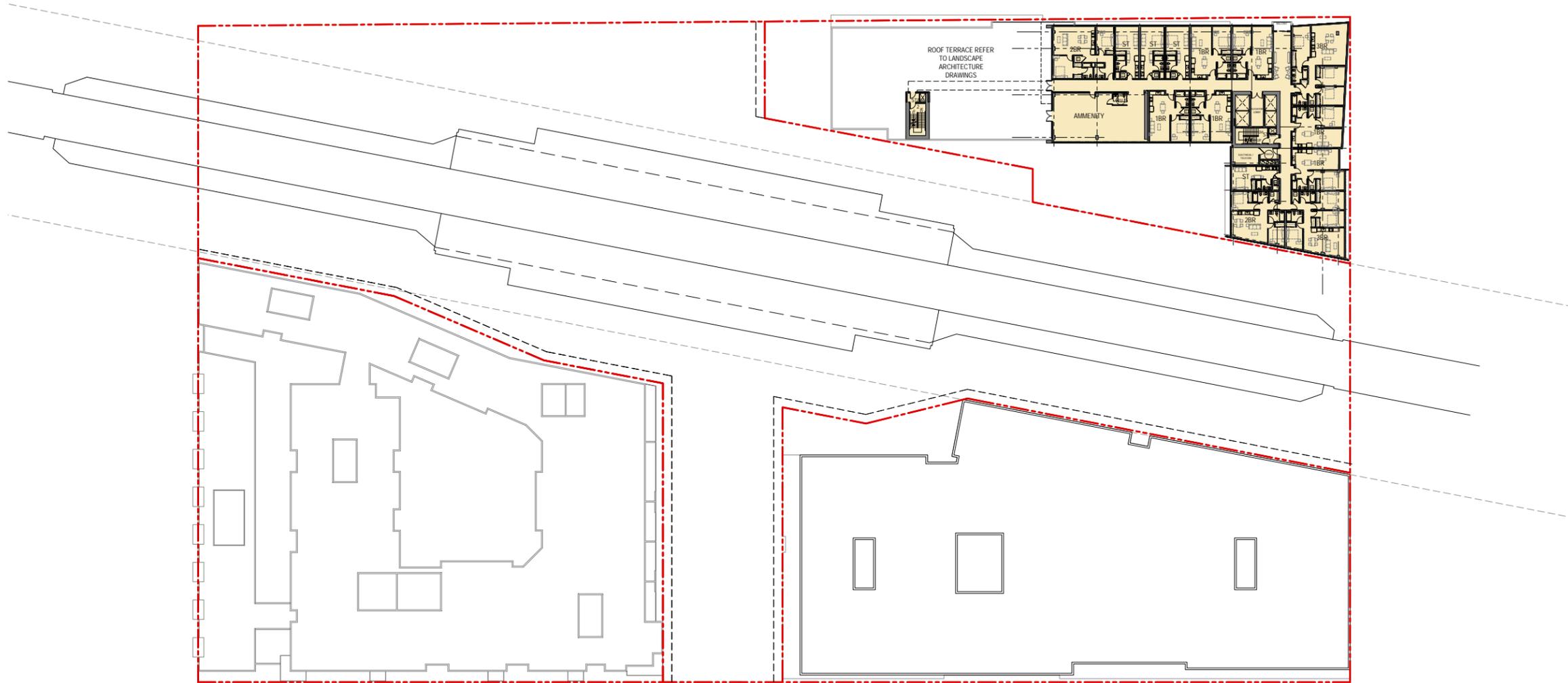
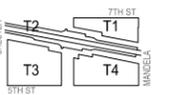
jrdv ARCHITECTS
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The Cathedral Building
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Oakland, CA 94612
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PROJECT TEAM

BEF ENGINEERS
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(925) 945-2200
BKT100
PGA DESIGN
444 17th Street
Oakland
CA 94612
(510) 455-1284
STOK
949B Front Street
San Francisco
CA 94111
(415) 329-7100
stök

STAMP

KEY



PROJ #: 168-153 WO BART
DATE: July 22, 20
SCALE: 1/32"=1'-0"
TITLE:

19TH
FLOOR
PLAN

SHEET: **A11.08**



PDP PACKAGE

PROJECT

MANDELA STATION @ WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM

INF ENGINEERS
1646 N CALIFORNIA
BLVD, SUITE 400
WALNUT CREEK
CA 94596
(925) 945-2200



PGA DESIGN
444 17th Street
Oakland
CA 94612
(510) 455-1284

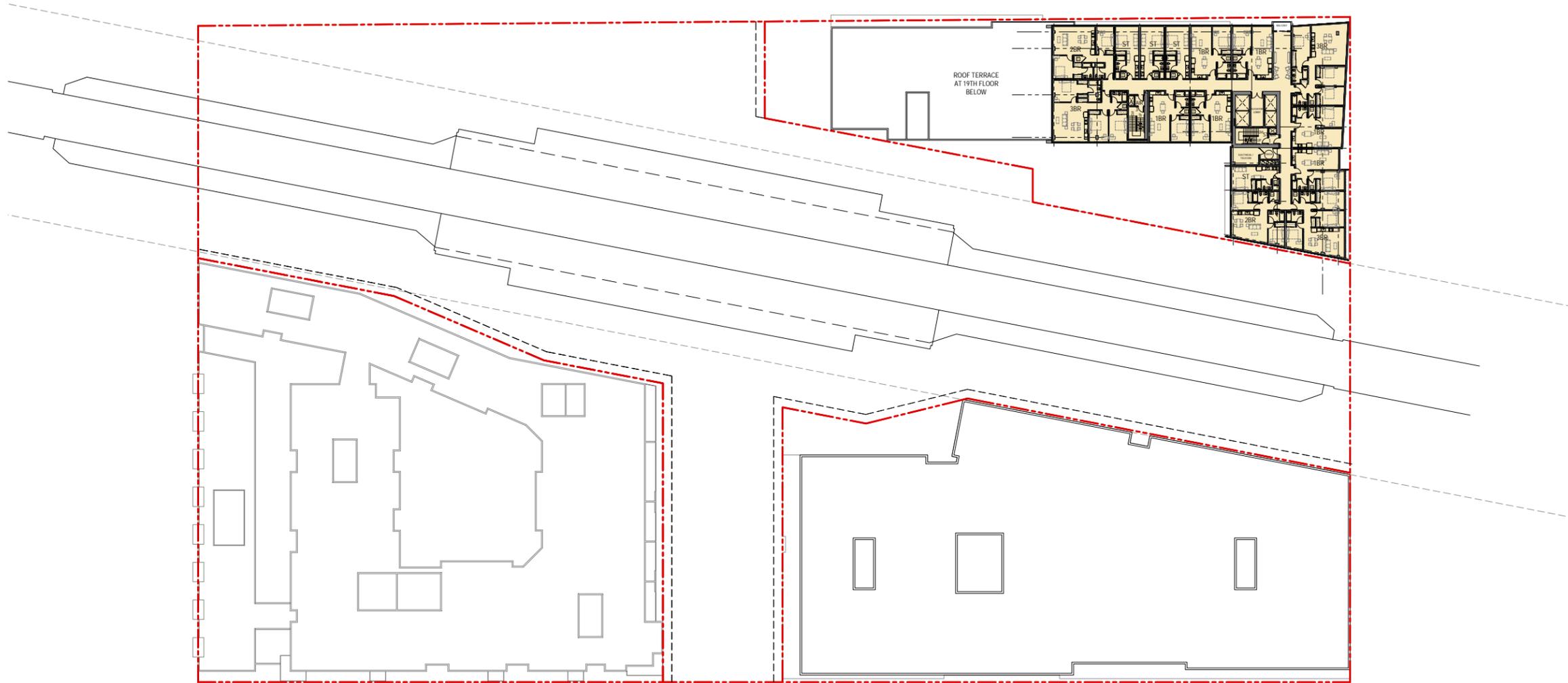
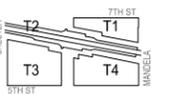


STOK
949B Front Street
San Francisco
CA 94111
(415) 329-7100



STAMP

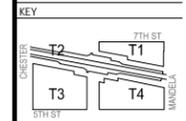
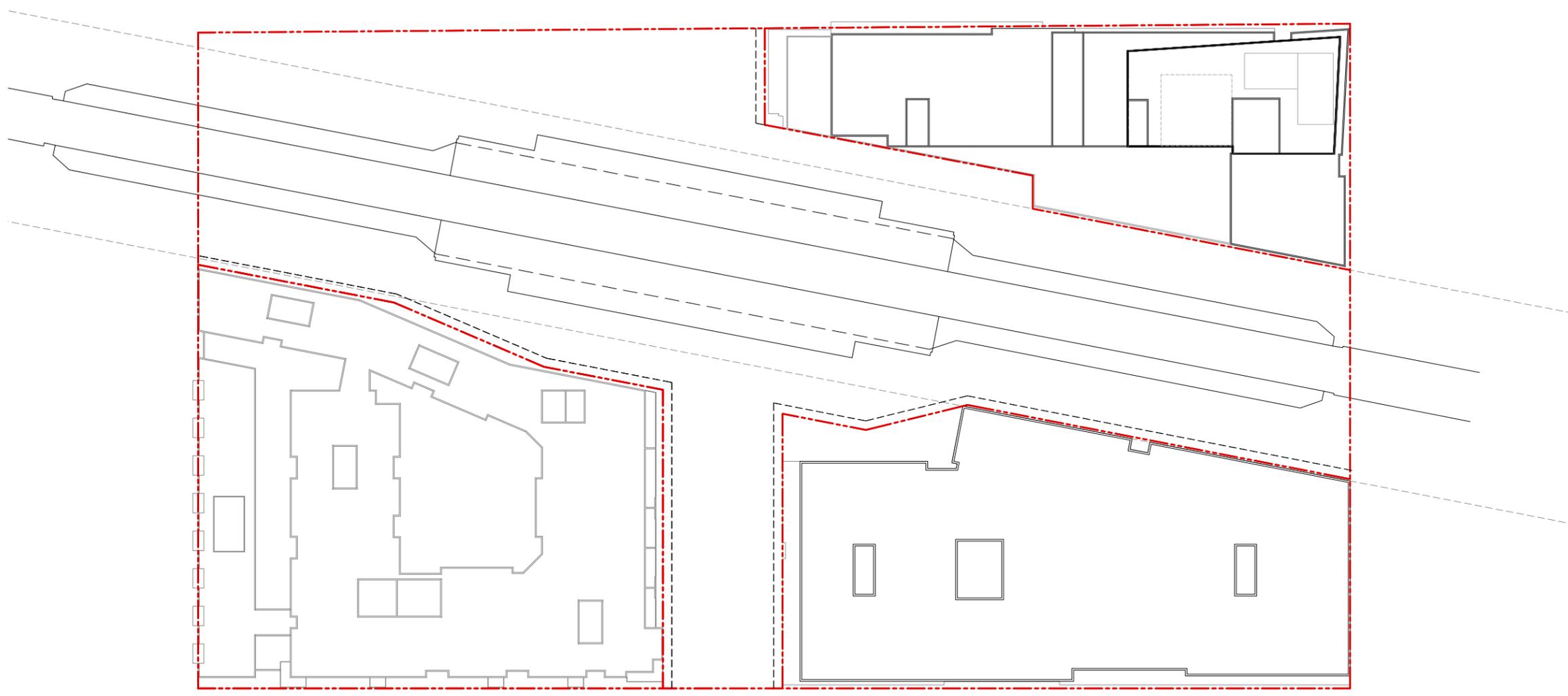
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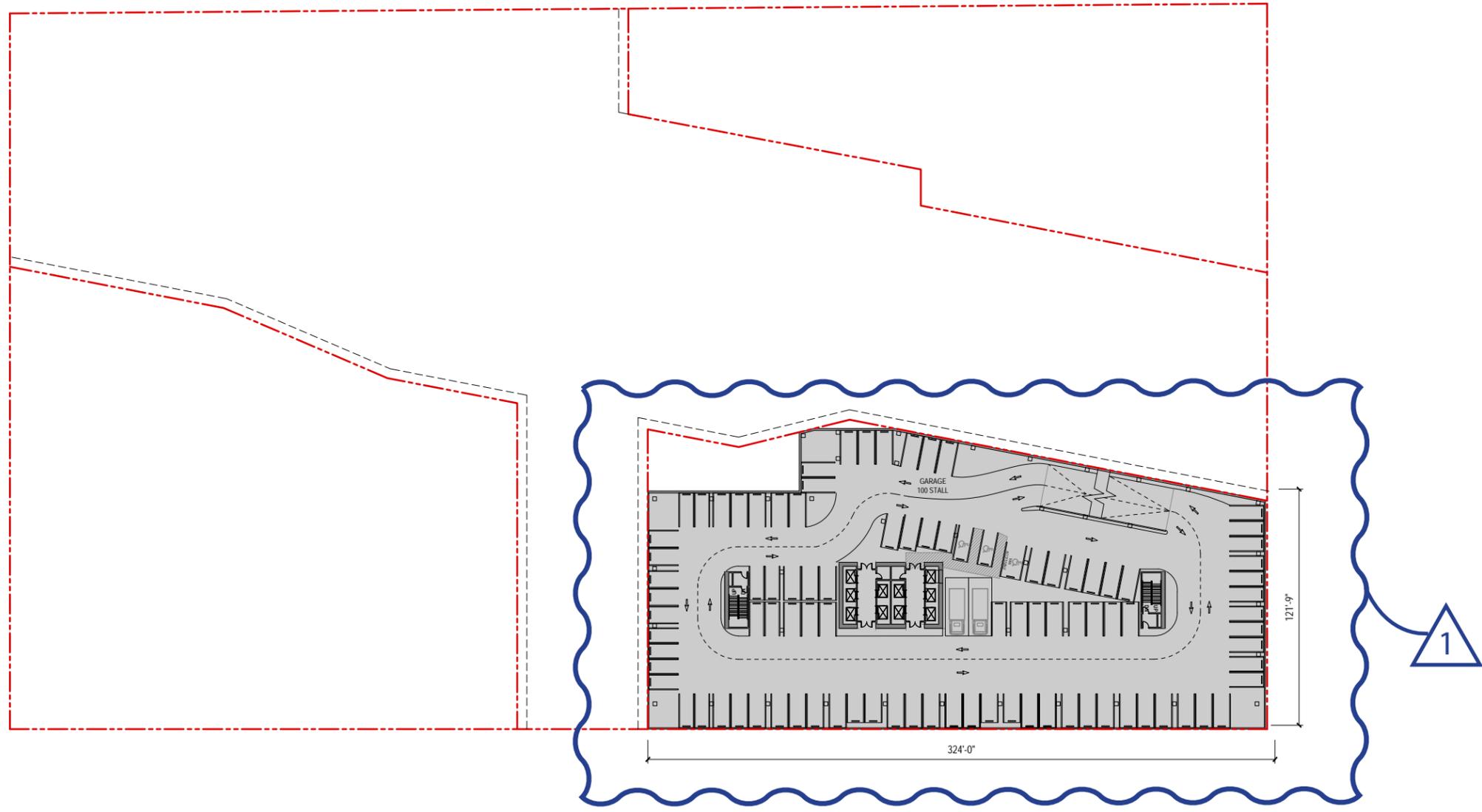


PROJ #: 168-153 WO BART
DATE: July 22, 20
SCALE: 1/32"=1'-0"
TITLE:

20TH-31TH
FLOOR
PLAN

SHEET: **A11.09**





Note:
The Applicant is requesting for a loading variance.



PDP PACKAGE

PROJECT

MANDELA STATION @ WEST OAKLAND BART
1451 7th St, Oakland, CA 94607

ARCHITECT



PROJECT TEAM

BNF ENGINEERS
1646 N. CALIFORNIA
BLVD, SUITE 400
WALNUT CREEK
CA 94596
(925) 945-2200



PGA DESIGN
444 17th Street
Oakland
CA 94612
(510) 455-1284

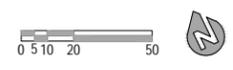
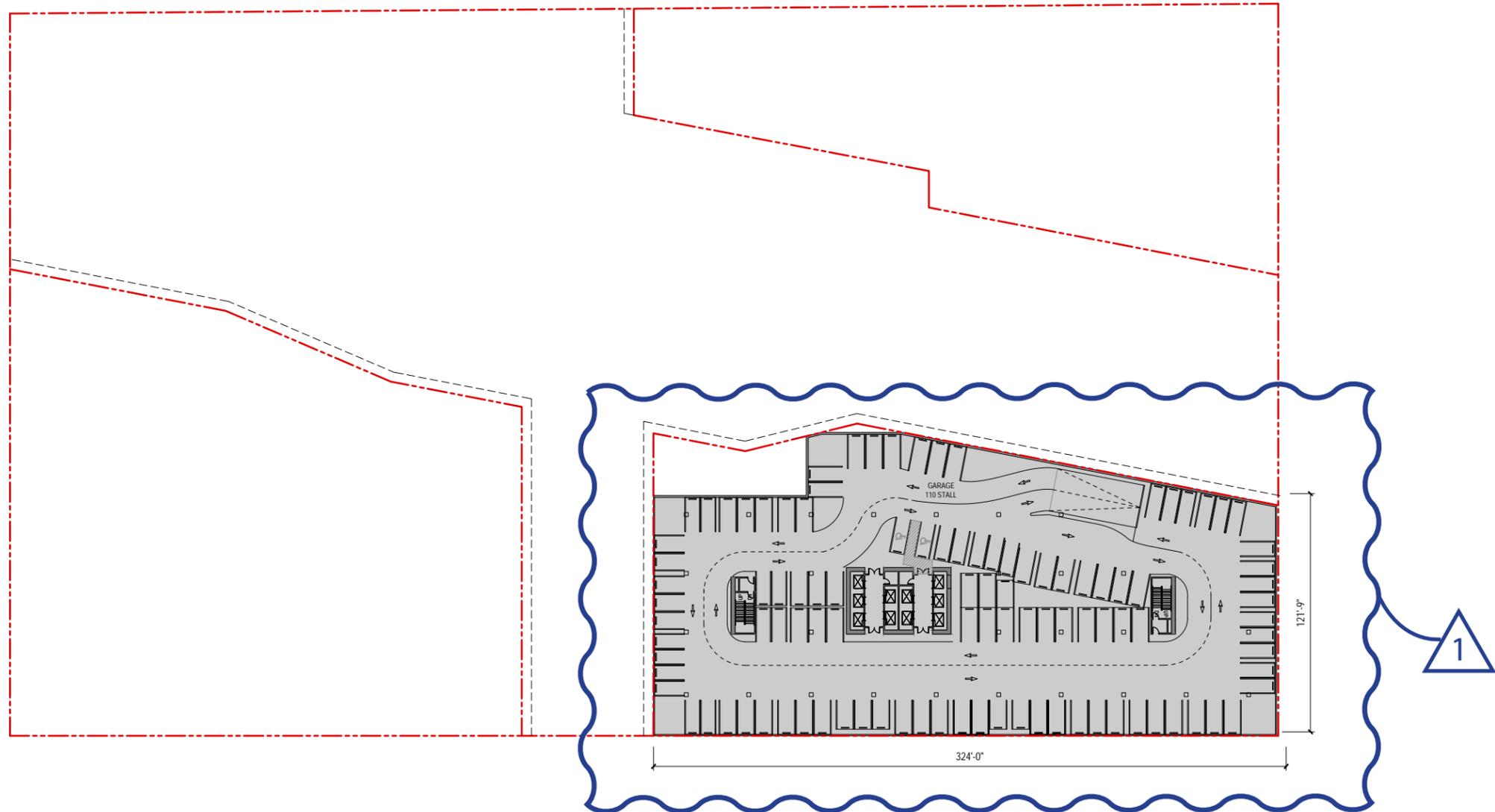
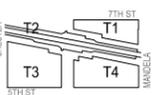


STOK
949B Front Street
San Francisco
CA 94111
(415) 329-7100



STAMP

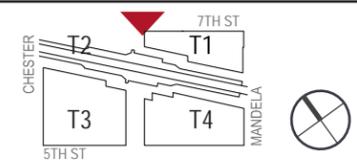
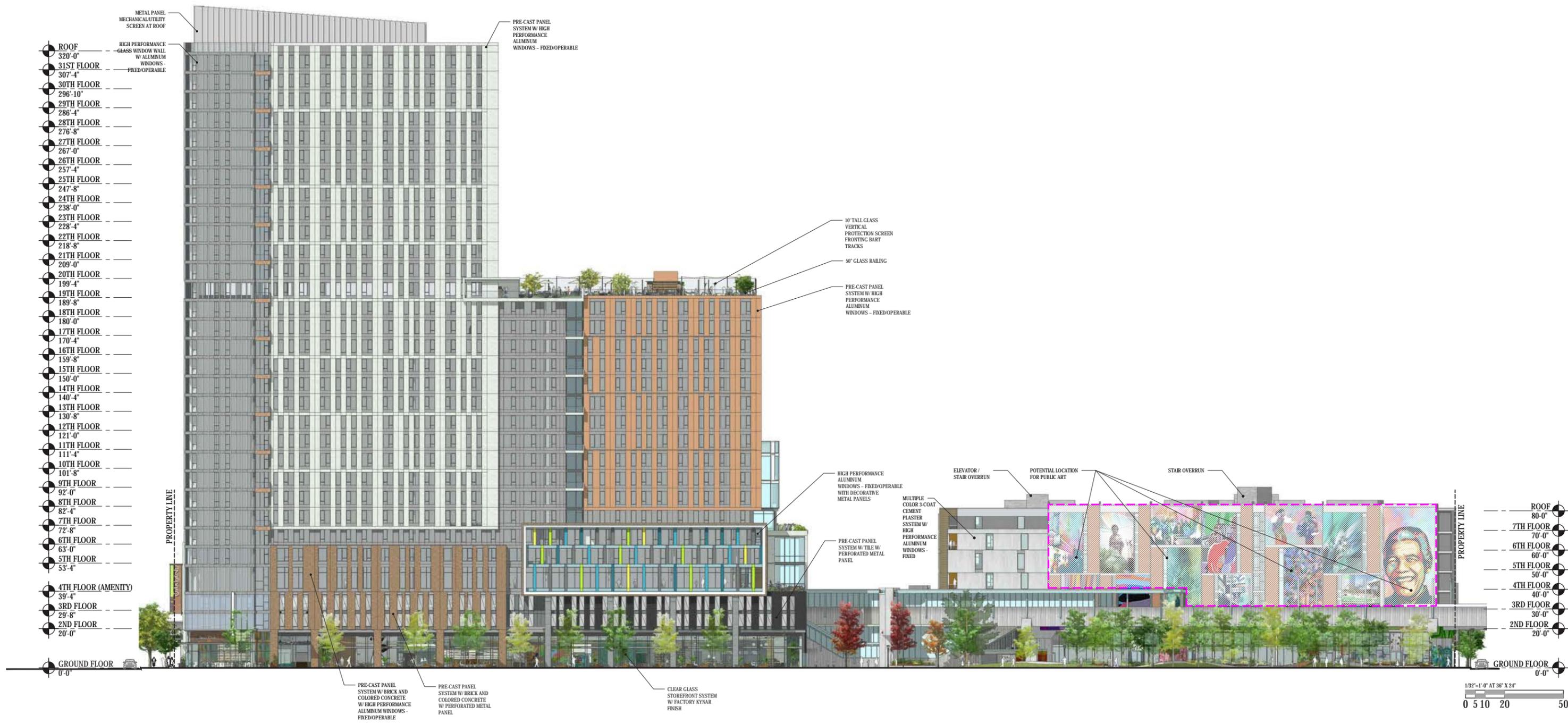
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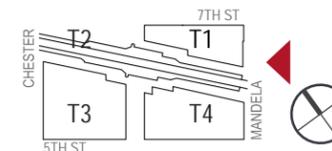


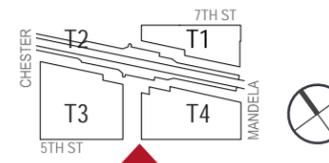
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DATE: July 22, 20
SCALE: 1/32"=1'-0"
TITLE:

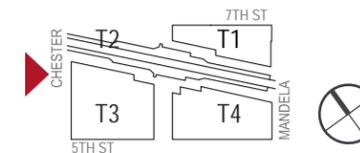
B2
FLOOR
PLAN

SHEET: A11.12

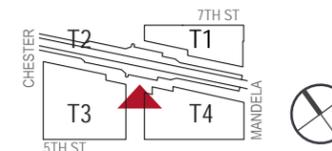
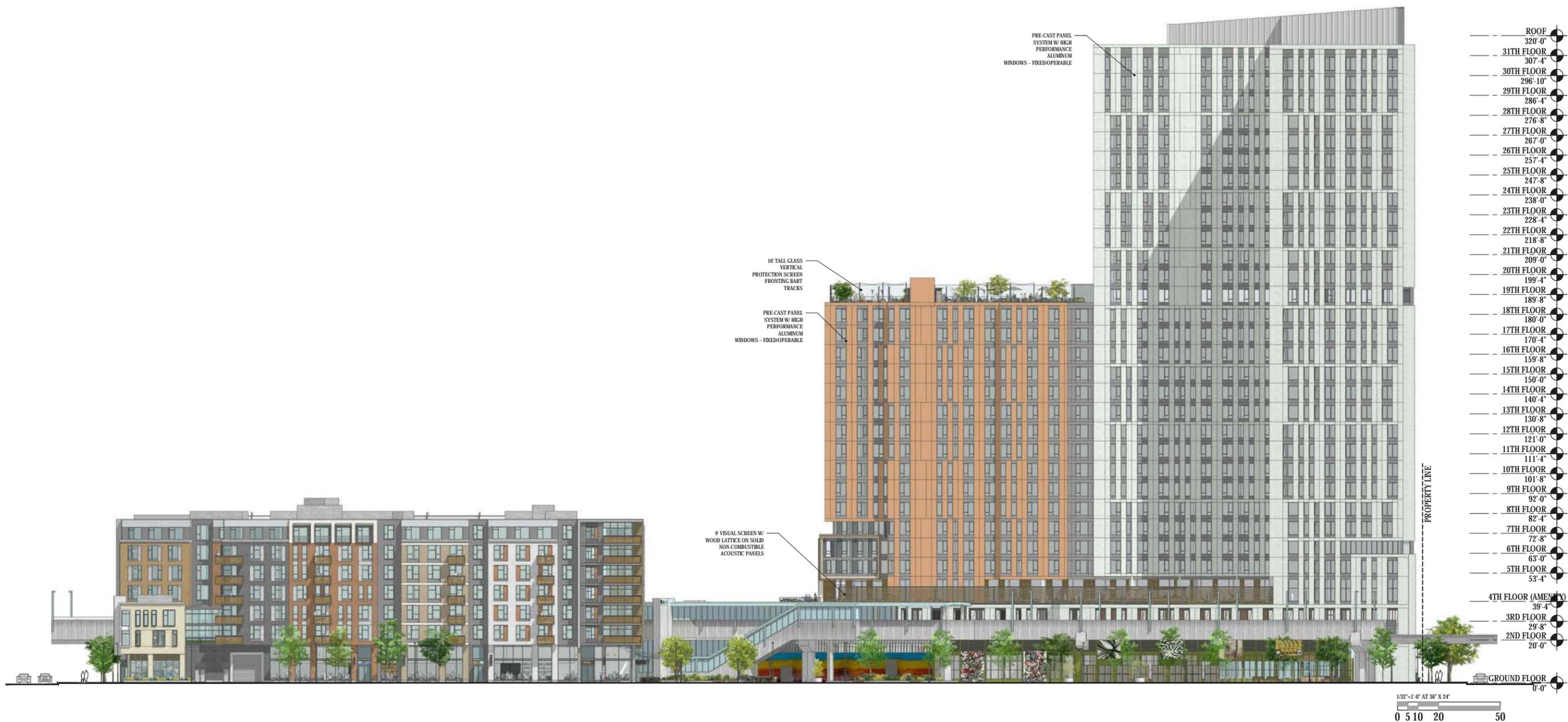












1 5

