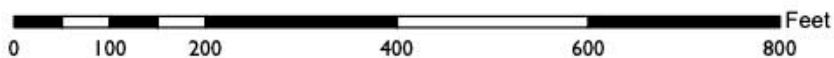


**Case File Number: PLN20108-PUDF02**

**July 20, 2022**

<b>Location:</b>	<b>Lake Merritt Bart (51-9th Street &amp; 107-8TH ST)</b>
<b>Accessor's Parcel Number:</b>	001 016900100 & 001 017100200
<b>Proposal:</b>	The proposed project is a Final Development Permit (FDP) application for Lake Merritt Bart Phase One, Horizontal improvements, part of the previously approved Planned Unit Development (PUD) for Lake Merritt Bart, located at <b>51-9<sup>th</sup> Street</b> (APN 001 016900100). The Lake Merritt Bart PUD was approved on May 19, 2021. The PUD's conditions of approval specify two Phases. Phase One of the PUD includes three FDPs: the FDP for horizontal improvements, the FDP for Building A, and the FDP for Building B. This is the first FDP application for the development at Lake Merritt Bart. The first FDP includes all horizontal improvements on two City Blocks. Block One is the City block bounded by 9 <sup>th</sup> Street, Fallon Street, 8 <sup>th</sup> Street, and Oak Street; inclusive of the adjacent public right-of-way. Block Two is the City block bounded by 8 <sup>th</sup> Street, Oak Street, 7 <sup>th</sup> Street, and Madison Street; inclusive of the adjacent public right-of-way.
<b>Applicant:</b>	East Bay Asian Local Development Corporation (EBALDC), James Perez
<b>Phone Number:</b>	(510) 512-2444
<b>Owner:</b>	San Francisco Bay Area Rapid Transit District (BART)
<b>Case File Number:</b>	PLN20108-PUDF02
<b>Planning Permits Required:</b>	FDP, compliance with CEQA.
<b>General Plan:</b>	Central Business District
<b>Zoning:</b>	Lake Merritt Station Area District Pedestrian - 2 Commercial Zone (D-LM-2)
<b>Environmental Determination:</b>	This FDP relies on the Lake Merritt Station Area Plan Environmental Impact Report (EIR), certified on July 28, 2014, and is entirely within the scope of, and was contemplated in, the CEQA Analysis for the Lake Merritt Bart PUD. The Lake Merritt Station Area Plan EIR can be reviewed at: <a href="https://cao-94612.s3.amazonaws.com/documents/Final-Environmental-Impact-Report-FEIR-July-28-2014.pdf">https://cao-94612.s3.amazonaws.com/documents/Final-Environmental-Impact-Report-FEIR-July-28-2014.pdf</a> The Lake Merritt Bart PUD CEQA Analysis can be reviewed at: <a href="https://cao-94612.s3.amazonaws.com/documents/LMBART-CEQA-Checklist-Addendum_Final_signed-1.pdf">https://cao-94612.s3.amazonaws.com/documents/LMBART-CEQA-Checklist-Addendum_Final_signed-1.pdf</a> . Since it was contemplated in the previous Lake Merritt Bart PUD CEQA Analysis, the project meets all requirements under CEQA Guidelines Sections 15164, 15183, and 15168. As a result, no supplemental environmental review is required in accordance with Public Resources Code Section 21083.3 and Section 21166, and CEQA Guidelines Sections 15162 through 15164, as well as 15168.
<b>Historic Status:</b>	n/a
<b>City Council District:</b>	2 – Nikki Fortunato Bas
<b>Status:</b>	Under Review
<b>Staff Recommendation:</b>	Approval of FDP subject to project Conditions of Approval.
<b>Finality of Decision:</b>	Appealable to City Council within ten (10) days
<b>For further information:</b>	Contact case planner <b>Karen August</b> by e-mail at <a href="mailto:kaugust@oaklandca.gov">kaugust@oaklandca.gov</a> .

# CITY OF OAKLAND PLANNING COMMISSION



Case File: PLN20108-PUDF02  
Applicant: East Bay Asian Local Development Corporation (EBALDC)  
Address: 51 9th Street, 107 8th Street  
Zone: D-LM-2  
Height Area: LM-275

## SUMMARY

The proposed project is a Final Development Permit (FDP) application for Lake Merritt BART Horizontal Improvements, part of the previously approved Planned Unit Development (PUD) for Lake Merritt BART, located at **51-9<sup>th</sup> Street** (APN 001 016900100). The Lake Merritt BART PUD was approved on May 19, 2021 (2021-LMB-PUD). The 2021-LMB-PUD's conditions of approval specifies two phases. Phase One of the 2021-LMB-PUD includes three FDPs: the FDP for all horizontal improvements (both Block One & Block Two), the FDP for Building A, and the FDP for Building B. For context, Phase Two of the PUD includes two FDPs: the FDP for Building C and the FDP for Building D.

This FDP is for the horizontal improvements delineated in the 2021-LMB-PUD. The proposed design includes schematic design for Block One and conceptual design for Block Two (see the Project Description section on Page 4 for more information). The horizontal improvements for Block One are divided into sub-phases: Sub-Phase 1.1. and Sub-Phase 1.2.

## PROPERTY DESCRIPTION

### *Project Site and Surrounding Area*

Block 1 is bounded by 9th Street to the north, Fallon St to the east, 8th Street to the south, and Oak Street to west. The block currently contains BART parking, two small BART station head houses, and a small BART plaza. The block is surrounded by Laney College to the east, the BART plaza to the west, small scale commercial buildings to the south, and a pair of residential rowhouses to the north. Block Two is bounded by 8<sup>th</sup> Street, Oak Street, 7<sup>th</sup> Street, and Madison Street. Other nearby neighbors include Chinatown, the Oakland Museum of California, Alameda County Courthouse, Lake Merritt, and Jack London Square. The site is on the eastern boundary of the Central Business District (General Plan and Estuary Policy Plan Amendments; City of Oakland, Department of Planning and Building July 3, 2014).

## PROJECT BACKGROUND

### *Project History*

Approved by the City Planning Commission on May 19, 2021, the 2021-LMB-PUD encompasses both the 60,260-square foot "Block 1" located at 51 9th Street and the 59,978-square foot "Block 2" at 107 8th Street. As noted above, Block 1 is bounded by 9th Street, Fallon Street, 8th Street, and Oak Street. Currently this block serves the public transportation hub as a surface parking lot owned and operated by BART, and the BART tunnel runs underneath the centre of the site from Oak to Fallon. Additionally, this block has two station headhouse entrances on Oak Street, at the corners of 8th Street and 9th Street, which are not part of the 2021-LMB-PUD. The Project's Block 2 is bounded by 8th Street, Oak Street, 7th Street, and Madison Street. Block 2 is currently occupied by an existing BART-owned office building and its private parking lot.

In Spring of 2022, the applicant team, applied for two separate FDPs as part of Phase One of the 2021-LMB-PUD. This FDP is for the horizontal improvements associated with the project. The companion FDP application is for Building B, an affordable housing development (subject to consideration under a separate staff report and recommendation).

In April 2022, the related Tree Permit for the 2021-LMB-PUD was extended to May 7, 2023.

In May 2022, both FDPs for the Horizontal Improvements and Building B were presented at the City's May 25<sup>th</sup> Design Review Committee hearing. Detailed comments were provided to the applicant regarding the clarity of the design sheets, clarity of the proposed phasing, and scope for the required horizontal improvements.

**PROJECT DESCRIPTION**

This FDP (PLN20108-PUDF02) includes all horizontal improvements included in the 2021-LMB-PUD, including both Block One and Block Two.

Block One is the city block bounded by 9<sup>th</sup> Street, Fallon Street, 8<sup>th</sup> Street, and Oak Street; inclusive of the adjacent public right-of-way. Block one consists of five areas, the footprint of Building A or Lot 1, the footprint of Building B or Lot 2, the “Paseo” in-between Building A and Building B or Lot 3, the “West Plaza” between Oak Street and Buildings A, B, and the Paseo or *Remainder Parcel*, and the perimeter public right-of-way. (For further details see sheets L1.0 & C4.1)

Block Two is the city block bounded by 8<sup>th</sup> Street, Oak Street, 7<sup>th</sup> Street, and Madison Street; inclusive of the adjacent public right-of-way. Block Two consist of three areas, the footprint of Building C or Lot 1, the footprint of Building D or Lot 2, and the perimeter public right-of-way. (For further details see sheets L1.0 & C4.2. Please note full set of designs references are provided as **Attachment C, Proposed Plans, LMB-FDP-Horizontal\_dated 22-06-21.**

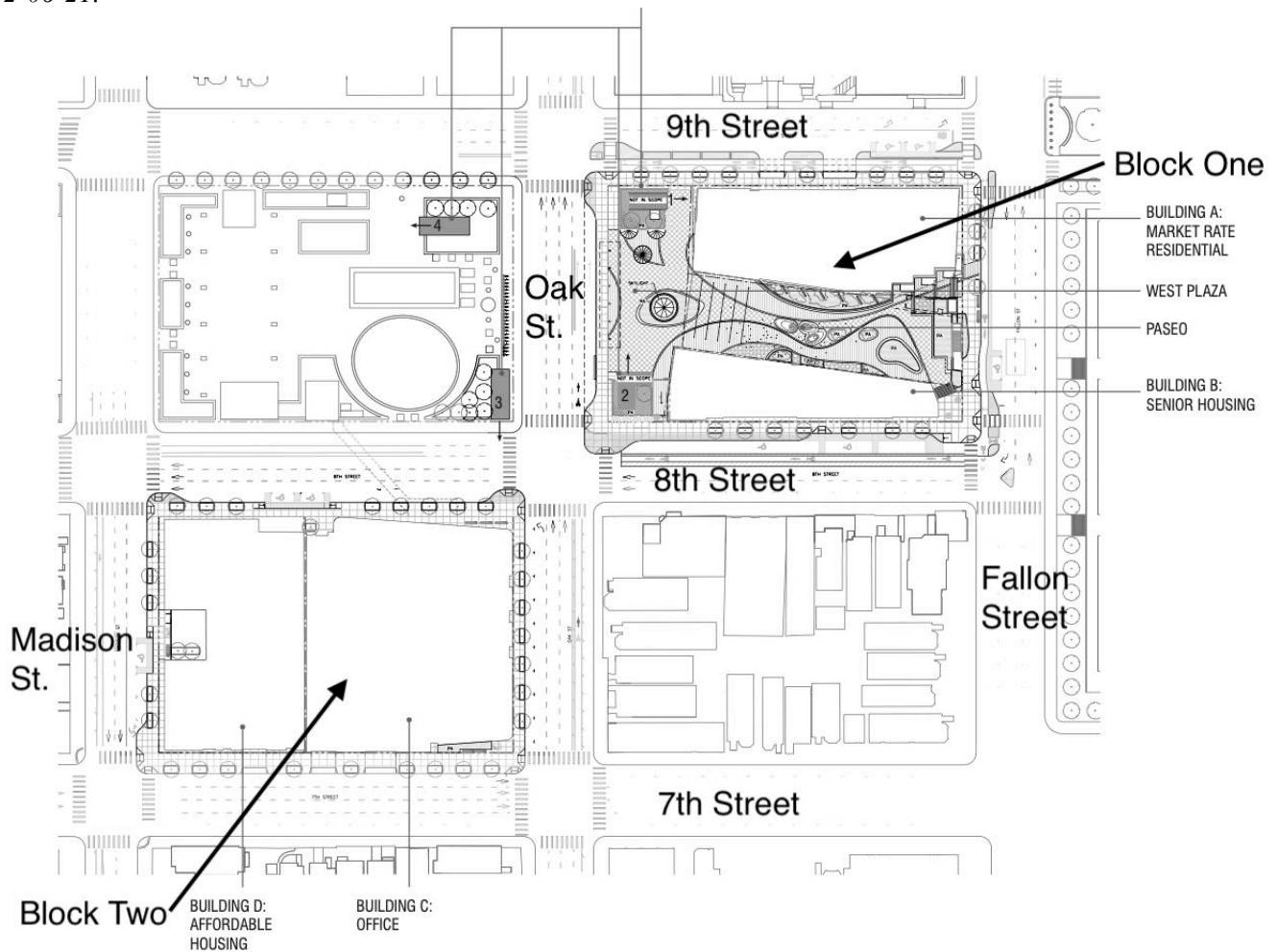


Image of Block One & Two, and sub-sections. Design Sheet L1.0 ‘Area Plan’; Attachment C, Proposed Plans, LMB-FDP-Horizontal\_dated 22-06-21

The proposed project is for Block One and is divided into two sub-phases, Sub-Phase 1.1 and Sub-Phase 1.2. The current FDP proposes Sub-Phase 1.1 horizontal improvement to be constructed concurrently with the affordable senior housing Building B and for Phase 1.2 to be bonded to ensure delivery of improvements to coincide with market-rate residential Building A. Block Two is divided into two sub-phases, Sub-Phase 1.3 and Sub-Phase 1.4.

The scope of Sub-Phase 1.1 includes: the horizontal improvements along Fallon Street, the majority of 8th street, and the majority of the public Paseo. The improvements to the Paseo comply with the majority of the 2021-LMB-PUD's design guidelines for the Paseo. A small area of the Paseo includes temporary hard landscaping and temporary ADA access ramp adjacent to a street level parking lot and the future location for Building A.

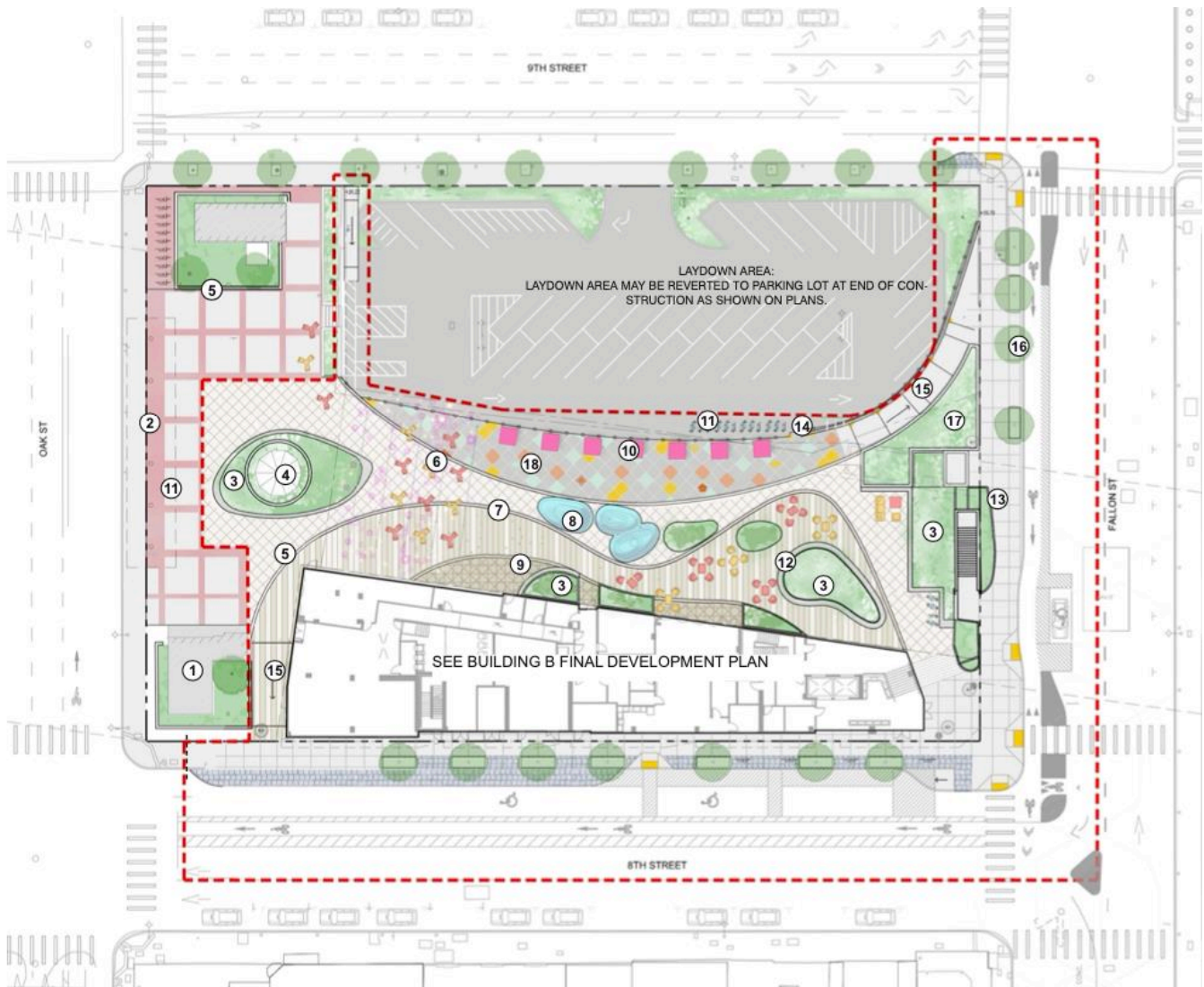


Image of the proposed build out of Sub-Phase 1.1. Design Sheet L4.01B 'Illustrative Materials; Phase 1.1'; Attachment C, Proposed Plans, LMB-FDP-Horizontal dated 22-06-21

Sub-Phase 1.2 is projected to be constructed at the same time as construction of Building A and will deliver the permanent improvements to replace the temporary hard landscaping and temporary ADA ramp adjacent to Building A. Sub-Phase 1.2 will also complete the hard landscaping improvements to the Oak Street side of the Paseo (i.e., the “West Plaza” adjacent to Oak Street), complete public improvements to 8th Street, construct all improvements on Oak Street, and the majority of improvements to 9th street.



Image of the proposed build out of Sub-Phase 1.2. Design Sheet L4.01 'Illustrative Materials; Phase 1.2'; Attachment C, Proposed Plans, LMB-FDP-Horizontal\_dated 22-06-21

Sub-Phase 1.3 is projected to be constructed at the same time as construction of Building C and will deliver all horizontal improvements on 8<sup>th</sup> Street, Oak Street, and 7<sup>th</sup> Street.

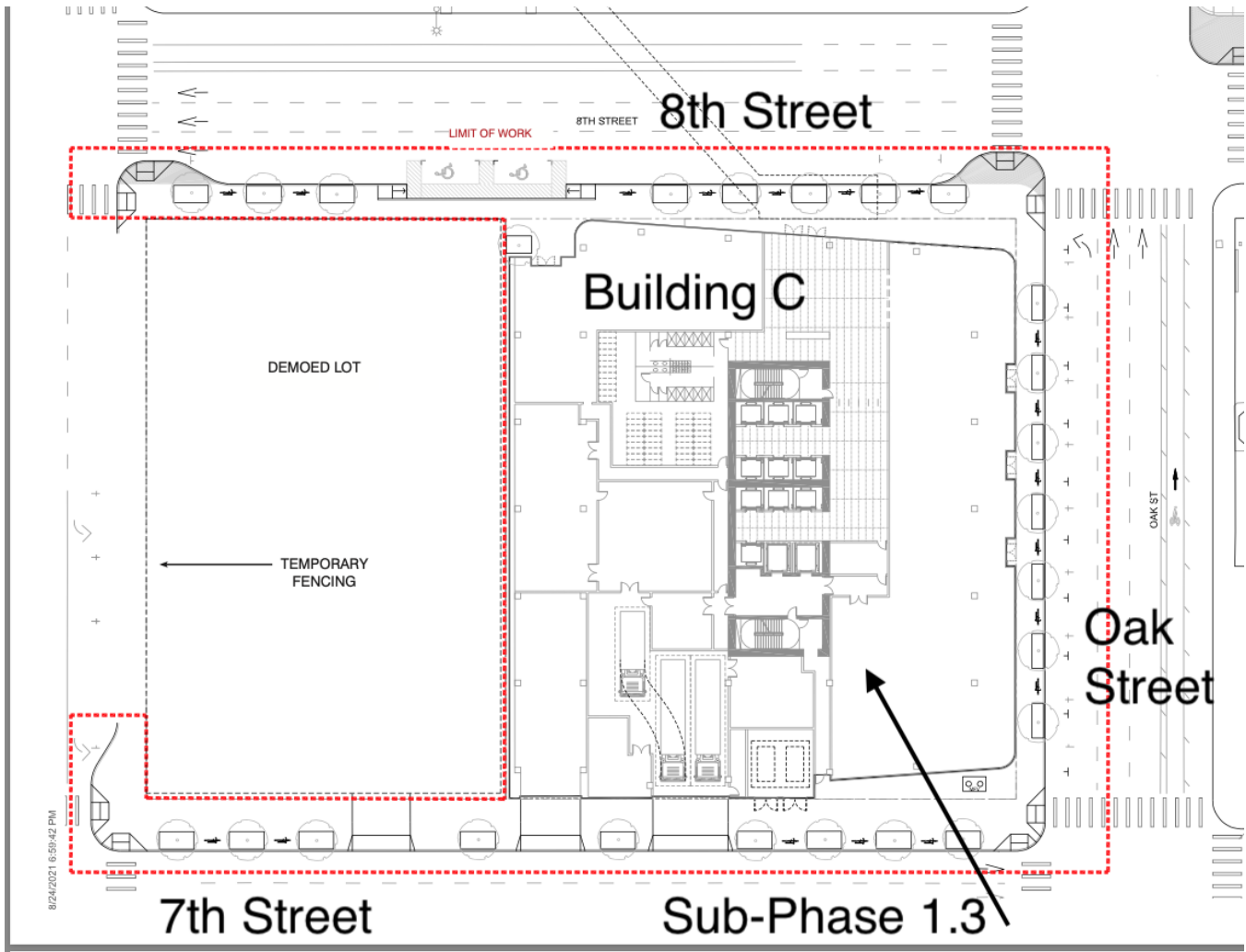


Image of the proposed build out of Sub-Phase 1.3 (outlined in red dash). Design Sheet L3.3; Attachment C, Proposed Plans, LMB-FDP-Horizontal\_dated 22-06-21

Sub-Phase 1.4 is projected to be constructed at the same time as construction of Building D and will deliver all horizontal improvements on Madison Street.

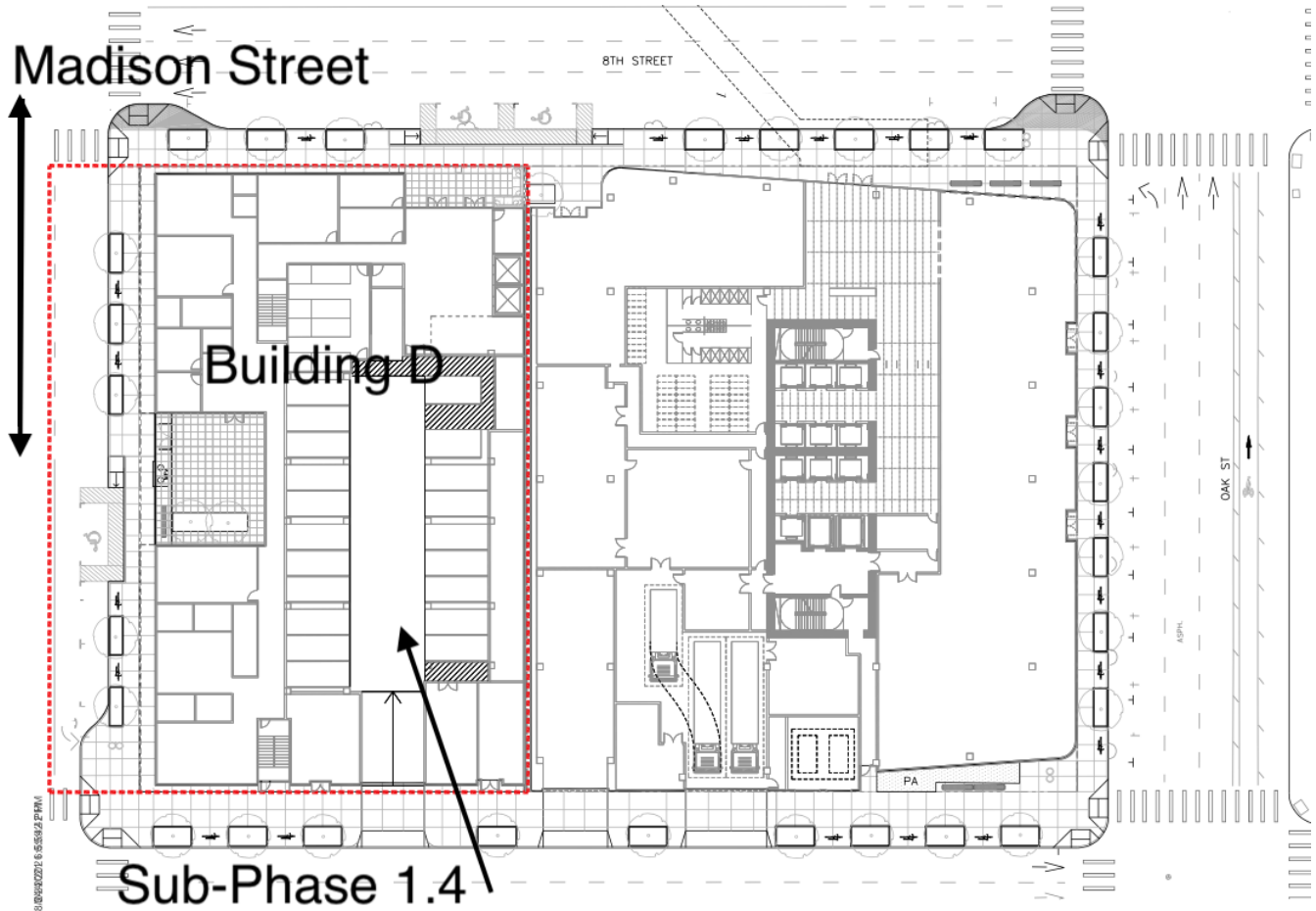


Image of the proposed build out of Sub-Phase 1.4 (outlined in red dash). Design Sheet L3.4; Attachment C, Proposed Plans, LMB-FDP-Horizontal\_dated 22-06-21

The complete set of Design Sheets, plans, elevations and illustrative are provided in **Attachment C** to this report.

### GENERAL PLAN ANALYSIS

#### *Central Business District General Plan designation*

The General Plan land use designation for this site is Central Business District. The classification is intended to “encourage, support, and enhance the downtown area as a high-density mixed use urban center of regional importance and a primary hub for business, communications, office, government, high technology, retail, entertainment, and transportation in Northern California.”

The desired character and use for the CBD classification includes a mix of large-scale offices, commercial, urban (high-rise) residential, institutional, open space, cultural, educational, arts, entertainment, service, community facilities, and visitor uses.



The following is an analysis of how the proposed project meets applicable General Plan objectives (staff analysis in indented, italicized text below each objective):

- Objective T2. Provide mixed use, transit-oriented development that encourages public transit use and increases pedestrian and bicycle trips at major transportation nodes.

*The proposed project provides a mixed-use, transit-oriented development at the Lake Merritt BART station, including residential, office, and retail. The project provides streetscape improvements that improve the conditions for pedestrians and bicyclists and includes a paseo to improve connections to the Lake Merritt BART station.*

- Objective D1. Enhance the identify of Downtown Oakland and its distinctive districts; Policy D1.8 Planning for the Channel Park Arts, Educational, and Cultural Center. The area south of Lake Merritt that includes Laney College, the Henry J. Kaiser Auditorium, the Oakland Museum, and Alameda County offices should be enhanced as a walkable, bicycle-friendly educational, cultural and institutional center in downtown Oakland. Efforts to strengthen this area's identity and create transportation linkages with the Jack London Waterfront, City Center, and the Financial District, and BART should be promoted.

*The proposed project creates a transit-oriented development at the Lake Merritt BART station that helps create a node of activity to connect Laney College and the Oakland Museum to the BART station, with improved bicycle and pedestrian connections and a mid-block activated paseo.*

- Objective D3. Create a pedestrian-friendly downtown

*The project improves the sidewalks on the four frontages of each block, providing a minimum 5.5-foot clear width for pedestrians on all frontages. The project also includes intersection improvements that improve pedestrian safety. The streetscape improvements also include landscaping, street furniture, and other amenities. The pedestrian paseo provides mid-block pedestrian access to the BART station.*

- Objective D5. Enhance the safety and perception of safety downtown at all hours

*The proposed project (Limited to Building B and the hard Landscaping) is being designed to activate the transit node at all hours, with ground floor active uses, activation of the paseo, and incorporation of Crime Prevention through Environmental Design (CPTED) techniques.*

- Objective D7. Facilitate and promote downtown Oakland's position as the primary office center for the region.

*This FDP is Phase One (Limited to Building B and the Horizontal Improvements) of a two-phased PUD. The PUD proposed also provides 500,000 square feet of office space located in Building C, with direct access to the BART station. Building B is limited to 100% affordable, deed restricted, Senior housing units and ground-floor commercial space and related ancillary uses.*

- Objective D10. Maximize housing opportunities in the downtown to create a better sense of community

*The proposed project provides 97 new residential, affordable units of varying size and affordability levels.*

- Objective D11. Foster mixed use developments to help create a diverse, lively, and vibrant downtown

*The proposed project provides a mixed-use transit-oriented development with ground floor retail. The pedestrian paseo envisions a food hub near the BART head houses, with a commercial kitchen, restaurants, and outdoor seating. The corner of 8<sup>th</sup> and Oak will also be a focal point for vibrant activity.*

#### *Lake Merritt Station Area Plan policies*

The Lake Merritt Station Area Plan provides land use policies for the plan area and, more specifically, for the Lake Merritt BART Station Area District.

#### Area-Wide Land Use Policies

LU-2 High intensity development potential. Support transit-oriented development and accommodate regional growth projections by promoting high intensity and high-density development in the Planning Area.

*The Phase One includes 97 new residential, affordable units of varying size and affordability levels and a pedestrian paseo and food hub near the BART head houses, contributing to achieving regional growth projections by providing senior housing and thus creating a safer neighborhood with more eyes on the street and diverse denizens.*

LU-3 Ground floor commercial uses. Expand active commercial uses, including retail and restaurants, throughout the Planning Area. This expansion supports an enhanced regional destination, building on and complementing the existing success of the Chinatown Commercial Center and diversifying retail options as an expansion of Oakland's Central Business District.

*The Project provides ground floor limited-service restaurant space around the Lake Merritt BART station entrances and retail along Oak St.*

LU-4 Active ground floor uses. Encourage active uses in new buildings on key streets in neighborhood hubs in order to transform key streets into activated pedestrian connections over time and expand the vibrancy and activity that already exists in some areas. These active ground floor uses should be located at the street edge, or at the edge of parks, plazas, or other public spaces. Activated neighborhood hubs include:

LU-5 Lake Merritt BART Station Area: key streets through this hub include Oak Street, Madison Street (excluding Madison Square Park), 8th Street, and 9th Street

*The Project provides ground floor commercial uses (as described above) as well as residential lobbies, residential amenity space, and daycare activities to activate the ground floor.*

LU-7 Diverse housing types. Ensure a diverse community by incentivizing a range of housing types, including housing for individuals and families of all sizes and all income levels.

*The overall PUD project includes diverse housing types, including family-oriented affordable housing in Building D, affordable senior housing in Building B, and a mix of unit types in the market-rate residential Building A. The proposed project is for one part of this diversity, Building B.*

#### Land Use Policies for the Lake Merritt BART Station Area District

LU-26 High intensity development. Promote high intensity development on the BART-owned blocks to support transit-oriented development. Ensure neighborhood compatibility through application of design guidelines.

*The Project complies with the Lake Merritt Station Area Plan Design Guidelines as well as the project specific design guidelines, ensuring compatibility of the project with the overarching land use entitlements and policies.*

LU-27 Community benefit. New development on the Lake Merritt BART blocks should reflect the unique community heritage of Chinatown, serve the existing and future community, and incorporate public amenities.

*The Project serves the existing and future community by providing affordable housing options for families and seniors and provides community gathering spaces in the paseo.*

LU-28 Community involvement. Work closely with the community and BART to develop the desired program of uses for the Lake Merritt BART blocks and ensure the provision of an appropriate range of community services, public uses, and amenities throughout the area.

*The Project applicant has led extensive community engagement to help shape the Project. Engagement efforts included stakeholder meetings and interviews, Community Design Workshops, and community surveys.*

LU-29 Catalyst development. Promote development on the Lake Merritt BART blocks that acts as a catalyst project that creates an active neighborhood hub and serves as part of activated spines along 8th, 9th, and Oak Streets, connecting the heart of Chinatown, the Lake Merritt BART Station, and Laney College.

*The Project will serve as a catalyst within the broader neighborhood, creating a food hub and community gathering space in the paseo, providing safe and comfortable pedestrian and bicycle connections to the BART station, and providing 97 units of the PUD's proposed 557 residential units.*

## ZONING ANALYSIS

The 2021-LMB-PUD is located within the D-LM Lake Merritt Station Area District Zones. The intent of the Lake Merritt Station Area District (D-LM) Zones is to implement the Lake Merritt Station Area Plan. In general, development in this zoning district shall be consistent with the Lake Merritt Station Area Plan, of a high-quality design, and include active ground floor uses where appropriate and feasible. The 2021-LMB-PUD conforms to the Zoning regulations of the District and Area Plan.

Six overarching objects of the 2021-LMB-PUD Design Guidelines applicable to the Phase One are listed below.

1. Create a more active and vibrant Lake Merritt Station Area District to serve and attract residents, businesses, students, and visitors;

*The Project creates a mixed-use, transit-oriented development on one city block of the Lake Merritt Station Area that will transform an existing BART parking lot into a mixed-use building with 100% affordable housing, with a paseo providing mid-block pedestrian connections to the Lake Merritt BART station.*

2. Increase activity and vibrancy in the area by encouraging vital retail nodes that provide services, restaurants, and shopping opportunities;

*The commercial spaces in Block 1 will create a food hub, with restaurants and outdoor seating to create a food destination in the neighborhood.*

3. Improve connections between the Lake Merritt BART Station and major destinations outside the Station Area District;

*The pedestrian paseo provides an important connection between Laney College and the BART station.*

4. Improve safety and pedestrian-orientation;

*The proposed project provides pedestrian improvements including improved sidewalks with minimum 5.5 ft. clear width, improved intersections with curb bulbs, curb ramps, and crosswalks, and activation to improve safety.*

5. Identify additional recreation and open space opportunities and improve existing resources; and

*The pedestrian paseo in Block 1 provides increased landscaping and greening of the block as well as opportunities for gathering and potential play space for children.*

6. Encourage and enhance a pedestrian-oriented streetscape.

*The project includes improvements to the streetscape on all frontages, which provide pedestrian-oriented streetscapes with improved sidewalks, landscaping, and street furniture.*

The approved 2021-LMB-PUD produced 206 itemized Design Guidelines. Each of the 2021-LMB-PUD’s Design Guidelines are address in the attached Zoning Matrix. Please see **Attachment B, Findings Approval/Zoning Regulations Matrix** for a comprehensive Zoning Regulations Matrix that reviews for consistence with the 2021-LMB-PUD and grants the assumption that all horizontal improvements on Block One and Block Two are included.

A few key Design Guidelines for the Horizontal FDP include the following, for the exhaustive review of the 2021-LMB-PUD’s Design Guidelines please see **Attachment B, Findings Approval/Zoning Regulations Matrix**:

	General Guidelines for Blocks 1 & 2	Shared Outdoor Spaces	Shared outdoor spaces should include plantings, benches, lighting and other appropriate elements to create an inviting and useful space for the residents. Shared outdoor spaces should be designed to accommodate children where feasible by incorporating play structures and play areas. Shared outdoor spaces such as the publicly accessible open space Paseo, the courtyard, the open space at the upper floor step backs and/or the rooftops, should be designed to have ample daylight and to be sheltered from the wind.	Horizontal	Yes
					Paseo design meets this design guidelines to include planting, lighting, and places for children. Outdoor spaces at buildings are optimized for sun exposure.

Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	Street furnishings should be provided in transit waiting areas and as needed to facilitate multi-modal transfers from bike or scooter or bus to BART. (LMSAP/DG-131: Furnishings)	Horizontal	Yes	Benches underneath the bus canopy orient towards Oak and are directly adjacent to scooter corals and bike lockers and bike parking.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Streetscape / Business Frontage Zone	Elegant vertical circulation with clear sightlines to the Paseo should be maintained at entry points mid-block in the business frontage zone.	Horizontal	Yes	Design of massing and materials could be further refined to create "elegant" vertical circulation, however the minimum requirement has been met. The design of the visual connection and circulation from Fallon Street into the Paseo and through to Oak Street is a priority.

As demonstrated in the Zoning Matrix, the proposed project meets the regulator requirements delineated in 2021-LMB-PUD if all Horizontal improvements on Block One and Block Two are included in Phase One.

**ENVIRONMENTAL DETERMINATION**

This FDP relies on the Lake Merritt Station Area Plan Environmental Impact Report (EIR), certified on July 28, 2014, and is entirely within the scope of, and was contemplated in, the CEQA Analysis for the Lake Merritt BART PUD. The Lake Merritt Station Area Plan EIR can be reviewed at: <https://cao-94612.s3.amazonaws.com/documents/Final-Environmental-Impact-Report-FEIR-July-28-2014.pdf>  
 The Lake Merritt Bart PUD CEQA Analysis can be reviewed at: [https://cao-94612.s3.amazonaws.com/documents/LMBART-CEQA-Checklist-Addendum\\_Final\\_signed-1.pdf](https://cao-94612.s3.amazonaws.com/documents/LMBART-CEQA-Checklist-Addendum_Final_signed-1.pdf)

Since it was contemplated in the previous Lake Merritt BART PUD CEQA Analysis, the proposed project meets all requirements under CEQA Guidelines Sections 15164, 15183, and 15168. As a result, no supplemental environmental review is required in accordance with Public Resources Code Section 21083.3 and Section 21166, and CEQA Guidelines Sections 15162 through 15164, as well as 15168.

**KEY ISSUES AND IMPACTS**

*Issues*

In general, staff finds the proposed project design to be consistent with the approved 2021-LMB-PUD. The Paseo adds a visually interesting public space that will be inviting to all public users. The public right-of-way improvements upgrade ADA infrastructure to required safety standards and provide access to the proposed Senior Housing.

However, four issues remain outstanding, as noted below:

- ISSUE 1: CLARITY OF DELIVERABLES: The DRC requested greater clarity regarding the extent of the horizontal improvements.
  - The applicant provided a memo explaining that the clarity issue was addressed by adding design sheet L4.01B. Design sheet L4.01B with the page title: “Illustrative Material Phase 1.1” is dedicated to Phase 1.1 and does increase the readability of the phasing proposal. However, key details are still missing. On sheet L4.01B, a conceptual sketch is provided of the ADA ramp connecting the Paseo to Fallon Street, labeled on the site plan as “Temporary ADA Ramp”.



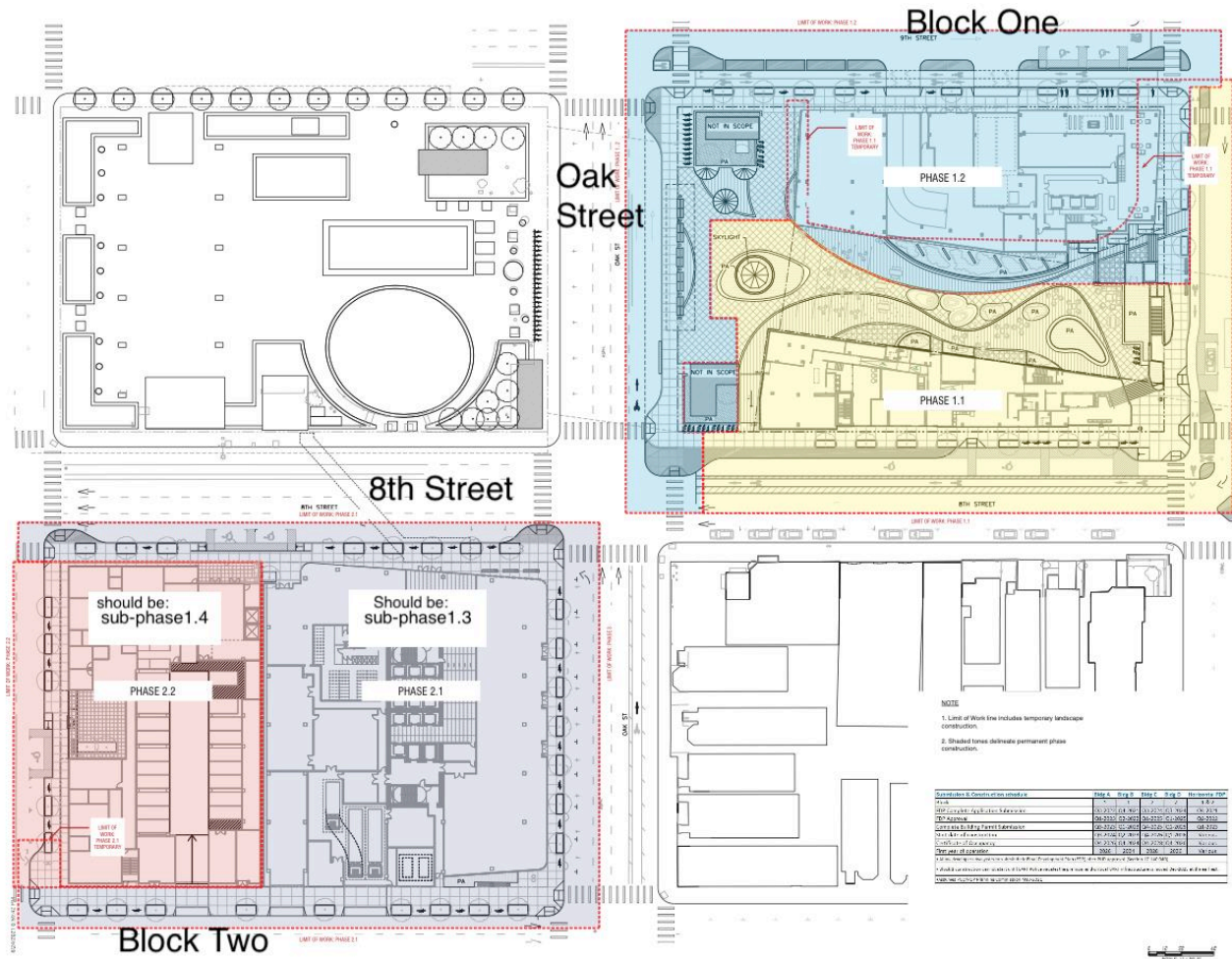
PHASE 1.1 CONNECTION TO EAST AT FALLON ST.

*Image of the proposed build out of Phase 1.1. Design Sheet L4.01B ‘Illustrative Materials; Phase 1.1’; Attachment C, Proposed Plans, LMB-FDP-Horizontal\_dated 22-06-21*

- The sketch also includes a tall fence between the ADA ramp and the street level parking lot, labeled on the site plan as “Temporary Fence with Colorful Posts”. The fence appears to be roughly double the height of a person walking in the parking lot.
- However, dimensions and materials of the of the ramp and fence are not provided. Staff is not able to determine if the ramp meets ADA requirements nor if the fence meets zoning regulations.
- Sub-Phase 1.2 is not clearly labeled throughout the design sheet. Some design sheets include a subheading to the page title that identifies the proposed phase subsections; however, this is not consistent through the design sheets. For example, Design Sheet L4.01 with the page title: “Illustrative Materials Phase 1.2” is a focused design sheet dedicated to the completion of Sub-Phase 1.2. Design Sheet L4.02, titled “Illustrative Material Phase 2.2”. There is an equivalent sheet titled “Illustrative Material Phase 2.1”. However, other sheets do not identify the proposed phase; e.g., sheets L4.1, L4.2, L5.1, L5.2, L6.1, L6.2, L7.1, L7.2, etc.
  - Staff Recommendations:
  - *Include a condition of approval to require revision of design sheets to make phasing more transparent. This will make the phasing more easily understood for future review of project requirement for later phased P-jobs and FDPs.*

- *On design sheet L4.01B, include dimensions of site plan features delivered as temporary hard landscaping delivered with the completion of Sub-Phase 1.1. This includes the ADA ramp and fence.*
- *On design sheet L4.01B, add conceptual sketch of connection between new landscaping of Paseo to the Oak Street side of the Paseo that is not included with Sub-Phase 1.1, i.e., the later phased “West Plaza” or Remainder Parcel.*
- *Specify how this “West Plaza” or Remainder Parcel area will be used by the public during the construction of Building A.*
- *Create three new design sheets that mirror focus of deliverables of sub-phases akin to L4.01B. E.g., Sub-Phase 1.2, Sub-Phase 1.3, and Sub-Phase 1.4.*
- *Given that a section of the Paseo is temporary and will need to be demolished for Sub-Phase 1.2, create a design sheet identifying the Sub-Phase 1.2 Demolition plan of the horizontal improvements completed in Sub-Phase 1.1.*
- *Consistently employ subheading titles “Sub-Phase 1.1, Sub-Phase 1.2, etc.” indicated the applicable phase on all design sheets*
- ***These recommendations are codified in the attached Conditions of Approval for this FDP.***

Issue 2: SCOPE OF HORIZONTAL IMPROVEMENT INCLUDED IN THE HORIZONTAL FDP: The proposal for the Horizontal FDP proposes two phases, and four sub-phases of the horizontal improvements. Design Sheets L3.0 to L3.4 label the horizontal improvements on Block Two as part of ‘Phase 2.1’ and ‘Phase 2.2’.



Design Sheet L3.0 'Phase Diagram'; Attachment C, Proposed Plans, LMB-FDP-Horizontal dated 22-06-21

- However, the 2021-LMB-PUD Conditions of Approval (COA) include a detailed delineation of the Phasing. The item identified as *Part 1, #19* of the 2021-LMB-PUD COA, specifies that the first FDP must include all horizontal improvements on the two city blocks identified as Block One and Block Two. The 2021-LMB-PUD COA specify two phases. The first phase requires three FDPs (all of the horizontal improvements, and one for each of the two buildings) while the second phase requires two FDPs (one for each of the two buildings).
- The follow outline is established as Conditions of Approval 19, 20, 21. The full 2021-LMB-PUD Conditions of Approval are provided as **Attachment D**.
- The 2021-LMB-PUD's COA organized the phases and relevant permits as follow:
  - ◆ PHASE ONE
    - FDP 1: All horizontal improvements, including both Blocks 1 and 2;
    - FDP 2: Building A
    - FDP 3: Building B
  - ◆ PHASE TWO
    - FDP 1: Building C



- FDP 2: Building D
  - *Staff Recommendations:*
  - *Revise Design Sheets to make phasing adhere 2021-LMB-PUD COA.*
  - *Label Horizontal improvements on Block 2 as Sub-Phases 1.3 and 1.4*
  - *Where applicable, update all Design Sheets with subheading titles “Sub-Phase 1.3, Sub-Phase 1.4”*
  - *Create new design sheet specifying the delivery and security of the satisfaction of the City of Oakland timeline*
  - *Specify that Block Two Horizontal Improvement will come back to DRC for reconfirmation at time of first Phase Two FDP*
  - *Include a condition of approval to clearly establish how the horizontal FDP may be delivered in phases, as follows:*
    - *Phase 1: Block One: Applicant shall apply for construction-related permits for the entirety of Phase One of the horizontal FDP. The applicant may elect to delay delivery of Sub-Phase 1.2 until Certificate of Occupancy for Building B, subject to an Improvement Agreement and appropriate security acceptable to the City of Oakland.*
      - *Sub-Phase 1.1: Sub-Phase 1.1 shall be delivered prior to Certificate of Occupancy for Building B.*
      - *Sub-Phase 1.2: As noted above, Sub-Phase 1.2 shall be delivered prior to Certificate of Occupancy for Building A, unless the applicant enters into an Improvement Agreement that guarantees delivery of improvements prior to issuance of the Certificate of Occupancy for Building A.*
    - *Phase 2: Block Two: At the time of approval of this FDP, the design for Phase 2 (Block Two) is conceptual to allow for future schematic design of the buildings planned for that block and for further development of right-of-way improvement plans by the City of Oakland. Therefore, Phase Two of this FDP shall be refined to schematic design level and subject to review by the Design Review Committee of the Planning Commission and approval by Bureau of Planning and OakDOT staff prior to approval of the first vertical FDP for Block Two.*
      - *Sub-Phase 1.3: Sub-Phase 1.3 shall be delivered prior to Certificate of Occupancy for Building C (or whichever building is constructed first on Block 2).*
      - *Sub-Phase 1.4: Sub-Phase 1.4 shall be delivered prior to Certificate of Occupancy for Building D, unless the applicant enters into an Improvement Agreement that guarantees delivery of improvements prior to issuance of the Certificate of Occupancy for Building D (or whichever building is constructed last on Block 2).*
  - ***These recommendations are codified in the attached Conditions of Approval for this FDP.***

THIRD: THE DIVIDE OF HORIZONTAL IMPROVEMENTS BETWEEN SUB-PHASE 1.1 AND SUB-PHASE 1.2: Staff has reason to include more of the proposed horizontal improvement of the FDP in Sub-Phase 1.1.

- At the May 25th DRC hearing, Commissioners discussed the value of including the whole of the Paseo (from Fallon Street to Oak Street) in what is now understood as Sub-Phase 1.1. As proposed, the improvements to the Paseo will abruptly end roughly one-third from Oak Street.

- DRC Commissioners also questioned the lack of inclusion of the public right-of-way improvements along Oak Street.
  - The applicant has replied in a memo that it would be cost prohibitive to include the horizontal improvements, stating:
    - “The proposed Horizontal FDP phasing plan responds to several competing objectives including the operational requirements of BART, desire to bolster the Project’s competitiveness for affordable housing and transportation funding, and logistical construction constraints... projects in the City of Oakland have recently been getting “stuck” (particularly at the CDLAC stage – which is the last piece of financing needed in most affordable housing projects) as a result of working in an extremely high-cost local environment. We are hoping to get in front of this by being very deliberate with the level of offsite work that this affordable housing project can deliver.”
- Staff agrees that the horizontal improvement to 9<sup>th</sup> Street, along the Street Frontage of Building A, are not practical to include in Sub-Phase 1.1, given the later construction of Building A. However, in alignment with the City’s Transit First policy (Resolution No. 73036 C.M.S.), transit access improvements to and from the site should be prioritized to be completed with Sub-Phase 1.1.
- A key function of the Paseo is to create a pedestrian passage through the site and increase easy access to BART. With the work abruptly stopping roughly one-third from Oak Street, it may create underutilized public space that would no longer meet the design guidelines set out in the 2021-LMB-PUD.
- Additionally, staff questions why Building B’s Street Fronting public right-of-way horizontal improvements are not included with the construction of Building B? Staff is aware that developers of Building A may later question why they should be responsible for the Street Fronting public right-of-way horizontal improvements for a different building (Building B).
  - *Staff Recommendations:*
  - *Sub-Phase 1.1 shall include the horizontal improvements along all of Building B’s Street Fronting public right-of-way. This includes all horizontal improvements on 8th Street and horizontal improvements on Oak Street up to the North-west corner of Building B. With attention to the following:*
    - *The bulb-out as shown in Horizontal Plans for sub-phase 1.2 on the corner of 8th and Oak Street as Sub-Phase 1.1, to the extent feasible as determined by Oak-DOT City Staff.*
  - *Staff also recommends that the Oak Street section of the Paseo labeled as the “West Plaza” or Remainder Parcel be included in Sub-Phase 1.1 and conditions as a publicly accessible space during the later staging and construction of Building A.*
  - *The Remainder of the Horizontal improvements for Block One, including the majority of Oak Street and majority of 9<sup>th</sup> street shall be Bonded in line with the 2021-LMB-PUD COA.*
  - *These recommendations are codified in the attached Conditions of Approval for this FDP.*

## RECOMMENDATION

The proposed Lake Merritt BART Horizontal FDP is consistent with and constitutes the previously approved Preliminary Development Plan. Staff specifically recommends that the Planning Commission:

1. Rely on the Lake Merritt Station Area Plan Environmental Impact Report (EIR), certified on July 28, 2014 as adequate under CEQA for analysis of the Lake Merritt BART Vertical Final Development Permit pursuant to CEQA Guidelines Section 15162, and based on the attached findings (and incorporated herein by reference);
- Approve the Lake Merritt PUD FDP #1 for horizontal improvements (case file PLN20108-PUDF02), subject to the following recommendations listed below and duplicated in the attached Conditions of Approval for this FDP as **Attachment A**.
  1. Requirement: The project applicant shall revise Design sheets to make phasing more transparent and easily understood for future review of project requirements for later phased P-jobs and FDPs according to the following:
    - On design sheet L4.01B, include dimensions of site plan features delivered as temporary hard landscaping delivered with the completion of Sub-Phase 1.1. This includes the ADA ramp and fence.
    - On design sheet L4.01B, add conceptual sketch of connection between new landscaping of Paseo to the Oak Street side of the Paseo that is not included with Sub-Phase 1.1, i.e., the later phased “West Plaza” or Remainder Parcel.
    - Specify how this “West Plaza” or Remainder Parcel area will be used by the public during the construction of Building A.
    - Create three new design sheets that mirror focus of deliverables of sub-phases akin to L4.01B. E.g., Sub-Phase 1.2, Sub-Phase 1.3, and Sub-Phase 1.4.
    - Given that a section of the Block One Paseo is temporary and will need to be demolished for Sub-Phase 1.2, create a design sheet identifying the Sub-Phase 1.2 Demolition plan of the horizontal improvements completed in Sub-Phase 1.1.
    - Consistently employ subheading titles “Sub-Phase 1.2, Sub-Phase 1.3, or Sub-Phase 1.4 indicating the applicable phase on all design sheets

When Required: Prior to application for construction-related permit

Initial Approval: N/A

Monitoring/Inspection: Planning and Building Department

2. Requirement: The applicant shall abide by the following phasing plan in adhere to 2021-LMB-PUD COA:
  - A. **Phase 1: Block One**: Applicant shall apply for construction-related permits for the entirety of the horizontal FDP for Block One. The applicant may elect to delay delivery of Sub-Phase 1.2 until after Certificate of Occupancy for Building B, subject to an Improvement Agreement and appropriate security acceptable to the City of Oakland so that the improvements are guaranteed by the applicant. The applicant shall apply for consideration for the Improvement Agreement and security to the satisfaction of the City of Oakland prior to, or contemporaneous with, applying for any construction-related permit for Phase 1: Block One. In any event, Sub-Phase 1.2 shall be delivered prior to Certificate of Occupancy being issued for Building A.

- Sub-Phase 1.1: Sub-Phase 1.1 shall be delivered prior to Certificate of Occupancy for Building B.
- Sub-Phase 1.2: As noted above, Sub-Phase 1.2 shall be delivered prior to Certificate of Occupancy for Building A. The applicant may delay delivery of the improvements to after the construction of Building B by entering into an Improvement Agreement that guarantees delivery of improvements prior to issuance of the Certificate of Occupancy for Building A. The applicant shall apply for the Improvement Agreement and security to the satisfaction of the City of Oakland for Sub-Phase 1.2 prior

**B. Phase 2: Block Two:** The project applicant agrees that the design of the buildings for Phase 2 (Block Two) is conceptual to allow for future schematic design of the buildings planned for that block and for further development of right-of-way improvement plans by the City of Oakland. The design for Phase 2 will be considered at the time of approval of the FDP for Phase 2. Therefore, the Phase 2 horizontal improvements of this FDP shall be refined to schematic design level and subject to review by the Design Review Committee of the Planning Commission and approval by Bureau of Planning/OakDOT staff prior to approval of the first vertical FDP for Block Two.

- Sub-Phase 1.3: Sub-Phase 1.3 shall be delivered prior to Certificate of Occupancy for Building C (or whichever building is constructed first on Block 2).
- Sub-Phase 1.4: Sub-Phase 1.4 shall be delivered prior to Certificate of Occupancy for Building C, unless the applicant enters into an Improvement Agreement that guarantees delivery of improvements prior to issuance of the Certificate of Occupancy for Building D (or whichever building is constructed last on Block 2).

When Required: Prior to construction-related permit and ongoing

Initial Approval: N/A

Monitoring/Inspection: Department of Planning and Building and Department of Transportation

3. Requirement: The project applicant shall expand the scope of horizontal improvements included in Sub-Phase 1.1 to include the horizontal improvements along all of Building B's street fronting public right-of-way and all of Paseo through to Oak Street; with attention to the "bulb-out" on the corner of 8th and Oak Street.

When Required: Prior to application for construction-related permit

Initial Approval: N/A

Monitoring/Inspection: Planning and Building Department

- 4. Requirement: The project applicant is required to submit Sub-Phases 1.2, 1.3, and 1.4 to return to the Design Review Committee at time of P-Job submittal, with Bureau of Planning and Department of Transportation approval authority to review and approve the revisions. The project applicant may bundle to the Design Review Committee Sub-Phase 1.2, 1.3, or 1.4 with the corresponding building FDP. If the Sub-Phase is considered with the corresponding Building, it shall return to Planning Commission (rather than Planning/OakDOT Staff) with the corresponding Building for final approval.

When Required: Prior to application for construction-related permit

Initial Approval: N/A

Monitoring/Inspection: Planning and Building Department

Prepared by:



Karen August, Historic Preservation Planner III

Reviewed by:



Catherine Payne, Development Planning Manager  
Bureau of Planning

Approved for forwarding to the  
City Planning Commission by:



Ed Manasse, Deputy Director of Planning

**Attachment:**

- A. Conditions For Approval for the FDP, PLN20108-PUDF02;
- B. Proposed Plans, *LMB-FDP-Horizontal\_dated 22-06-21*
- C. 2021-LMB-PUD---DESIGN GUIDELINES
- D. Findings Approval/ Zoning Regulations Matrix

- E. 2021 Lake Merritt BART PUD COA
- F. The Lake Merritt Station Area Plan EIR: <https://cao-94612.s3.amazonaws.com/documents/Final-Environmental-Impact-Report-FEIR-July-28-2014.pdf>
- G. The Lake Merritt BART PUD CEQA Analysis: [https://cao-94612.s3.amazonaws.com/documents/LMBART-CEQA-Checklist-Addendum\\_Final\\_signed-1.pdf](https://cao-94612.s3.amazonaws.com/documents/LMBART-CEQA-Checklist-Addendum_Final_signed-1.pdf)



## ATTACHMENT A

### Conditions of Approval For Horizontal FDP And Phasing Plan In Addition To Those Included in the PUD

1. **Requirement:** The project applicant shall revise Design sheets to make phasing more transparent and easily understood for future review of project requirements for later phased P-jobs and FDPs according to the following:
  - On design sheet L4.01B, include dimensions of site plan features delivered as temporary hard landscaping delivered with the completion of Sub-Phase 1.1. This includes the ADA ramp and fence.
  - On design sheet L4.01B, add conceptual sketch of connection between new landscaping of Paseo to the Oak Street side of the Paseo that is not included with Sub-Phase 1.1, i.e., the later phased “West Plaza” or Remainder Parcel.
  - Specify how this “West Plaza” or Remainder Parcel area will be used by the public during the construction of Building A.
  - Create three new design sheets that mirror focus of deliverables of sub-phases akin to L4.01B. E.g., Sub-Phase 1.2, Sub-Phase 1.3, and Sub-Phase 1.4.
  - Given that a section of the Block One Paseo is temporary and will need to be demolished for Sub-Phase 1.2, create a design sheet identifying the Sub-Phase 1.2 Demolition plan of the horizontal improvements completed in Sub-Phase 1.1.
  - Consistently employ subheading titles “Sub-Phase 1.2, Sub-Phase 1.3, or Sub-Phase 1.4 indicating the applicable phase on all design sheets

**When Required:** Prior to application for construction-related permit

**Initial Approval:** N/A

**Monitoring/Inspection:** Planning and Building Department

2. **Requirement:** The applicant shall abide by the following phasing plan in adhere to 2021-LMB-PUD COA:

**A. Phase 1: Block One:** Applicant shall apply for construction-related permits for the entirety of the horizontal FDP for Block One. The applicant may elect to delay delivery of Sub-Phase 1.2 until after Certificate of Occupancy for Building B, subject to an Improvement Agreement and appropriate security acceptable to the City of Oakland so that the improvements are guaranteed by the applicant. The applicant shall apply for consideration for the Improvement Agreement and security to the satisfaction of the City of Oakland prior to, or contemporaneous with, applying for any construction-related permit for Phase 1: Block One. In any event, Sub-Phase 1.2 shall be delivered prior to Certificate of Occupancy being issued for Building A.

- Sub-Phase 1.1: Sub-Phase 1.1 shall be delivered prior to Certificate of Occupancy for Building B.
- Sub-Phase 1.2: As noted above, Sub-Phase 1.2 shall be delivered prior to Certificate of Occupancy for Building A. The applicant may delay



delivery of the improvements to after the construction of Building B by entering into an Improvement Agreement that guarantees delivery of improvements prior to issuance of the Certificate of Occupancy for Building A. The applicant shall apply for the Improvement Agreement and security to the satisfaction of the City of Oakland for Sub-Phase 1.2 prior

**B. Phase 2: Block Two:** The project applicant agrees that the design of the buildings for Phase 2 (Block Two) is conceptual to allow for future schematic design of the buildings planned for that block and for further development of right-of-way improvement plans by the City of Oakland. The design for Phase 2 will be considered at the time of approval of the FDP for Phase 2. Therefore, the Phase 2 horizontal improvements of this FDP shall be refined to schematic design level and subject to review by the Design Review Committee of the Planning Commission and approval by Bureau of Planning/OakDOT staff prior to approval of the first vertical FDP for Block Two.

- Sub-Phase 1.3: Sub-Phase 1.3 shall be delivered prior to Certificate of Occupancy for Building C (or whichever building is constructed first on Block 2).
- Sub-Phase 1.4: Sub-Phase 1.4 shall be delivered prior to Certificate of Occupancy for Building C, unless the applicant enters into an Improvement Agreement that guarantees delivery of improvements prior to issuance of the Certificate of Occupancy for Building D (or whichever building is constructed last on Block 2).

When Required: Prior to construction-related permit and ongoing

Initial Approval: N/A

Monitoring/Inspection: Department of Planning and Building and Department of Transportation

3. Requirement: The project applicant shall expand the scope of horizontal improvements included in Sub-Phase 1.1 to include the horizontal improvements along all of Building B's street fronting public right-of-way and all of Paseo through to Oak Street; with attention to the "bulb-out" on the corner of 8th and Oak Street.

When Required: Prior to application for construction-related permit

Initial Approval: N/A

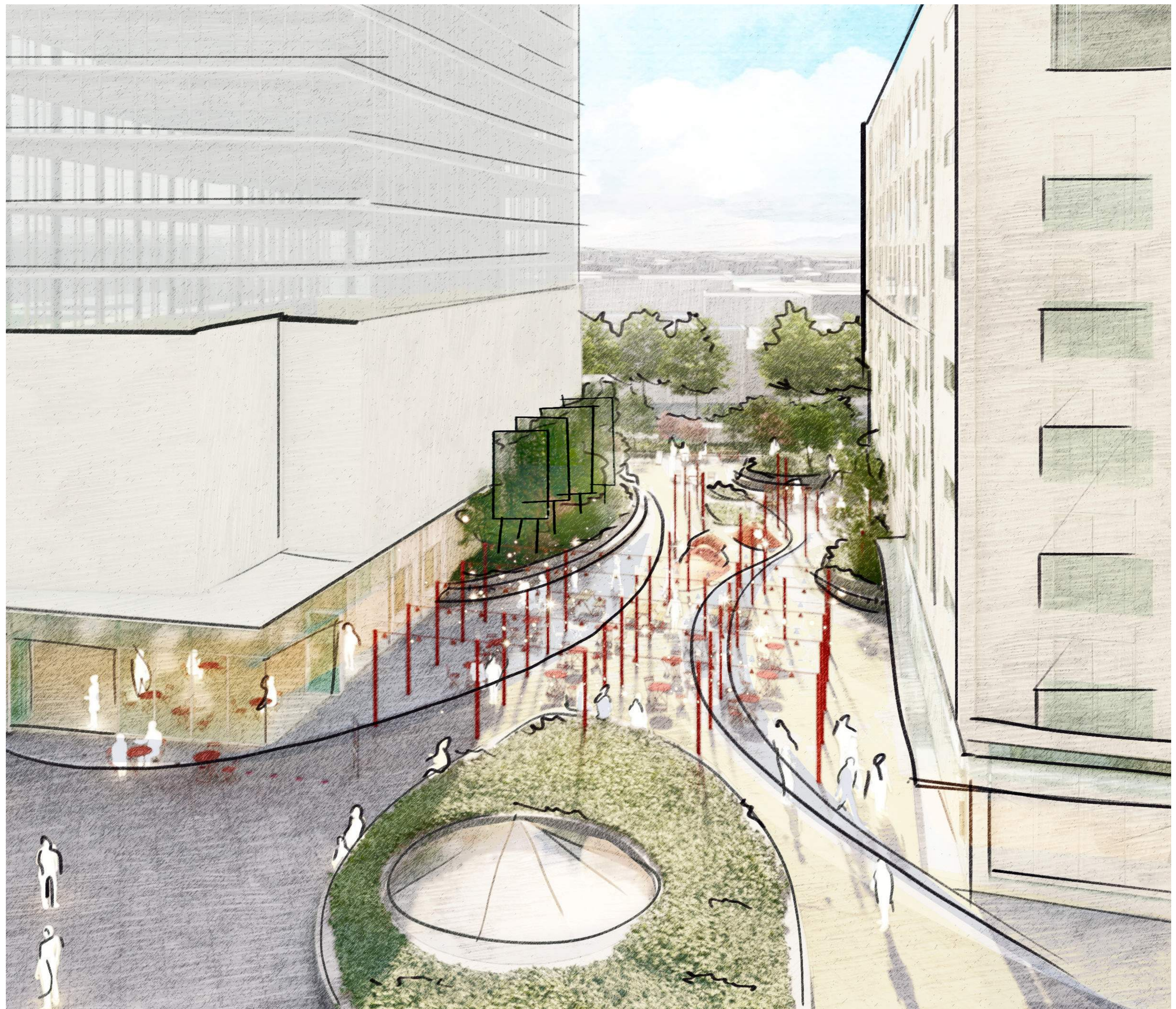
Monitoring/Inspection: Planning and Building Department

4. Requirement: The project applicant is required to submit Sub-Phases 1.2, 1.3, and 1.4 to return to the Design Review Committee at time of P-Job submittal, with Bureau of Planning and Department of Transportation approval authority to review and approve the revisions. The project applicant may bundle to the Design Review Committee Sub-Phase 1.2, 1.3, or 1.4 with the corresponding building FDP. If the Sub-Phase is considered with the corresponding Building, it shall return to Planning Commission (rather than Planning/OakDOT Staff) with the corresponding Building for final approval.

When Required: Prior to application for construction-related permit

Initial Approval: N/A

Monitoring/Inspection: Planning and Building Department



## Paseo Food Court Zone

### NOTES:

- ① See Civil Drawings for limit of project sidewalk improvements.
- ② Street access plan striping shown for planning purposes only.
- ③ Paseo design under study. Furniture is movable. Typical conditions shown.
- ④ All open space elements to be maintained by BART will be designed to BFS standards.
- ⑤ The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads. Plant areas shown over the tunnel structure must comply with BFS standards.
- ⑥ BART security cameras to be included as agreed to with BART.
- ⑦ Property lines between the headhouse and building frontage of Building A and Building B will be called out in the paving with physical markings. See Civil Drawings for Property Lines
- ⑧ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping and crosswalks as necessary.

### NOTE:

"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval" and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

### SHEET INDEX

SHEET NO.	
0.00	COVER PAGE AND SHEET INDEX
L0.0	SITE PHOTOS
L0.1	SITE PHOTOS
L1.0	AREA PLAN
L1.1	URBAN PLANNING
L2.0	EXISTING TREES
L2.1	EXISTING TREES
L2.2	EXISTING TREES
L2.3	EXISTING TREES
L3.0	PHASE DIAGRAM
L3.1	PHASE 1.1
L3.2	PHASE 1.2
L3.3	PHASE 2.1
L3.4	PHASE 2.2
L3.5	TRUCK TURNING RADIUS DIAGRAM
L3.6	PASEO CROSS SECTIONS
C1.0	TITLE SHEET
C2.0	OVERALL SHEET INDEX
C2.1	EXISTING CONDITIONS (BLOCK 1)
C2.2	EXISTING CONDITIONS (BLOCK 2)
C3.1	DEMOLITION PLAN (BLOCK 1)
C3.2	DEMOLITION PLAN (BLOCK 2)
C4.1	SITE PLAN (BLOCK 1)
C4.2	SITE PLAN (BLOCK 2)
C5.1	GRADING PLAN (BLOCK 1)
C5.2	GRADING PLAN (BLOCK 2)
C5.3	SECTIONS
C6.1	UTILITY PLAN (BLOCK 1)
C6.2	UTILITY PLAN (BLOCK 2)
C7.1	STORMWATER PLAN (BLOCK 1)
C7.2	STORMWATER PLAN (BLOCK 2)
C8.1	DETAILS
L4.00	RENDERED VIEWS
L4.01	ILLUSTRATIVE MATERIALS PHASE 1.2 (BLOCK 1)
L4.01B	ILLUSTRATIVE MATERIALS PHASE 1.1 (BLOCK 1)
L4.02	ILLUSTRATIVE MATERIALS (BLOCK 2)
L4.03	MATERIALS
L4.1	LANDSCAPE PLAN (BLOCK 1)
L4.2	LANDSCAPE PLAN (BLOCK 2)
L5.1	FURNITURE PLAN (BLOCK 1)
L5.2	LIGHTS AND FURNITURE PLAN (BLOCK 2)
L6.1	MATERIALS PLAN (BLOCK 1)
L6.2	MATERIALS PLAN (BLOCK 2)
L7.1	PLANTING PLAN (BLOCK 1)
L7.2	PLANTING PLAN (BLOCK 2)
L7.3	PLANTING PALETTE
L7.4	PLANTING PALETTE (BLOCK 1)
L8.0	GRADING PLAN (BLOCK 1)
L8.1	DRAINAGE DETAILS
L9.1	SIGN PLAN (BLOCK 1)
L9.2	SIGN PLAN (BLOCK 2)
L10.1	9TH ST SECTION (BLOCK 1)
L10.2	FALLON ST SECTION (BLOCK 1)
L10.3	8TH ST SECTION (BLOCK 1)
L10.4	OAK ST SECTION (BLOCK 1)
L10.5	8TH ST SECTION (BLOCK 2)
L10.6	OAK ST SECTION (BLOCK 2)
L10.7	7TH ST SECTION (BLOCK 2)
L10.8	MADISON ST SECTION (BLOCK 2)
L10.9	BART INFRASTRUCTURE (BLOCK 2)
L11.1	PRELIMINARY LIGHTING CONCEPTS
L11.2	PRELIMINARY LIGHTING LAYOUT
L11.3	PRELIMINARY PHOTOMETRIC CALCULATION
L11.4	FIXTURE CUT SHEET
L11.5	FIXTURE CUT SHEET
L11.6	FIXTURE CUT SHEET



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

### ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

### REVISION SCHEDULE

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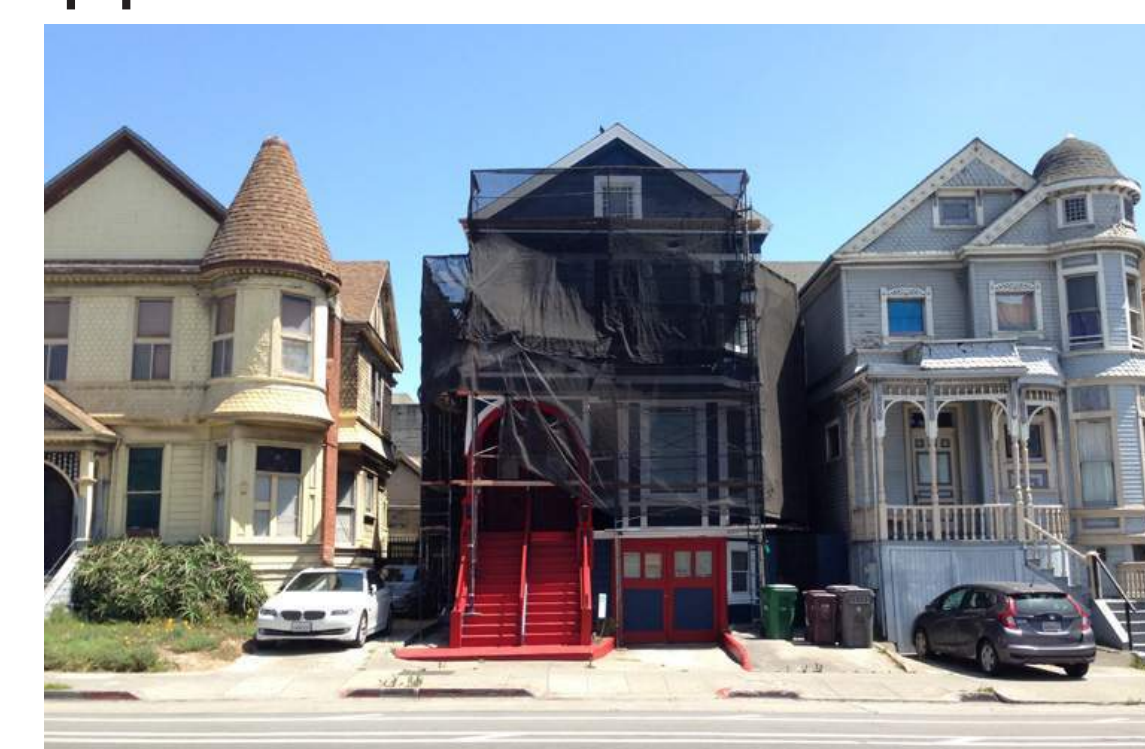
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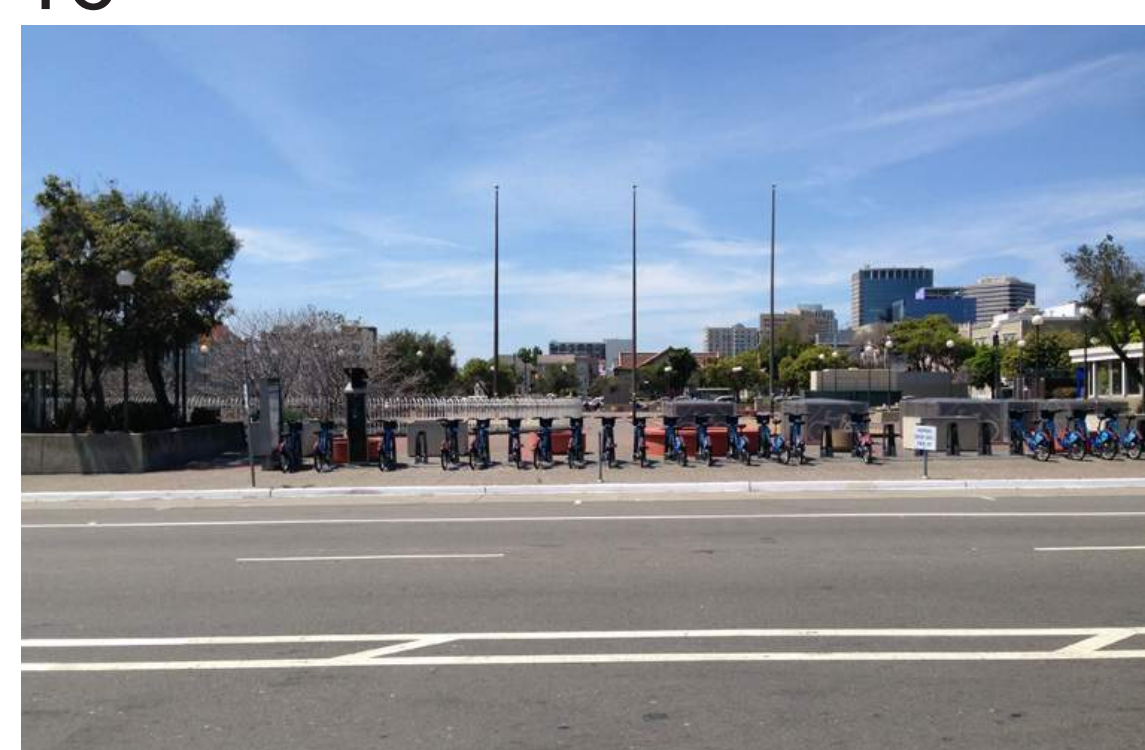
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LAKE MERRITT BART HORIZONTAL FDP Oakland, CA 94607

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ISSUE SCHEDULE	
50 SD - HORIZONTAL FDP	10/29/2021
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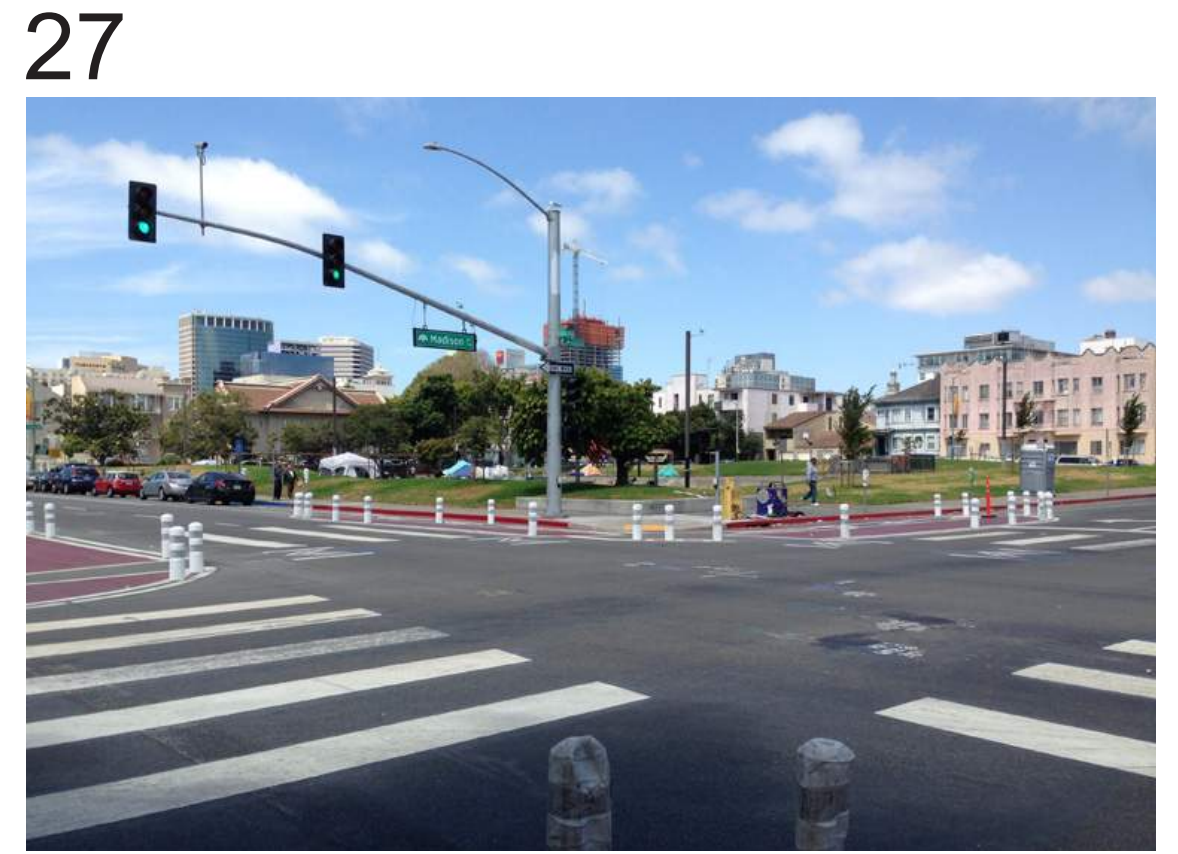
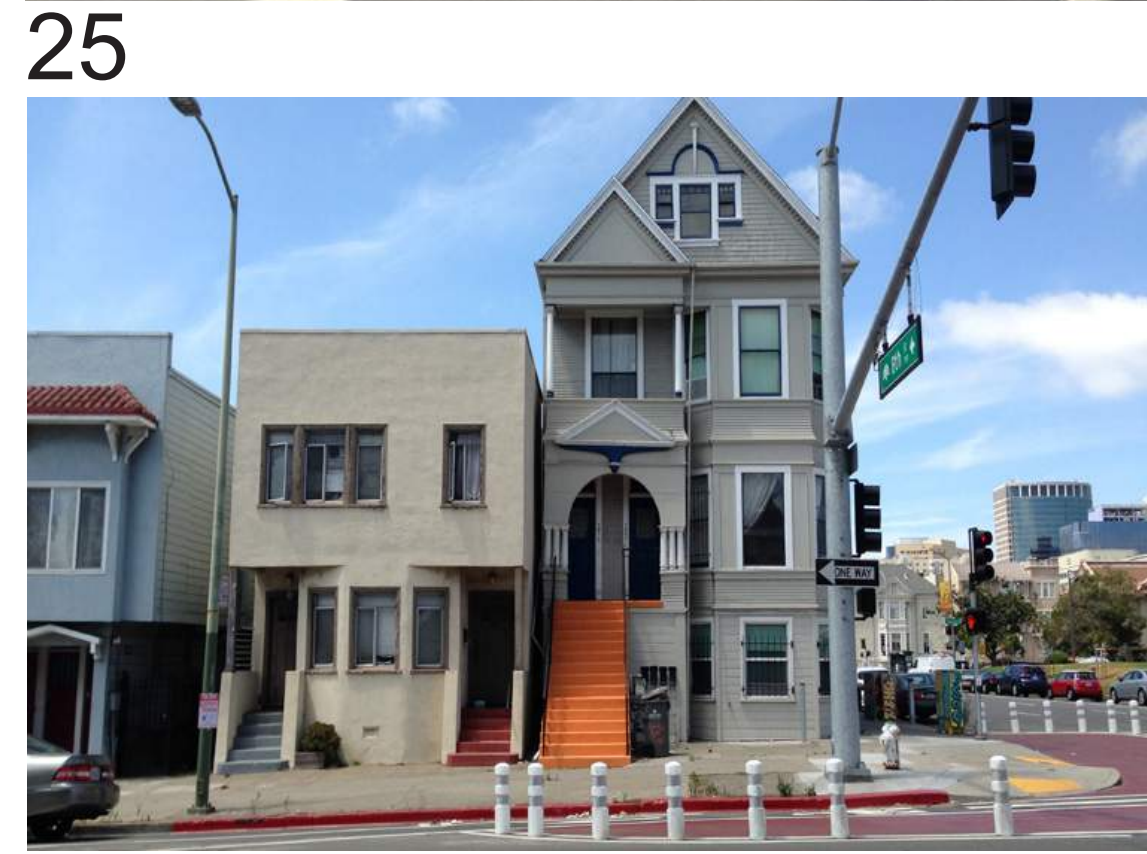
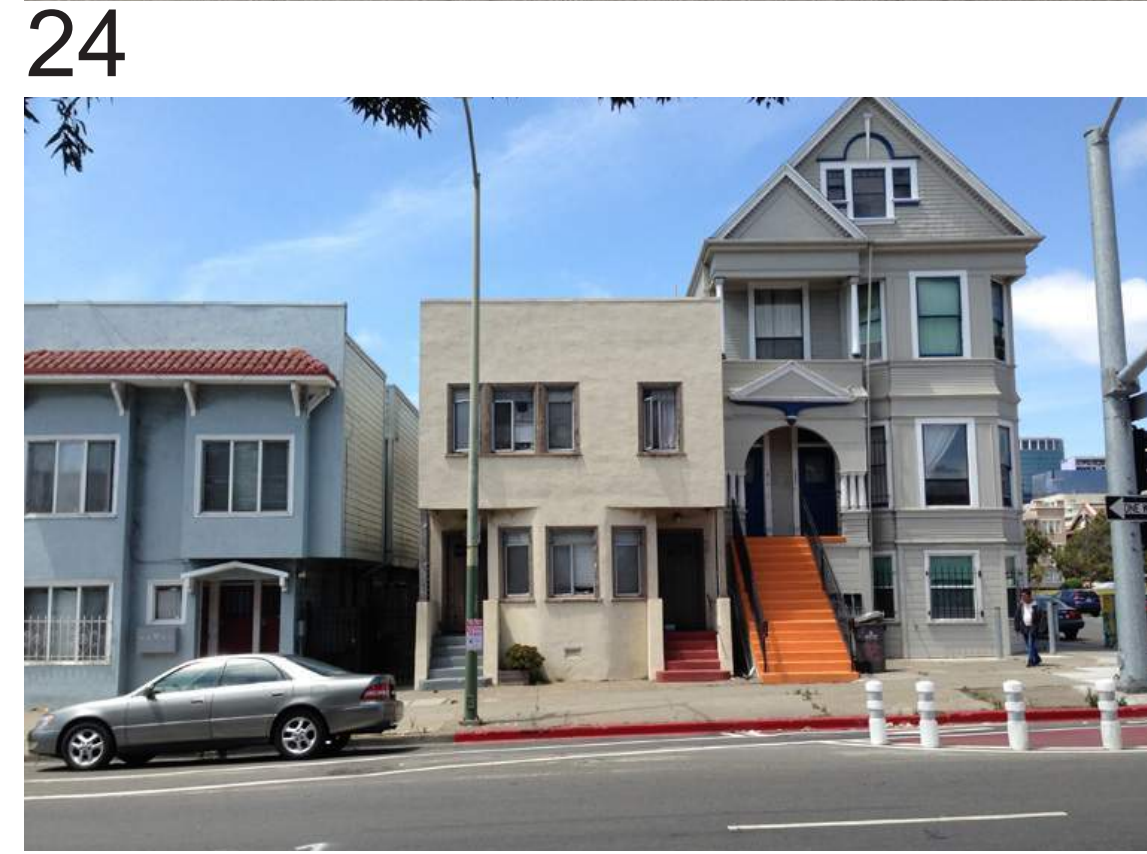
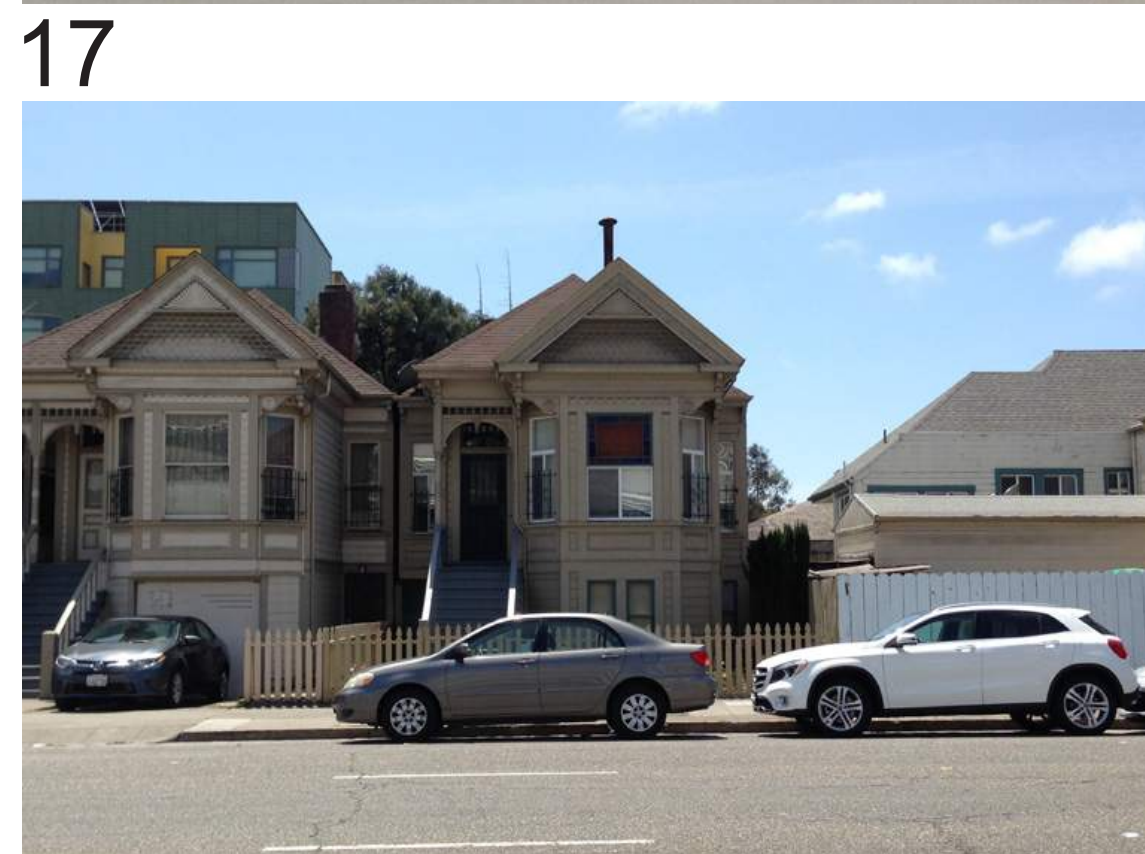
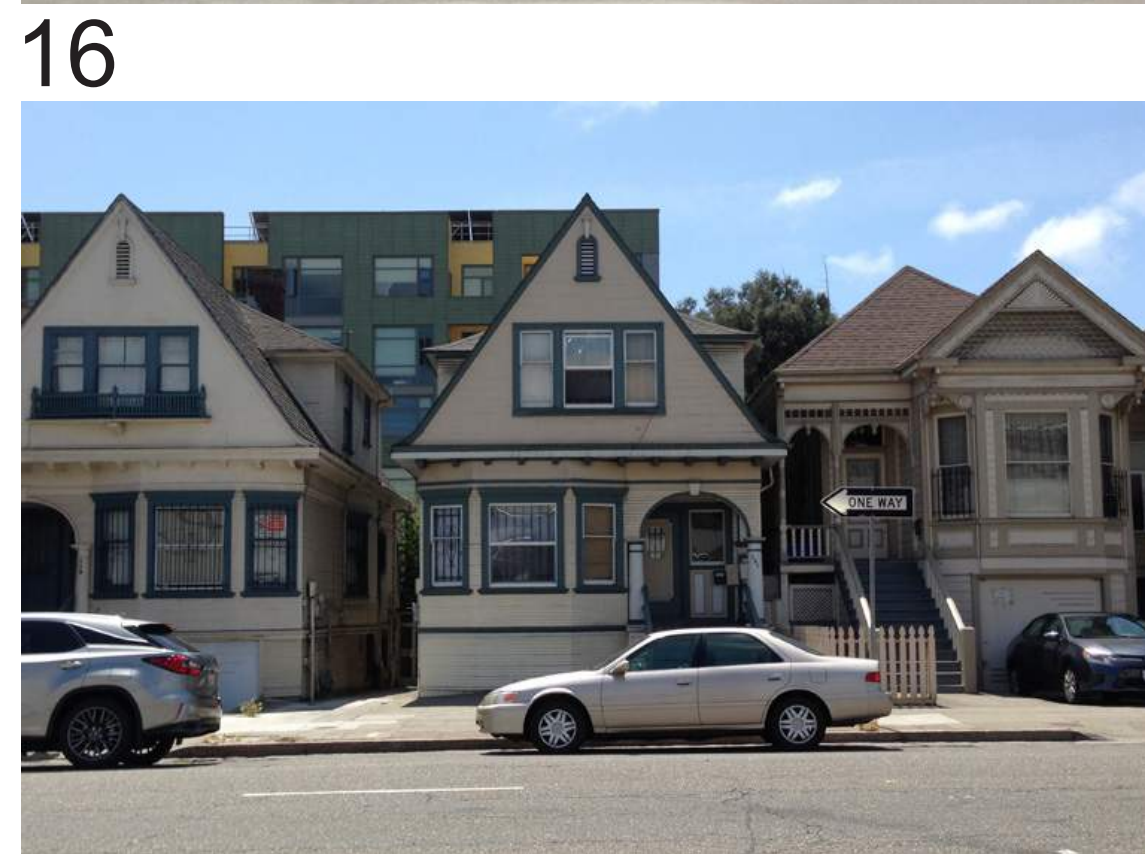
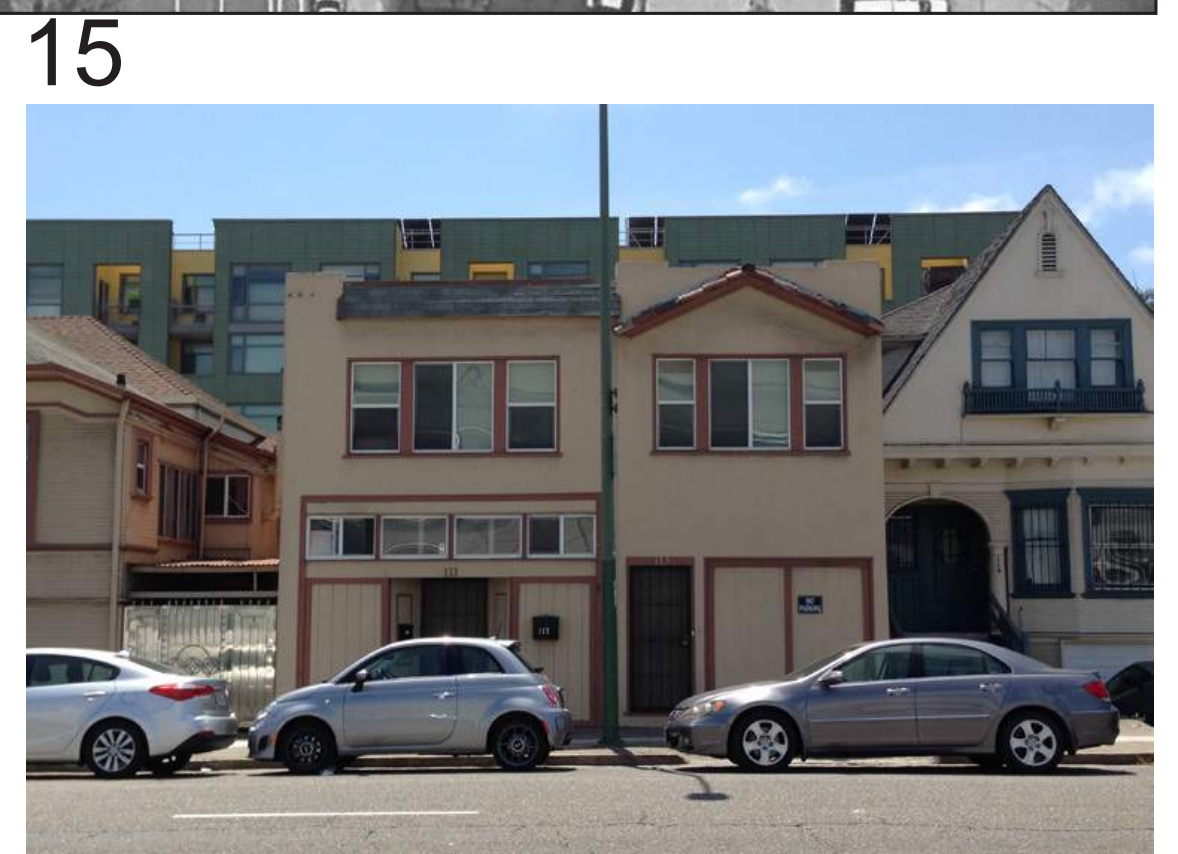
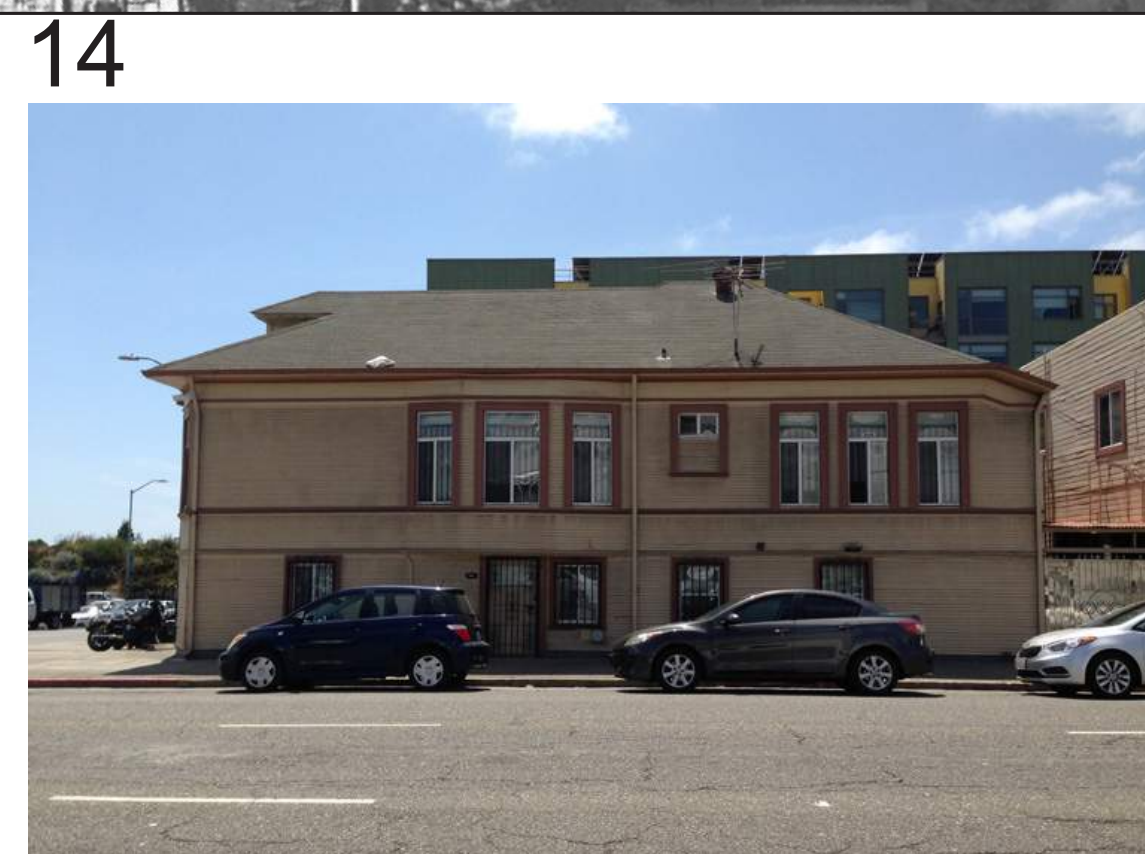
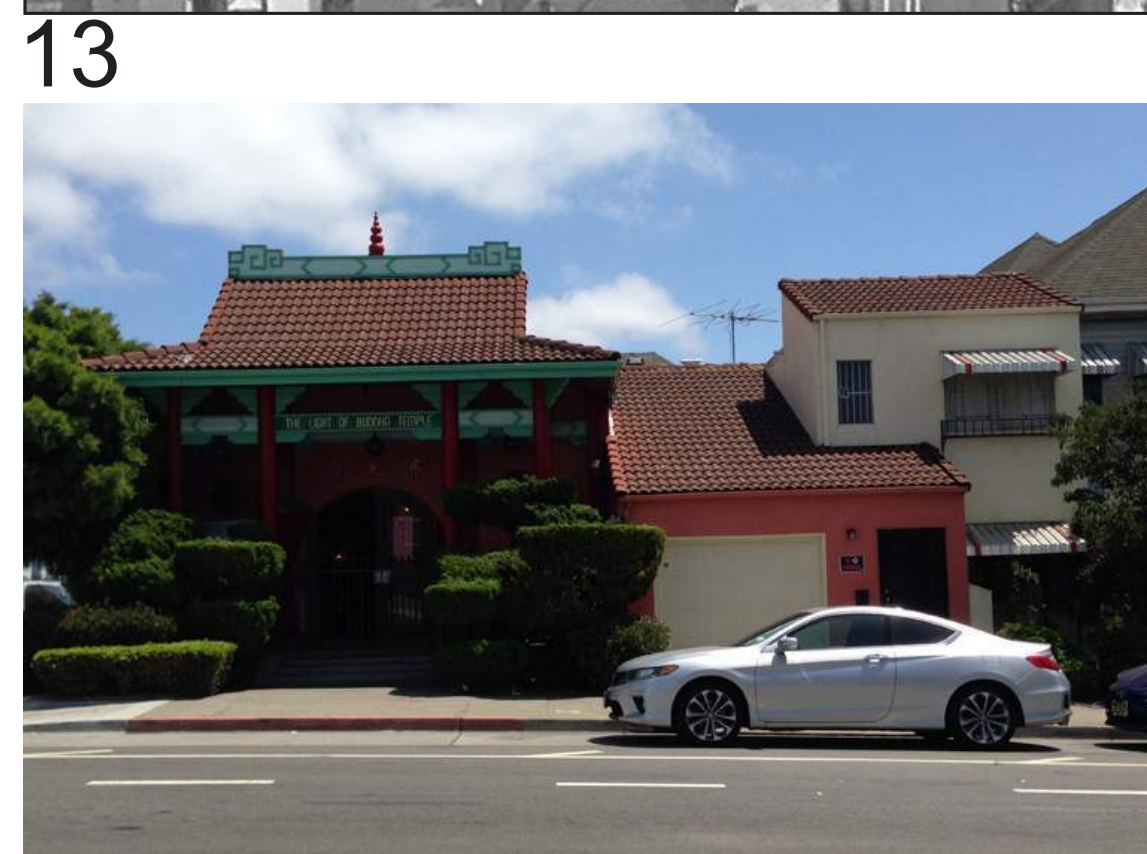
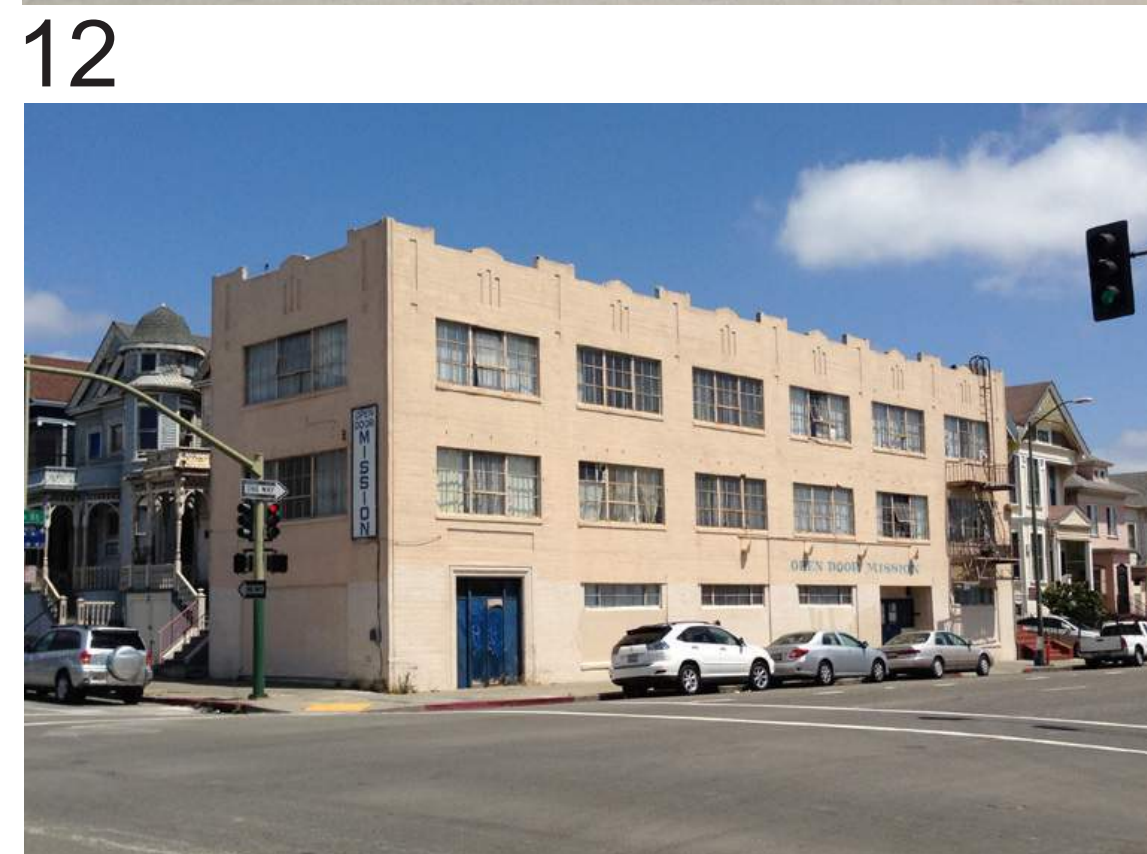
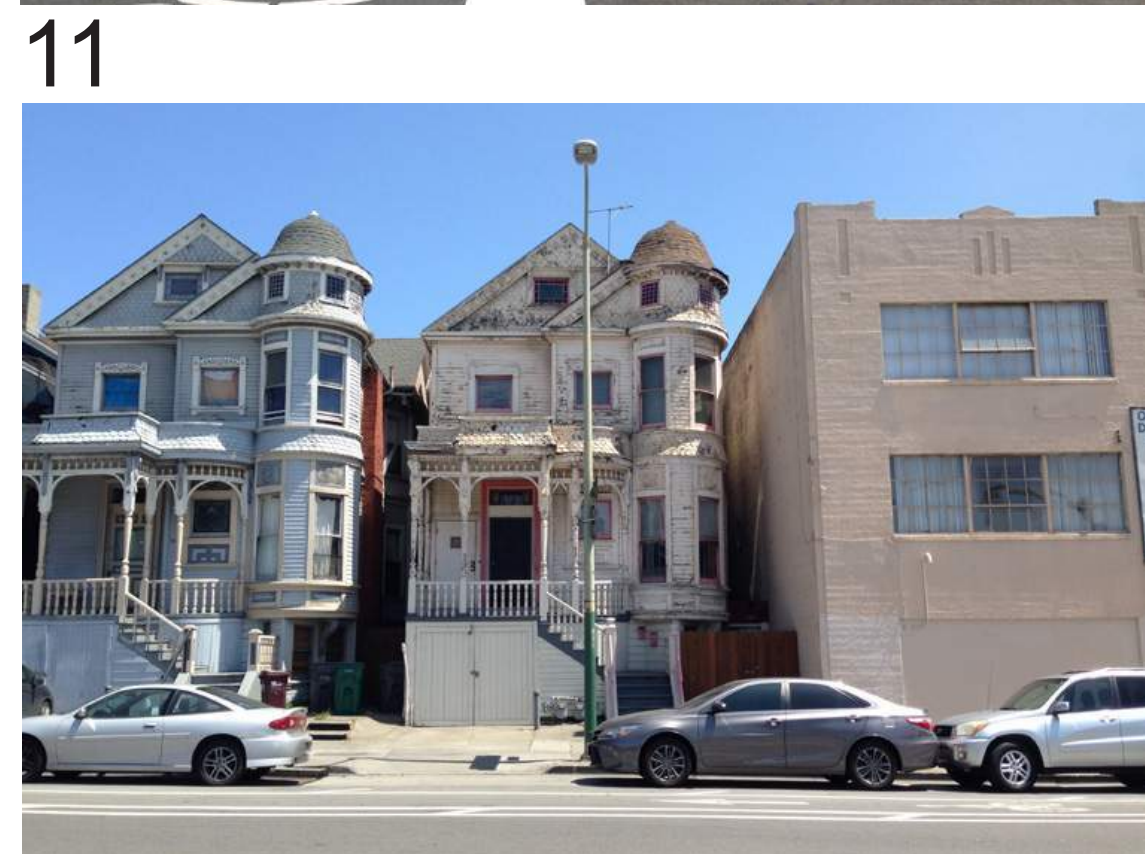
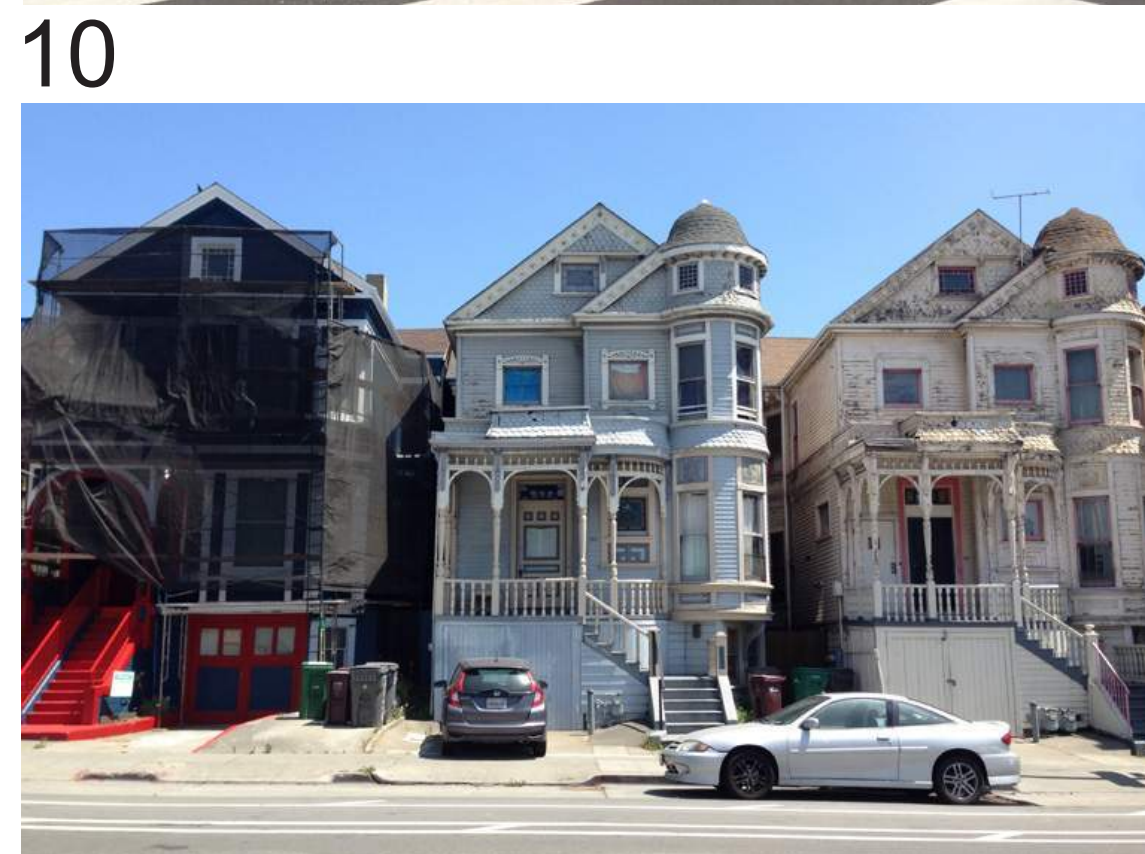
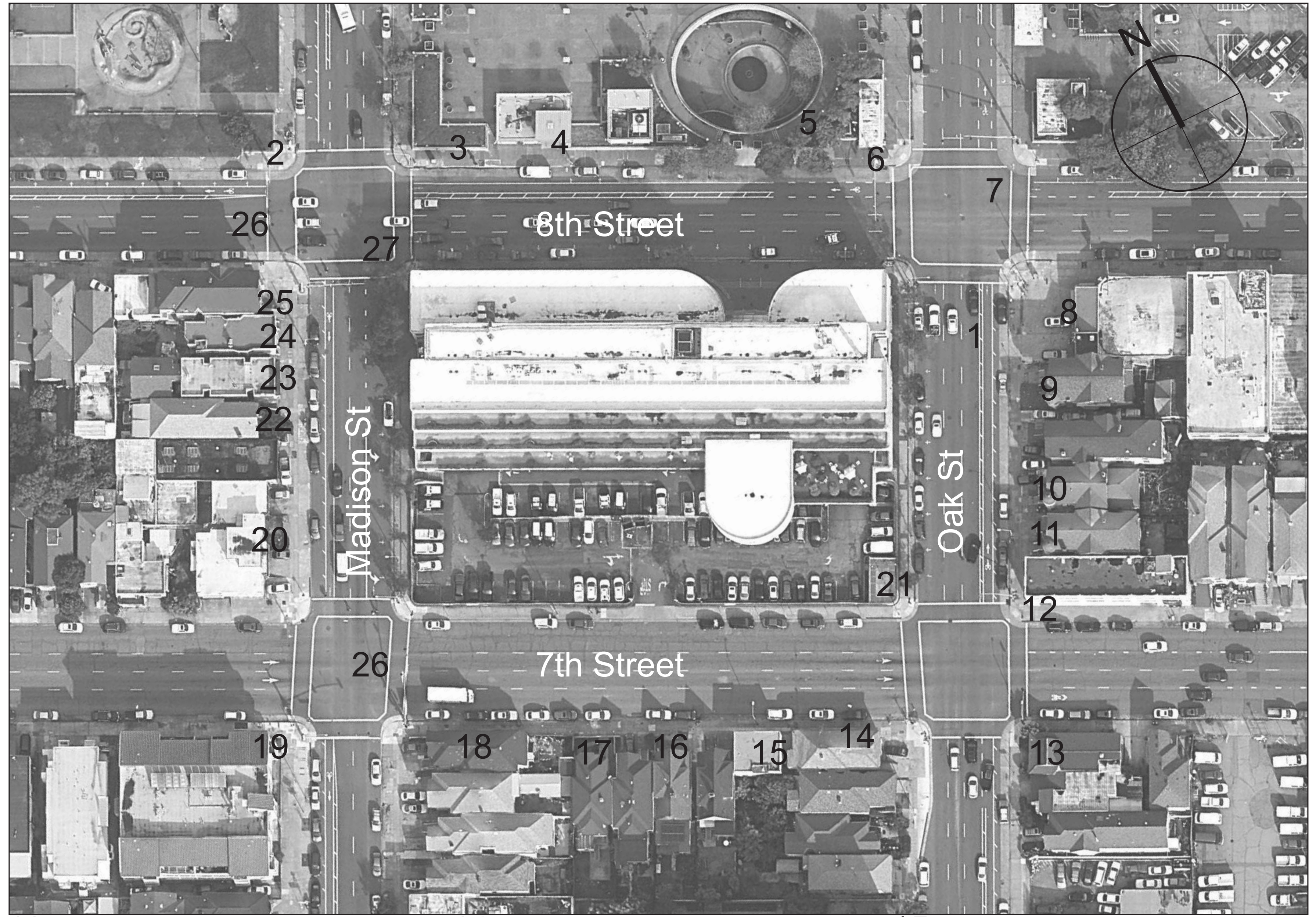
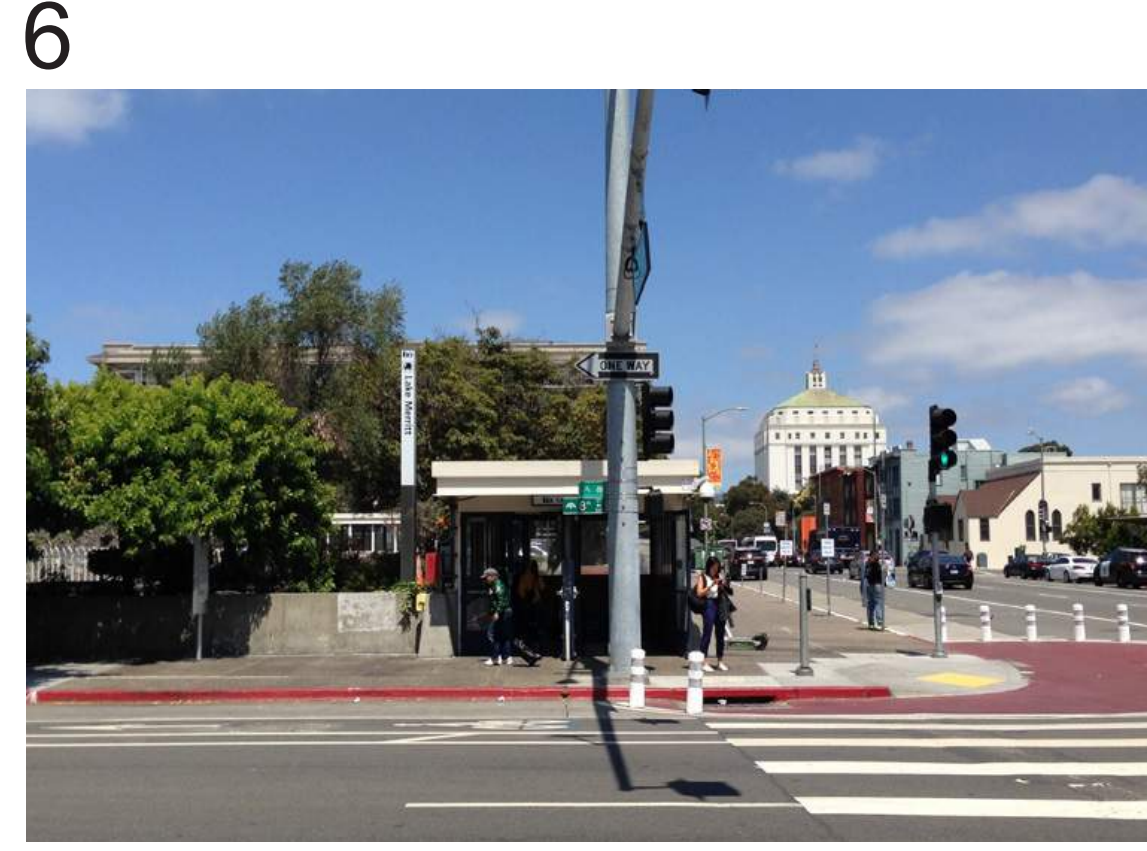
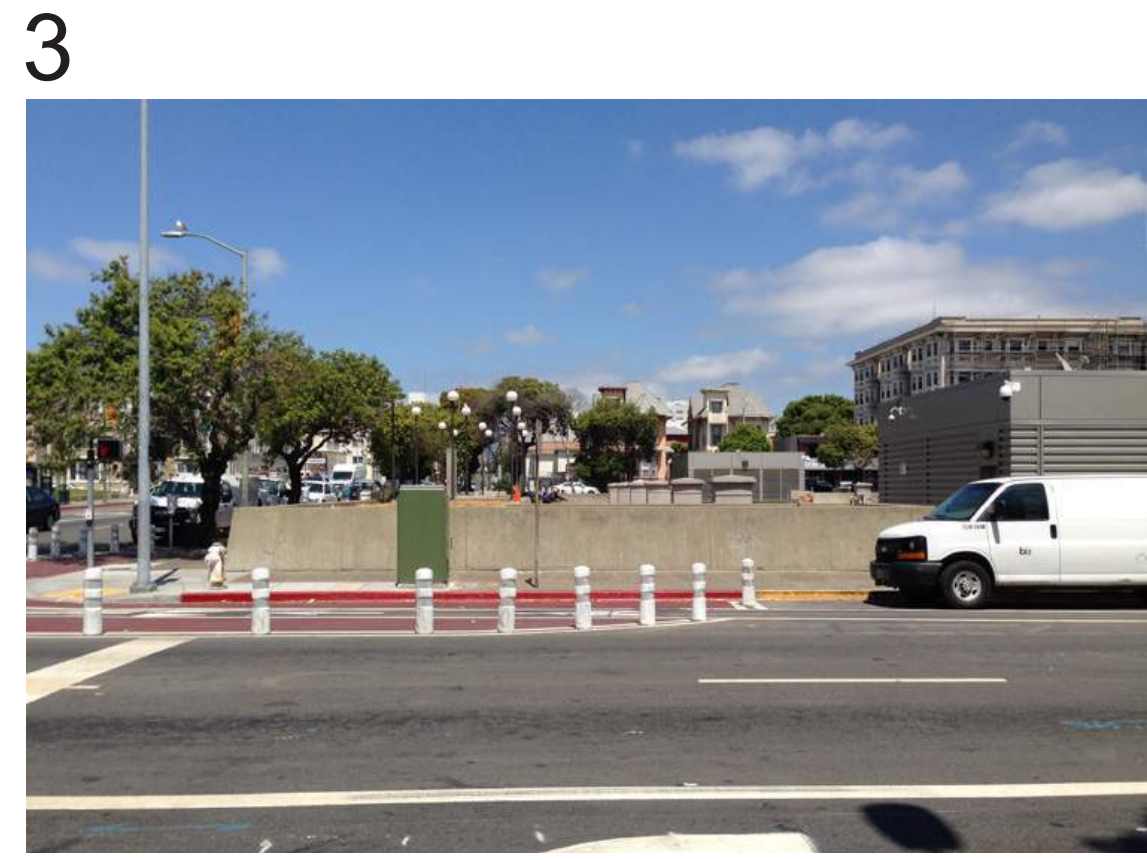
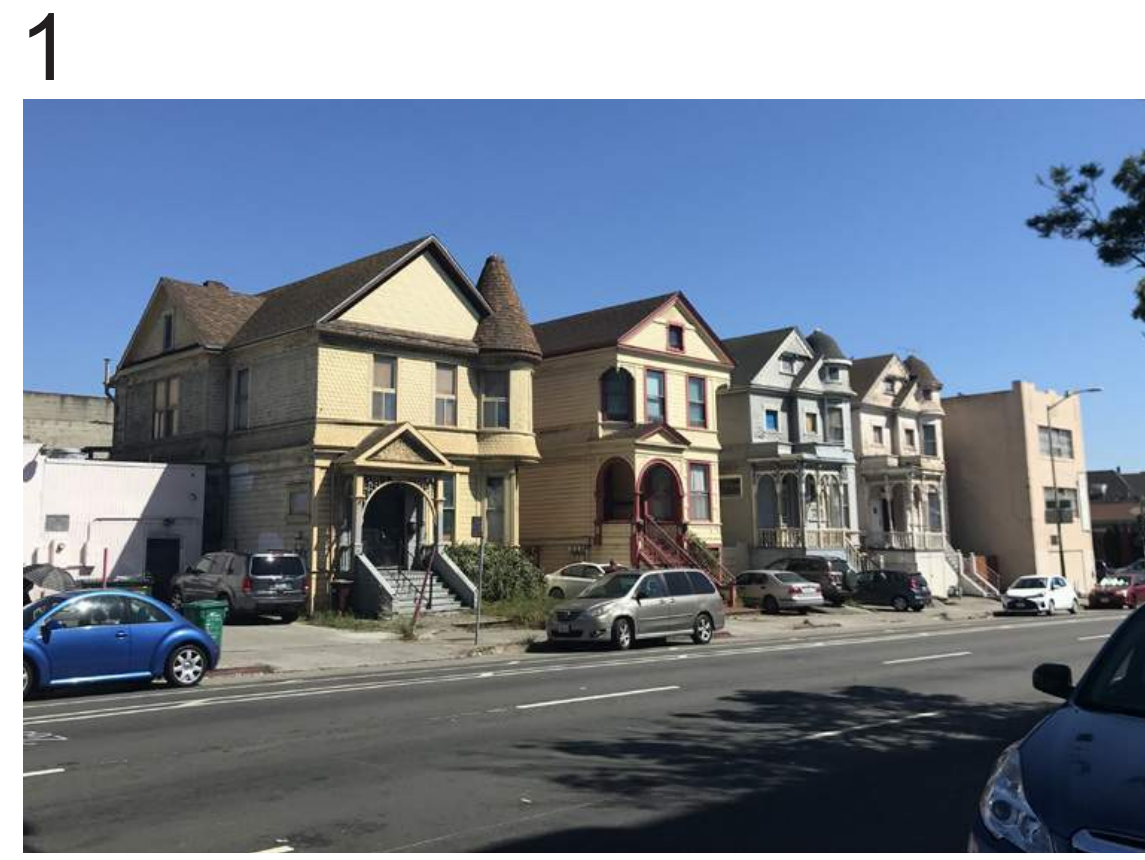
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PRELIMINARY - Not for Construction -



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

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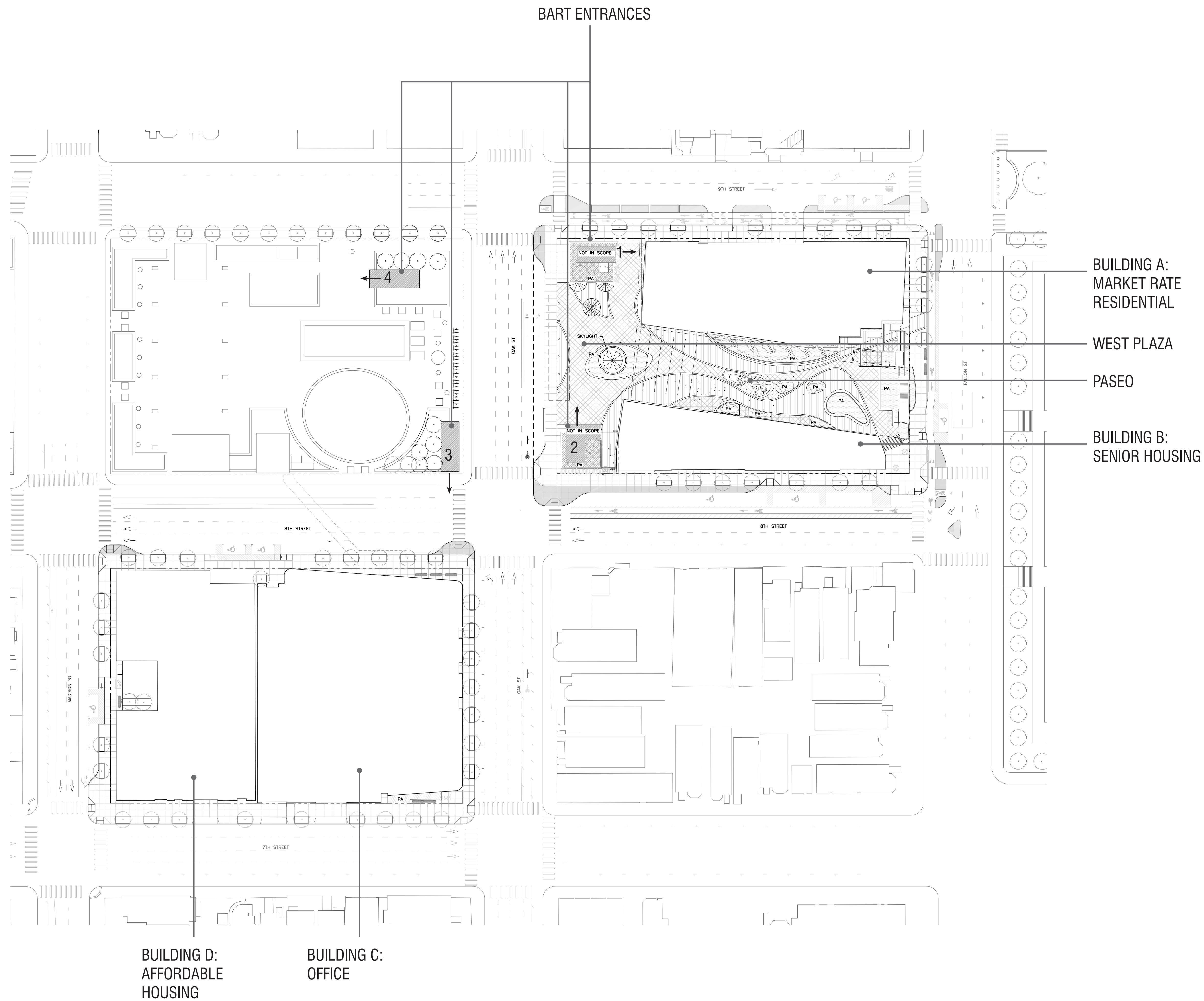
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**SITE PHOTOS**

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PRELIMINARY - Not for Construction -



- BUILDING A:  
MARKET RATE  
RESIDENTIAL
- WEST PLAZA
- PASEO
- BUILDING B:  
SENIOR HOUSING

- BUILDING D:  
AFFORDABLE  
HOUSING
- BUILDING C:  
OFFICE

**NOTE:**  
 "BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the the project as presened is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

Baseline design and construction in the West Plaza is fully funded by the developer.

Existing bus shelter, headhouses, and skylight to remain.



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

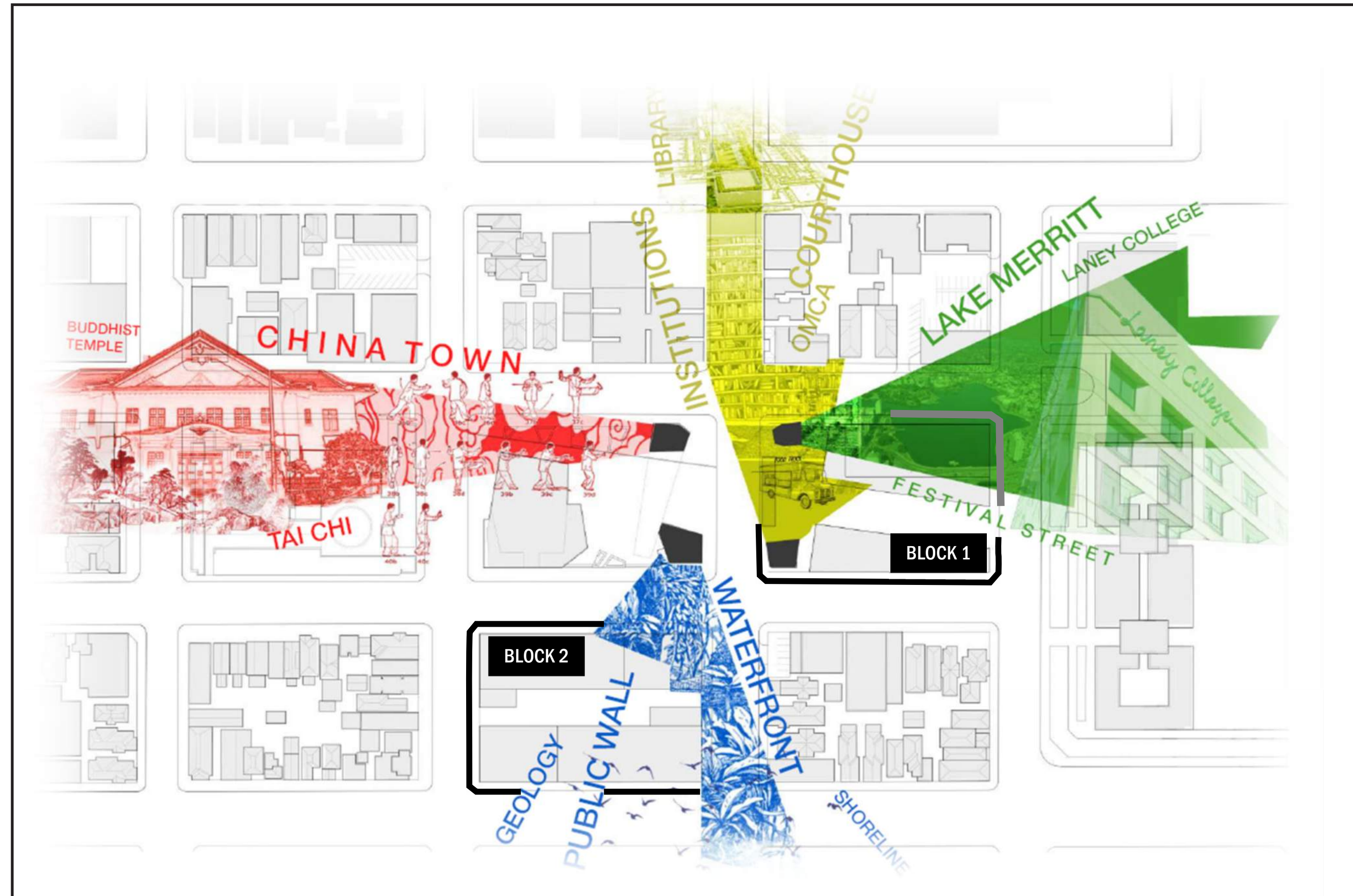
ISSUE SCHEDULE		
50 SD - HORIZONTAL FDP		10/29/2021
100 SD - HORIZONTAL FDP		12/17/2021
100 SD - HORIZONTAL FDP		05/02/2022
100 SD - HORIZONTAL FDP2		06/16/2022

REVISION SCHEDULE	
NO.   ISSUE	DATE

JOB NUMBER:	1808
DRAWN BY:	Author
CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx
SCALE:	As indicated
PHASE:	1
BLOCK:	1 AND 2
TITLE:	

**AREA PLAN**

SHEET:  
**L1.0**



BART STATION CARDINAL DIRECTIONS AND WAYFINDING



CONNECT CULTURE AND NATURE

### Connecting Transit to Place

Transit oriented development can enhance the arrival of riders and be a tool to connect people to places, cultures, and ecology. The proposed open space design for the Lake Merritt BART Redevelopment connects this important station with the existing communities and institutions surrounding the station.

Connections to the Oakland Museum of Art, Laney College, the Waterfront, and most importantly Chinatown are the basis of the design organization and will be the inspiration for culture, art, and planting selections. The development of an active pedestrian environment on all sides of the new buildings were designed into the basic fabric of the site. The ground floor and open space for the project is integrated into a complementary whole. Community serving uses and activation on the ground floor will provide eyes on the street as well as new community resources in the new development.

Multi-modal connections and the importance of space planning for the many new and evolving forms of transportation are designed into the base condition of the project with maximum flexibility for the anticipated changes of the future.

Careful consideration of existing BART infrastructure was integrated into the overall project. Advance planning for maintenance, access, and the significant responsibilities of BART Police and other staff were highlighted and designed into the new project to increase flexibility and improve security.



TIE INTO CHINATOWN THROUGH OPEN SPACE



STAMP:

ISSUE SCHEDULE		
50 SD - HORIZONTAL FDP		10/29/2021
100 SD - HORIZONTAL FDP		12/17/2021
100 SD - HORIZONTAL FDP		05/02/2022
100 SD - HORIZONTAL FDP#2		06/16/2022

REVISION SCHEDULE	
NO.	ISSUE DATE

JOB NUMBER:	1808
DRAWN BY:	Author
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ISSUE DATE:	xx/xx/xxxx
SCALE:	As indicated
PHASE:	1
BLOCK:	1

TITLE:  
**URBAN PLANNING**

SHEET:  
**L1.1**



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE		
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100 SD - HORIZONTAL FDP#2	06/16/2022	

REVISION SCHEDULE	
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JOB NUMBER:	1808
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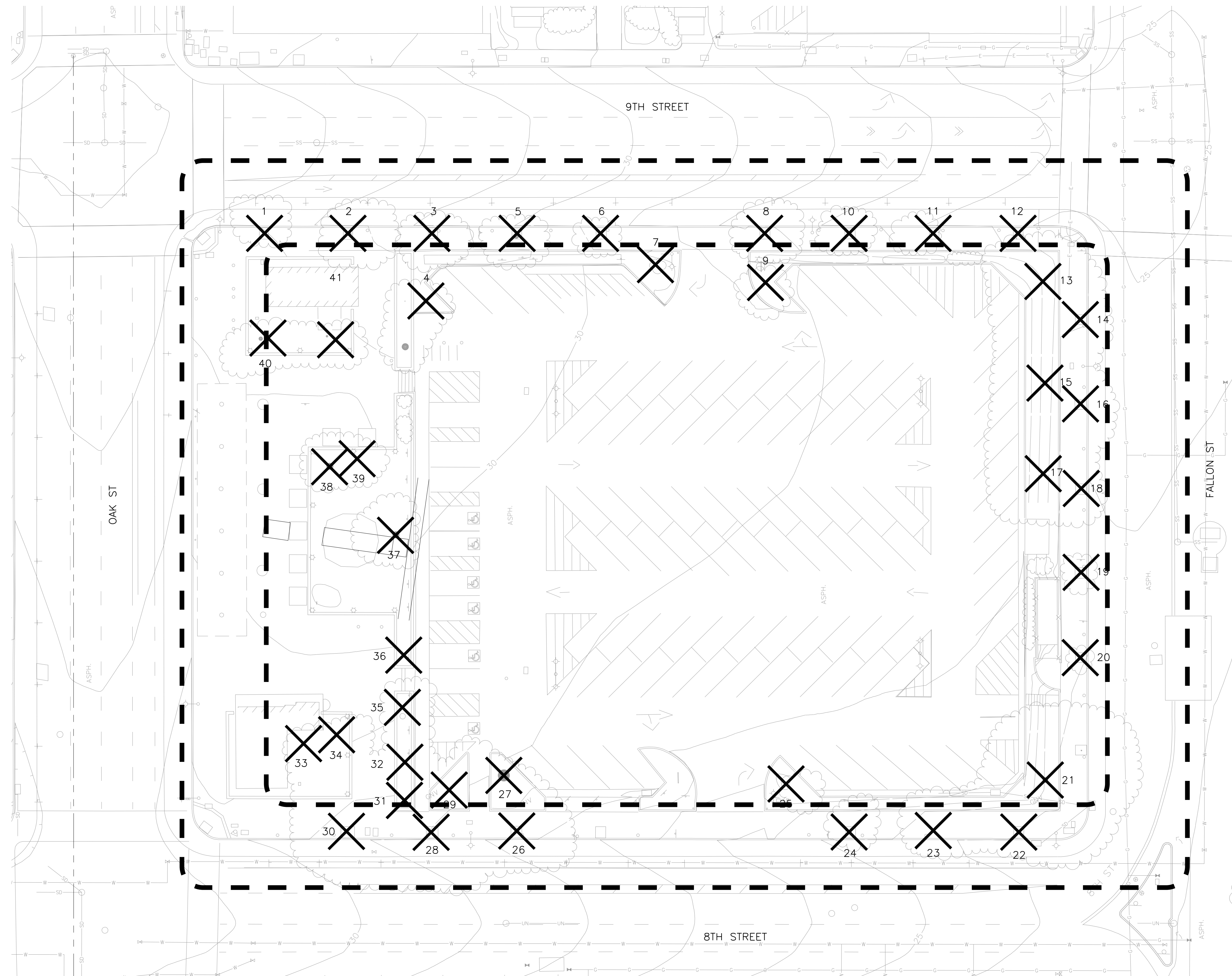
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BLOCK:	1

TITLE:  
**EXISTING TREES**

SHEET:

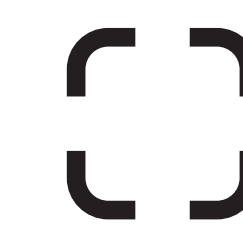
**L2.0**

PRELIMINARY - Not for Construction -



Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
1	13"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
2	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
3	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
4	18"	Yes	<i>Olea europaea</i>	Olive Tree
5	24"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
6	6"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
7	10"	Yes	<i>Olea europaea</i>	Olive Tree
8	12"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
9	14"	Yes	<i>Olea europaea</i>	Olive Tree
10	8"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
11	7"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
12	14"	Yes	STUMP	STUMP
13	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
14	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
15	13"	Yes	<i>Platanus racemosa</i>	California Sycamore
16	15"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
17	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
18	5"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
19	14"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
20	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
21	24"	Yes	<i>Platanus racemosa</i>	California Sycamore
22	20"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
23	10"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
24	7"	Yes	<i>Rhus lancea</i>	African Sumac
25	14"	Yes	<i>Olea europaea</i>	Olive Tree
26	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
27	51"	Yes	<i>Olea europaea</i>	Olive Tree
28	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
29	14"	Yes	<i>Olea europaea</i>	Olive Tree
30	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
31	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
32	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
33	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
34	12"	Yes	<i>Olea europaea</i>	Olive Tree
35	15"	Yes	<i>Olea europaea</i>	Olive Tree
36	9"	Yes	<i>Pittosporum undulatum</i>	Victorian box
37	30"	Yes	<i>Olea europaea</i>	Olive Tree
38	30"	Yes	<i>Olea europaea</i>	Olive Tree
39	13"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
40	25"	Yes	<i>Olea europaea</i>	Olive Tree
41	20"	Yes	<i>Pittosporum undulatum</i>	Victorian box

LEGEND



30' OFFSET FROM CONSTRUCTION (LOW) FOR TREE ID AND PRESERVATION PURPOSES

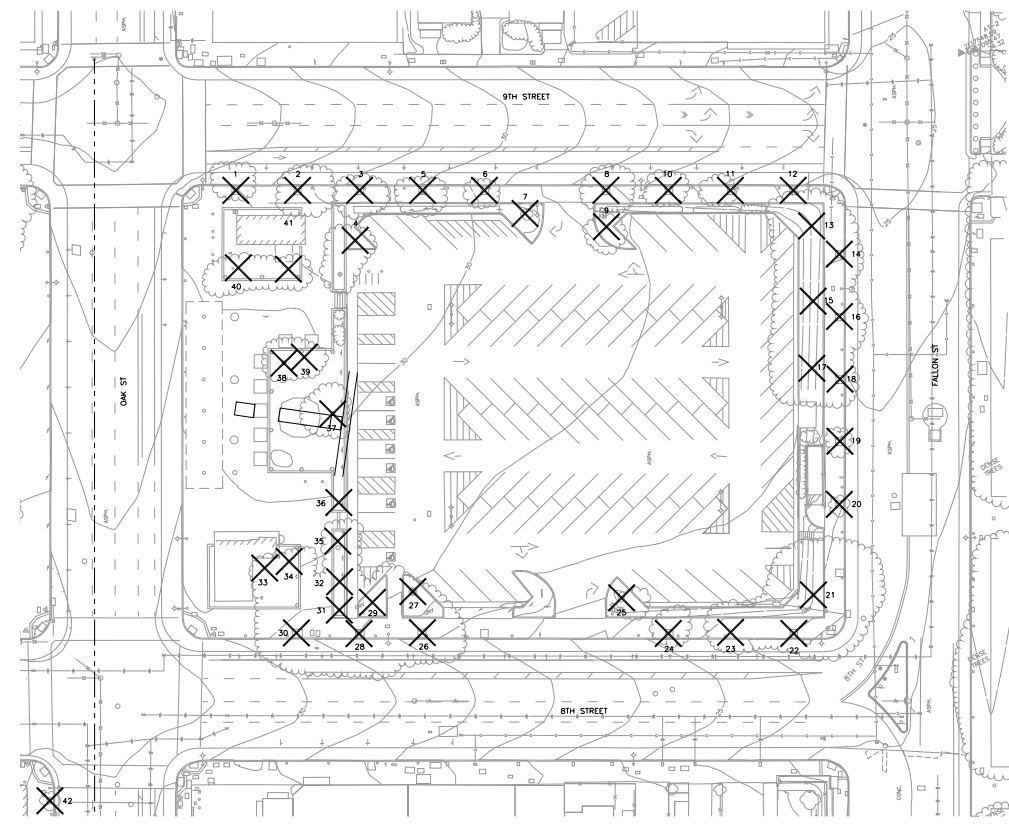
NOTE:

The following are Protected Trees according to City Ordinance Chapter 12.36. PROTECTED TREES:  
A. Any Coast Live Oak tree that is larger than 4 inches DBH. B. Any tree (except Eucalyptus) that is larger than 9 inches DBH. (Eucalyptus trees and up to 5 Monterey Pines per acre are not considered Protected Trees under this section. Monterey Pines must be inspected and verified by the Public Works Agency- Tree Division prior to their removal. C. Any tree of any size located in the public right-of-way (including street trees).

NOTE ON REMOVAL:

1. We are recommending removing 40 protected trees because of overall poor structure and poor health.
2. To meet the needs of a multi modal transit center, new sidewalks designs, bike paths, pick up and drop off, ADA multi-modal drop off will all require the relocation and demolition of sidewalks. This will disrupt the roots and will create larger soil volume areas.
3. The master plans aim is to make a cohesive neighborhood identity (coordinating with Madison Sq. Park, Bart plaza (TOF), and Block 1 + 2) and to have a cohesive plant palette.
4. See page L0.8 and L0.81 for new tree planting. Intent of design to provide street trees within the scope of the project.





Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
1	13"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
2	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
3	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
4	18"	Yes	<i>Olea europaea</i>	Olive Tree
5	24"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
6	6"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
7	10"	Yes	<i>Olea europaea</i>	Olive Tree
8	12"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
9	14"	Yes	<i>Olea europaea</i>	Olive Tree
10	8"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
11	7"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
12	14"	Yes	STUMP	STUMP
13	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
14	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
15	13"	Yes	<i>Platanus racemosa</i>	California Sycamore
16	15"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
17	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
18	5"	Yes	<i>Afracarpus falcatus</i>	African Fir Pine
19	14"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
20	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
21	24"	Yes	<i>Platanus racemosa</i>	California Sycamore
22	20"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
23	10"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
24	7"	Yes	<i>Rhus lancea</i>	African Sumac
25	14"	Yes	<i>Olea europaea</i>	Olive Tree
26	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
27	51"	Yes	<i>Olea europaea</i>	Olive Tree
28	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
29	14"	Yes	<i>Olea europaea</i>	Olive Tree
30	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
31	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
32	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
33	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
34	12"	Yes	<i>Olea europaea</i>	Olive Tree
35	15"	Yes	<i>Olea europaea</i>	Olive Tree
36	9"	Yes	<i>Pittosporum undulatum</i>	Victorian box
37	30"	Yes	<i>Olea europaea</i>	Olive Tree
38	30"	Yes	<i>Olea europaea</i>	Olive Tree
39	13"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
40	25"	Yes	<i>Olea europaea</i>	Olive Tree
41	20"	Yes	<i>Pittosporum undulatum</i>	Victorian box



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP#2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx

SCALE: As indicated  
PHASE: 1  
BLOCK: 1

TITLE:  
**EXISTING TREES**

SHEET:  
**L2.1**

PRELIMINARY - Not for Construction -



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

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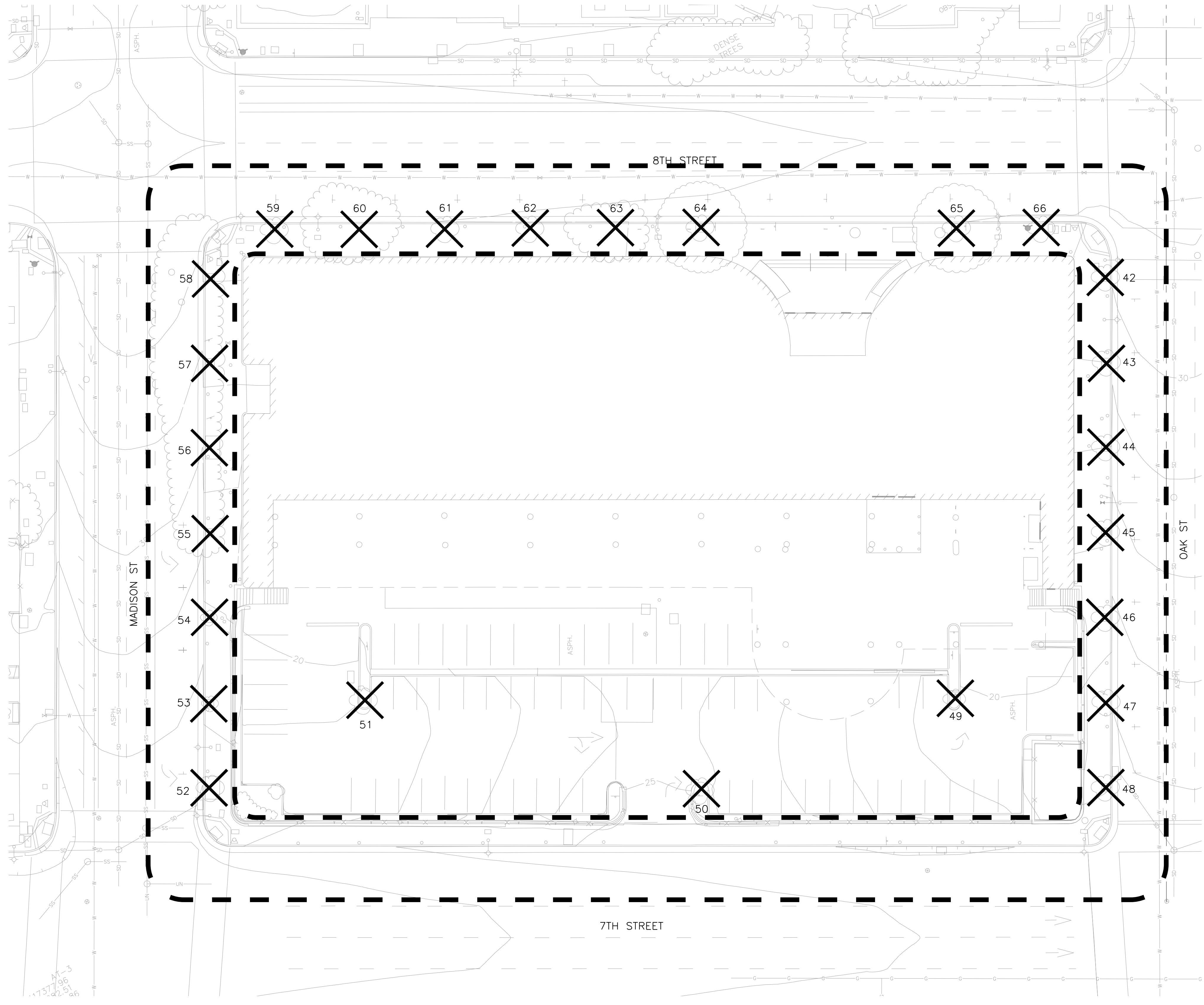
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BLOCK: 2

TITLE:  
**EXISTING TREES**

SHEET:

**L2.2**

PRELIMINARY - Not for Construction -

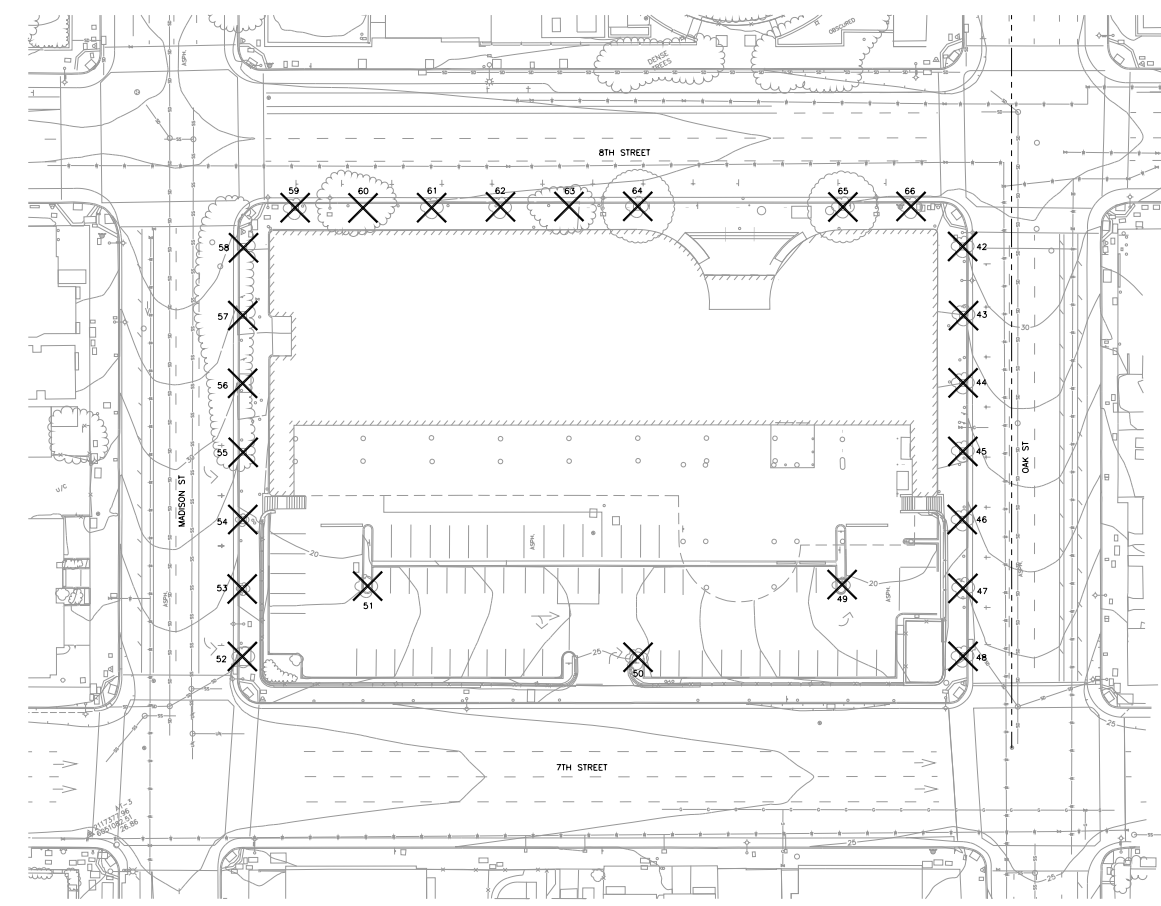


Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
42	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
43	13"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
44	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
45	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
46	3"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
47	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
48	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
49	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
50	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
51	7"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
52	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
53	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
54	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
55	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
56	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
57	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
58	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
59	8"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
60	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
61	6"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
62	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
63	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
64	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
65	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
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LEGEND  
 30' OFFSET FROM CONSTRUCTION (LOW) FOR TREE ID AND PRESERVATION PURPOSES

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47	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
48	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
49	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
50	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
51	7"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
52	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
53	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
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55	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
56	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
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58	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
59	8"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
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62	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
63	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
64	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
65	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
66	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash



# LUMA

## LAKE MERRITT BART HORIZONTAL FDP Oakland, CA 94607

STAMP:

### ISSUE SCHEDULE

NO.	ISSUE	DATE
50	SD - HORIZONTAL FDP	10/29/2021
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100	SD - HORIZONTAL FDP	05/02/2022
100	SD - HORIZONTAL FDP#2	06/16/2022

### REVISION SCHEDULE

NO.	ISSUE	DATE
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JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx

SCALE: As indicated

PHASE: 1

BLOCK: 1 AND 2

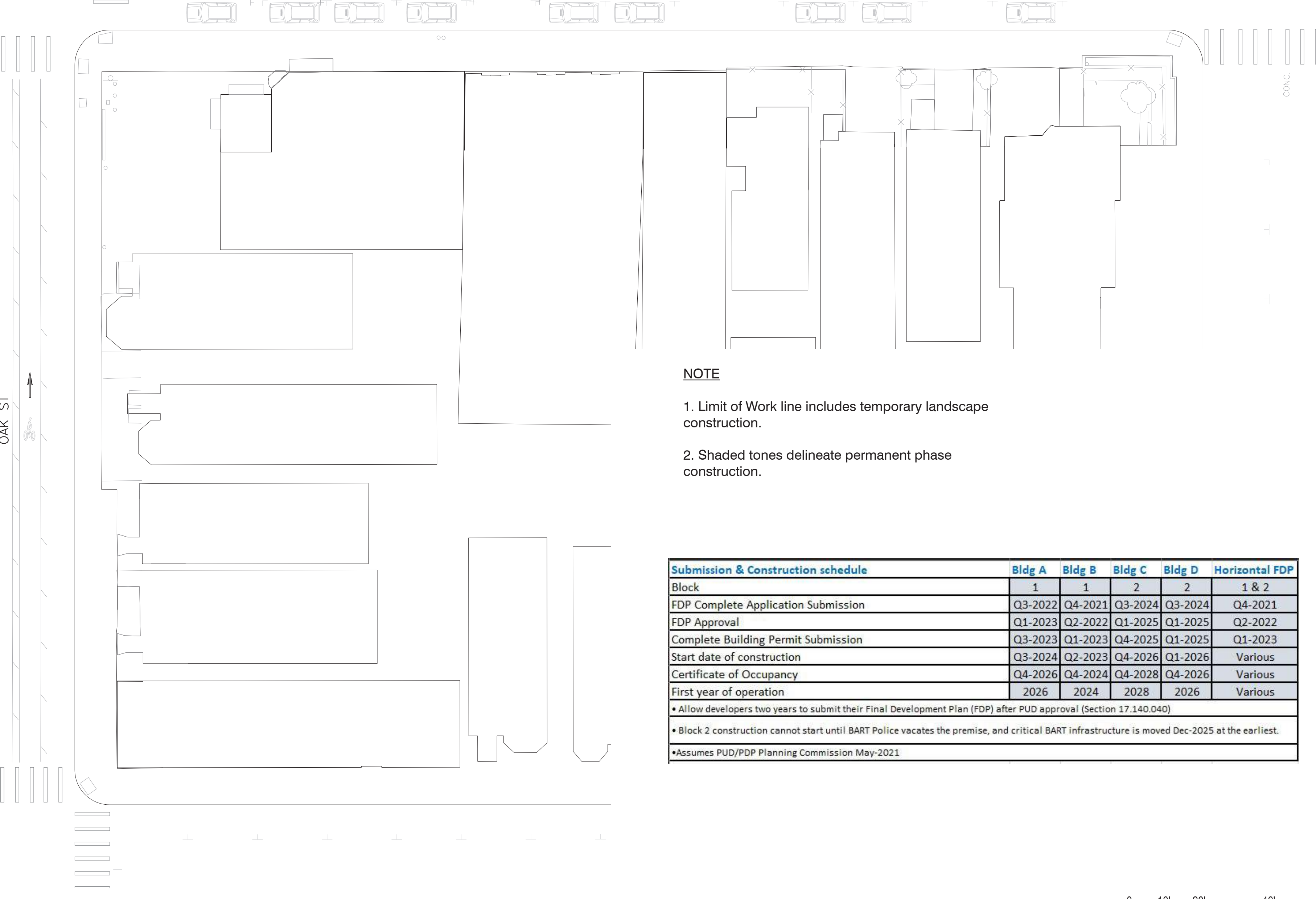
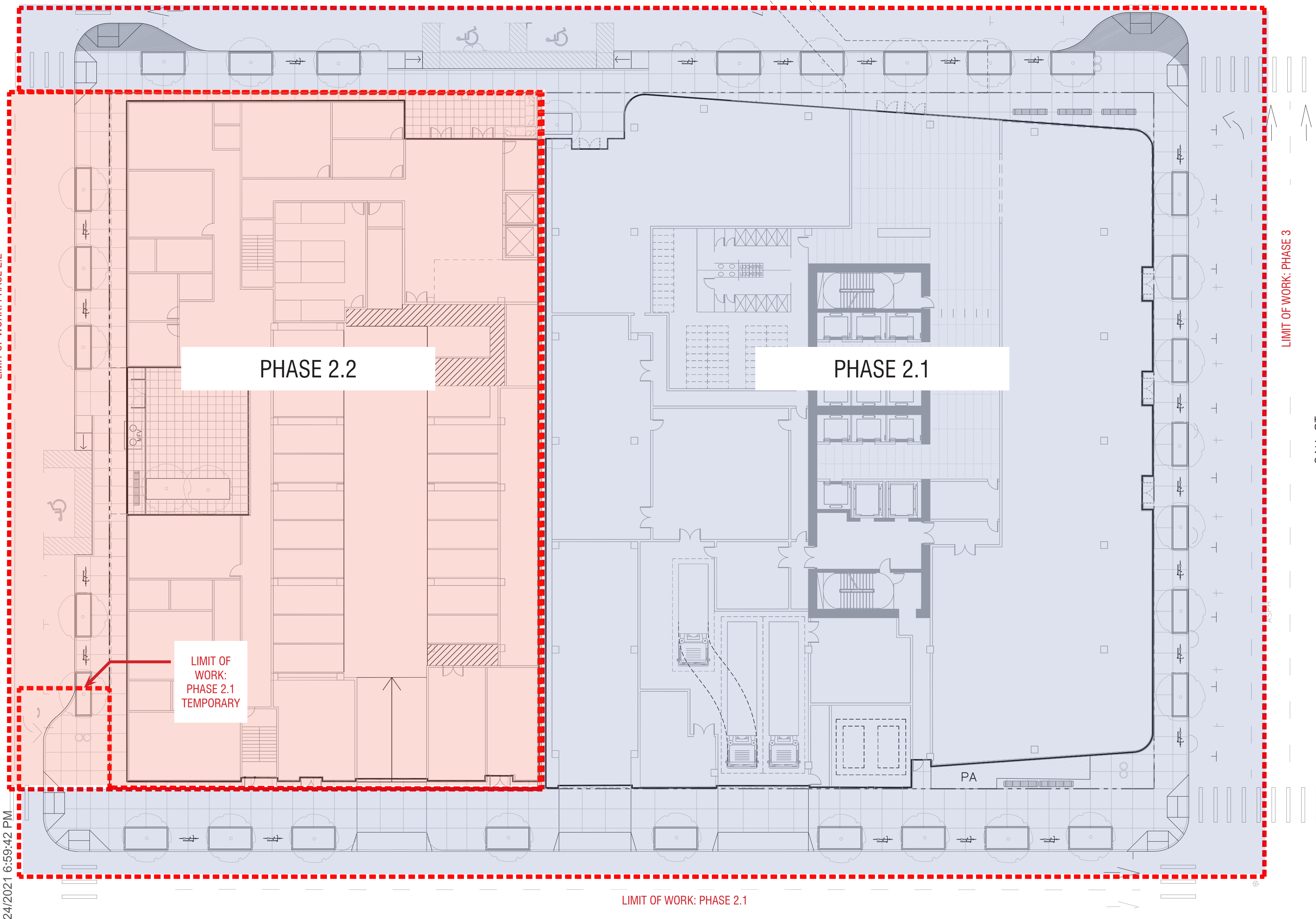
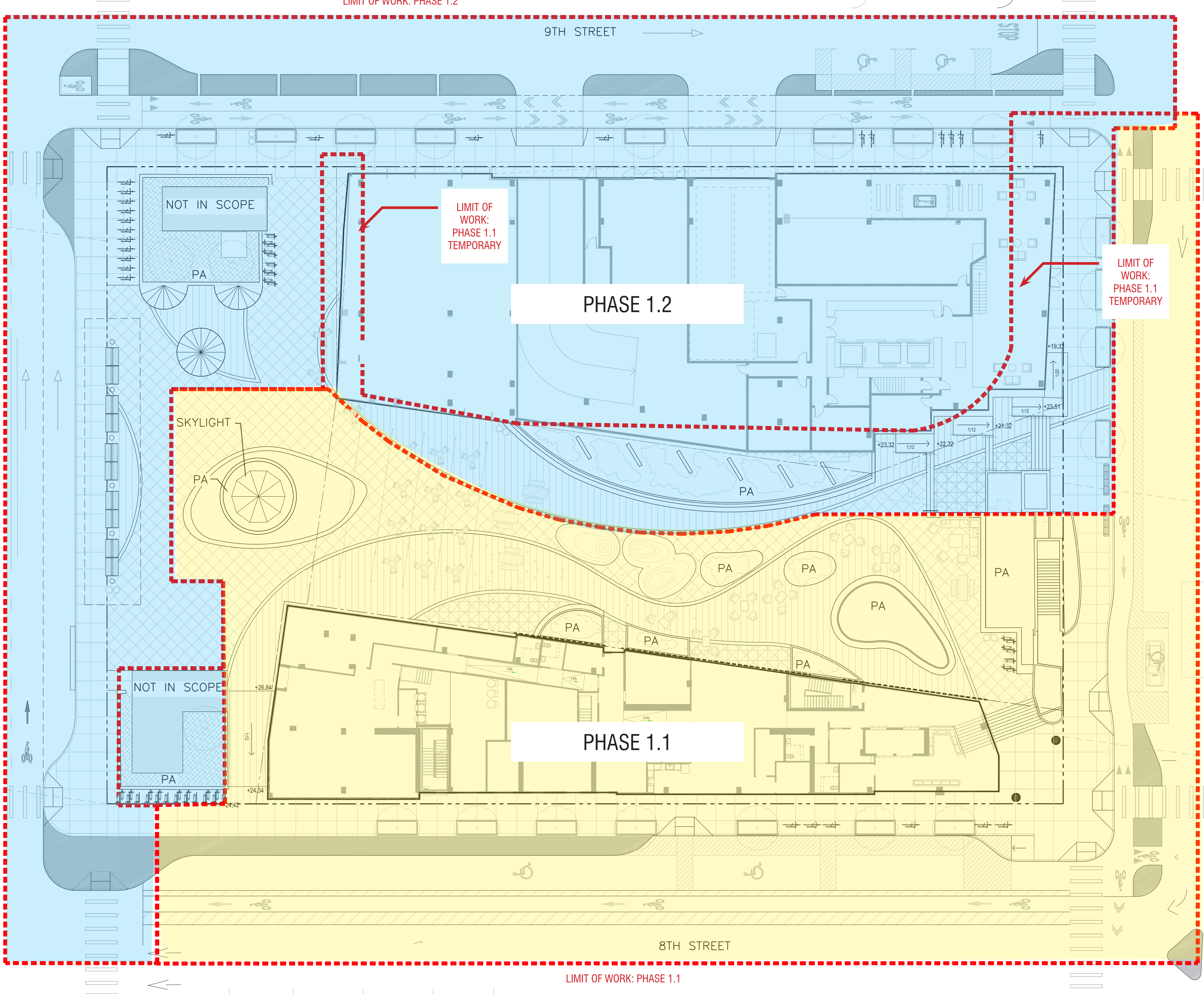
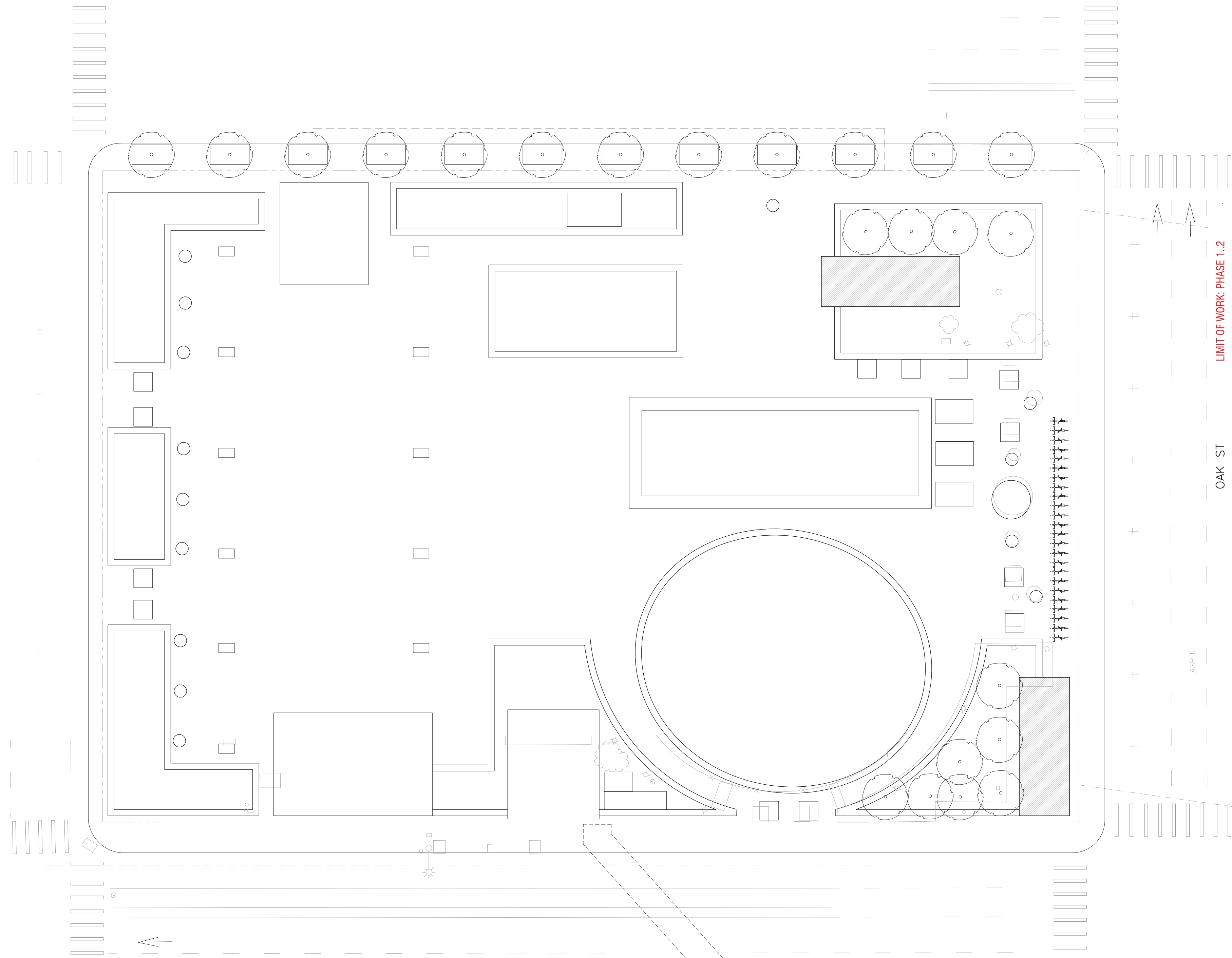
TITLE:

EXISTING TREES

SHEET:

# L2.3

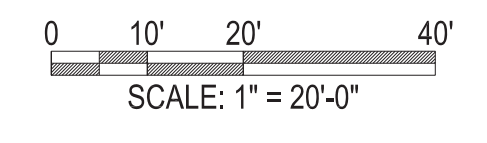
PRELIMINARY - Not for Construction -



**NOTE**

1. Limit of Work line includes temporary landscape construction.
2. Shaded tones delineate permanent phase construction.

Submission & Construction schedule	Bldg A	Bldg B	Bldg C	Bldg D	Horizontal FDP
Block	1	1	2	2	1 & 2
FDP Complete Application Submission	Q3-2022	Q4-2021	Q3-2024	Q3-2024	Q4-2021
FDP Approval	Q1-2023	Q2-2022	Q1-2025	Q1-2025	Q2-2022
Complete Building Permit Submission	Q3-2023	Q1-2023	Q4-2025	Q1-2025	Q1-2023
Start date of construction	Q3-2024	Q2-2023	Q4-2026	Q1-2026	Various
Certificate of Occupancy	Q4-2026	Q4-2024	Q4-2028	Q4-2026	Various
First year of operation	2026	2024	2028	2026	Various
<ul style="list-style-type: none"> <li>• Allow developers two years to submit their Final Development Plan (FDP) after PUD approval (Section 17.140.040)</li> <li>• Block 2 construction cannot start until BART Police vacates the premise, and critical BART infrastructure is moved Dec-2025 at the earliest.</li> <li>• Assumes PUD/PDP Planning Commission May-2021</li> </ul>					



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

**ISSUE SCHEDULE**

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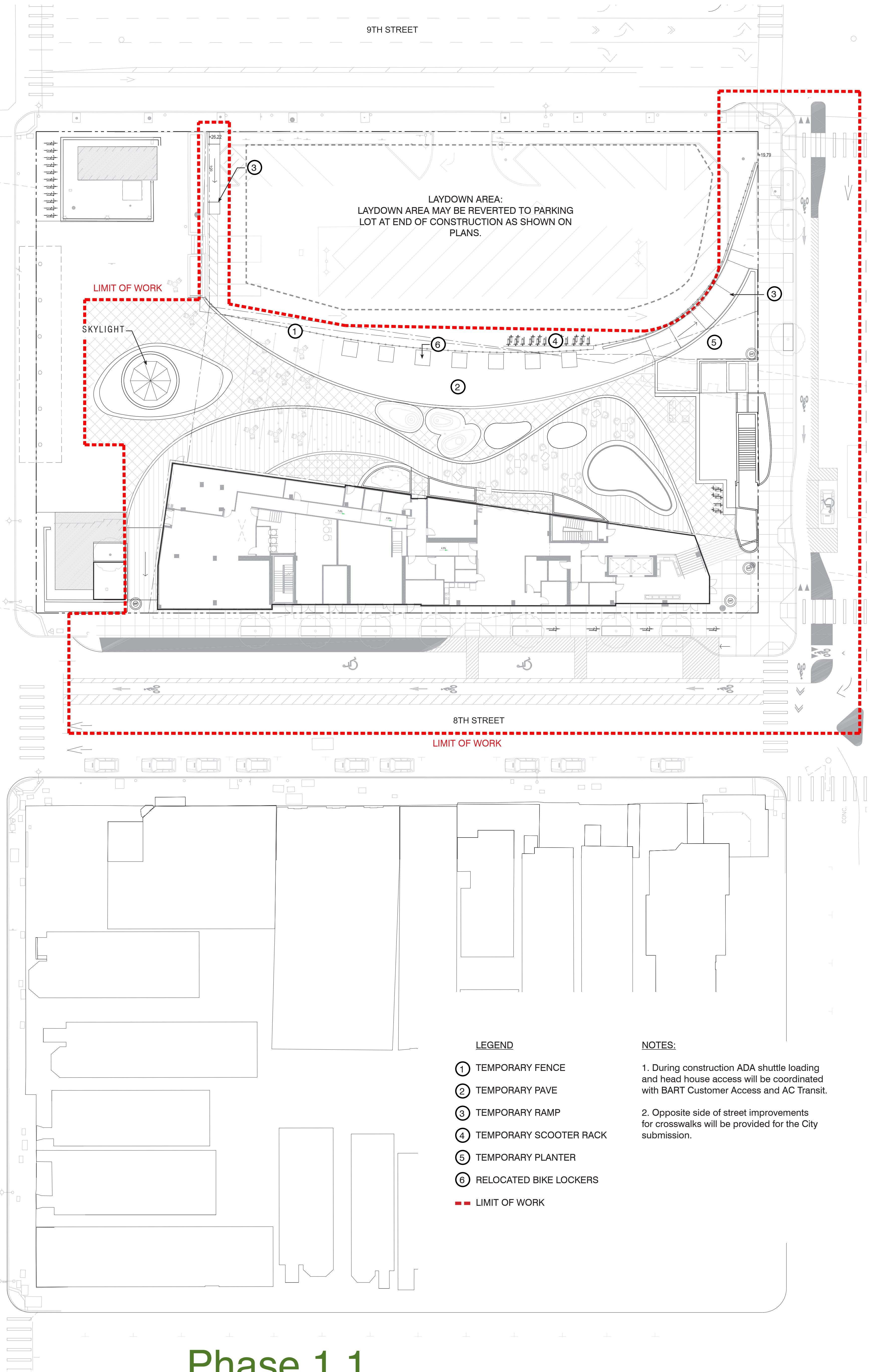
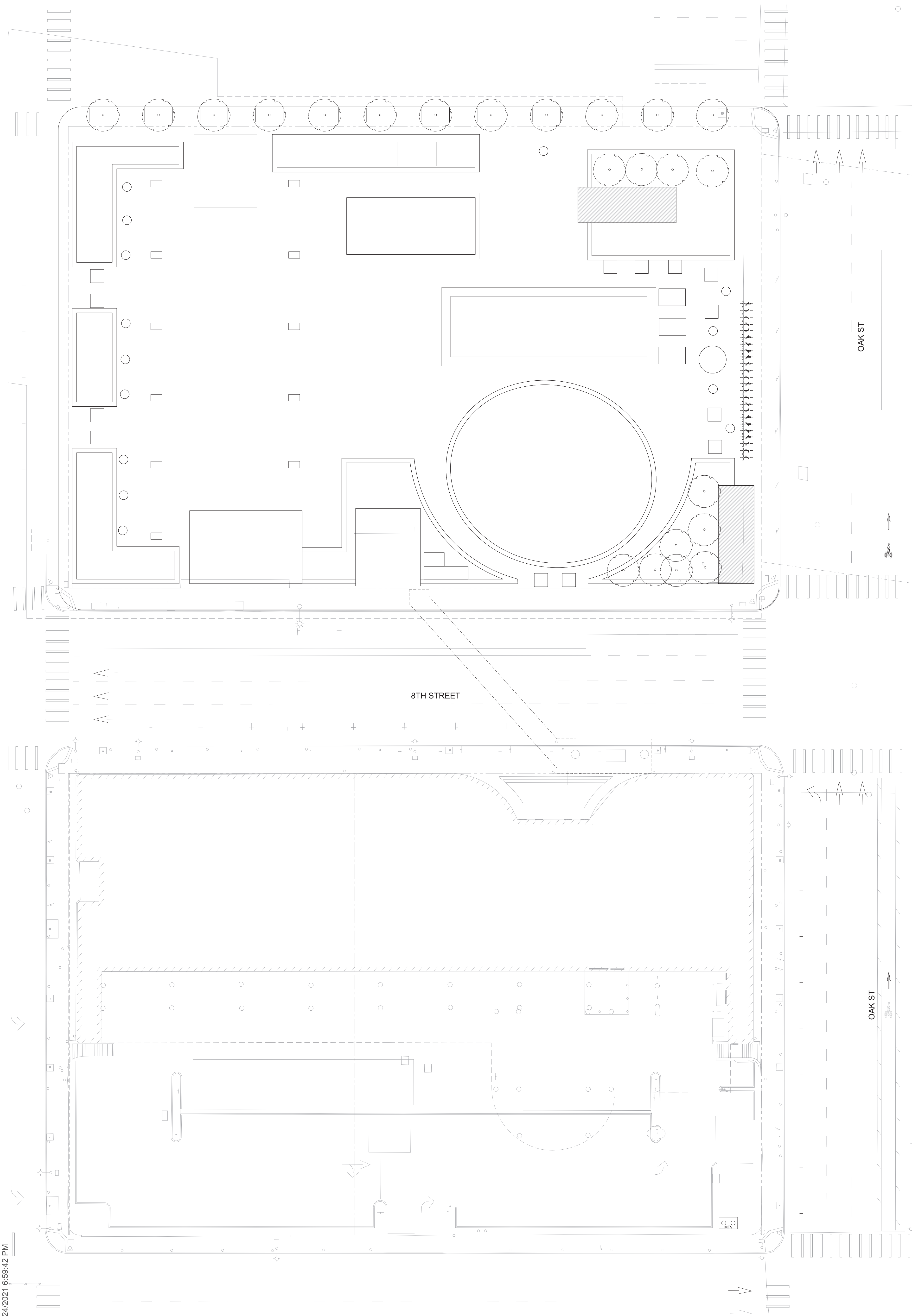
**REVISION SCHEDULE**  
NO. | ISSUE | DATE

JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx  
SCALE: As indicated  
PHASE: 1  
BLOCK: 1 AND 2  
TITLE: PHASE DIAGRAM

SHEET:  
**L3.0**

PRELIMINARY - Not for Construction -

8/24/2021 6:59:42 PM



# Phase 1.1

- LEGEND**
- ① TEMPORARY FENCE
  - ② TEMPORARY PAVE
  - ③ TEMPORARY RAMP
  - ④ TEMPORARY SCOOTER RACK
  - ⑤ TEMPORARY PLANTER
  - ⑥ RELOCATED BIKE LOCKERS
  - LIMIT OF WORK

**NOTES:**

1. During construction ADA shuttle loading and head house access will be coordinated with BART Customer Access and AC Transit.
2. Opposite side of street improvements for crosswalks will be provided for the City submission.

**EINWILLERKUEHL**  
LANDSCAPE ARCHITECTURE  
318 HARRISON STREET SUITE 301  
OAKLAND CALIFORNIA 94607  
(510) 891-1696

**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA INVESTMENT GROUP**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**BKF ENGINEERS**  
255 SHORELINE DR, SUITE 200  
REDWOOD CITY, CA 94065

**LUMA LIGHTING DESIGN**  
48 GOLDEN GATE AVENUE  
SAN FRANCISCO, CA 94102



# LAKE MERRITT BART HORIZONTAL FDP

Oakland, CA 94607

STAMP:

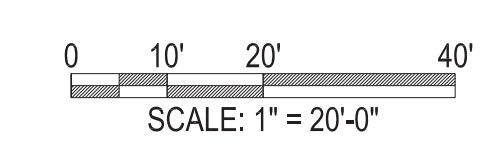
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100 SD - HORIZONTAL FDP	12/17/2021	
100 SD - HORIZONTAL FDP	05/02/2022	
100 SD - HORIZONTAL FDP2	06/16/2022	

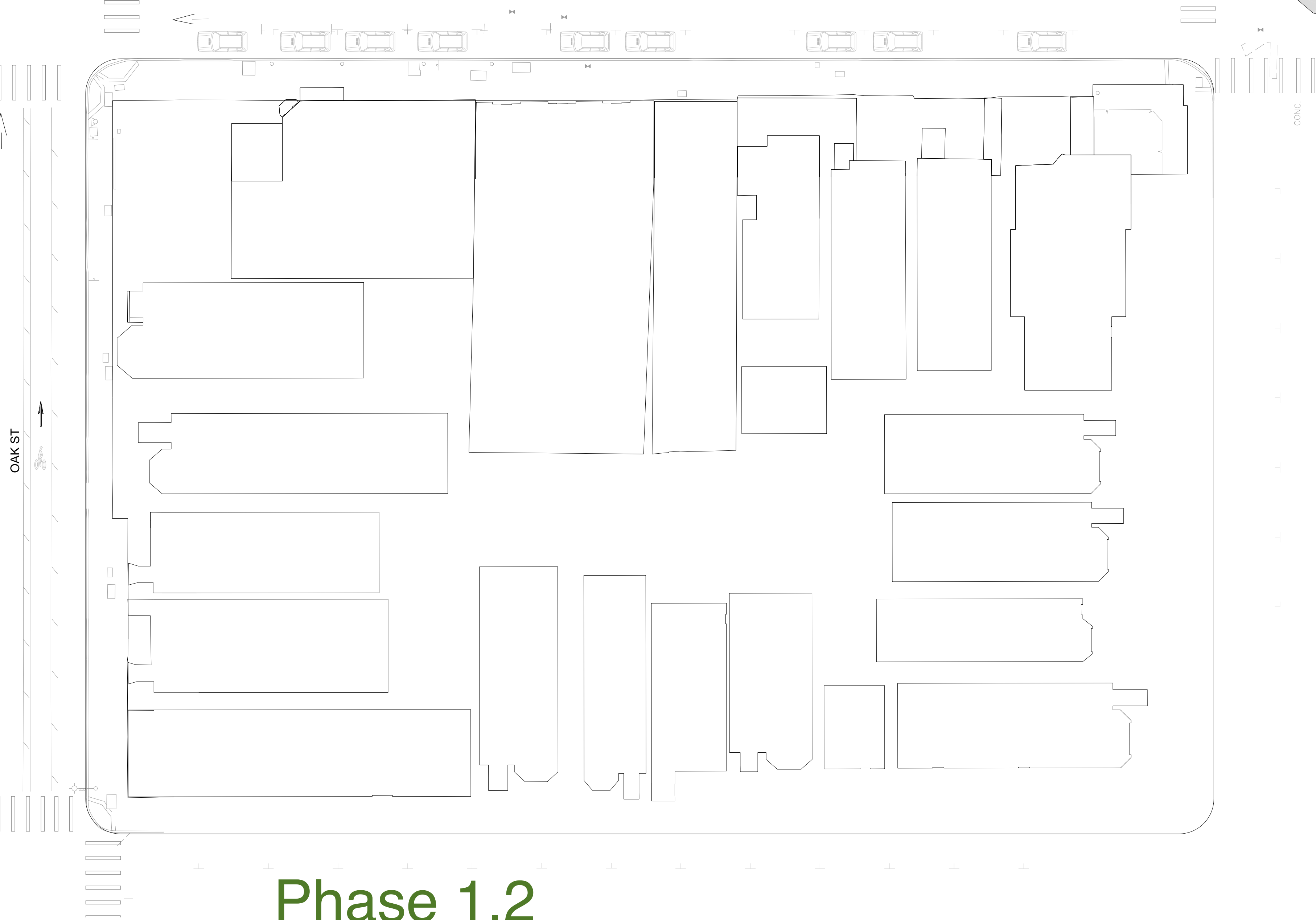
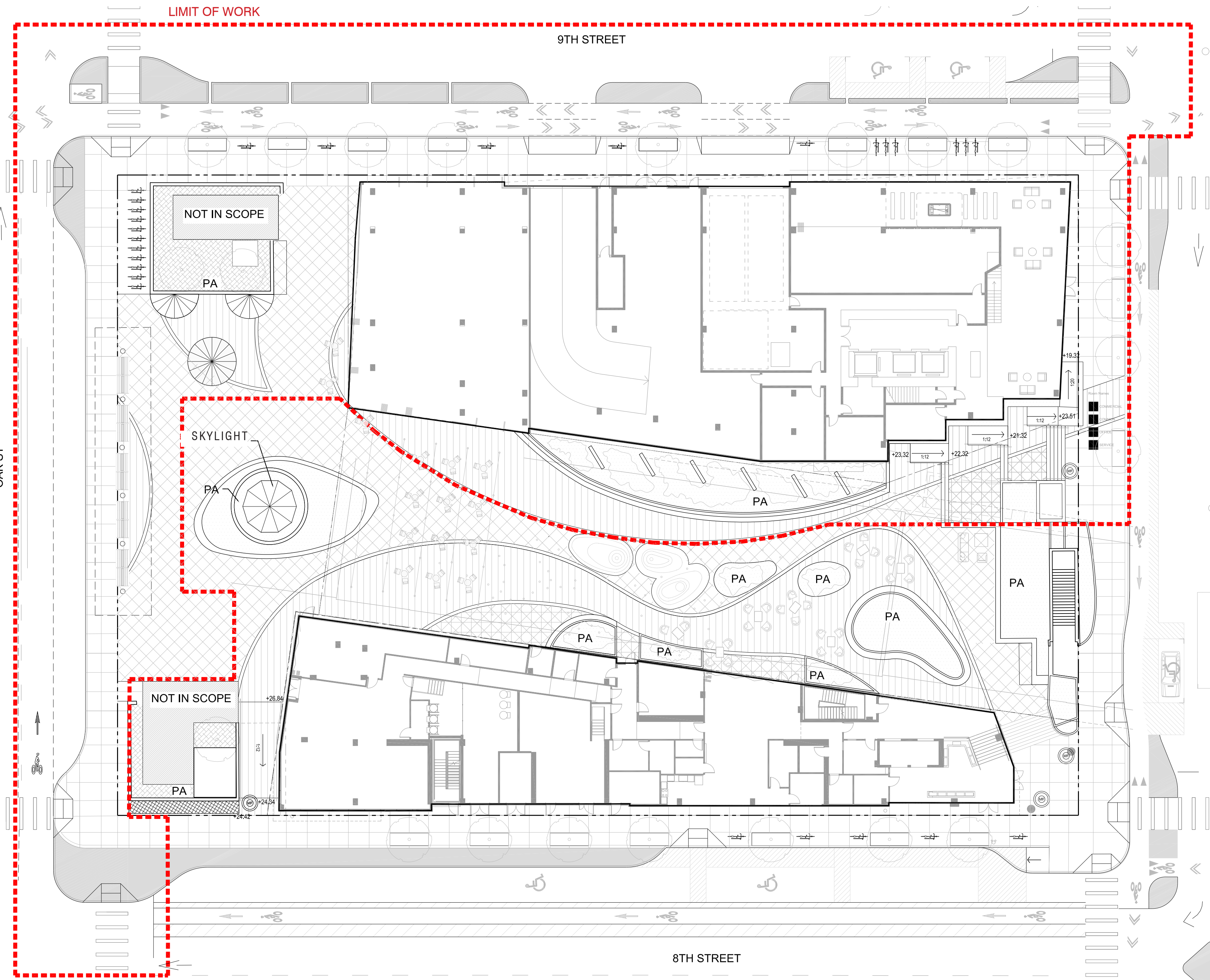
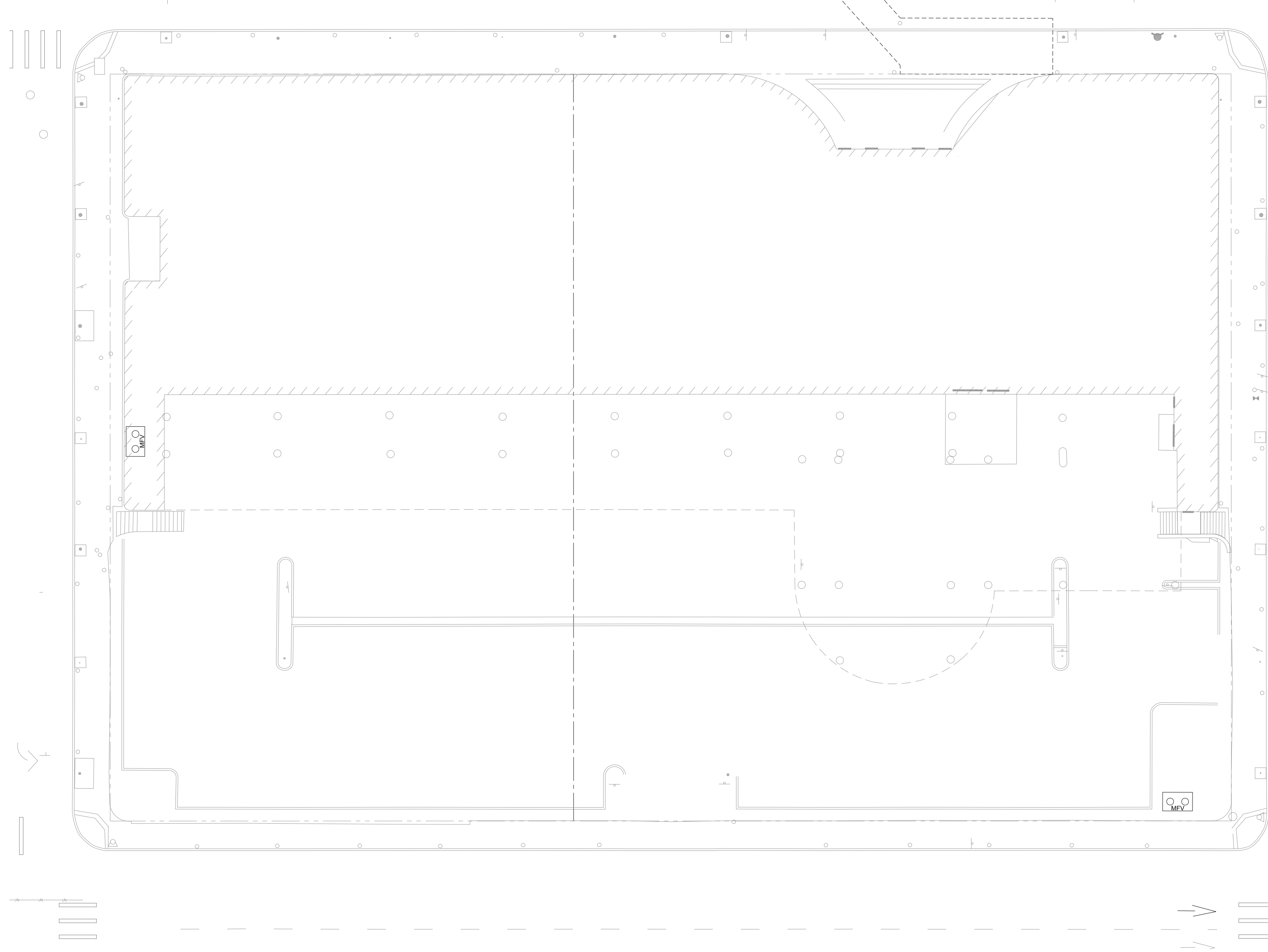
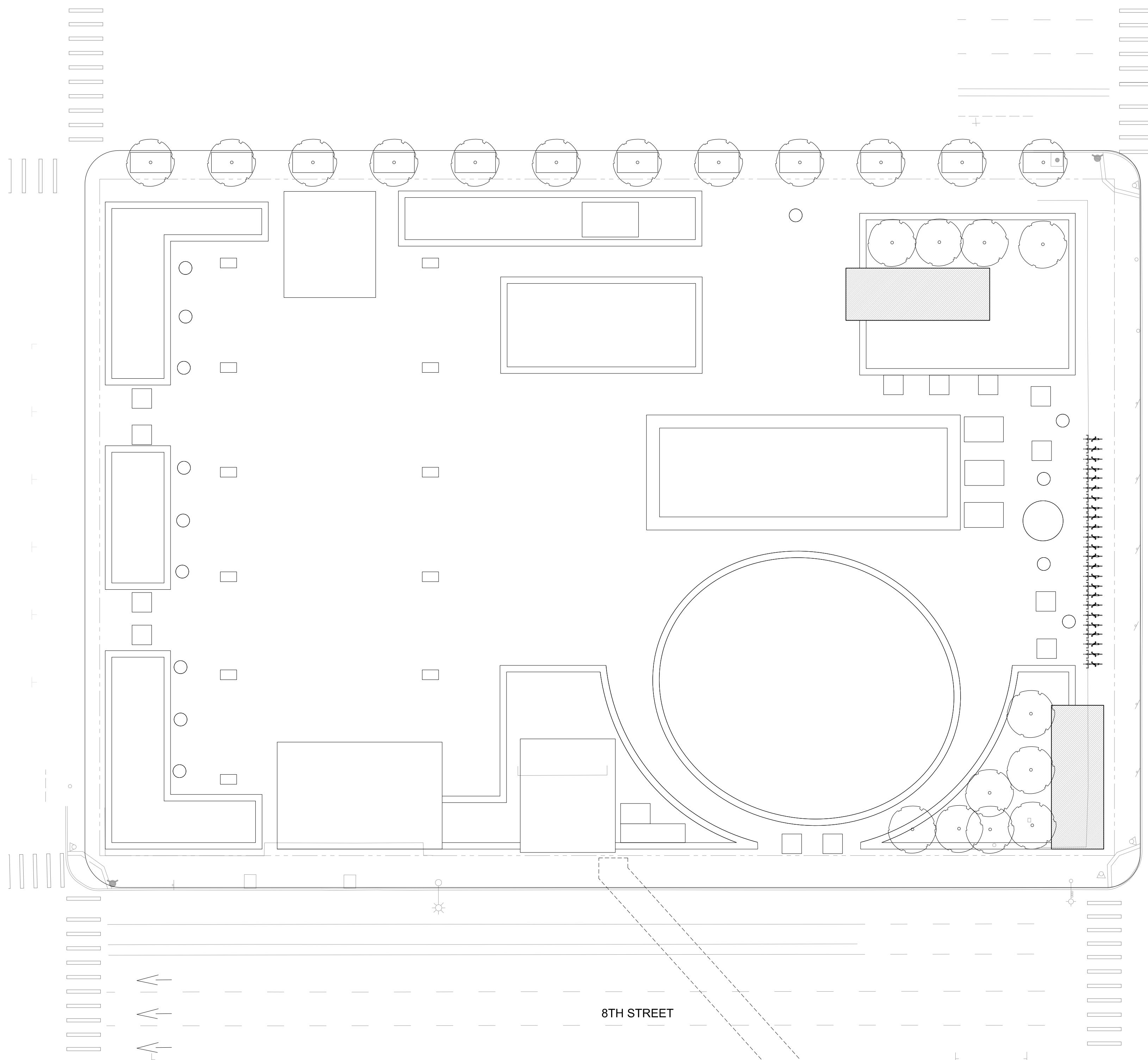
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NO.	ISSUE DATE

JOB NUMBER: 1808  
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 CHECKED BY: Checker  
 ISSUE DATE: xx/xx/xxxx  
 SCALE: As indicated  
 PHASE: 1  
 BLOCK: 1 AND 2  
 TITLE: **PHASE 1.1**

# L3.1

8/24/2021 6:59:42 PM





# Phase 1.2

0 10' 20' 40'  
SCALE: 1" = 20'-0"

**EINWILLERKUEHL**  
LANDSCAPE ARCHITECTURE  
318 HARRISON STREET SUITE 301  
OAKLAND CALIFORNIA 94607  
(510) 891-1696

**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94621

**STRADA INVESTMENT GROUP**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**BKF ENGINEERS**  
255 SHORELINE DR, SUITE 200  
REDWOOD CITY, CA 94065

**LUMA LIGHTING DESIGN**  
48 GOLDEN GATE AVENUE  
SAN FRANCISCO, CA 94102

**LUMA**

# LAKE MERRITT BART HORIZONTAL FDP

Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP#2	06/16/2022

REVISION SCHEDULE

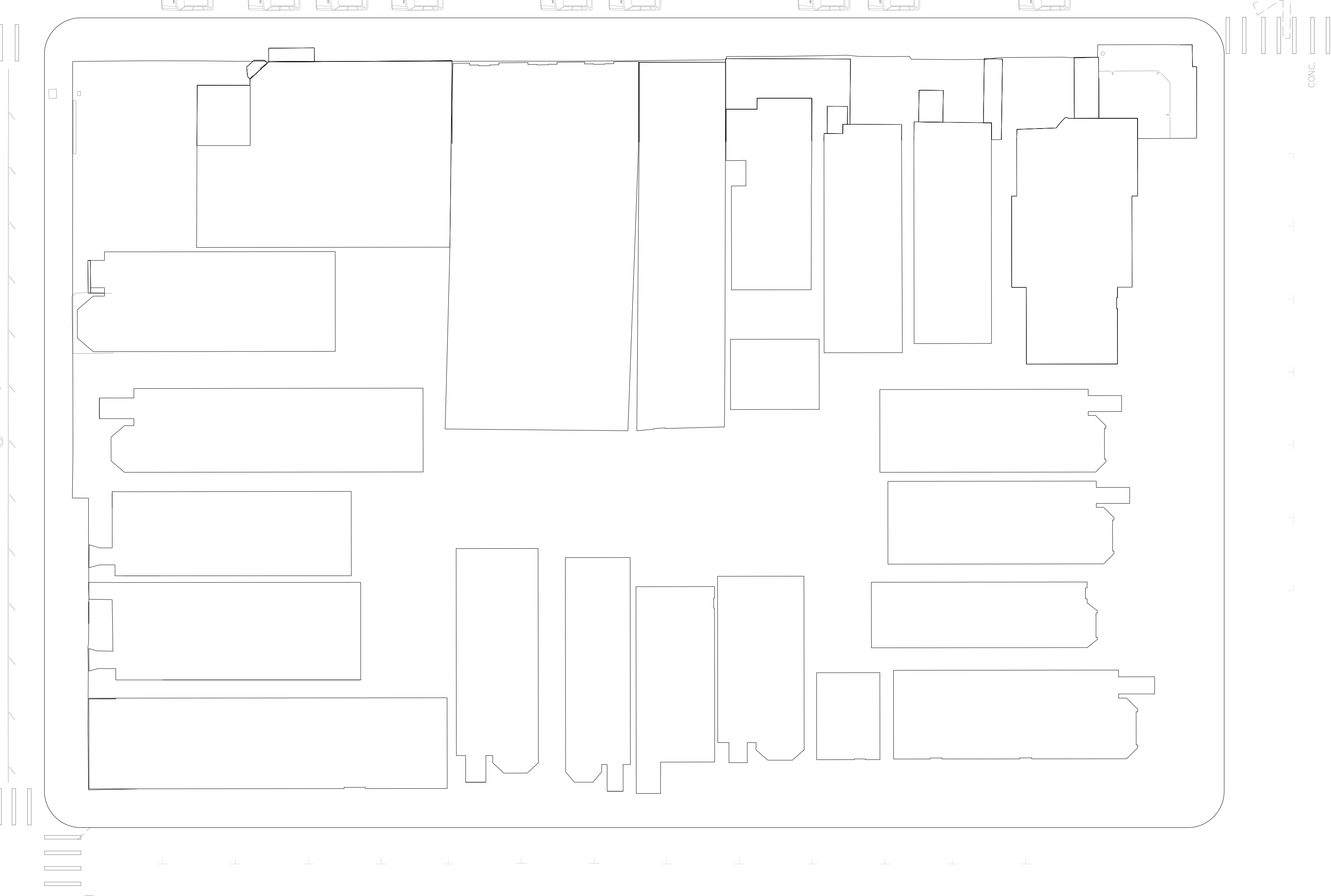
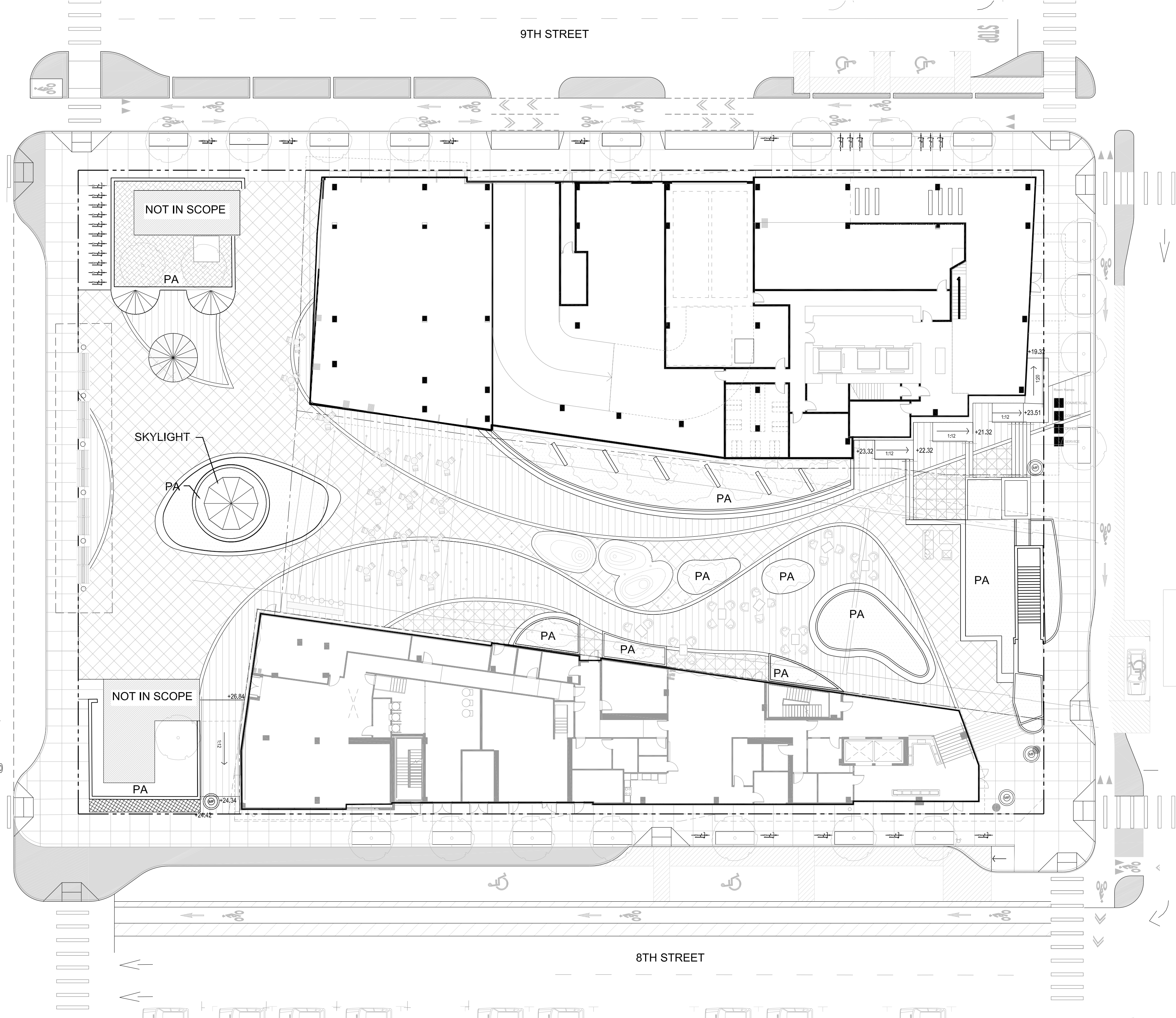
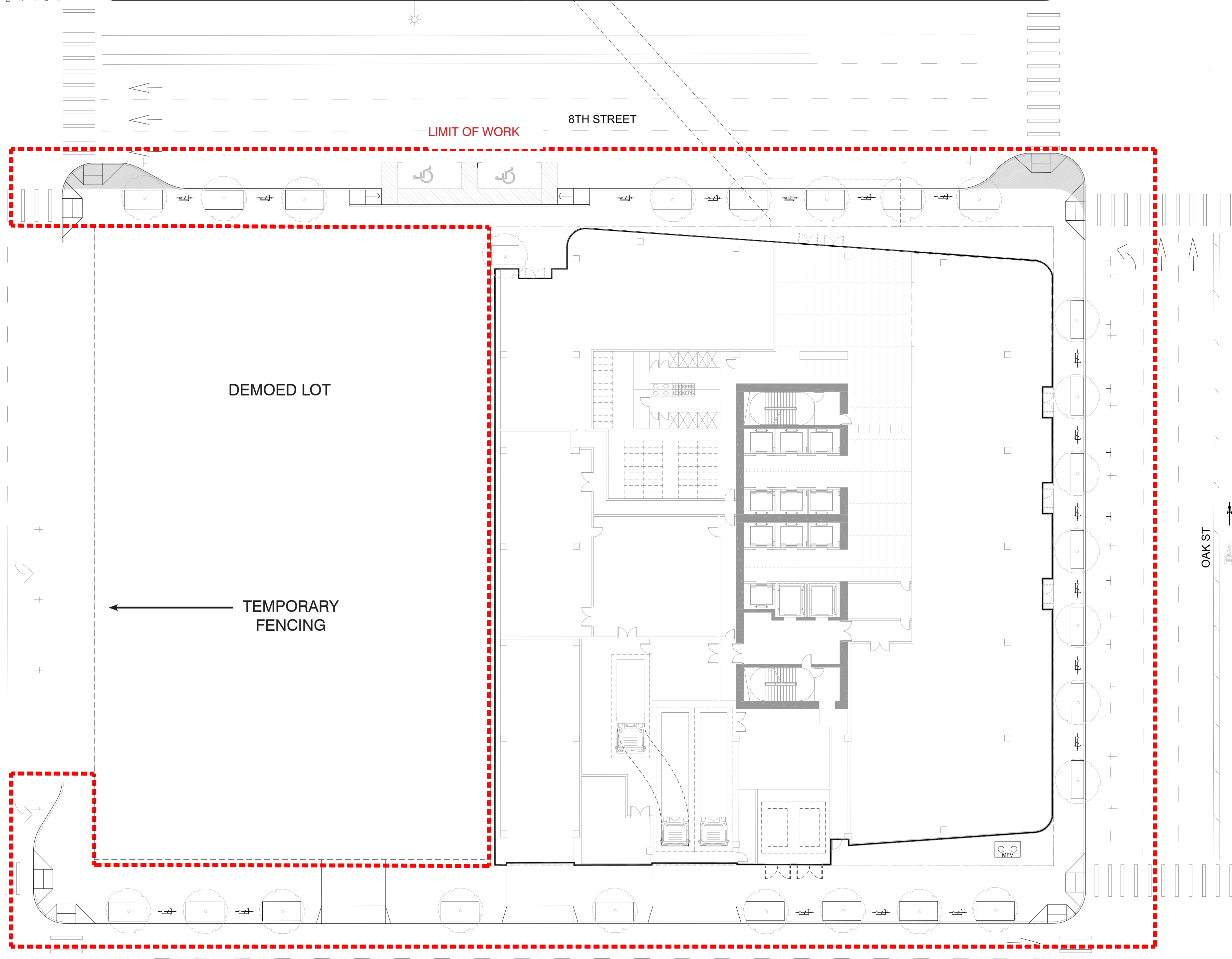
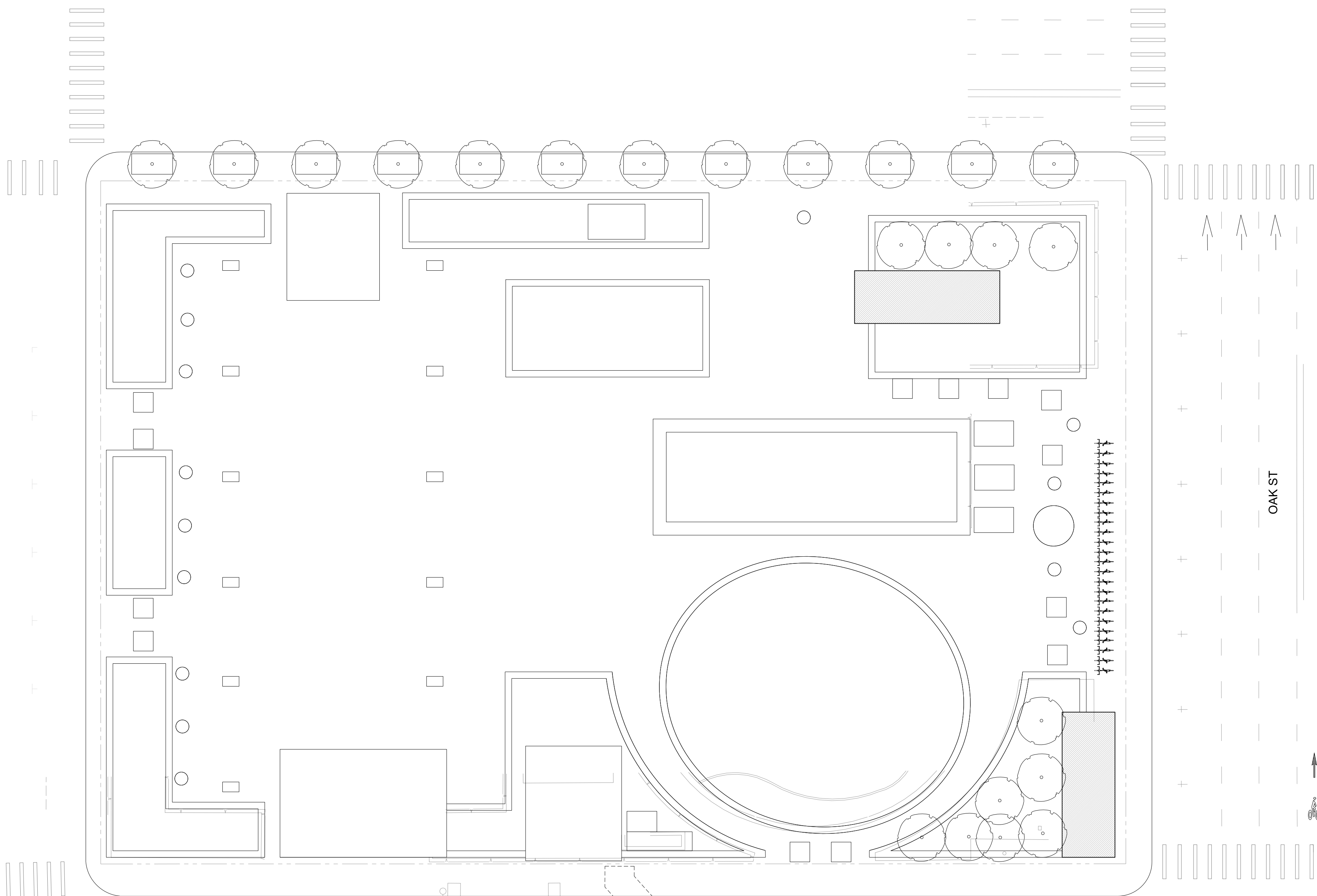
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CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx  
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PHASE: 1  
BLOCK: 1 AND 2  
TITLE: **PHASE 1.2**

SHEET:  
**L3.2**

PRELIMINARY - Not for Construction -  
© 2022 EINWILLERKUEHL LANDSCAPE ARCHITECTURE & URBAN DESIGN

8/24/2021 6:59:42 PM



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

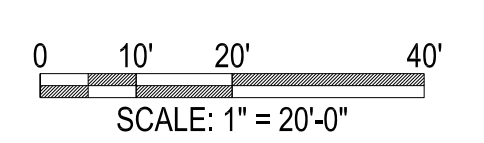
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100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP#2	06/16/2022

REVISION SCHEDULE

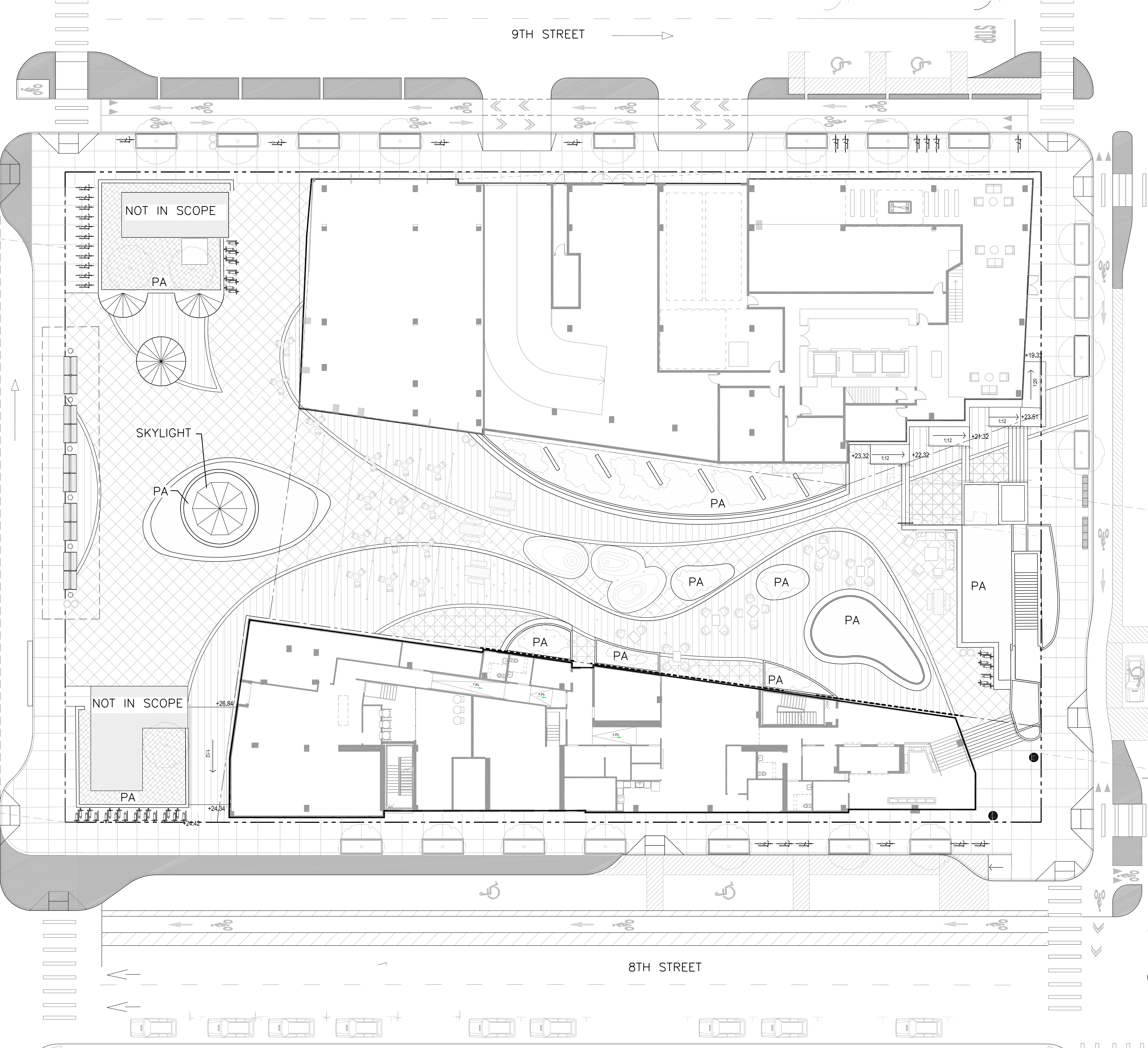
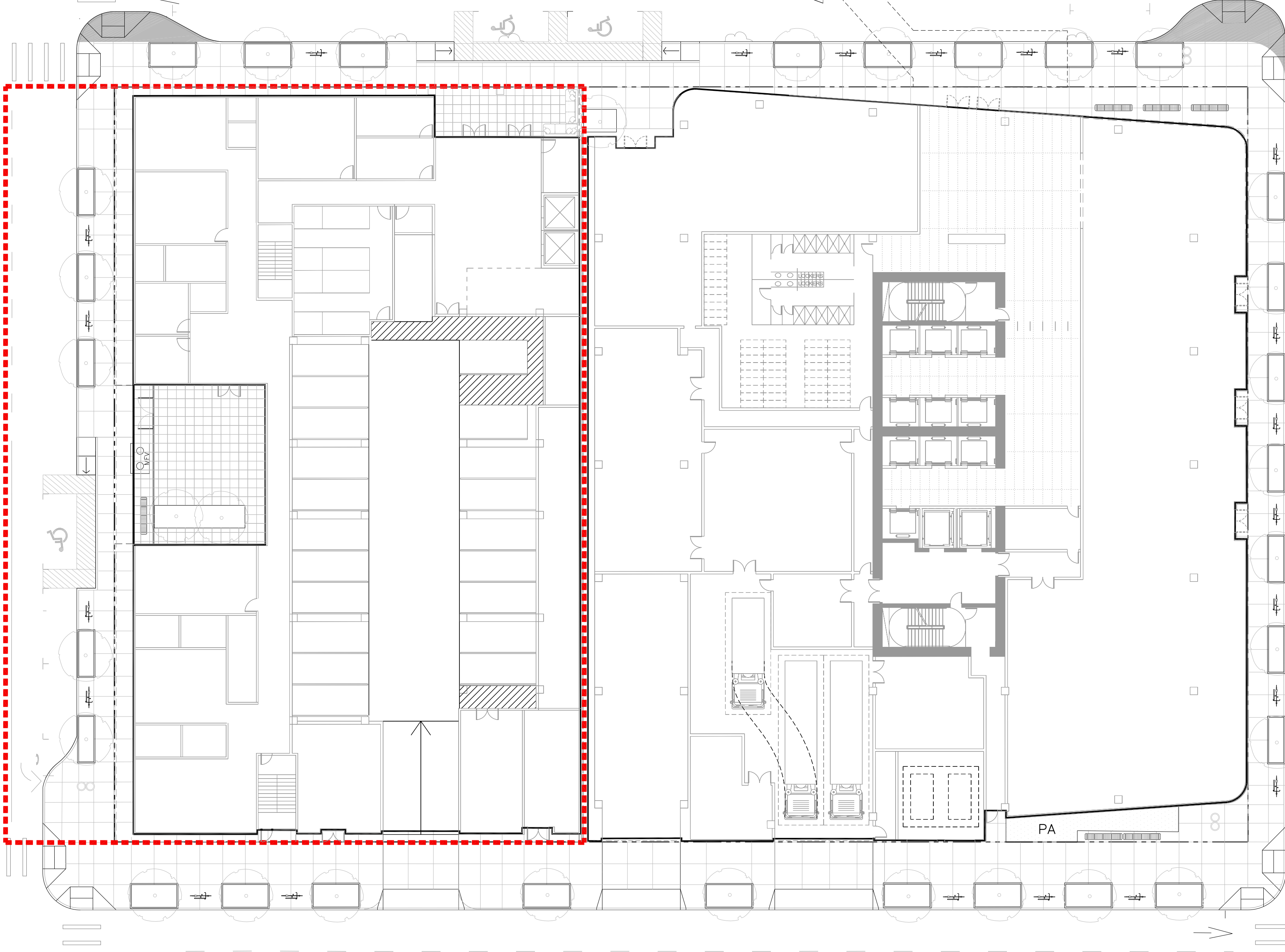
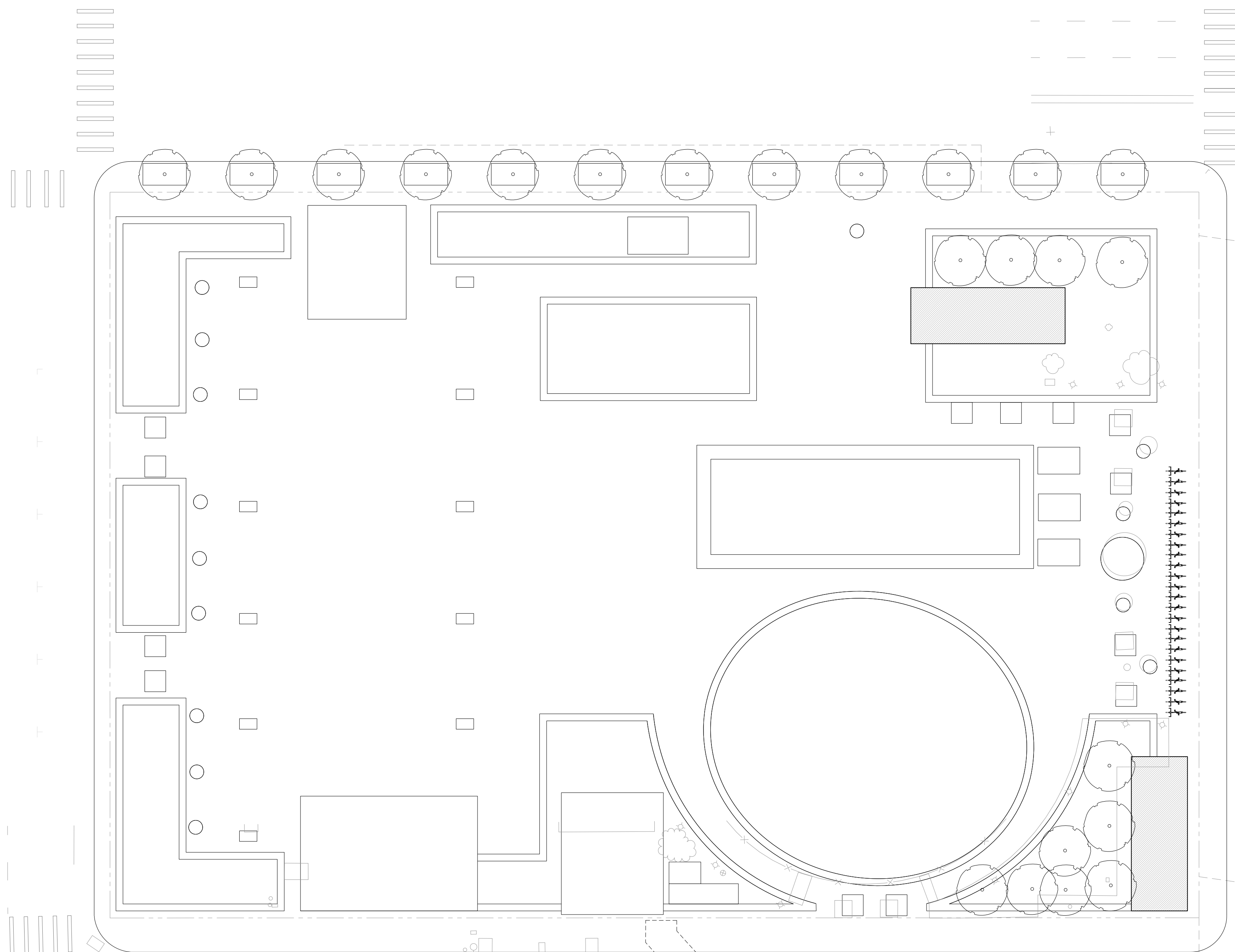
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CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx  
SCALE: As indicated  
PHASE: 1  
BLOCK: 1 AND 2  
TITLE: **PHASE 2.1**

SHEET:  
**L3.3**



8/24/2021 6:59:42 PM



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP#2	06/16/2022

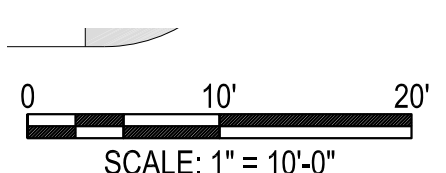
REVISION SCHEDULE

NO.	ISSUE	DATE
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PHASE: 1  
BLOCK: 1 AND 2  
TITLE: PHASE 2.2

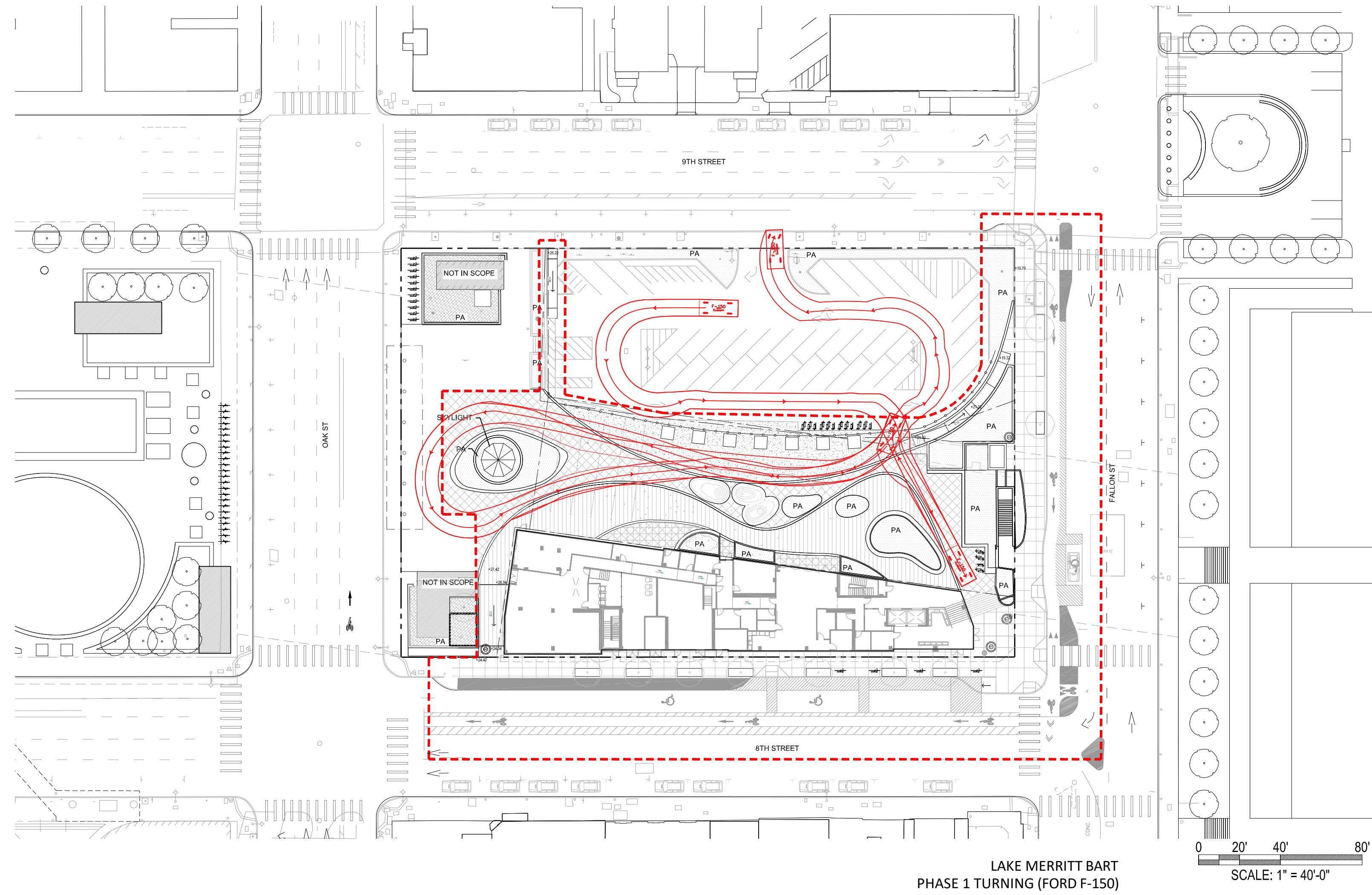
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PRELIMINARY - Not for Construction -

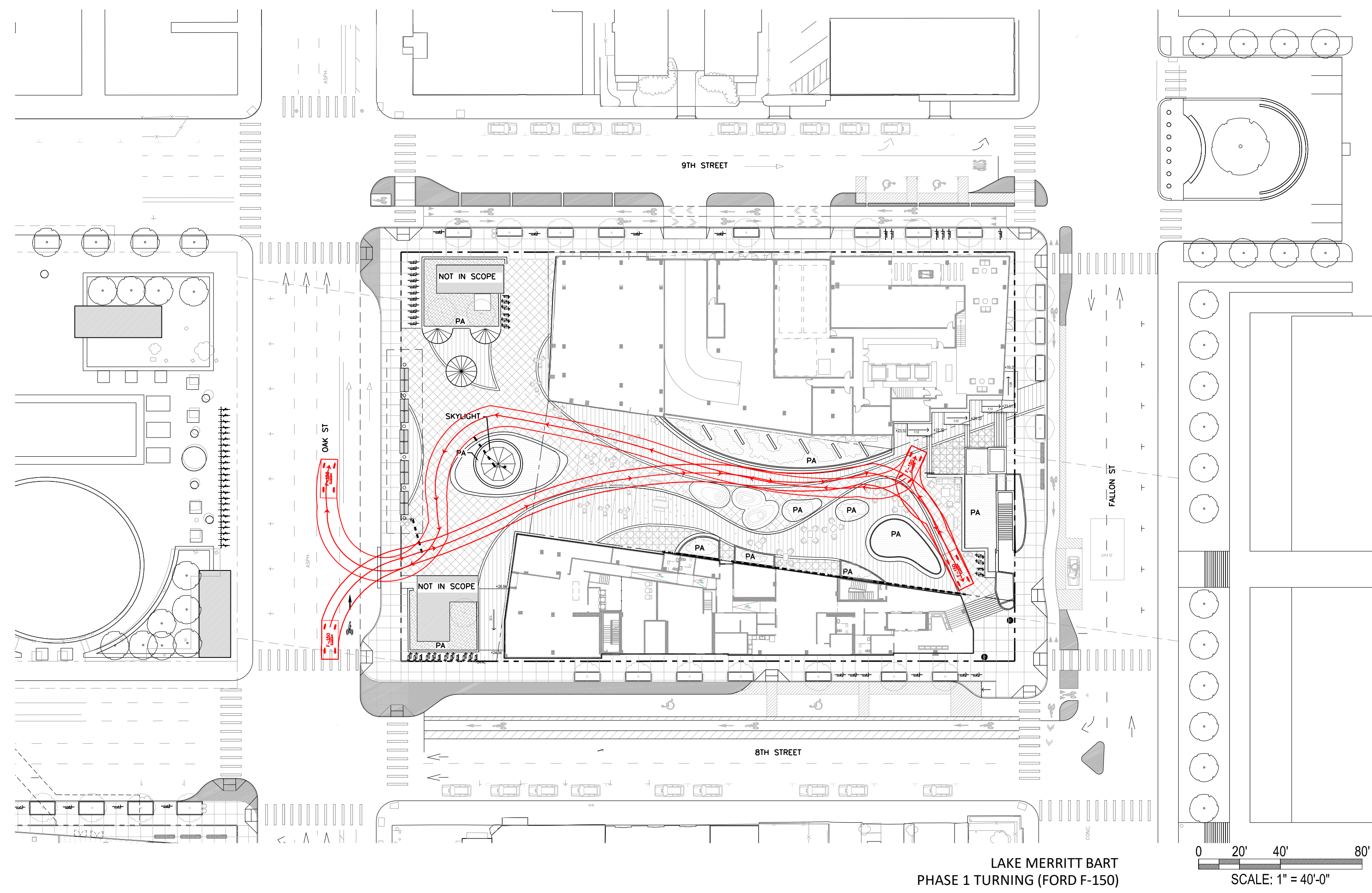


8/30/2022 6:55:52 AM





**PHASE 1.1**  
 LIGHTWEIGHT TRUCK TURNING RADIUS DIAGRAM-  
 -ENTRY FROM LAYDOWN AREA



**PHASE 1.2**  
 LIGHTWEIGHT TRUCK TURNING RADIUS DIAGRAM  
 -ENTRY FROM ROLL CURB ON OAK ST.

**NOTE:**

1. Paseo design under study. Furniture is movable. Typical conditions shown.
2. All open space elements to be maintained by BART will be designed to BFS standards.

**NOTE:**

"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval" and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

STAMP:

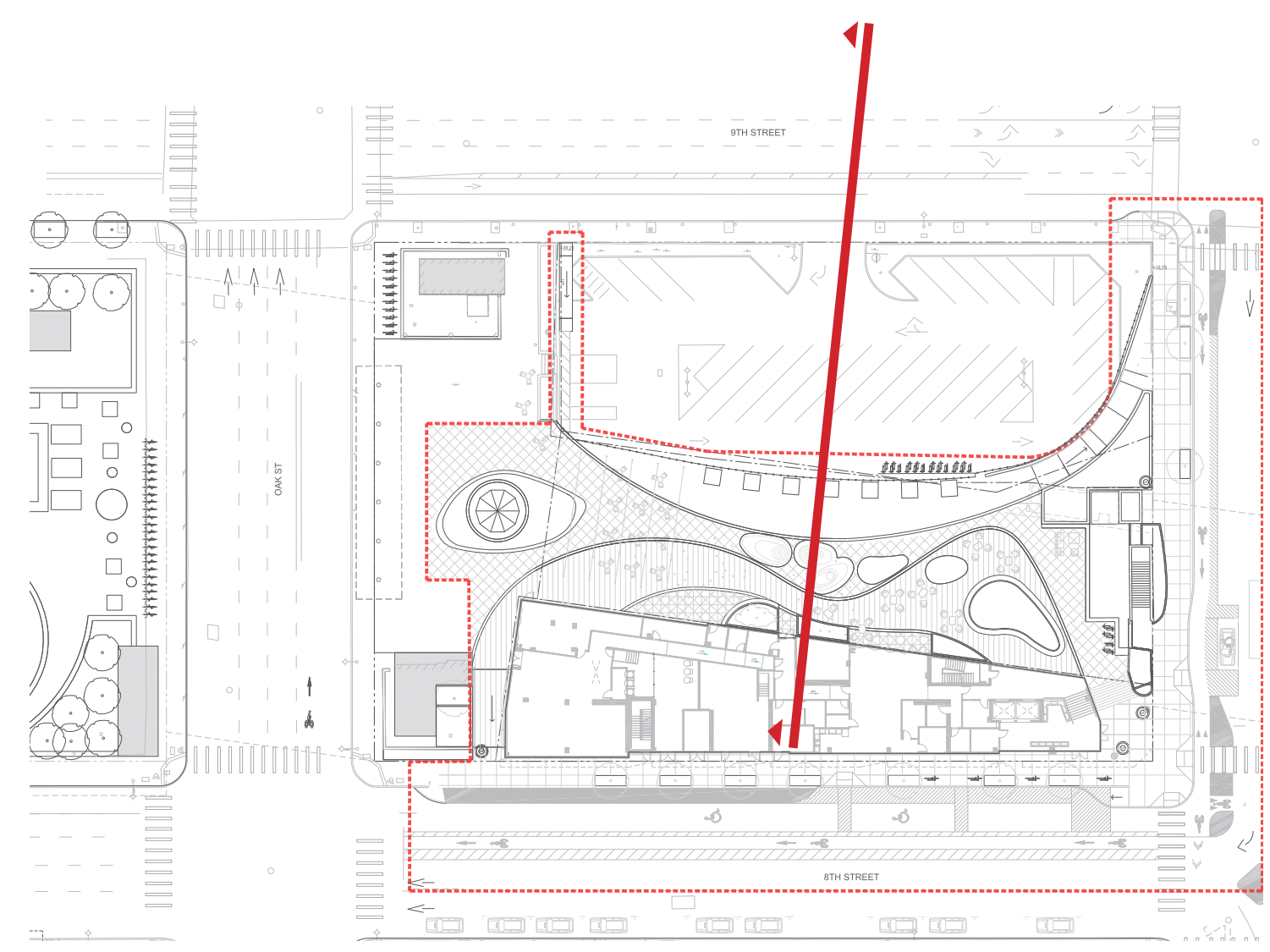
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100 SD - HORIZONTAL FDP	12/17/2021	
100 SD - HORIZONTAL FDP	05/02/2022	
100 SD - HORIZONTAL FDP2	06/16/2022	

REVISION SCHEDULE	
NO.	DATE

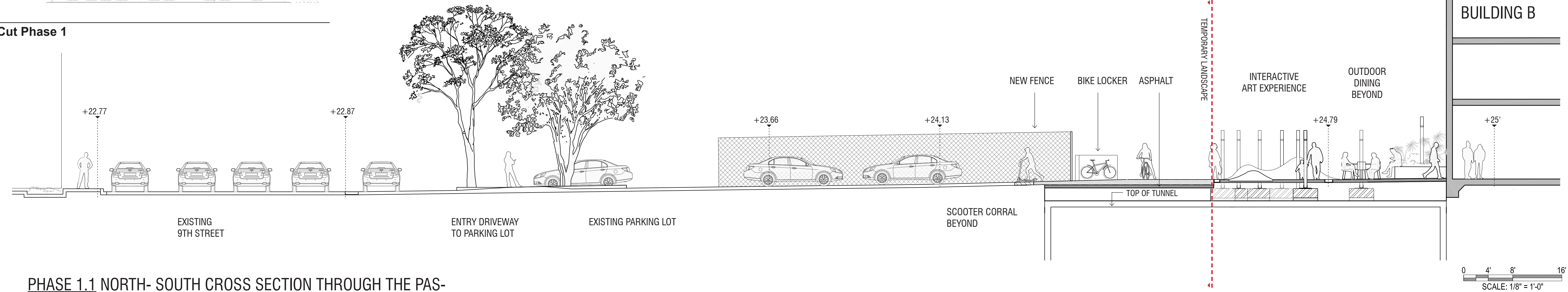
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DRAWN BY:	Author
CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx
SCALE:	As indicated
PHASE:	1
BLOCK:	1

TITLE:  
**TRUCK TURNING RADIUS DIAGRAMS**

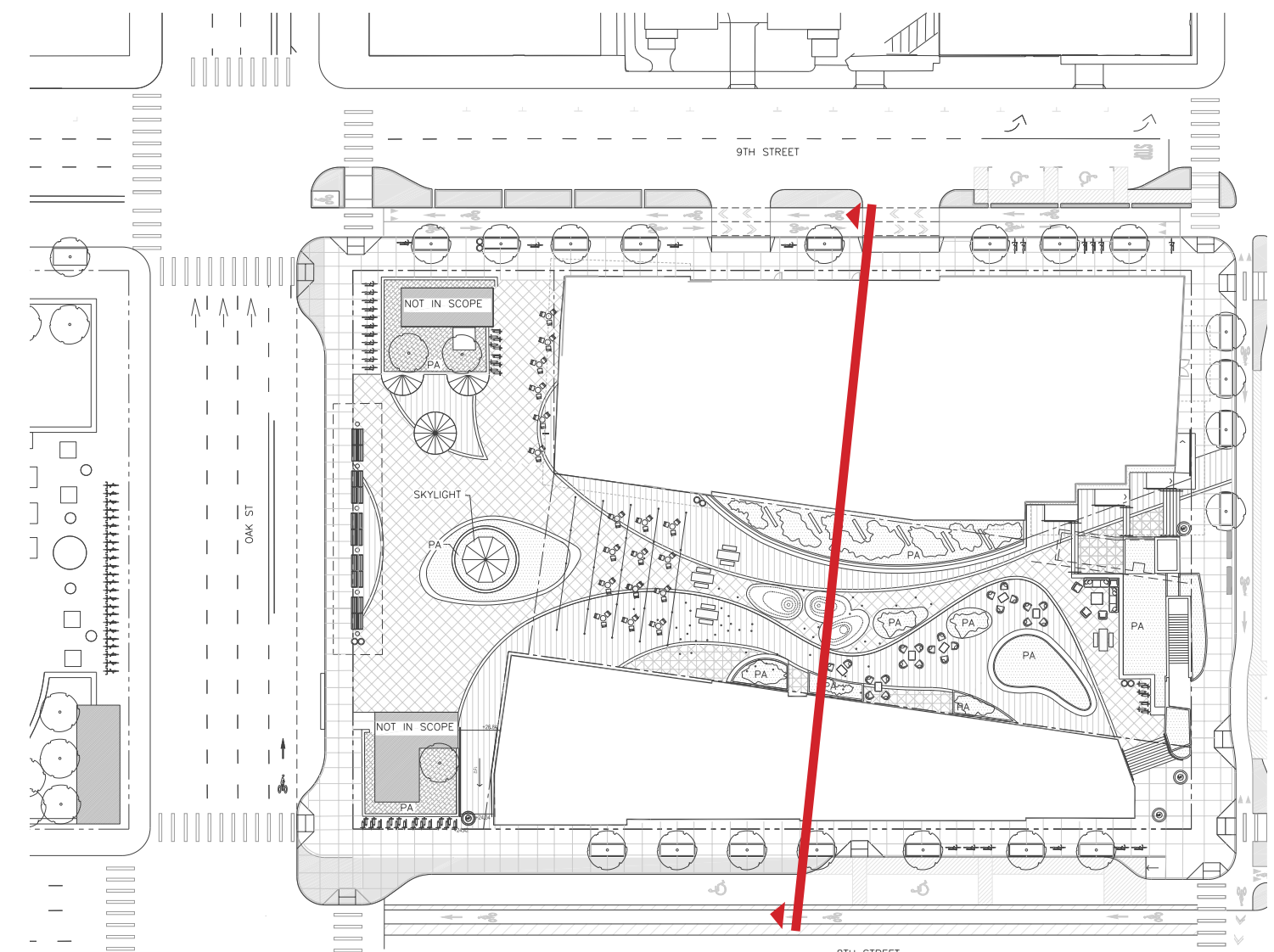
SHEET:  
**L3.5**



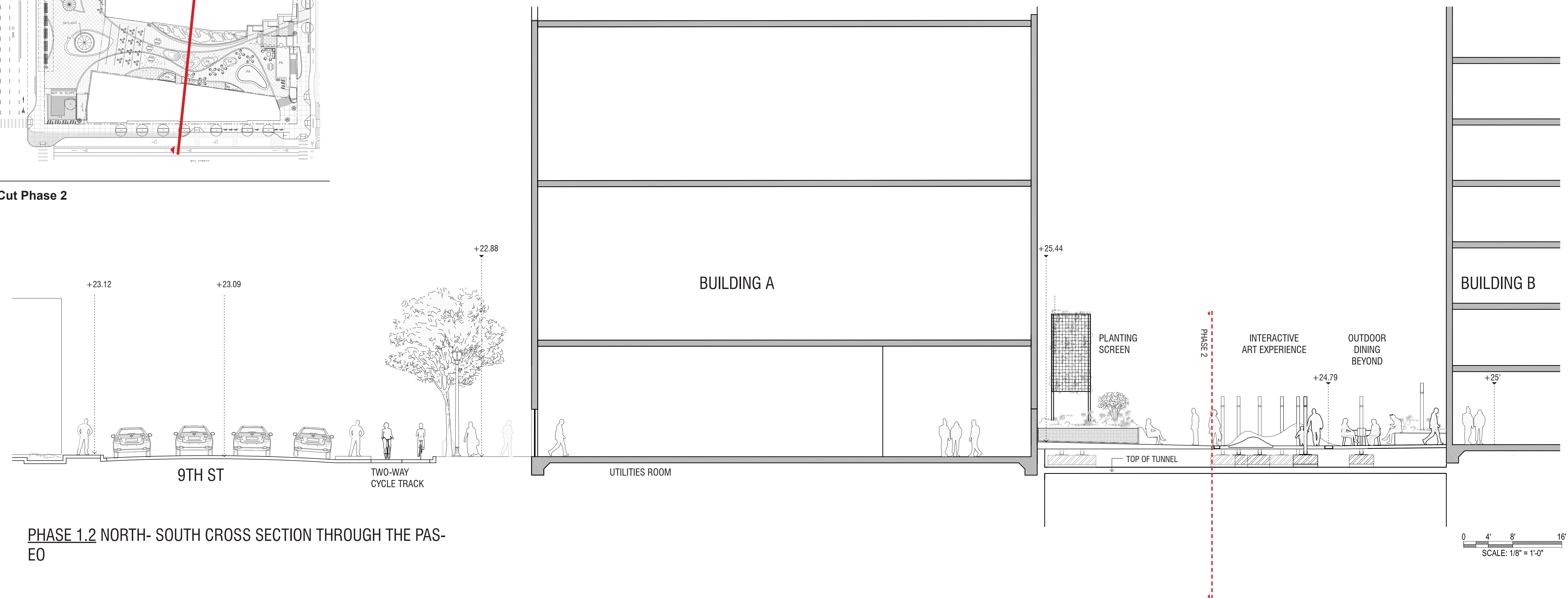
Section Cut Phase 1



PHASE 1.1 NORTH- SOUTH CROSS SECTION THROUGH THE PASEO



Section Cut Phase 2



PHASE 1.2 NORTH- SOUTH CROSS SECTION THROUGH THE PASEO



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE		
50 SD - HORIZONTAL FDP	10/29/2021	
100 SD - HORIZONTAL FDP	12/17/2021	
100 SD - HORIZONTAL FDP	05/02/2022	
100 SD - HORIZONTAL FDP2	06/16/2022	

REVISION SCHEDULE	
NO.	DATE

JOB NUMBER:	1808
DRAWN BY:	Author
CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx

SCALE:	As indicated
PHASE:	1
BLOCK:	1

TITLE:  
**PASEO CROSS SECTION**

SHEET:  
**L3.6**

# LAKE MERRITT BART DEVELOPMENT

## FDP PACKAGE – HORIZONTAL

CITY OF OAKLAND

ALAMEDA COUNTY

STATE OF CALIFORNIA

### PROJECT DESCRIPTION

**BLOCK 1**  
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 1, BOUND BY 8TH STREET, FALLON STREET, 9TH STREET, AND OAK STREET. A NEW MARKET RATE RESIDENTIAL BUILDING (BUILDING A), A NEW SENIOR HOUSING BUILDING (BUILDING B), AND A NEW PEDESTRIAN PASEO WILL BE CONSTRUCTED ON THE PROPERTY SITE AND WILL MAINTAIN AND IMPROVE ACCESS TO THE EXISTING LAKE MERRITT BART STATION.

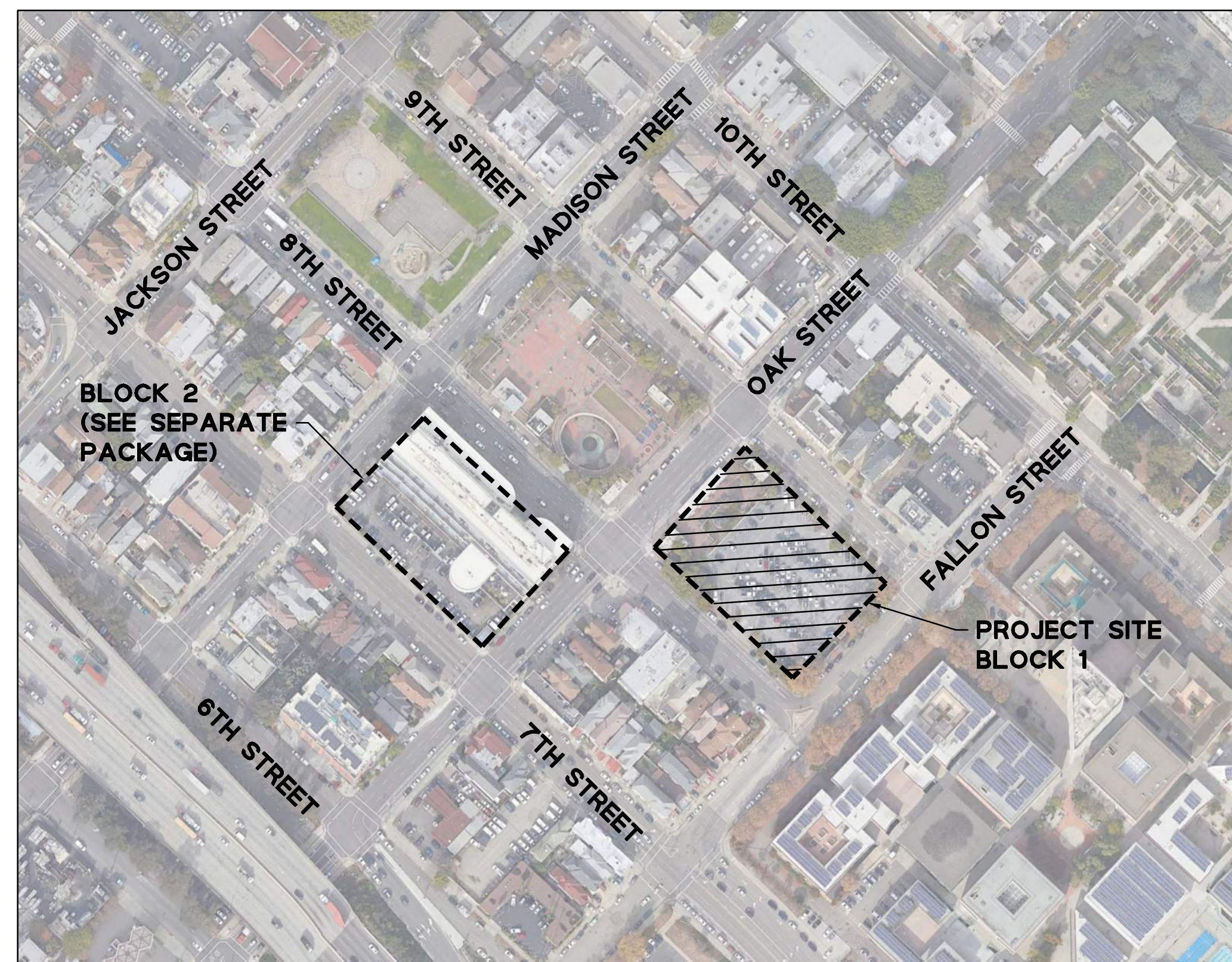
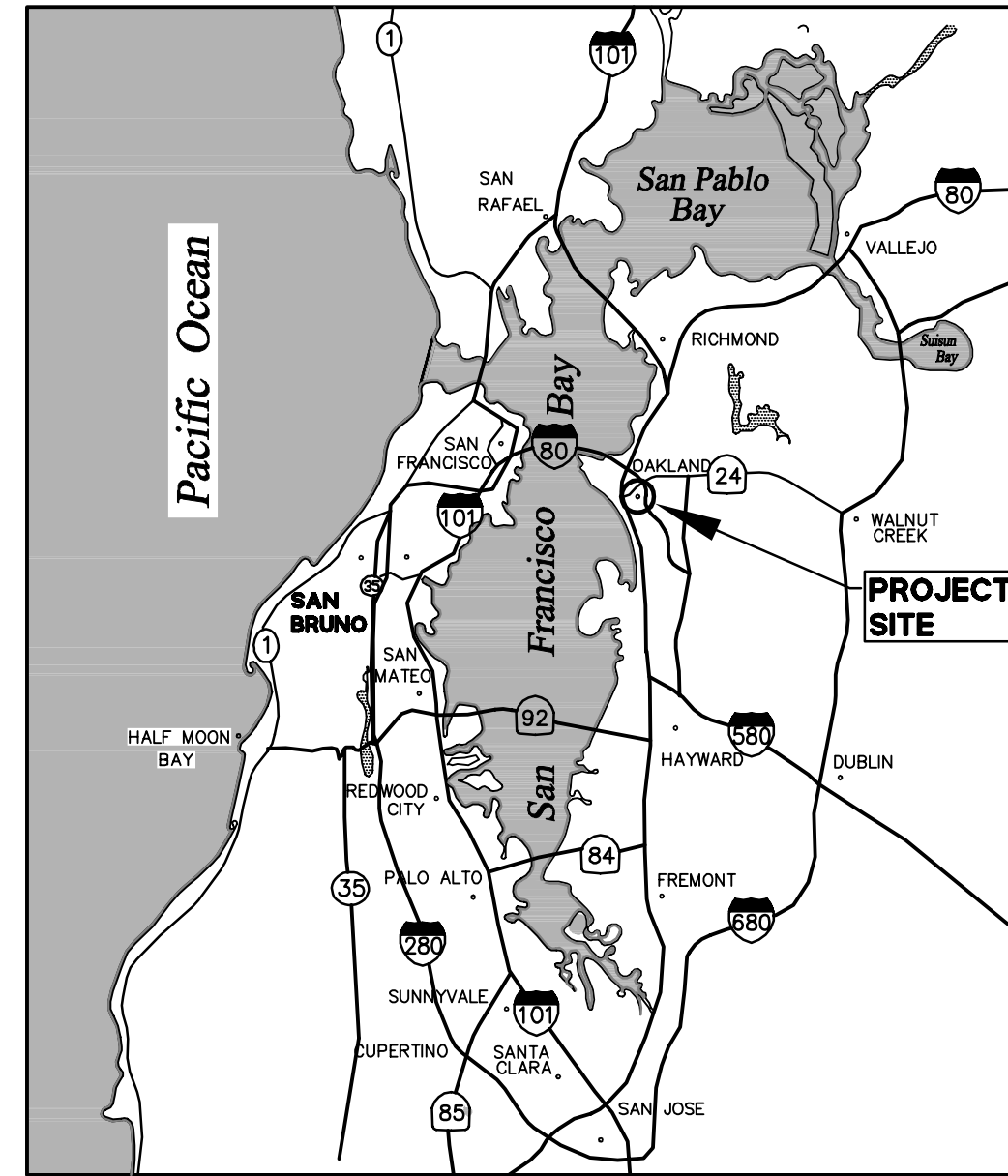
**BLOCK 2**  
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 2, BOUND BY 7TH STREET, OAK STREET, 8TH STREET, AND MADISON STREET. A NEW OFFICE BUILDING (BUILDING C) AND A NEW AFFORDABLE HOUSING BUILDING (BUILDING D) WILL BE CONSTRUCTED ON THE PROPERTY SITE.

### GENERAL NOTES

- SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY UNDER THE SUPERVISION OF DAVIS THRESH, PLS #6868, PERFORMED ON MAY 13TH, MAY 15TH, AND MAY 22ND, 2019. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
  - FEMA DESIGNATED FLOOD ZONE:** PURSUANT TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, COMMUNITY NO. 06001C0067H, EFFECTIVE DATE DECEMBER 21, 2018, THE SUBJECT PROPERTY LIES WITHIN FLOOD ZONE 'X' – AREAS DETERMINED OF MINIMAL FLOOD HAZARD.
  - UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES, AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
  - BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HEREON IS BASED UPON RESOLUTIONS OF RECORD STREET AND LOT DIMENSIONS AND COLLECTED STREET MONUMENT LOCATIONS WITHIN THE SURROUNDING STREETS. MONUMENT COLLECTION WAS CONDUCTED ON APRIL 19, 2019. NO CURRENT MAP OR RECORD OF SURVEY CURRENTLY EXISTS FOR THE MAPPED BLOCKS; DEEDS MAKE REFERENCE TO KELLERSBERGER'S MAP OF OAKLAND FILED IN BOOK 7 OF MISCELLANEOUS MAPS AT PAGE 3, ALAMEDA COUNTY RECORDS.
  - HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.
  - BENCHMARK:** FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD88)
- ELEVATIONS SHOWN ARE ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT.; SUBTRACTING NAVD88 ELEVATIONS BY 5.68' WILL RESULT IN SHOWN COVD ELEVATIONS.

### PROJECT DATA

<b>OWNERS:</b>	STRADA INVESTMENT GROUP 101 MISSION STREET, SUITE 420 SAN FRANCISCO, CA 94105 PHONE: (415) 263-9151 CONTACT: WILLIAM GOODMAN	EAST BAY ASIAN LOCAL DEVELOPMENT CORP 1825 SAN PABLO AVENUE, SUITE 200 OAKLAND, CA 94612 PHONE: (510) 512-2444 CONTACT: JAMES PEREZ
<b>ARCHITECTS:</b>	PYATOK ARCHITECTS 1611 TELEGRAPH AVENUE, SUITE 200 OAKLAND, CA 94612 PHONE: (510) 465-7010 CONTACT: PETER WALLER	SOLOMON CORDWELL BUENZ ARCHITECTS 255 CALIFORNIA STREET, 3RD FLOOR SAN FRANCISCO, CA 94111 PHONE: (415) 216-2450 CONTACT: CYRIL CHONG
<b>CIVIL ENGINEER:</b>	BKF ENGINEERS 255 SHORELINE DRIVE, SUITE 200 REDWOOD CITY, CA 94065 PHONE: (650) 482-6377 CONTACT: SIMON NORTH	
<b>LANDSCAPE ARCHITECT:</b>	ENWILLERKUEHL LANDSCAPE ARCHITECTURE 318 HARRISON STREET, SUITE 301 OAKLAND, CA 94607 PHONE: (510) 891-1696 CONTACT: SARAH KUEHL	
<b>ASSESSOR PARCEL NO.:</b>	001-0169-001 (BLOCK 1) 001-0171-002 (BLOCK 2)	
<b>EXISTING LAND USE:</b>	COMMERCIAL	
<b>PROPOSED LAND USE:</b>	MIXED USE	
<b>LAND AREA:</b>	1.383 ACRES (BLOCK 1) 1.377 ACRES (BLOCK 2)	
<b>UTILITY INFORMATION:</b>	WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD) FIRE PROTECTION: CITY OF OAKLAND / EBMUD SEWAGE DISPOSAL: CITY OF OAKLAND STORM DRAIN: CITY OF OAKLAND GAS: PACIFIC GAS & ELECTRIC (PG&E) ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E) TELEPHONE: AT&T CABLE TELEVISION: COMCAST	



VICINITY MAP  
NTS

### ABBREVIATIONS

AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BACK OF WALK
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EV	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
HCR	=	HANDICAP RAMP
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
P	=	POST
PKM	=	PARKING METER
SDCO	=	STORM DRAIN CLEANOUT
SDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UB	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	RIGHT OF WAY
---	---	CURB AND GUTTER
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWCUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	CATCH BASIN
---	---	SSMH
---	---	SDMH
---	---	SDI
---	---	WATER VALVE
---	---	GAS VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DCDA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	STREET LIGHT
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE

### SHEET INDEX

SHEET NO.	DESCRIPTION
C1.0	TITLE SHEET
C2.0	OVERALL SHEET INDEX
C2.1	EXISTING CONDITIONS (BLOCK 1)
C2.2	EXISTING CONDITIONS (BLOCK 2)
C3.1	DEMOLITION PLAN (BLOCK 1)
C3.2	DEMOLITION PLAN (BLOCK 2)
C4.1	SITE PLAN (BLOCK 1)
C4.2	SITE PLAN (BLOCK 2)
C5.1	GRADING PLAN (BLOCK 1)
C5.2	GRADING PLAN (BLOCK 2)
C5.3	SECTIONS
C6.1	UTILITY PLAN (BLOCK 1)
C6.2	UTILITY PLAN (BLOCK 2)
C7.1	STORMWATER PLAN (BLOCK 1)
C7.2	STORMWATER PLAN (BLOCK 2)
C8.1	DETAILS

### ENGINEER'S STATEMENT

THESE CONSTRUCTION DOCUMENTS HAVE BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

SIMON R. NORTH, P.E.  
PRINCIPAL/VICE PRESIDENT  
BKF ENGINEERS

DATE



LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

### ISSUE SCHEDULE

100 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/06/2021
100 SD - HORIZONTAL FDP	05/02/2022

### REVISION SCHEDULE

NO.	ISSUE	DATE
-----	-------	------

JOB NUMBER: PYK-1808 | BKF-20190110  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: AS NOTED  
TITLE: TITLE SHEET

SHEET:

C1.0





**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

100 SD - HORIZONTAL FDP	10/29/2021
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100 SD - HORIZONTAL FDP	05/02/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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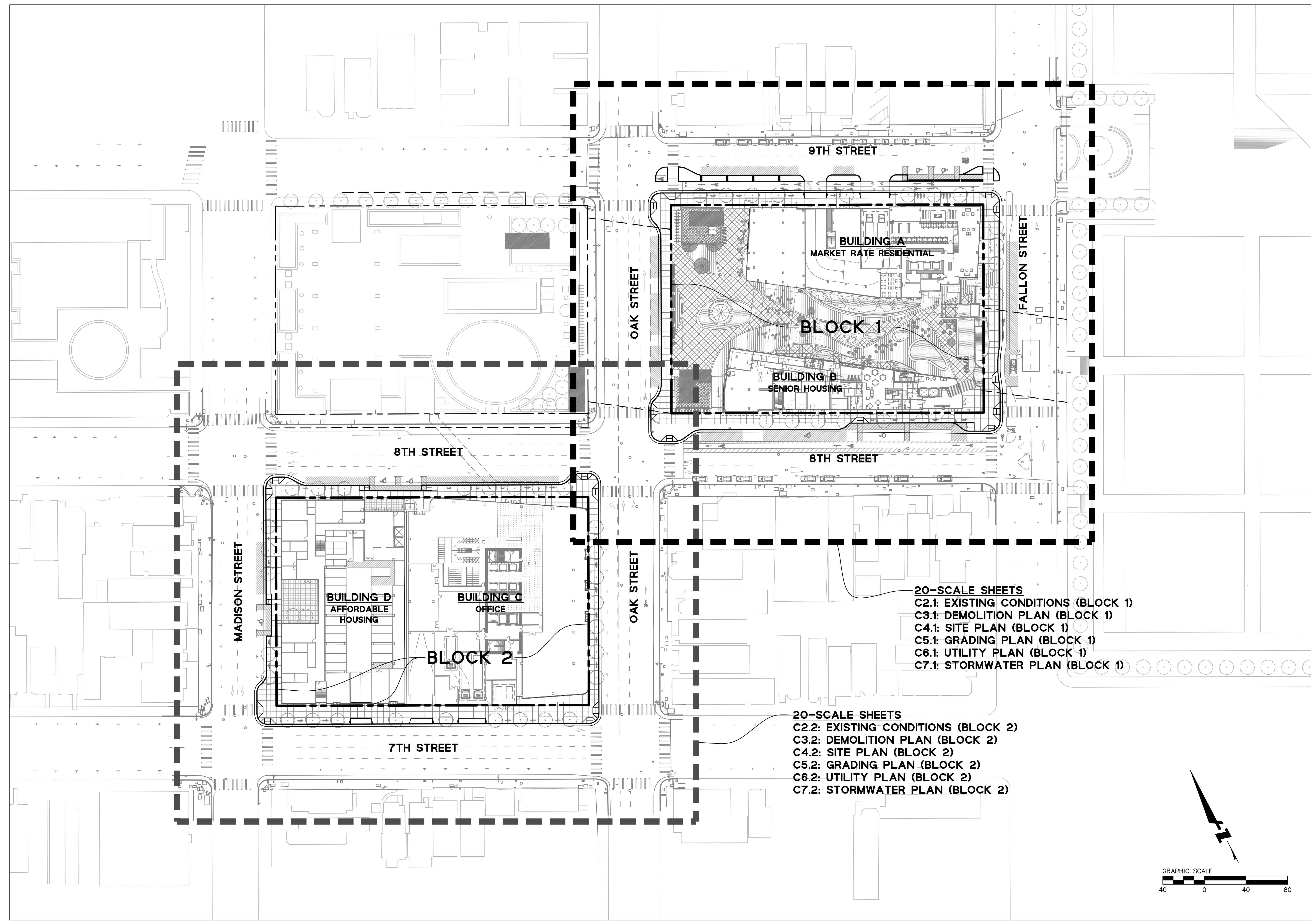
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ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 40'

TITLE:  
OVERALL SHEET INDEX

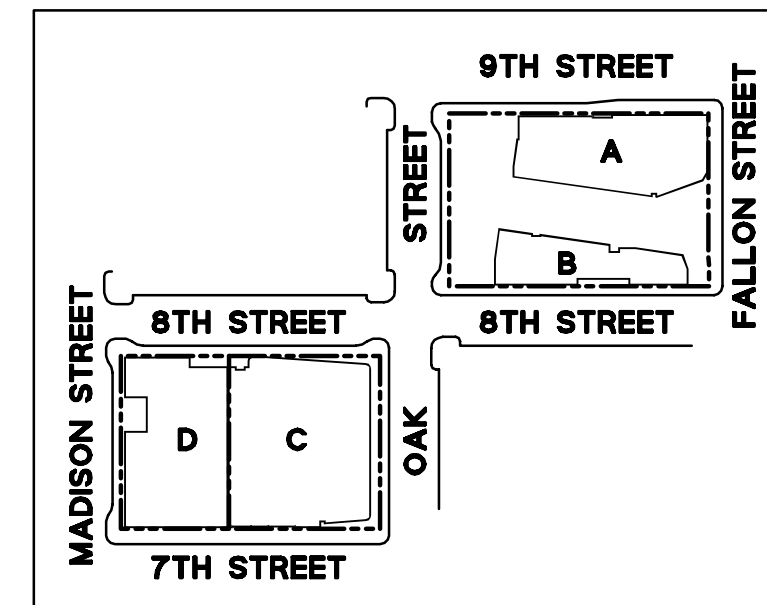
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**C2.0**

PRELIMINARY - Not for Construction -



- 20-SCALE SHEETS**  
 C2.1: EXISTING CONDITIONS (BLOCK 1)  
 C3.1: DEMOLITION PLAN (BLOCK 1)  
 C4.1: SITE PLAN (BLOCK 1)  
 C5.1: GRADING PLAN (BLOCK 1)  
 C6.1: UTILITY PLAN (BLOCK 1)  
 C7.1: STORMWATER PLAN (BLOCK 1)

- 20-SCALE SHEETS**  
 C2.2: EXISTING CONDITIONS (BLOCK 2)  
 C3.2: DEMOLITION PLAN (BLOCK 2)  
 C4.2: SITE PLAN (BLOCK 2)  
 C5.2: GRADING PLAN (BLOCK 2)  
 C6.2: UTILITY PLAN (BLOCK 2)  
 C7.2: STORMWATER PLAN (BLOCK 2)





**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

**ISSUE SCHEDULE**

100 SD - HORIZONTAL FDP	10/29/2021
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100 SD - HORIZONTAL FDP	05/02/2022

**REVISION SCHEDULE**

NO.	ISSUE	DATE
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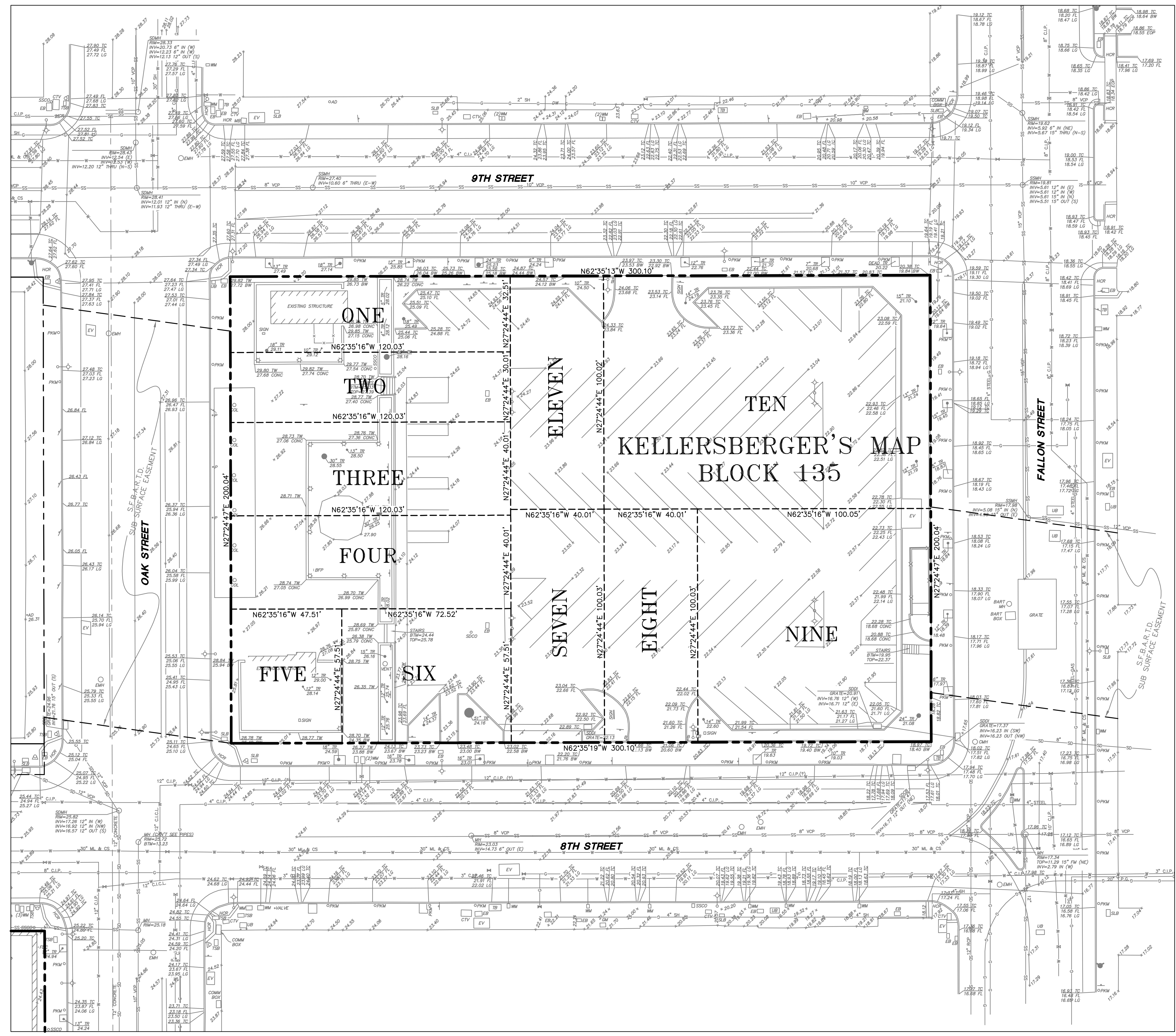
JOB NUMBER: PYK 1808 | BKF: 20190110  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE: EXISTING CONDITIONS (BLOCK 1)

SHEET:  
**C2.1**

PRELIMINARY - Not for Construction -



**ANNOTATION & LEGEND**

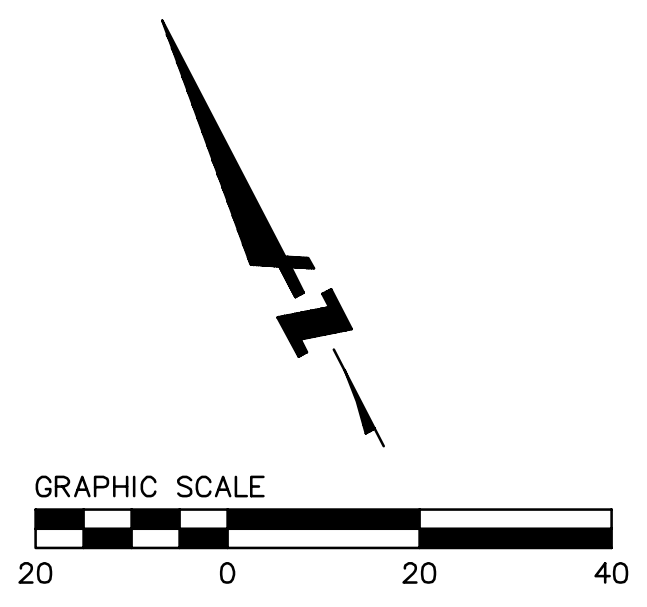
- AD = AREA DRAIN
- B = BOLLARD
- BFP = BACKFLOW PREVENTOR
- BR = BIKE RACK
- BW = BACK OF WALK
- CLDR = CENTERLINE OF DOOR
- COL = COMMUNICATION MANHOLE
- COMM = COMMUNICATION
- CONC = CONCRETE
- CTV = CABLE TELEVISION
- DW = DRIVEWAY
- EB = ELECTRICAL BOX
- EMH = ELECTRICAL MANHOLE
- EP = EDGE OF PAVEMENT
- EV = ELECTRICAL VAULT
- FDC = FIRE DEPARTMENT CONNECTION
- FL = FLOWLINE
- HCR = HANDICAP RAMP
- LG = LIP OF GUTTER
- LB = MAIL BOX
- MH = MANHOLE
- P = POST
- PKM = PARKING METER
- SDCO = STORM DRAIN CLEANOUT
- SDDI = STORM DRAIN DROP INLET
- SDMH = STORM DRAIN MANHOLE
- SLB = STREET LIGHTING BOX
- SSCO = SANITARY SEWER CLEANOUT
- SSMH = SANITARY SEWER MANHOLE
- TB = TELEPHONE BOX
- TC = TOP OF CURB
- TR = TREE
- TSB = TRAFFIC SIGNAL BOX
- TW = TOP OF WALL
- UB = UTILITY BOX
- UV = UTILITY VAULT
- WM = WATER METER
- WP = WATER PIPE
- = AREA/YARD LIGHT
- ⊠ = FIRE HYDRANT
- ⊡ = SIGN
- ⊙ = STREET LIGHT
- ⊞ = GAS VALVE
- ⊞ = WATER VALVE
- ⊞ = SIGNAL LIGHT
- = ELECTRICAL LINE
- = GAS LINE
- = STORM DRAIN LINE
- = SANITARY SEWER LINE
- = UNKNOWN UTILITY LINE
- = DOMESTIC WATER LINE

**NOTES**  
FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019  
ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

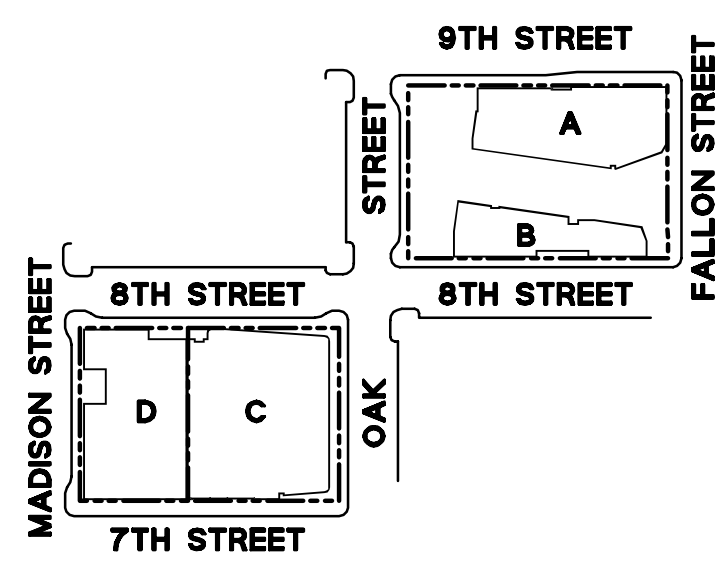
**HORIZONTAL CONTROL**  
HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS83), EPOCH 2017.00

**BENCHMARK**  
FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD83)

ELEVATIONS SHOWN ARE ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD83 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT.; SUBTRACTING NAVD83 ELEVATIONS BY 5.68' WILL RESULT IN SHOWN COVD ELEVATIONS.



*[Signature]*





LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

100 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/06/2021
100 SD - HORIZONTAL FDP	05/02/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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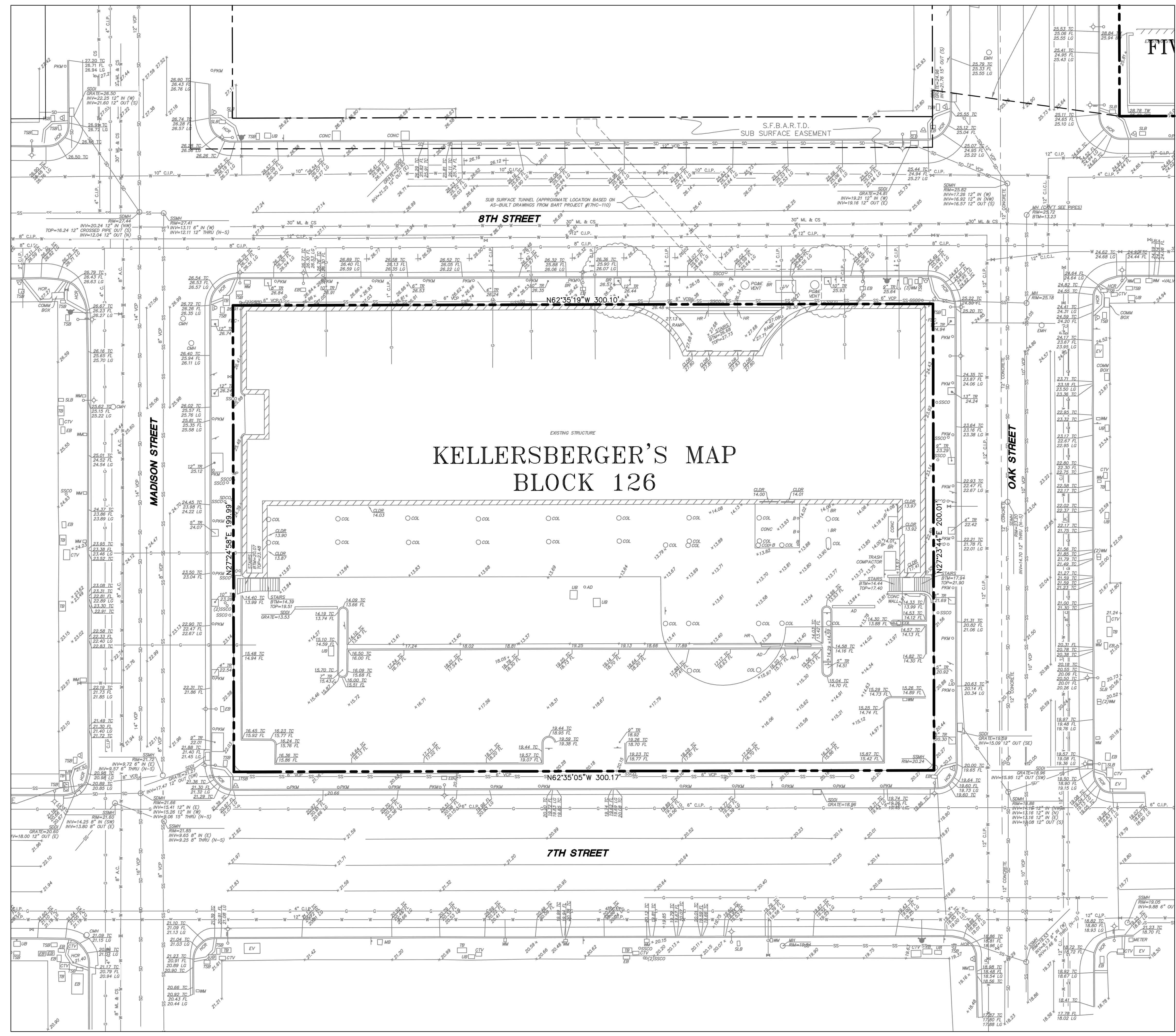
JOB NUMBER: PYK 1808 | BKF: 20190110  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE: EXISTING CONDITIONS (BLOCK 2)

SHEET:  
**C2.2**

PRELIMINARY - Not for Construction -



ANNOTATION & LEGEND

- AD = AREA DRAIN
- B = BOLLARD
- BFP = BACKFLOW PREVENTOR
- BR = BIKE RACK
- BW = BACK OF WALK
- CLDR = CENTERLINE OF DOOR
- COL = COMMUNICATION MANHOLE
- COMH = COMMUNICATION
- CONC = CONCRETE
- CTV = CABLE TELEVISION
- DW = DRIVEWAY
- EB = ELECTRICAL BOX
- EMH = ELECTRICAL MANHOLE
- EP = EDGE OF PAVEMENT
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- ⊙ = AREA/YARD LIGHT
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- ⊙ = STREET LIGHT
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- = GAS LINE
- = STORM DRAIN LINE
- = SANITARY SEWER LINE
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- = DOMESTIC WATER LINE

NOTES

FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

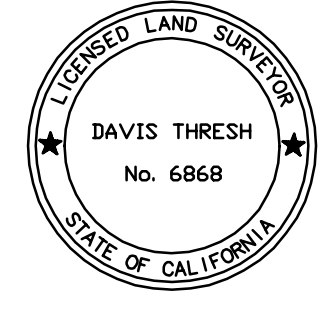
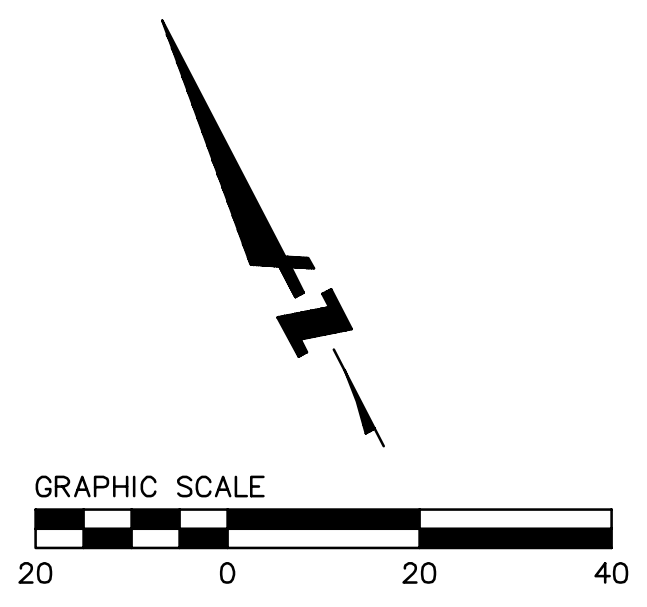
HORIZONTAL CONTROL

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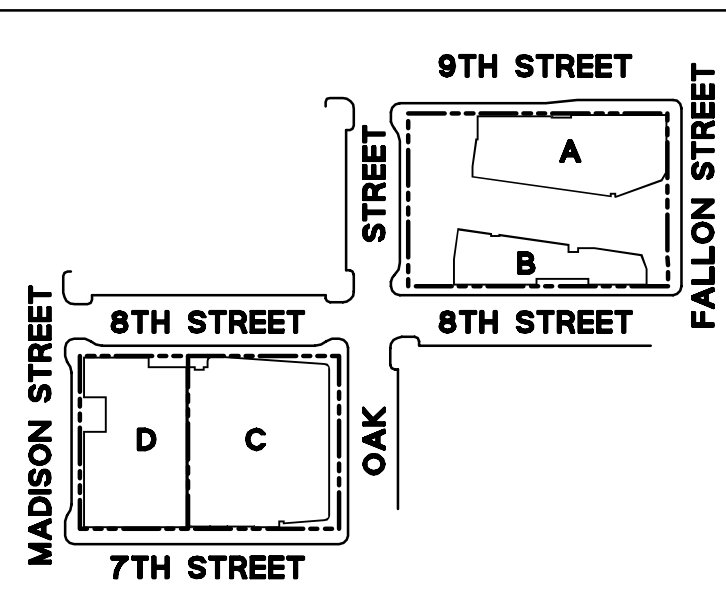
BENCHMARK

FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD88)

ELEVATIONS SHOWN ARE ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT. SUBTRACTING NAVD88 ELEVATIONS BY 5.68' WILL RESULT IN SHOWN COVD ELEVATIONS.



*[Handwritten signature]*





**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

100 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/06/2021
100 SD - HORIZONTAL FDP	05/02/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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JOB NUMBER: PYK 1808 | BKF: 20190110

DRAWN BY: DFS

CHECKED BY: SRN

ISSUE DATE: 05/02/2022

PHASE: 1

BLOCK: 1 + 2

SCALE: 1" = 20'

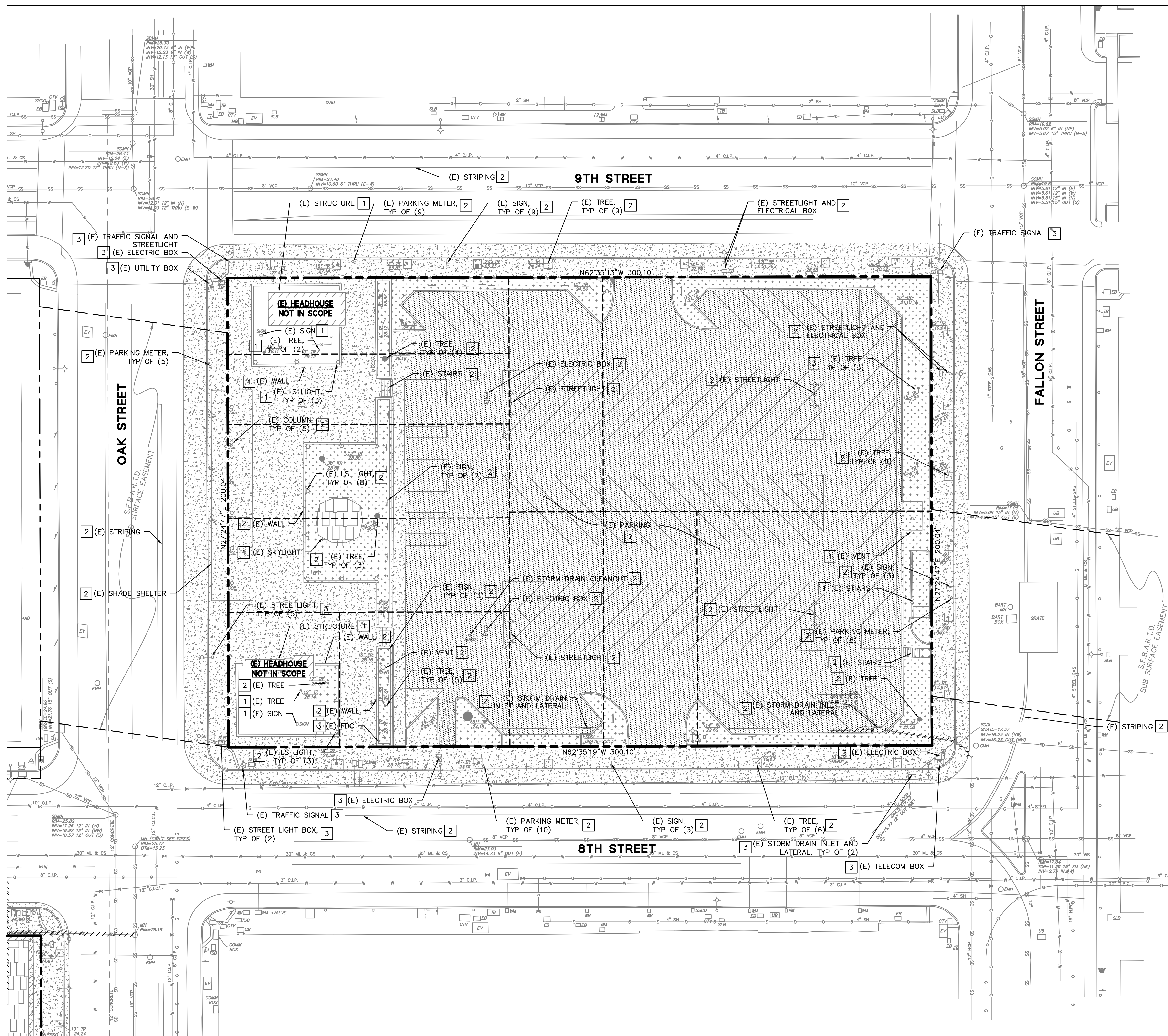
TITLE: DEMOLITION PLAN (BLOCK 1)

SHEET:

**C3.1**

PRELIMINARY - Not for Construction -

© 2021 EWINGKORRUEHL LANDSCAPE ARCHITECTURE & URBAN DESIGN

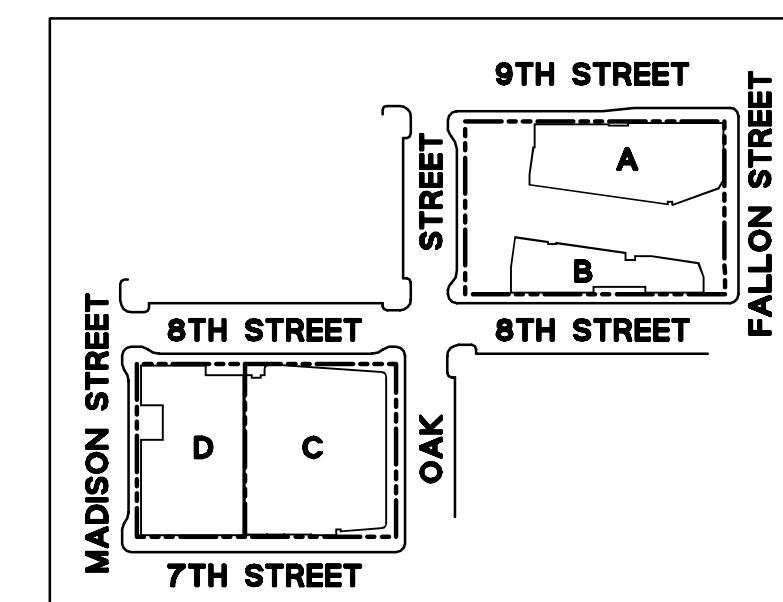
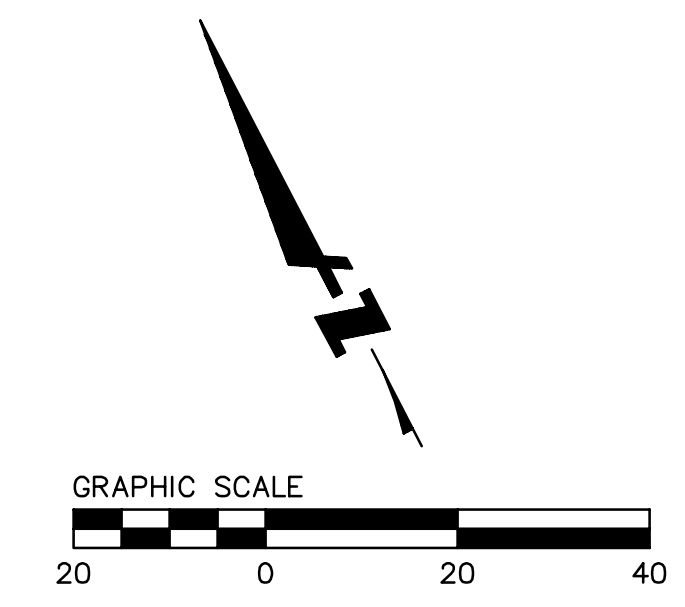


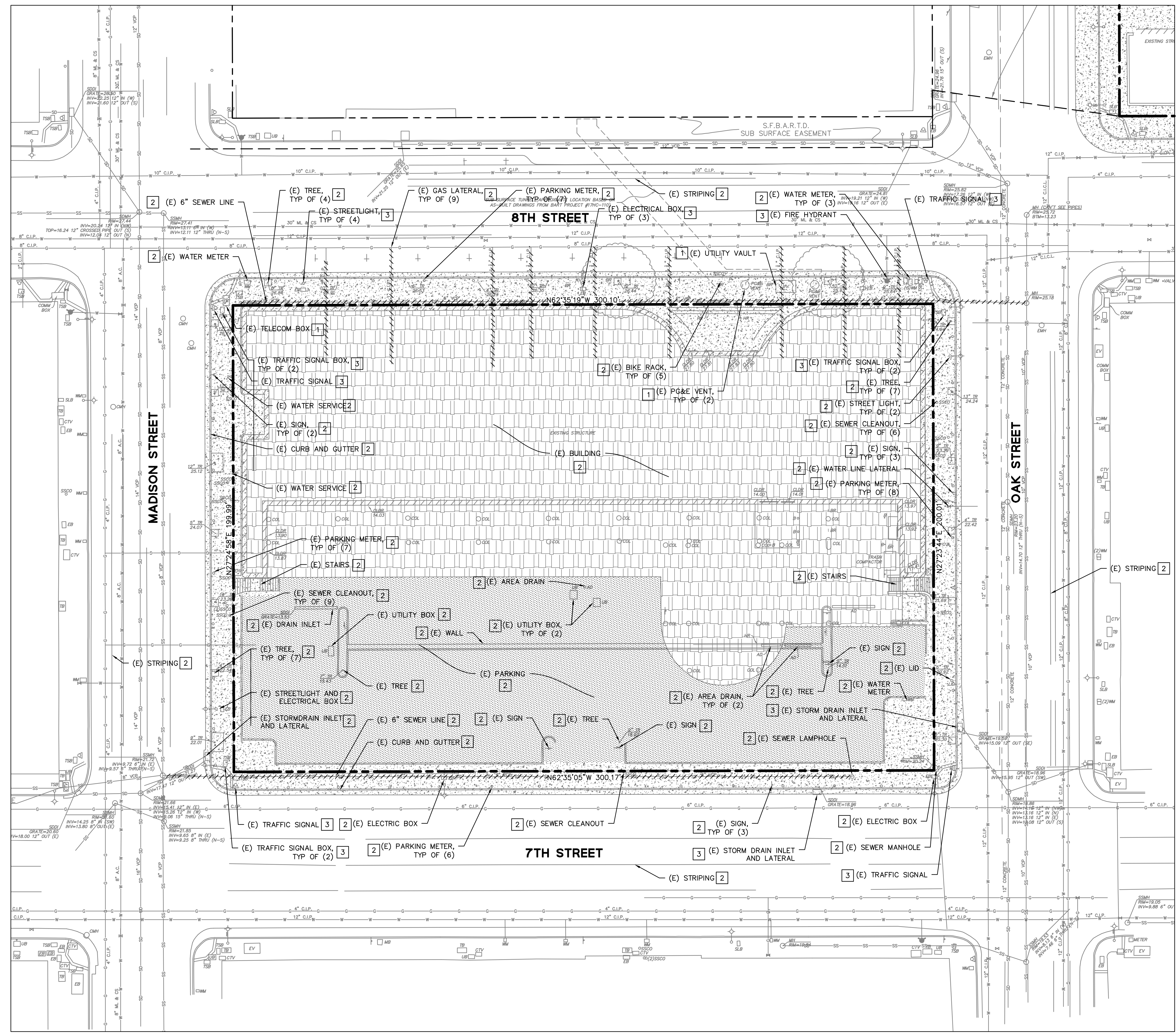
**DEMOLITION LEGEND:**

- SAWCUT DEMO & REMOVE EXISTING ASPHALT PARKING LOT & FULL DEPTH AC
- SAWCUT DEMO & REMOVE EXISTING CONCRETE HARDSCAPE, INCLUDING SIDEWALK, CURB, & GUTTER
- EXISTING BUILDING TO BE DEMOLISHED
- EXISTING LANDSCAPE TO BE REMOVED
- SAWCUT LINE
- EXISTING UTILITY LINE TO BE ABANDONED/REMOVED AS NEEDED

**DEMOLITION KEYNOTES:**

- 1** TO REMAIN, PROTECT IN PLACE
- 2** TO BE REMOVED
- 3** TO BE RELOCATED



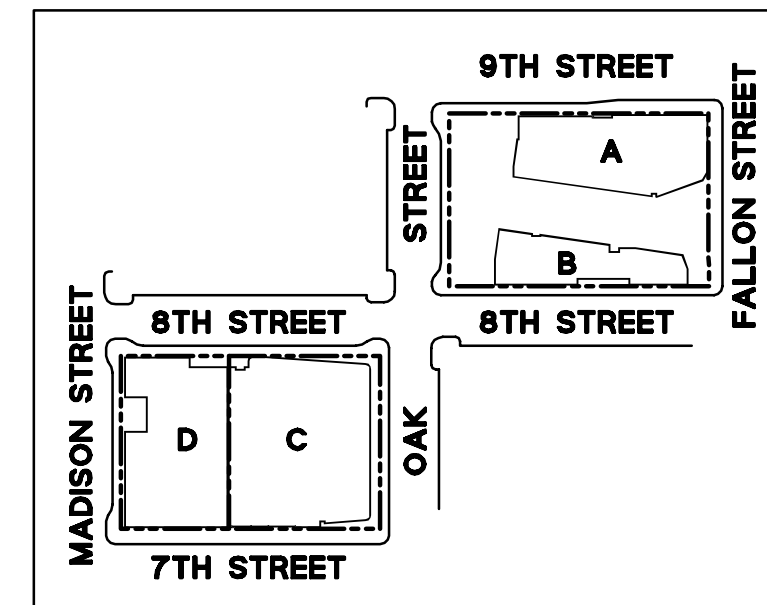
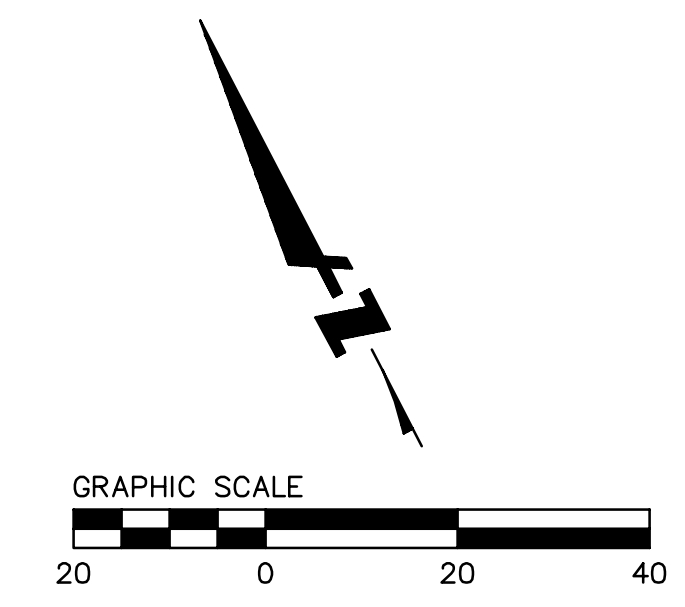


**DEMOLITION LEGEND:**

- SAWCUT DEMO & REMOVE EXISTING ASPHALT PARKING LOT & FULL DEPTH AC
- SAWCUT DEMO & REMOVE EXISTING CONCRETE HARDSCAPE, INCLUDING SIDEWALK, CURB, & GUTTER
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- EXISTING UTILITY LINE TO BE ABANDONED/REMOVED AS NEEDED

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- 2** TO BE REMOVED
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**REVISION SCHEDULE**

NO.	ISSUE	DATE
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JOB NUMBER: PYK 1808 | BKF: 20190110  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE: DEMOLITION PLAN (BLOCK 2)

SHEET:  
**C3.2**

PRELIMINARY - Not for Construction -





LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

100 SD - HORIZONTAL FDP	10/29/2021
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REVISION SCHEDULE

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JOB NUMBER: PYK 1808 | BKF: 20190110  
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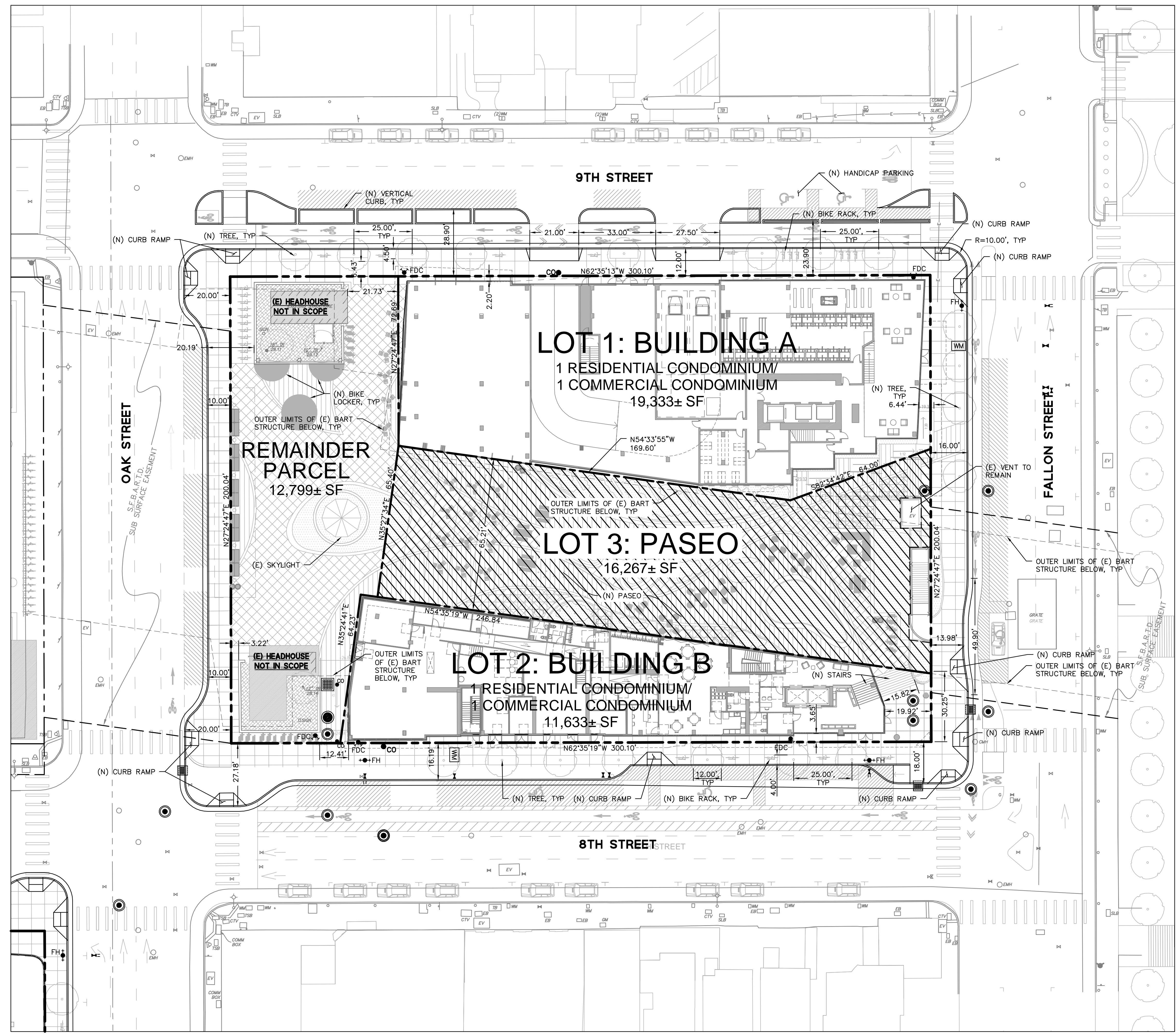
PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE:  
HORIZONTAL CONTROL PLAN  
(BLOCK 1)

SHEET:

C4.1

PRELIMINARY - Not for Construction -

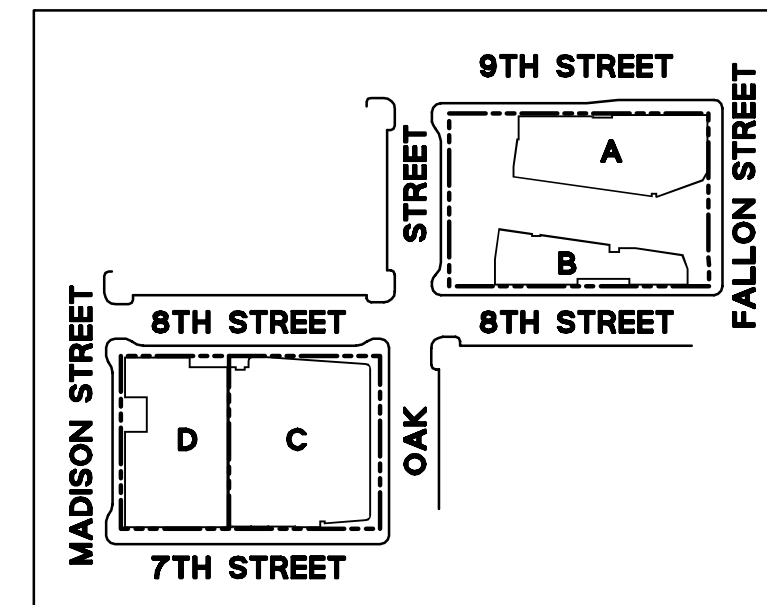
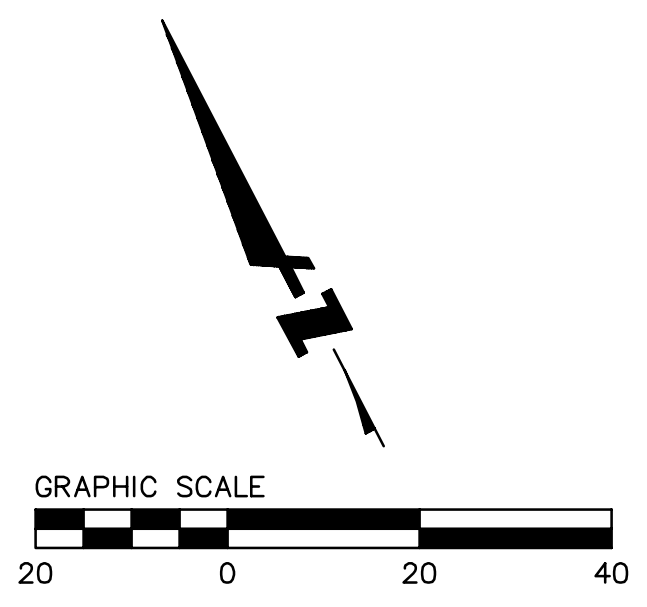


SITE PLAN NOTES:

1. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
2. SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
3. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
4. ALL PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS.
5. COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
6. SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
7. FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

LEGEND:

- BOUNDARY LINE
- LOT LINE
- - - EASEMENT LINE
- - - OUTER LIMITS OF (E) BART STRUCTURE BELOW
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE





**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE		
100 SD - HORIZONTAL FDP		10/29/2021
100 SD - HORIZONTAL FDP		12/06/2021
100 SD - HORIZONTAL FDP		05/02/2022

REVISION SCHEDULE		
NO.	ISSUE	DATE

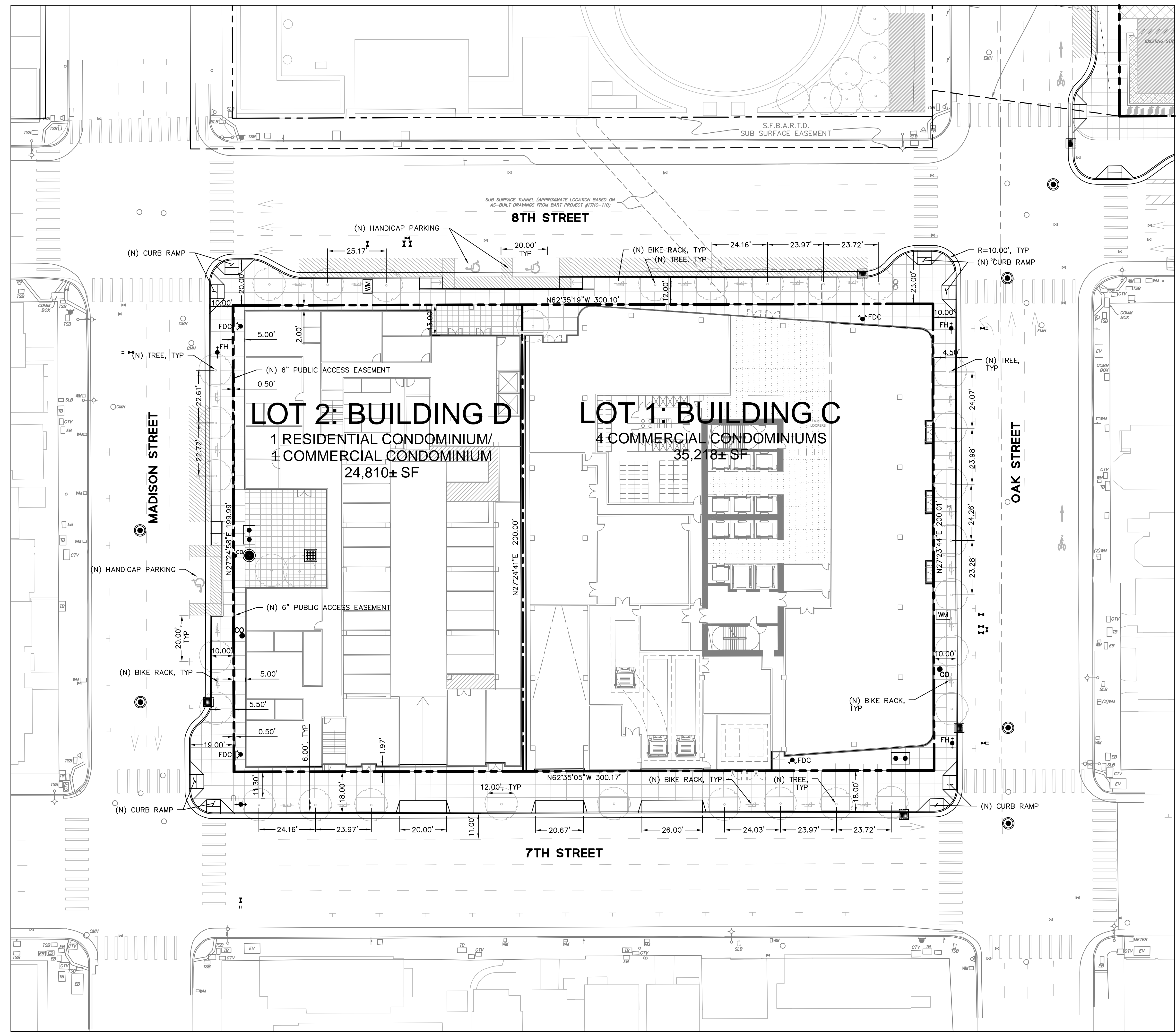
JOB NUMBER: PYK: 1808   BKF: 20190110	
DRAWN BY: DFS	
CHECKED BY: SRN	
ISSUE DATE: 05/02/2022	

PHASE: 1	
BLOCK: 1 + 2	
SCALE: 1" = 20'	

TITLE:  
HORIZONTAL CONTROL PLAN  
(BLOCK 2)

SHEET:  
**C4.2**

PRELIMINARY - Not for Construction -

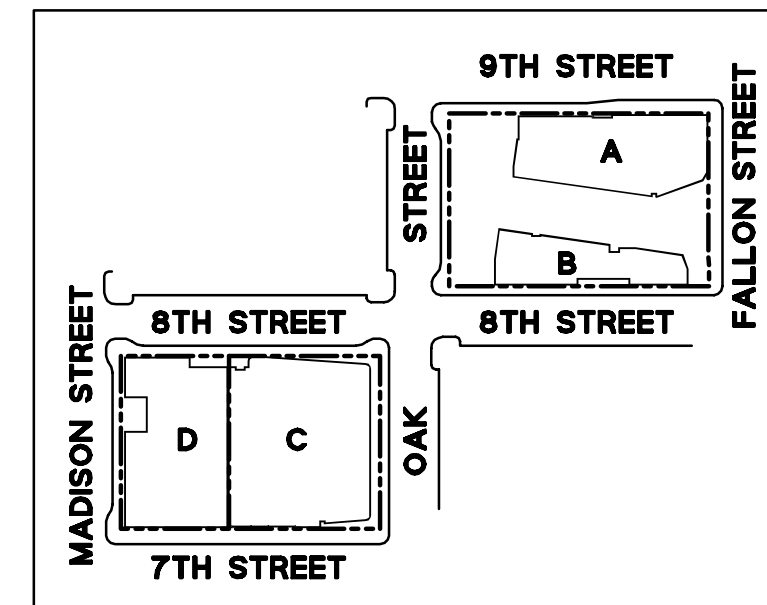
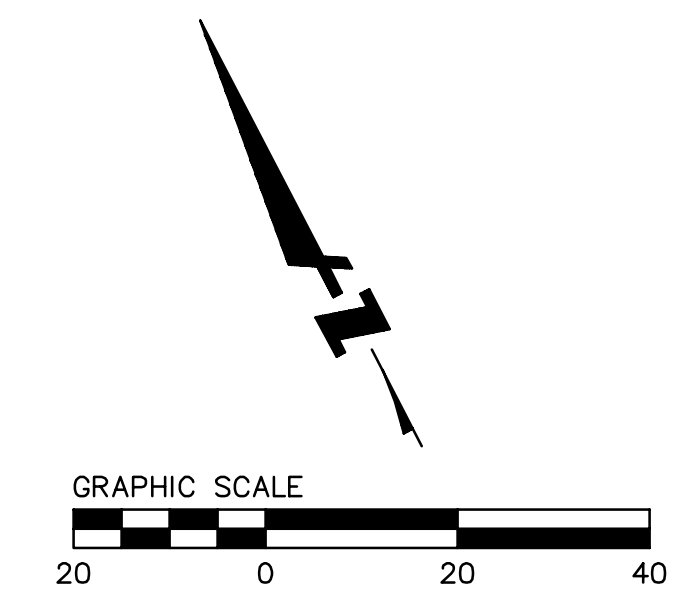


**SITE PLAN NOTES:**

1. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
2. SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
3. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
4. ALL PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS.
5. COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
6. SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
7. FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- - - EASEMENT LINE
- - - OUTER LIMITS OF (E) BART STRUCTURE BELOW
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE





**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

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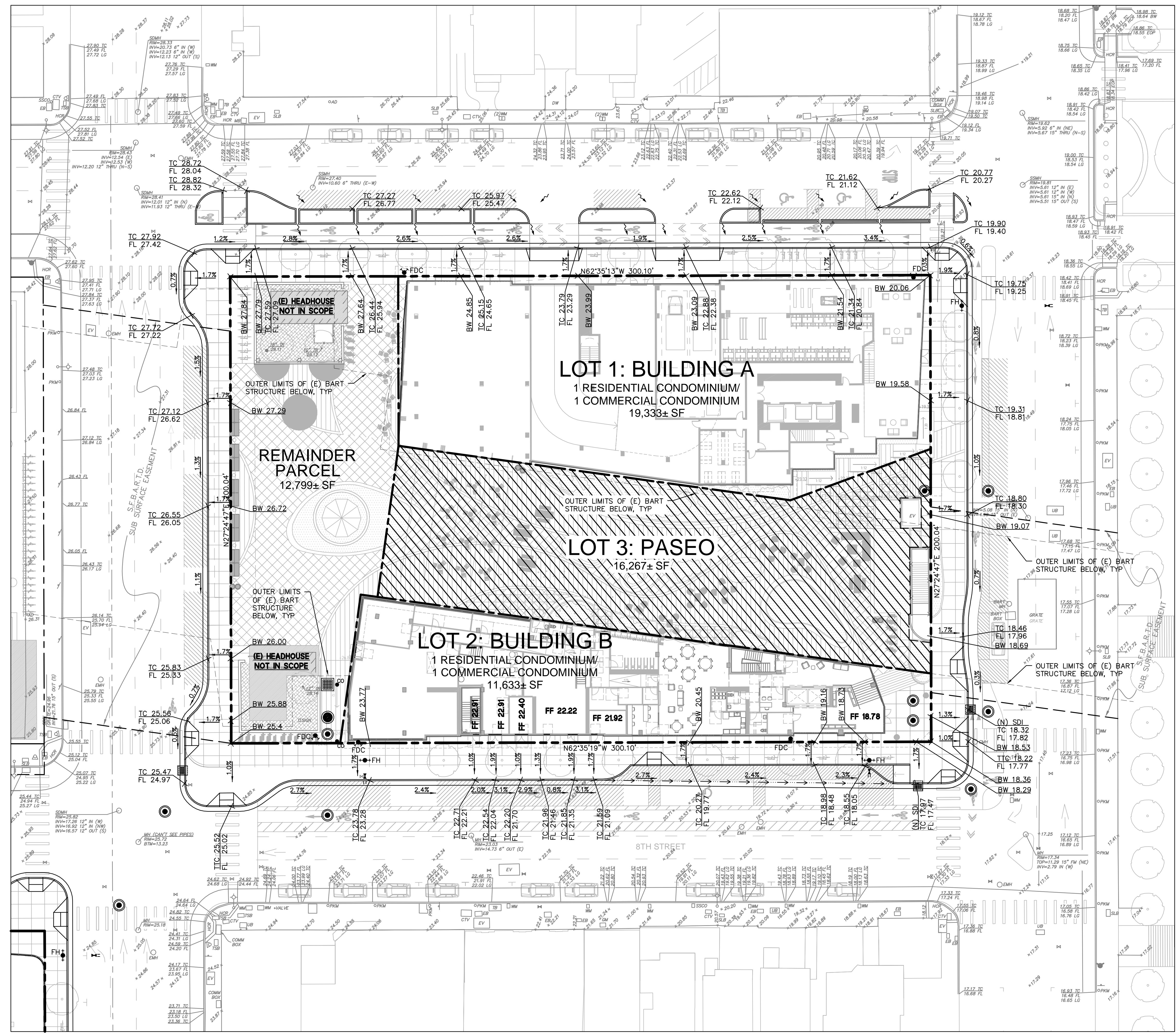
PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE: GRADING PLAN (BLOCK 1)

SHEET:

**C5.1**

PRELIMINARY - Not for Construction -

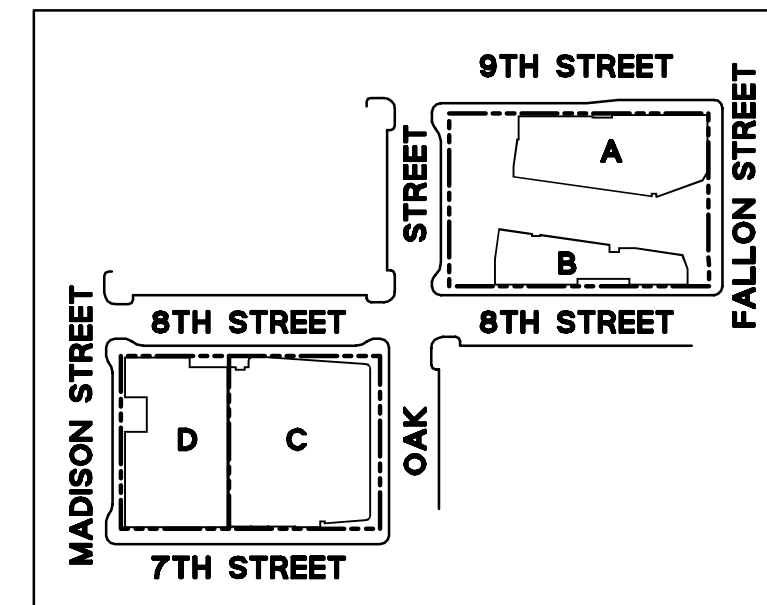
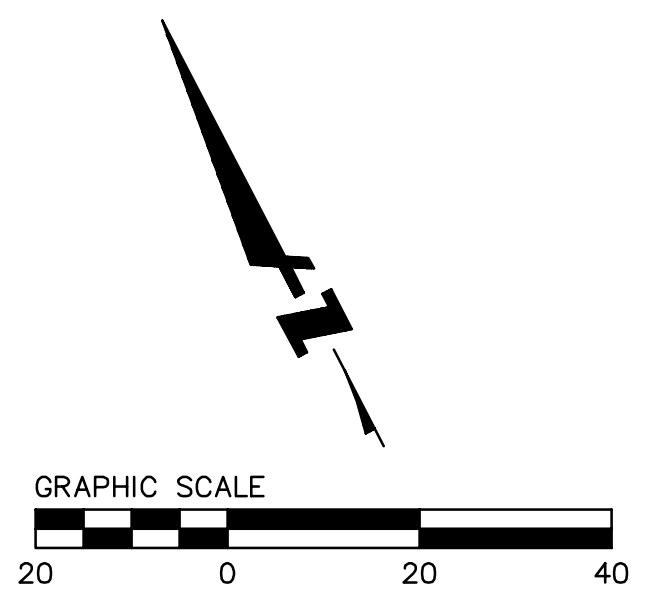


**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2% UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05'. HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- - - EASEMENT LINE
- - - OUTER LIMITS OF (E) BART STRUCTURE BELOW
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE





**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

100 SD - HORIZONTAL FDP	10/29/2021
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REVISION SCHEDULE

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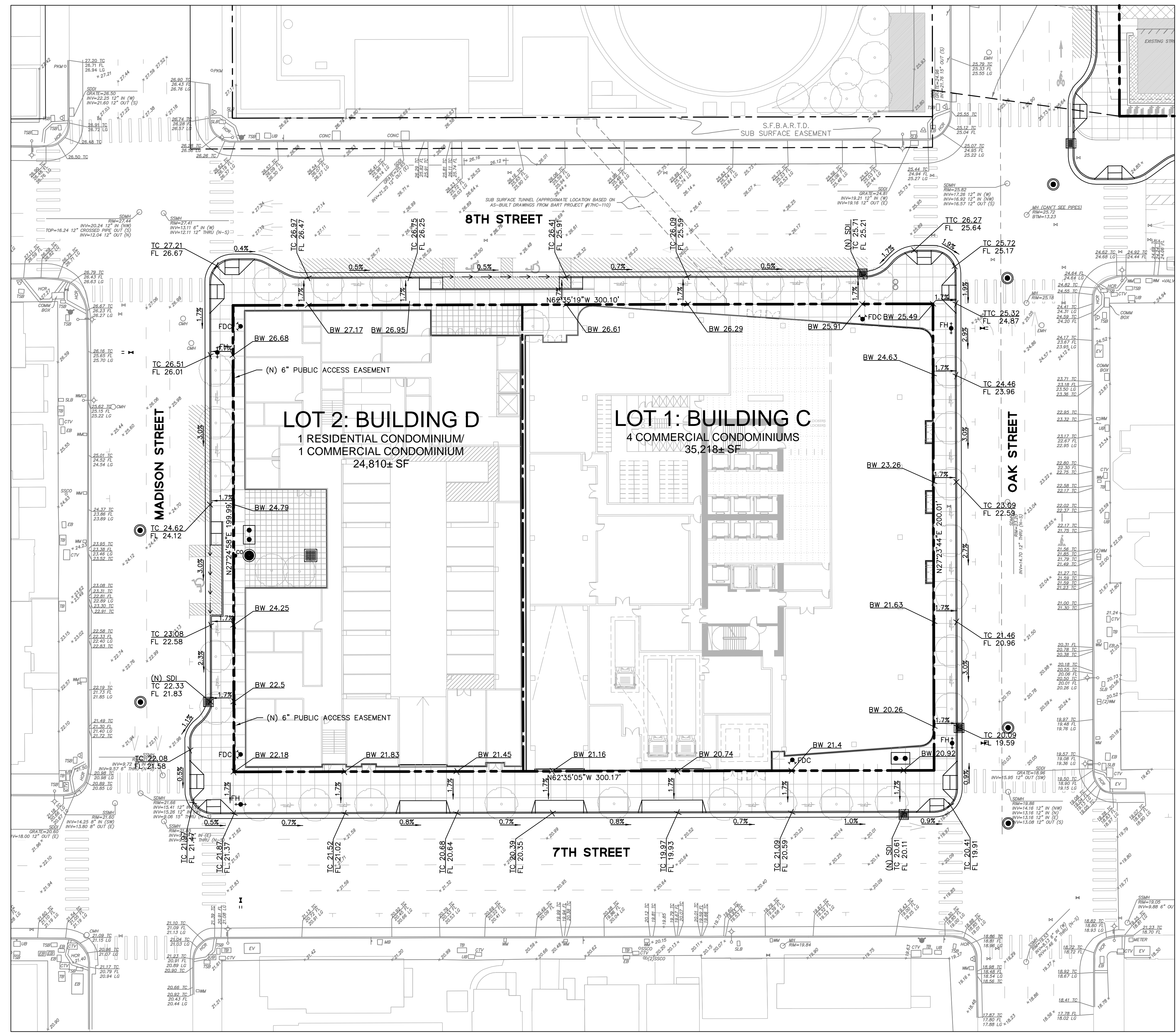
JOB NUMBER: PYK 1808 | BKF: 20190110  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE: GRADING PLAN (BLOCK 2)

SHEET:  
**C5.2**

PRELIMINARY - Not for Construction -

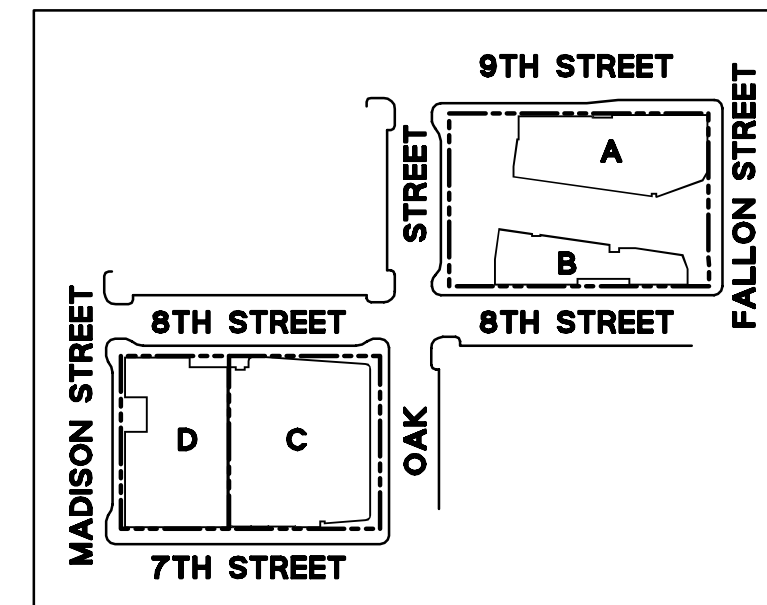
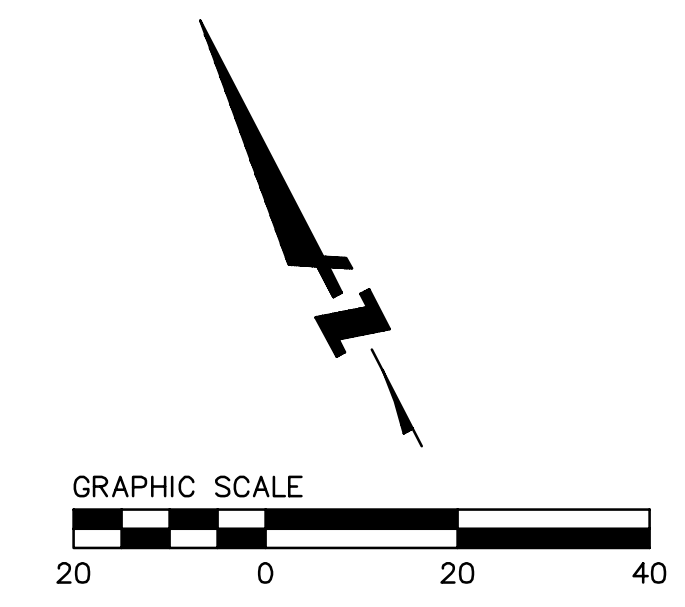


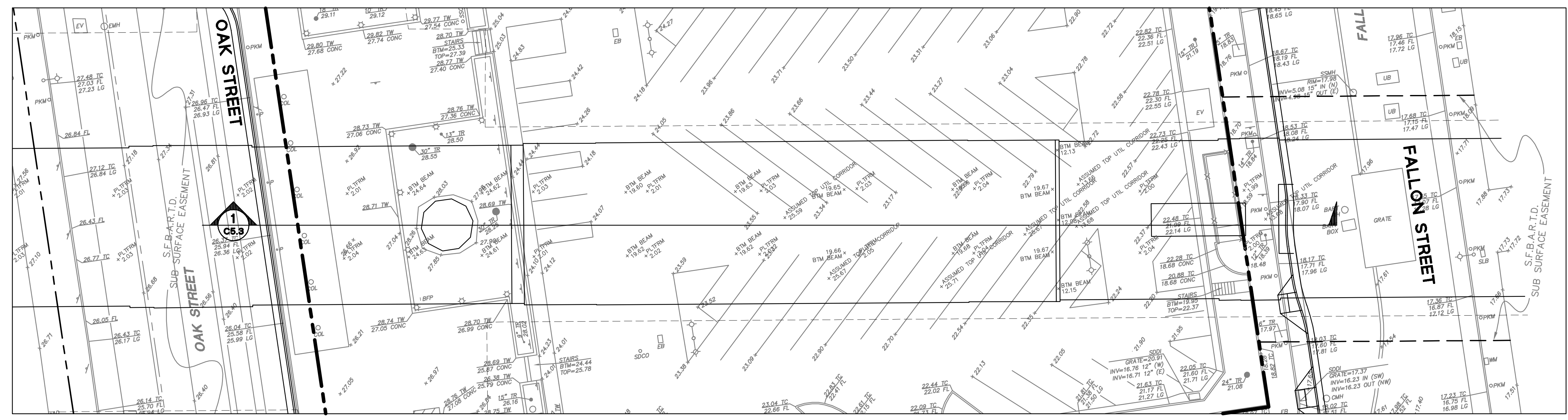
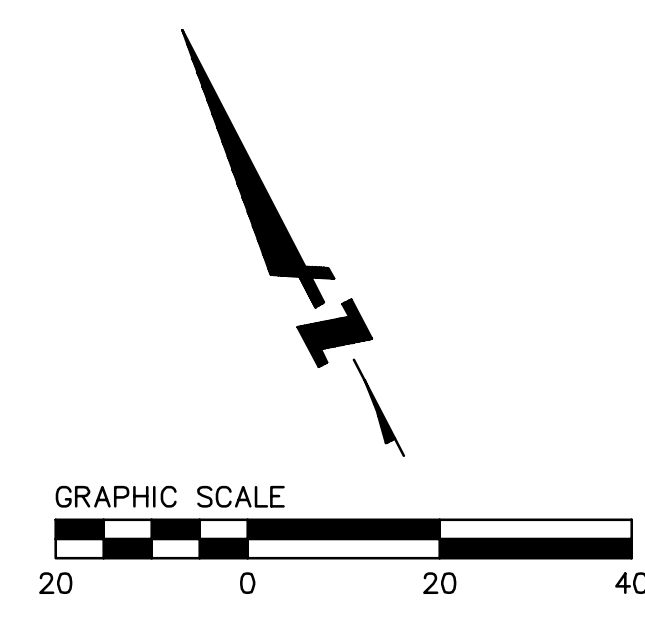
**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2% UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
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5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05'. HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

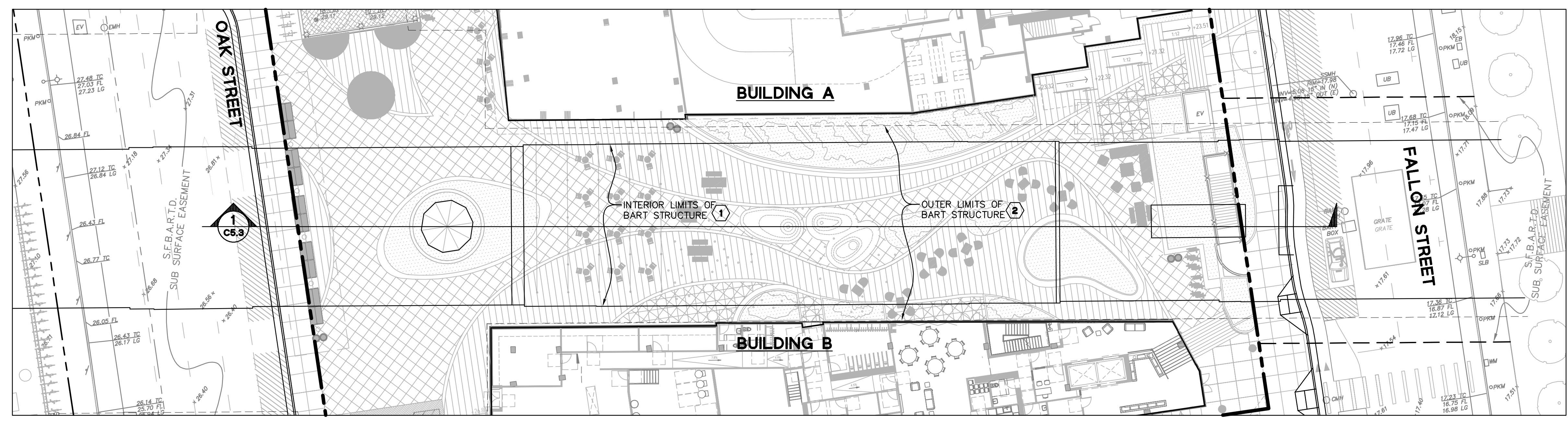
**LEGEND:**

- BOUNDARY LINE
- - - LOT LINE
- - - EASEMENT LINE
- - - OUTER LIMITS OF (E) BART STRUCTURE BELOW
- /// PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

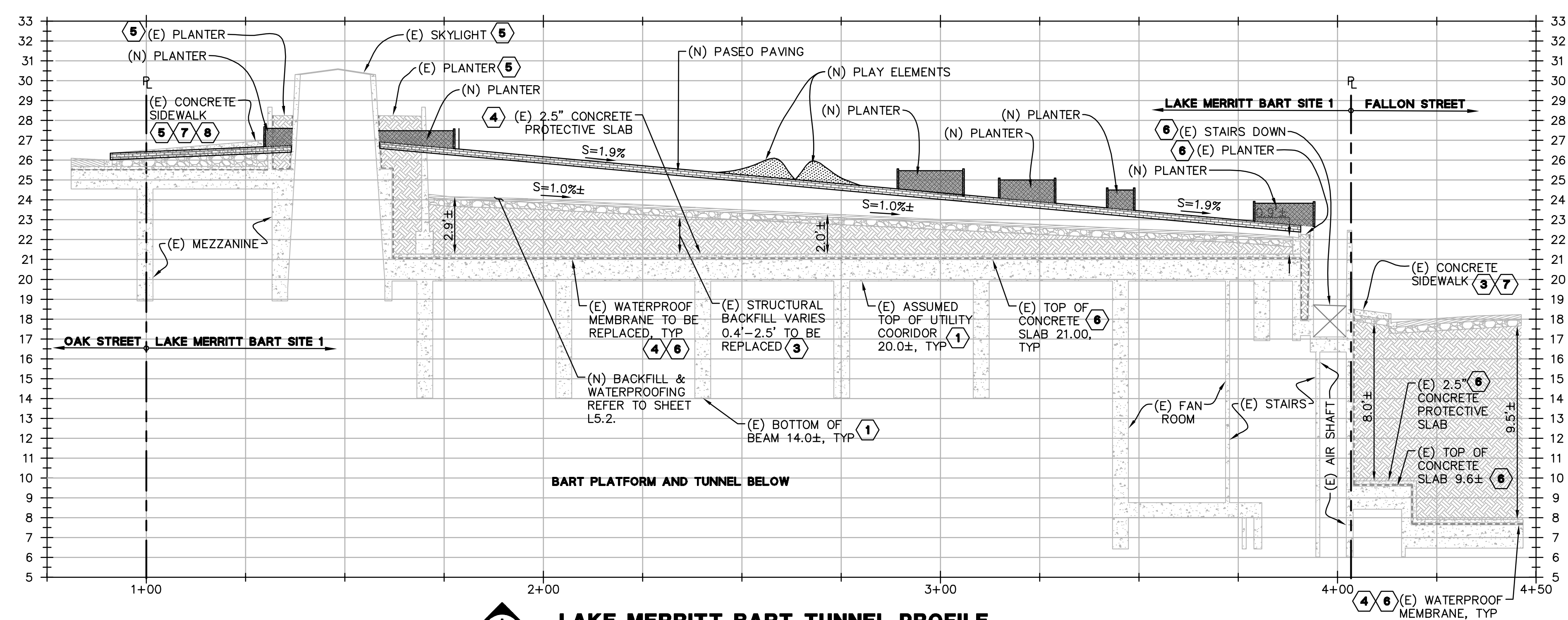




**EXISTING PLAN VIEW**  
SCALE: 1" = 20'



**PROPOSED PLAN VIEW**  
SCALE: 1" = 20'



**LAKE MERRITT BART TUNNEL PROFILE**  
HORZ SCALE: 1" = 20' - VERT SCALE: 1" = 4'

**NOTES:**

1. REFER TO THE KEYNOTES BELOW THAT REFERENCE VARIOUS BART AS-BUILT RECORD DRAWINGS USED TO CREATE THESE PROFILES AND SECTIONS. NOTE THAT ANY ELEVATIONS TAKEN FROM THESE AS-BUILT PLANS HAVE BEEN DECREASED BY 2.99' TO CORRELATE WITH THE NAVD88 SURVEY COORDINATE SYSTEM.

**KEYNOTES:**

- 1 INTERIOR TUNNEL LIMITS OBTAINED FROM SCAN INFORMATION COLLECTED ON 9/15/19, PER BKF "UNDERGROUND BART STATION EXHIBIT", DATED 9/23/19
- 2 EXTERIOR TUNNEL LIMITS WERE DETERMINED BY OFFSETTING THE CALCULATED, BEST-FIT CENTERLINE OF THE PLATFORM BY 31.33 FT, PER BKF "UNDERGROUND BART STATION EXHIBIT" DATED 9/23/19
- 3 REFER TO SHEET CT5-2 FROM BART CONTRACT #K0071-K007
- 4 REFER TO SHEET AR61-1 FROM BART CONTRACT #K0071-K007
- 5 REFER TO SHEET AR29-1 FROM BART CONTRACT #K0071-K007
- 6 REFER TO SHEET AR30-1 FROM BART CONTRACT #K0071-K007
- 7 REFER TO SHEET CT3-2 FROM BART CONTRACT #K0071-K007
- 8 REFER TO SHEET CT9-1 FROM BART CONTRACT #K0071-K007

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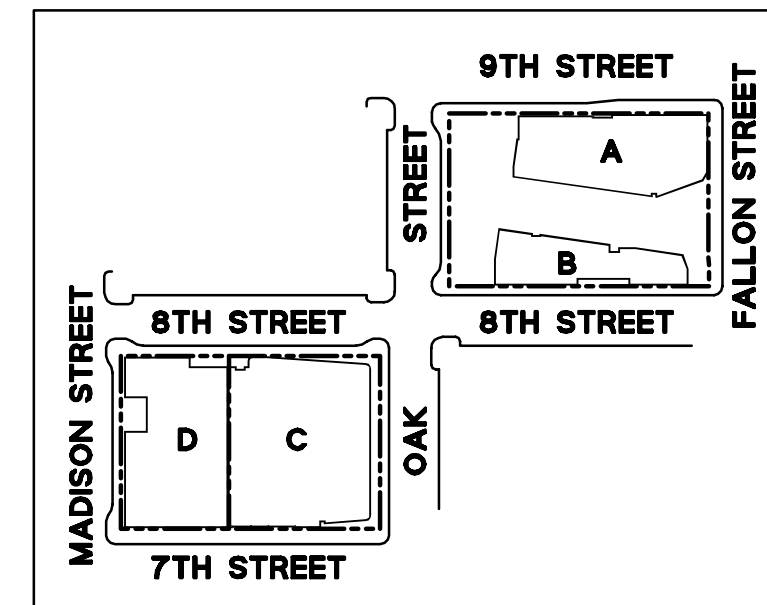
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PHASE: 1	
BLOCK: 1 + 2	
SCALE: 1" = 20'	

TITLE: SECTIONS

SHEET: **C5.3**

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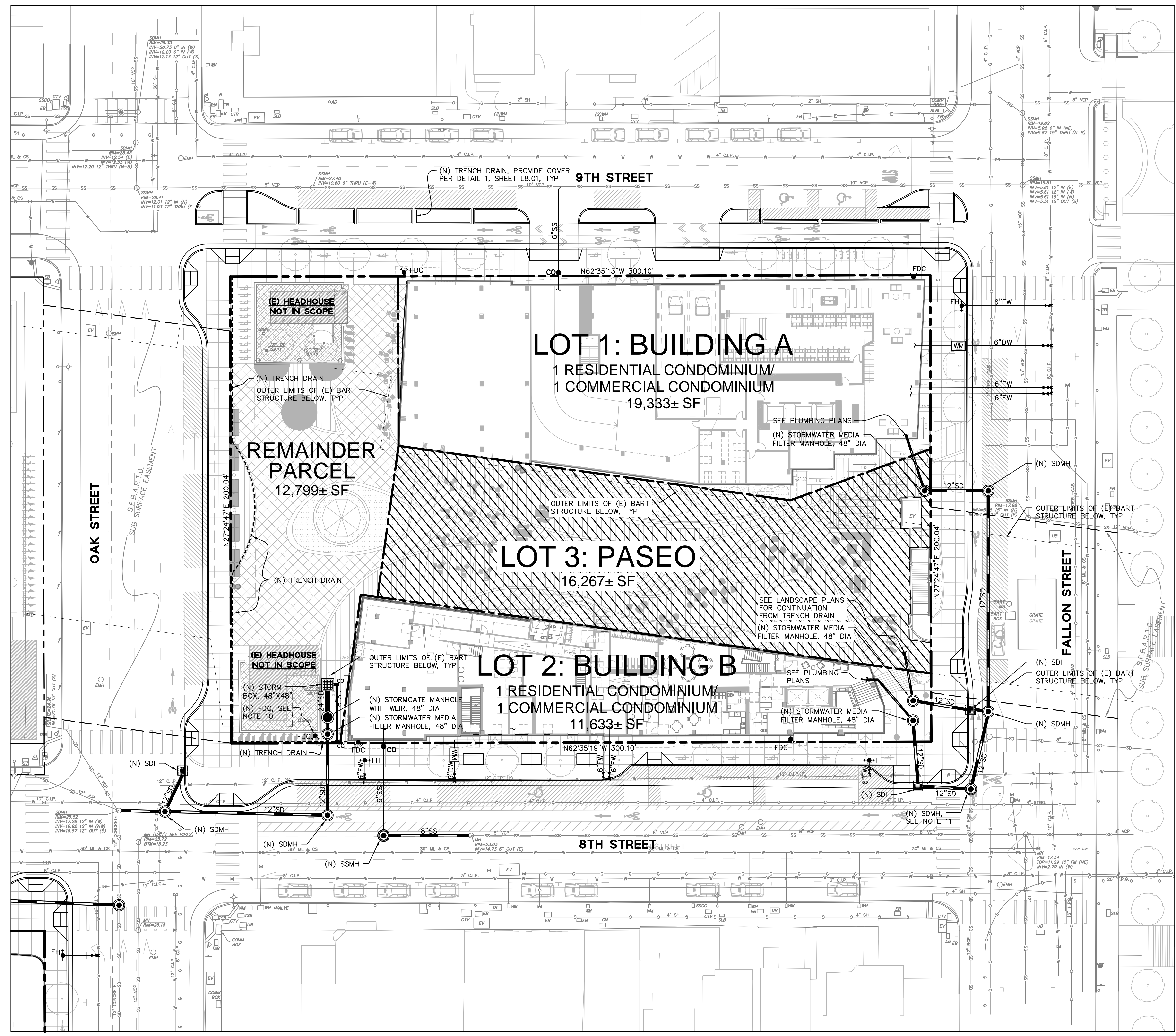
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BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE:  
UTILITY PLAN (BLOCK 1)

SHEET:

C6.1

PRELIMINARY - Not for Construction -

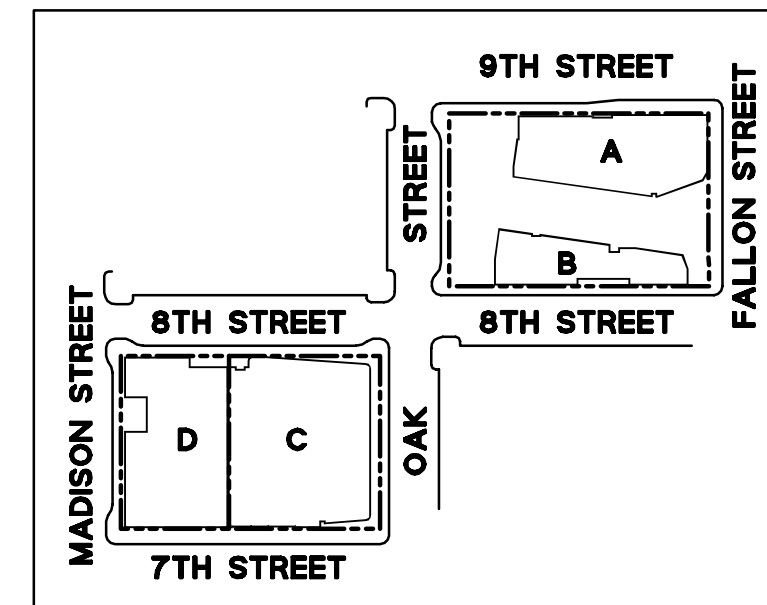
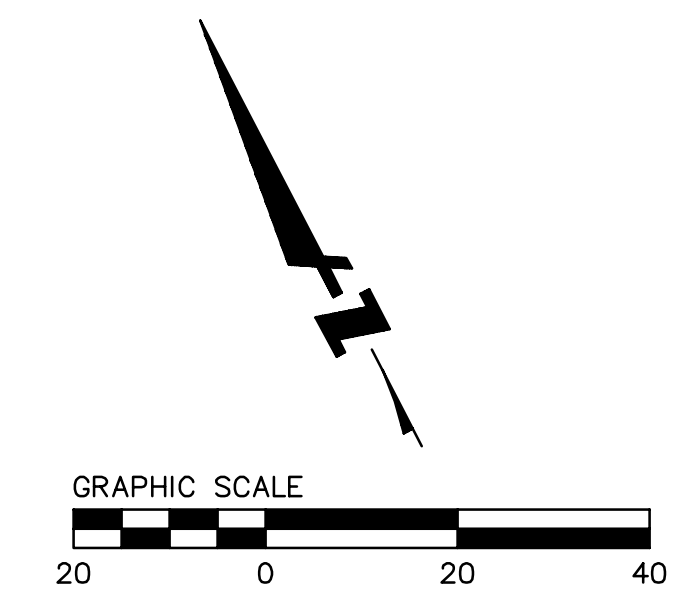


UTILITY NOTES:

1. INFORMATION REGARDING EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM RECORD DATA AND IS NOT MEANT TO BE A FULL CATALOG OF EXISTING CONDITIONS. CONTRACTOR SHALL CONDUCT FIELD INVESTIGATIONS, SUCH AS POT-HOLING, AS REQUIRED TO VERIFY THE LOCATIONS, ELEVATIONS, AND CONNECTION POINTS OF ALL EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES, WHETHER SHOWN ON THESE PLANS OR NOT, PRIOR TO THE COMMENCEMENT OF WORK AND UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
2. DOMESTIC WATER, FIRE WATER, AND SANITARY SEWER LATERAL SIZES TO BE CONFIRMED DURING THE DETAILED DESIGN PHASE.
3. CONTRACTOR SHALL COORDINATE GRAVITY UTILITY WORK WITH ALL JOINT TRENCH/RULE 20 UNDERGROUNDING WORK. IF JOINT TRENCH SCOPE OF WORK IS TO BE PERFORMED FIRST, CONTRACTOR SHALL STAKE LOCATIONS AND ELEVATIONS OF ALL PROPOSED GRAVITY UTILITY CROSSINGS. JOINT TRENCH TO BE INSTALLED WITH MINIMUM 12" VERTICAL CLEARANCE TO PROPOSED GRAVITY UTILITY AT ALL CROSSINGS.
4. ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
5. ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
6. ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
7. FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POT-HOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR CROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
8. DRAINS SHOWN ON CIVIL PLANS ARE NOT INTENDED TO BE THE FINAL NUMBER AND LOCATION OF ALL DRAINS. PLACEMENT AND NUMBER OF LANDSCAPING DRAINS ARE HIGHLY DEPENDENT ON GROUND COVER TYPE AND PLANT MATERIAL. CONTRACTOR SHALL ADD ADDITIONAL AREA DRAINS AS NEEDED AND AS DIRECTED BY THE LANDSCAPE ARCHITECT OR CIVIL ENGINEER.
9. THE LOCATION OF THE 12" WATER LINE IN 8TH STREET HAS BEEN SHOWN BASED ON RECORD INFORMATION PROVIDED BY EAST BAY MUD. EXACT LOCATION TO BE VERIFIED AND CONFIRMED PRIOR TO INSTALLATION OF THE PROPOSED DOMESTIC AND FIRE WATER SERVICES TO BUILDING B.
10. THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.
11. EXISTING STORM DRAIN MAIN INVERT AT THE INTERSECTION OF 8TH STREET AND FALLON STREET TO BE POT-HOLED AND VERIFIED.
12. STORMWATER CISTERN LOCATED WITHIN BUILDING B. SEE PLUMBING PLANS FOR CISTERN DETAILS AND LOCATION.

LEGEND:

- BOUNDARY LINE
- LOT LINE
- - - EASEMENT LINE
- - - OUTER LIMITS OF (E) BART STRUCTURE BELOW
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE





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PHASE: 1

BLOCK: 1 + 2

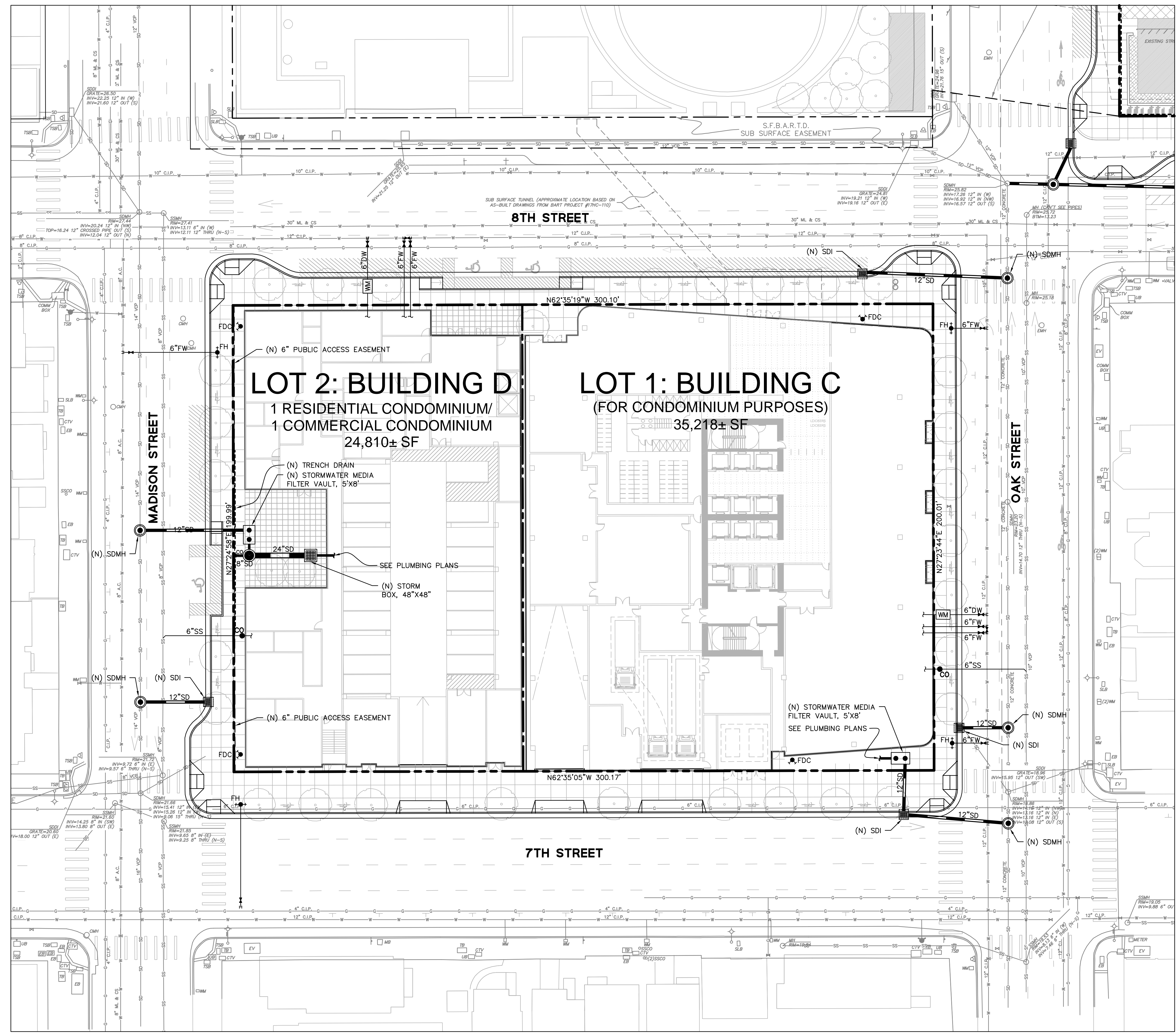
SCALE: 1" = 20'

TITLE: UTILITY PLAN (BLOCK 2)

SHEET:

**C6.2**

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**UTILITY NOTES:**

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- ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
- ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
- ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
- FOR FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POT-HOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR CROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
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- THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.
- EXISTING STORM DRAIN MAIN INVERT AT THE INTERSECTION OF 8TH STREET AND FALLON STREET TO BE POT-HOLED AND VERIFIED.
- STORMWATER CISTERN LOCATED WITHIN BUILDING B. SEE PLUMBING PLANS FOR CISTERN DETAILS AND LOCATION.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- OUTER LIMITS OF (E) BART STRUCTURE BELOW
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE



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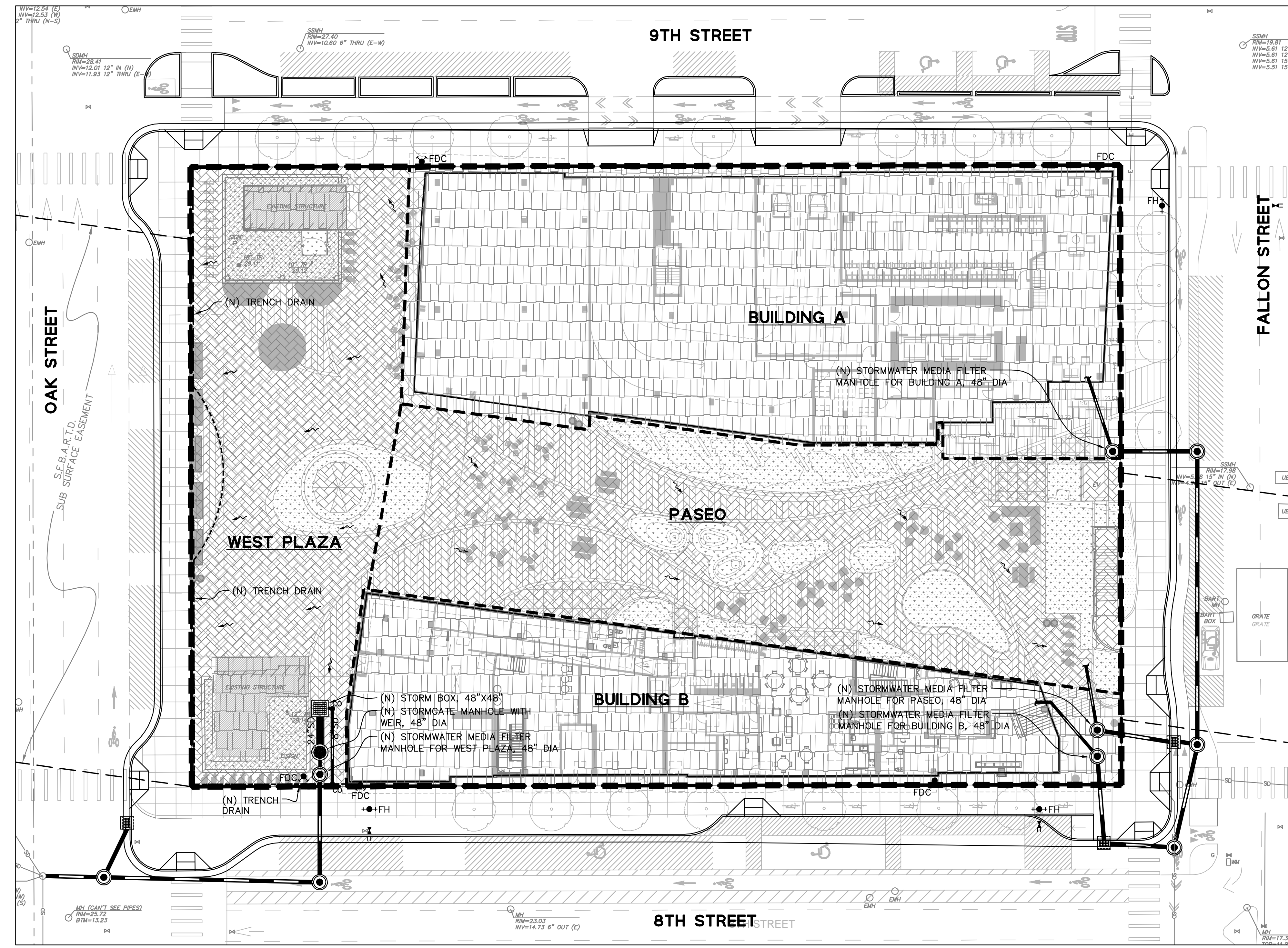
PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

TITLE: STORMWATER CONTROL PLAN (BLOCK 1)

SHEET:

C7.1

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STORMWATER COMPLIANCE DATA (BLOCK 1)

PER THE MUNICIPAL REGIONAL STORMWATER PERMIT ORDER NO. R2-0074, CERTAIN DEVELOPMENT PROJECTS THAT QUALIFY AS "SPECIAL PROJECTS" ARE ELIGIBLE FOR LOW IMPACT DESIGN TREATMENT REDUCTION CREDITS. THE LID TREATMENT REDUCTION CREDIT IS THE MAXIMUM PERCENTAGE OF THE AMOUNT OF RUNOFF THAT MAY BE TREATED WITH EITHER TREE-BOX-TYPE HIGH FLOWRATE BIOFILTERS OR VAULT-BASED HIGH FLOWRATE MEDIA FILTERS. THIS PROJECT IS CLASSIFIED AS A CATEGORY C SPECIAL PROJECT (TRANSIT ORIENTED) AND QUALIFIES FOR A TOTAL LID TREATMENT REDUCTION CREDIT OF 100% AS DESCRIBED BELOW.

CRITERIA FOR CATEGORY C (TRANSIT ORIENTED DEVELOPMENT) SPECIAL PROJECTS TO BE CONSIDERED A CATEGORY C SPECIAL PROJECT, A PROVISION C.3 REGULATED PROJECT MUST MEET ALL OF THE FOLLOWING CRITERIA:

- BE CHARACTERIZED AS A NON AUTO-RELATED LAND USE PROJECT. THAT IS, CATEGORY C SPECIFICALLY EXCLUDES ANY REGULATED PROJECT THAT IS A STAND-ALONE SURFACE PARKING LOT, CAR DEALERSHIP, AUTO AND TRUCK RENTAL FACILITY WITH ONSITE SURFACE STORAGE; FAST-FOOD RESTAURANT, BANK OR PHARMACY WITH DRIVE-THROUGH LANES; GAS STATION, CAR WASH, AUTO REPAIR AND SERVICE FACILITY; OR OTHER AUTO RELATED PROJECT UNRELATED TO THE CONCEPT OF TRANSIT-ORIENTED DEVELOPMENT.
- IF A COMMERCIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1.
- IF A RESIDENTIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST A DENSITY OF 25 DU/AC.
- IF A MIXED-USE DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1 OR A DENSITY OF 25 DU/AC.

100% LID TREATMENT REDUCTION CREDIT

- 50% REDUCTION CREDIT - PROJECT IS LOCATED WITHIN A 1/4 MILE RADIUS OF A TRANSIT HUB
- 30% REDUCTION CREDIT - MIXED USE PROJECT WITH DENSITY GREATER THAN 100 DU/ACRE
- 20% REDUCTION CREDIT - 0% OF TOTAL POST-PROJECT IMPERVIOUS SURFACE IS DEDICATED TO AT-GRADE, SURFACE PARKING

STORMWATER AREA SUMMARY

THE CITY OF OAKLAND STORM DRAINAGE DESIGN GUIDELINES ESTABLISHES A 25% GOAL FOR PEAK FLOW REDUCTION COMPARED TO EXISTING CONDITIONS, TO THE EXTENT POSSIBLE. DUE TO THE FACT THAT ALMOST 90% OF THE SITE IS COVERED BY PERMANENT STRUCTURE, INCLUDING THE EXISTING BART TUNNEL, THERE IS LIMITED SPACE FOR DETENTION MEASURES ONSITE. AS A RESULT, A 25% PEAK FLOW REDUCTION WAS ACCOMPLISHED IN ALL AREAS EXCEPT FOR THE PASEO.

EXISTING IMPERVIOUS SURFACE	PROPOSED IMPERVIOUS SURFACE
<b>BUILDING A</b>	<b>BUILDING A</b>
16,981 SF	19,333 SF
2,352 SF	0 SF
19,333 SF	19,333 SF
<b>BUILDING B</b>	<b>BUILDING B</b>
9,995 SF	11,627 SF
1,632 SF	0 SF
11,627 SF	11,627 SF
<b>PASEO</b>	<b>PASEO</b>
15,269 SF	12,059 SF
928 SF	4,208 SF
16,267 SF	16,267 SF
<b>WEST PLAZA</b>	<b>WEST PLAZA</b>
9,328 SF	10,984 SF
3,476 SF	1,820 SF
12,804 SF	12,804 SF

STORMWATER LID TREATMENT SIZING

TOTAL LID TREATMENT REDUCTION CREDIT = 100%  
TOTAL BLOCK 1 IMPERVIOUS AREA OF 54,489 SF ALLOWED TO BE TREATED W/ NON-LID TREATMENT MEASURES (MEDIA FILTER UNITS)

**BUILDING A**  
 $Q_{c3} = CIA = (0.9)(0.2 \text{ IN/HR})(19,333 \text{ SF})$   
 $Q_{c3} = 0.0806 \text{ CFS}$   
 A 48" MEDIA FILTER MANHOLE WITH (2) 12" & 12" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.11 CFS.

**BUILDING B**  
 $Q_{c3} = CIA = (0.9)(0.2 \text{ IN/HR})(11,627 \text{ SF})$   
 $Q_{c3} = 0.0484 \text{ CFS}$   
 A 48" MEDIA FILTER MANHOLE WITH (1) 12" & 12" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.05 CFS.

**PASEO**  
 $Q_{c3} = CIA = (0.9)(0.2 \text{ IN/HR})(12,059 \text{ SF})$   
 $Q_{c3} = 0.0502 \text{ CFS}$   
 A 48" MEDIA FILTER MANHOLE WITH (1) 12" & 18" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.07 CFS.

**WEST PLAZA**  
 $Q_{c3} = CIA = (0.9)(0.2 \text{ IN/HR})(10,984 \text{ SF})$   
 $Q_{c3} = 0.0458 \text{ CFS}$   
 A 48" MEDIA FILTER MANHOLE WITH (2) 12" CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.06 CFS.

STORMWATER PEAK FLOW REDUCTION

PEAK FLOW CALCULATED PER CITY OF OAKLAND STORM DRAINAGE DESIGN STANDARDS.  
DESIGN STORM = 10-YEAR MEAN ANNUAL PRECIPITATION (MAP) = 21 INCHES TIME OF CONCENTRATION = 5 MINUTES

**DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:**

**BUILDING A**  
 $Q_{EX-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(16,981 \text{ SF})$   
 $Q_{EX-10YR} = 1,221 \text{ CFS}$   
 $Q_{PR-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(19,333 \text{ SF})$   
 $Q_{PR-10YR} = 1,390 \text{ CFS}$   
 PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((1.390/1.221)*100) = -13.8\%$

**BUILDING B**  
 $Q_{EX-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(9,995 \text{ SF})$   
 $Q_{EX-10YR} = 0,719 \text{ CFS}$   
 $Q_{PR-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(11,627 \text{ SF})$   
 $Q_{PR-10YR} = 0,836 \text{ CFS}$   
 PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((0.836/0.719)*100) = -16.3\%$

**PASEO**  
 $Q_{EX-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(15,269 \text{ SF})$   
 $Q_{EX-10YR} = 1,098 \text{ CFS}$   
 $Q_{PR-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(12,059 \text{ SF})$   
 $Q_{PR-10YR} = 0,867 \text{ CFS}$   
 PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((0.867/1.098)*100) = +21.0\%$   
 THE PASEO IMPROVEMENTS PROVIDE A 21.0% PEAK FLOW REDUCTION.

**WEST PLAZA**  
 $Q_{EX-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(9,328 \text{ SF})$   
 $Q_{EX-10YR} = 0,671 \text{ CFS}$   
 $Q_{PR-10YR} = CIA = (0.90)(3.48 \text{ IN/HR})(10,984 \text{ SF})$   
 $Q_{PR-10YR} = 0,790 \text{ CFS}$   
 PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((0.790/0.671)*100) = -17.7\%$

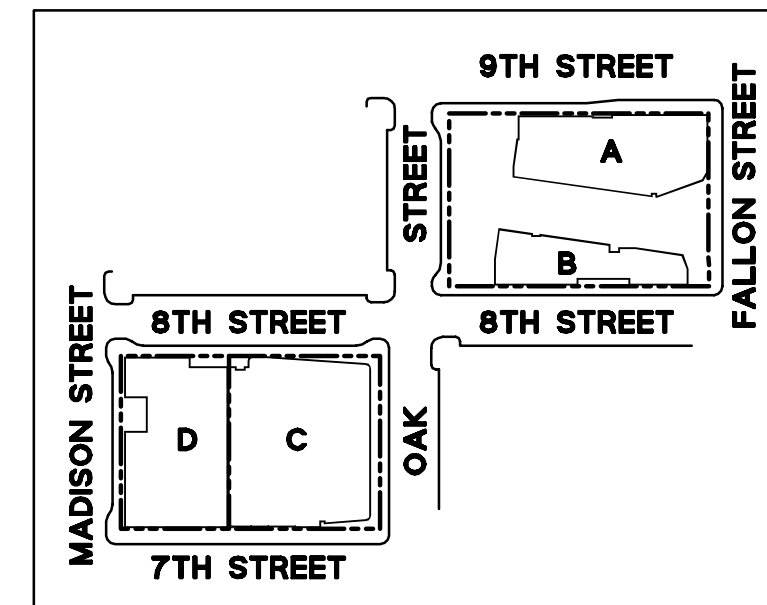
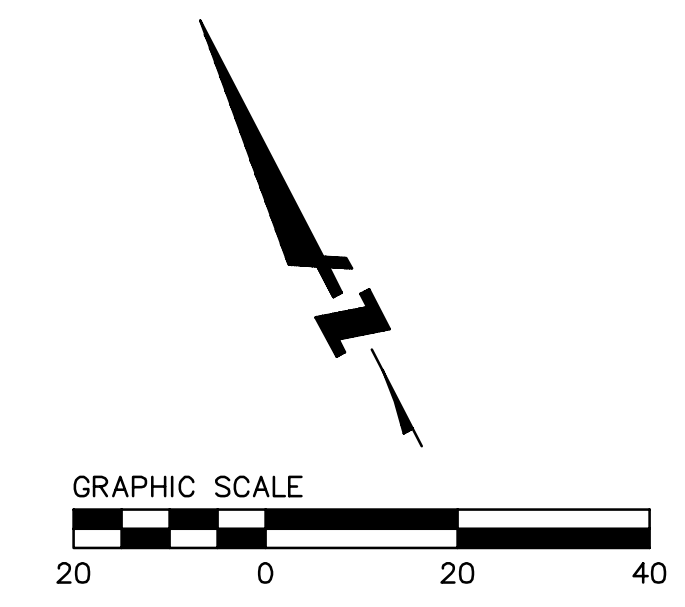
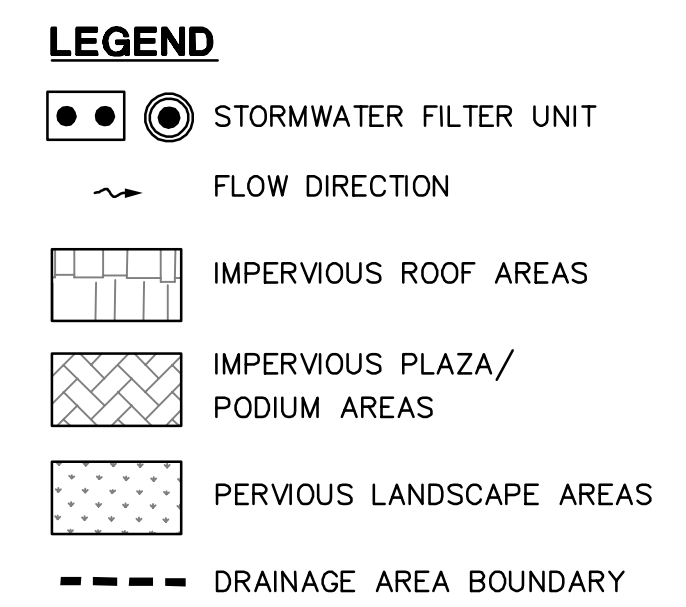
**DUE TO IMPLEMENTATION OF DETENTION TANK/CISTERN:**

**BUILDING A**  
 DETENTION VOLUME REQUIRED FOR 25%+13.8% REDUCTION  
 $V_{DET \text{ REQD}} (CF) = (0.25+0.138)(Q_{EX-10YR})^2 * T_c$   
 $V_{DET \text{ REQD}} (CF) = (0.388)(1,221 \text{ CFS})^2 * (5 \text{ MIN}) * (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQD}} (CF) = 213.2 \text{ CF OR } 1,597 \text{ GALLONS}$   
 DETENTION PIPE PROVIDED BY CISTERN IN BUILDING  
 $V_{DET \text{ PROVIDED}} = 7'X7' X 5' \text{ TALL CISTERN} = 245 \text{ CF}$   
 CISTERN TO BE LOCATED INSIDE THE BUILDING.

**BUILDING B**  
 DETENTION VOLUME REQUIRED FOR 25%+16.3% REDUCTION  
 $V_{DET \text{ REQD}} (CF) = (0.25+0.163)(Q_{EX-10YR})^2 * T_c$   
 $V_{DET \text{ REQD}} (CF) = (0.413)(0,719 \text{ CFS})^2 * (5 \text{ MIN}) * (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQD}} (CF) = 133.6 \text{ CF OR } 1,001 \text{ GALLONS}$   
 DETENTION PIPE PROVIDED BY CISTERN OUTSIDE THE BUILDING  
 $V_{DET \text{ PROVIDED}} = 6'X6' X 5' \text{ TALL CISTERN} = 180 \text{ CF}$   
 CISTERN TO BE LOCATED INSIDE THE BUILDING.

**DUE TO IMPLEMENTATION OF DETENTION PIPE:**

**WEST PLAZA**  
 DETENTION VOLUME REQUIRED FOR 15.2%+17.7% REDUCTION  
 $V_{DET \text{ REQD}} (CF) = (0.152+0.177)(Q_{EX-10YR})^2 * T_c$   
 $V_{DET \text{ REQD}} (CF) = (0.329)(0,671 \text{ CFS})^2 * (5 \text{ MIN}) * (60 \text{ SEC/MIN})$   
 $V_{DET \text{ REQD}} (CF) = 99.3 \text{ CF OR } 744 \text{ GALLONS}$   
 DETENTION PROVIDED BY 24" PIPE  
 $V_{DET \text{ PROVIDED}} = \pi * R^2 * LENGTH = \pi * (1.00FT)^2 * 8.7FT = 27 \text{ CF}$   
 DETENTION PROVIDED BY 8" PIPE  
 $V_{DET \text{ PROVIDED}} = \pi * R^2 * LENGTH = \pi * (0.33FT)^2 * 17FT = 9 \text{ CF}$   
 DETENTION PROVIDED BY 48" STORM BOX BASIN  
 $V_{DET \text{ PROVIDED}} = LENGTH * WIDTH * HEIGHT = 4.0FT * 4.0FT * 4.0FT = 64 \text{ CF}$   
 $V_{DET \text{ PROVIDED TOTAL}} = 100 \text{ CF}$   
 DETENTION PIPE TO BE LOCATED ADJACENT TO BUILDING B, IN THE WEST PLAZA.







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NO.	ISSUE	DATE
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JOB NUMBER: PJK-1808 | BKF-20190110  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

PHASE: 1  
BLOCK: 1 + 2  
SCALE: 1" = 20'

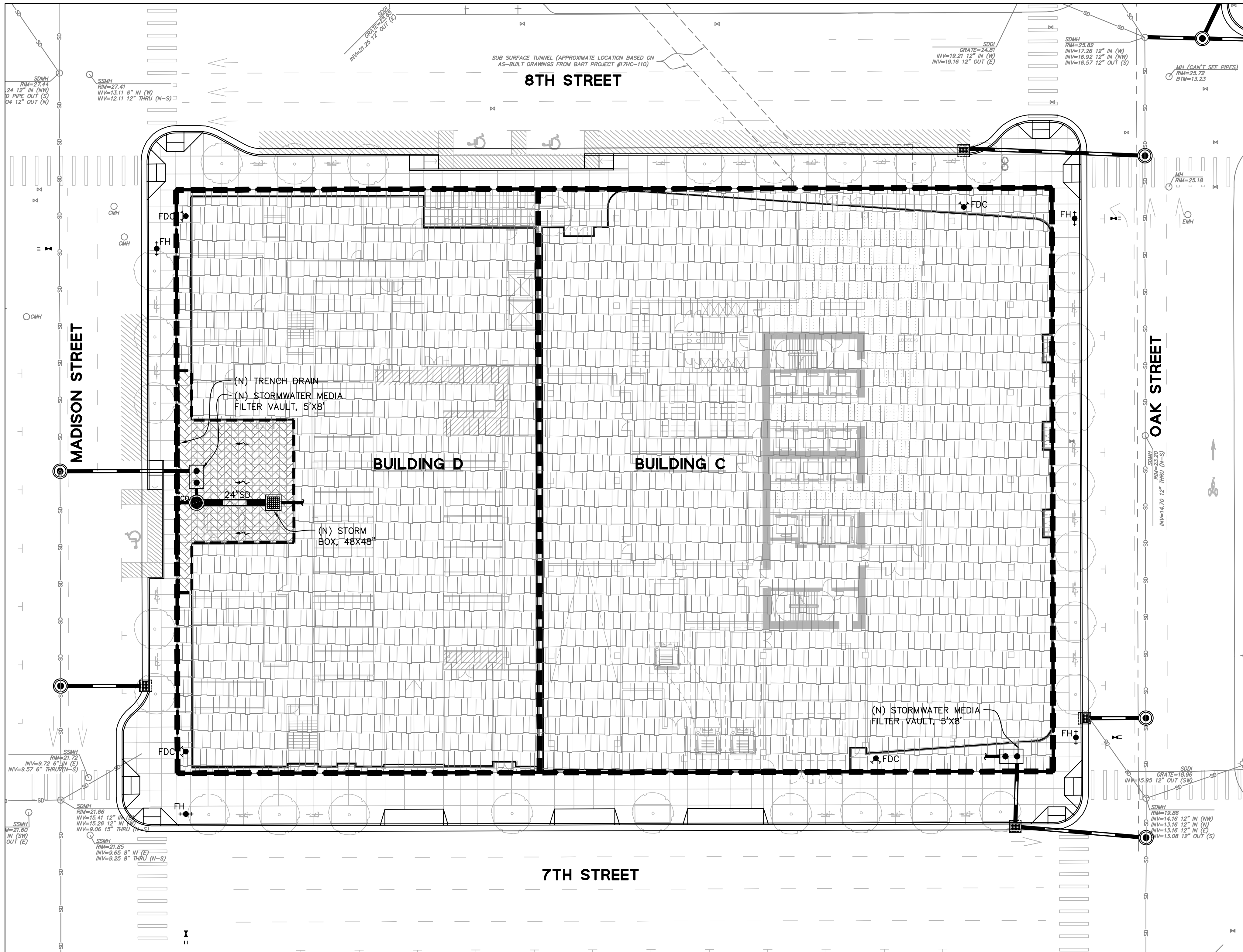
TITLE:  
STORMWATER CONTROL PLAN  
(BLOCK 2)

SHEET:

C7.2

PRELIMINARY - Not for Construction -

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STORMWATER COMPLIANCE DATA (BLOCK 2)

PER THE MUNICIPAL REGIONAL STORMWATER PERMIT ORDER NO. R2-0074, CERTAIN DEVELOPMENT PROJECTS THAT QUALIFY AS "SPECIAL PROJECTS" ARE ELIGIBLE FOR LOW IMPACT DESIGN TREATMENT REDUCTION CREDITS. THE LID TREATMENT REDUCTION CREDIT IS THE MAXIMUM PERCENTAGE OF THE AMOUNT OF RUNOFF THAT MAY BE TREATED WITH EITHER TREE-BOX-TYPE HIGH FLOWRATE BIOFILTERS OR VAULT-BASED HIGH FLOWRATE MEDIA FILTERS. THIS PROJECT IS CLASSIFIED AS A CATEGORY C SPECIAL PROJECT (TRANSIT ORIENTED) AND QUALIFIES FOR A TOTAL LID TREATMENT REDUCTION CREDIT OF 100% AS DESCRIBED BELOW.

CRITERIA FOR CATEGORY C (TRANSIT ORIENTED DEVELOPMENT) SPECIAL PROJECTS TO BE CONSIDERED A CATEGORY C SPECIAL PROJECT, A PROVISION C.3 REGULATED PROJECT MUST MEET ALL OF THE FOLLOWING CRITERIA:

1. BE CHARACTERIZED AS A NON AUTO-RELATED LAND USE PROJECT. THAT IS, CATEGORY C SPECIFICALLY EXCLUDES ANY REGULATED PROJECT THAT IS A STAND-ALONE SURFACE PARKING LOT; CAR DEALERSHIP; AUTO AND TRUCK RENTAL FACILITY WITH ON-SITE SURFACE STORAGE; FAST-FOOD RESTAURANT, BANK OR PHARMACY WITH DRIVE-THROUGH LANES; GAS STATION, CAR WASH, AUTO REPAIR AND SERVICE FACILITY; OR OTHER AUTO RELATED PROJECT UNRELATED TO THE CONCEPT OF TRANSIT-ORIENTED DEVELOPMENT.
2. IF A COMMERCIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1.
3. IF A RESIDENTIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST A DENSITY OF 25 DU/AC.
4. IF A MIXED-USE DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1 OR A DENSITY OF 25 DU/AC.

100% LID TREATMENT REDUCTION CREDIT

1. 50% REDUCTION CREDIT - PROJECT IS LOCATED WITHIN A 1/4 MILE RADIUS OF A TRANSIT HUB
2. 30% REDUCTION CREDIT - MIXED USE PROJECT WITH DENSITY GREATER THAN 100 DU/ACRE
3. 20% REDUCTION CREDIT - 0% OF TOTAL POST-PROJECT IMPERVIOUS SURFACE IS DEDICATED TO AT-GRADE, SURFACE PARKING

LEGEND

- ● STORMWATER FILTER UNIT
- FLOW DIRECTION
- ▨ IMPERVIOUS ROOF AREAS
- ▩ IMPERVIOUS PLAZA/PODIUM AREAS
- ▤ PERVIOUS LANDSCAPE AREAS
- - - DRAINAGE AREA BOUNDARY

STORMWATER AREA SUMMARY

THE CITY OF OAKLAND STORM DRAINAGE DESIGN GUIDELINES ESTABLISHES A 25% GOAL FOR PEAK FLOW REDUCTION COMPARED TO EXISTING CONDITIONS, TO THE EXTENT POSSIBLE. DUE TO THE FACT THAT ALMOST 90% OF THE SITE IS COVERED BY PERMANENT STRUCTURE, THERE IS LIMITED SPACE FOR DETENTION MEASURES ON-SITE. AS A RESULT, A 25% PEAK FLOW REDUCTION WAS ACCOMPLISHED AT BUILDING C, AND 16% FOR BUILDING D.

BUILDING C	
EXISTING IMPERVIOUS SURFACE	35,218 SF
EXISTING PERVIOUS SURFACE	0 SF
PROPOSED IMPERVIOUS SURFACE	35,218 SF
PROPOSED PERVIOUS SURFACE	0 SF
BUILDING D	
EXISTING IMPERVIOUS SURFACE	24,810 SF
EXISTING PERVIOUS SURFACE	0 SF
PROPOSED IMPERVIOUS SURFACE	24,810 SF
PROPOSED PERVIOUS SURFACE	0 SF

STORMWATER LID TREATMENT SIZING

TOTAL LID TREATMENT REDUCTION CREDIT = 100%  
TOTAL BLOCK 2 IMPERVIOUS AREA OF 60,028 SF ALLOWED TO BE TREATED W/ NON-LID TREATMENT MEASURES (MEDIA FILTER UNITS)

BUILDING C	
$Q_{c.3} = CIA = (0.9)(0.2 \text{ IN/HR})(35,218 \text{ SF})$	$Q_{c.3} = 0.1467 \text{ CFS}$
A 5'x8' MEDIA FILTER VAULT WITH (3) 12"X12" STACKED CARTRIDGES TO BE PROVIDED WITHIN BUILDING. TREATMENT FLOW CAPACITY = 0.16 CFS.	
BUILDING D	
$Q_{c.3} = CIA = (0.9)(0.2 \text{ IN/HR})(24,810 \text{ SF})$	$Q_{c.3} = 0.1033 \text{ CFS}$
A 5'x8' MEDIA FILTER VAULT WITH (3) 18" CARTRIDGES TO BE PROVIDED WITHIN BUILDING. TREATMENT FLOW CAPACITY = 0.12 CFS.	

STORMWATER PEAK FLOW REDUCTION

PEAK FLOW CALCULATED PER CITY OF OAKLAND STORM DRAINAGE DESIGN STANDARDS.

DESIGN STORM = 10-YEAR  
MEAN ANNUAL PRECIPITATION (MAP) = 21 INCHES  
TIME OF CONCENTRATION = 5 MINUTES

DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:

BUILDING C	
$Q_{ex-10yr} = CIA = (0.90)(3.48 \text{ IN/HR})(35,218 \text{ SF})$	$Q_{ex-10yr} = 2,532 \text{ CFS}$
$Q_{pr-10yr} = CIA = (0.90)(3.48 \text{ IN/HR})(24,810 \text{ SF})$	$Q_{pr-10yr} = 1,784 \text{ CFS}$
PERCENT REDUCTION IN PEAK FLOW $\% \text{ REDUCTION} = 100 - ((1,784 / 2,532) * 100) = 0.0\%$	
BUILDING D	
$Q_{ex-10yr} = CIA = (0.90)(3.48 \text{ IN/HR})(35,218 \text{ SF})$	$Q_{ex-10yr} = 2,532 \text{ CFS}$
$Q_{pr-10yr} = CIA = (0.90)(3.48 \text{ IN/HR})(24,810 \text{ SF})$	$Q_{pr-10yr} = 1,784 \text{ CFS}$
PERCENT REDUCTION IN PEAK FLOW $\% \text{ REDUCTION} = 100 - ((2,532 / 2,532) * 100) = 0.0\%$	

DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:

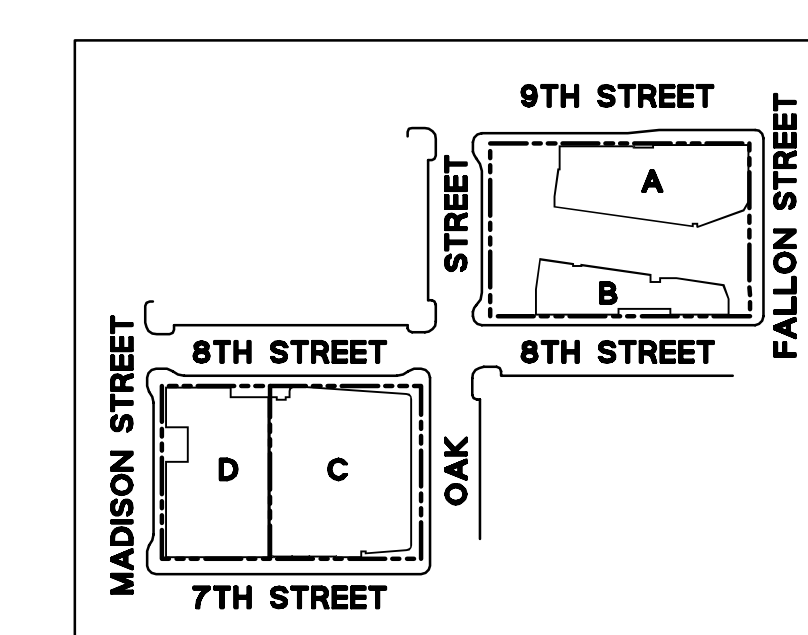
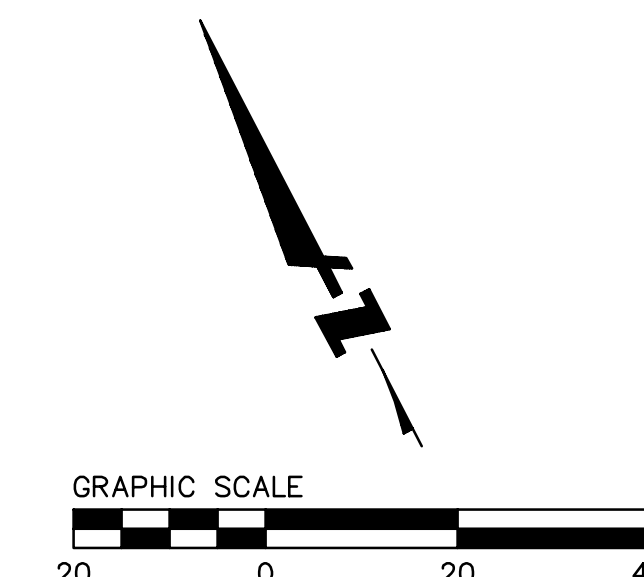
BUILDING D	
$Q_{ex-10yr} = CIA = (0.90)(3.48 \text{ IN/HR})(24,810 \text{ SF})$	$Q_{ex-10yr} = 1,784 \text{ CFS}$
$Q_{pr-10yr} = CIA = (0.90)(3.48 \text{ IN/HR})(24,810 \text{ SF})$	$Q_{pr-10yr} = 1,784 \text{ CFS}$
PERCENT REDUCTION IN PEAK FLOW $\% \text{ REDUCTION} = 100 - ((1,784 / 1,784) * 100) = 0.0\%$	

DUE TO IMPLEMENTATION OF DETENTION TANK/CISTERN:

BUILDING C	
DETENTION VOLUME REQUIRED FOR 25% REDUCTION	
$V_{det \text{ reqd}} (\text{CF}) = (0.25)(Q_{ex-10yr}) \cdot \frac{1}{2} T_c$	
$V_{det \text{ reqd}} (\text{CF}) = (0.25)(2,532 \text{ CFS}) \cdot \frac{1}{2} (5 \text{ MIN}) \cdot (60 \text{ SEC/MIN})$	
$V_{det \text{ reqd}} (\text{CF}) = 284.9 \text{ CF OR } 2,134 \text{ GALLONS}$	
DETENTION PIPE PROVIDED BY CISTERN IN BUILDING	
$V_{det \text{ provided}} = 7'x7' \times 6' \text{ TALL CISTERN} = 294 \text{ CF}$	
CISTERN TO BE LOCATED INSIDE THE BUILDING.	

DUE TO IMPLEMENTATION OF DETENTION PIPE:

BUILDING D	
DETENTION VOLUME REQUIRED FOR 16% REDUCTION	
$V_{det \text{ reqd}} (\text{CF}) = (0.16)(Q_{ex-10yr}) \cdot \frac{1}{2} T_c$	
$V_{det \text{ reqd}} (\text{CF}) = (0.16)(1,784 \text{ CFS}) \cdot \frac{1}{2} (5 \text{ MIN}) \cdot (60 \text{ SEC/MIN})$	
$V_{det \text{ reqd}} (\text{CF}) = 128.0 \text{ CF OR } 962 \text{ GALLONS}$	
DETENTION PROVIDED BY 24" PIPE	
$V_{det \text{ provided}} = \pi R^2 \cdot \text{LENGTH} = \pi (1.00\text{FT})^2 \cdot 21.0\text{FT} = 66 \text{ CF}$	
DETENTION PROVIDED BY 48" STORM BOX BASIN	
$V_{det \text{ provided}} = \text{LENGTH} \cdot \text{WIDTH} \cdot \text{HEIGHT} = 4.0\text{FT} \cdot 4.0\text{FT} \cdot 4.0\text{FT} = 64 \text{ CF}$	
$V_{det \text{ provided total}} = 130 \text{ CF}$	
DETENTION PIPE TO BE LOCATED ADJACENT TO THE BUILDING.	





LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE		
100 SD - HORIZONTAL FDP	10/29/2021	
100 SD - HORIZONTAL FDP	12/06/2021	
100 SD - HORIZONTAL FDP	05/02/2022	

REVISION SCHEDULE		
NO.	ISSUE	DATE

JOB NUMBER: PFK-1808 | BKF: 2019010  
DRAWN BY: DFS  
CHECKED BY: SRN  
ISSUE DATE: 05/02/2022

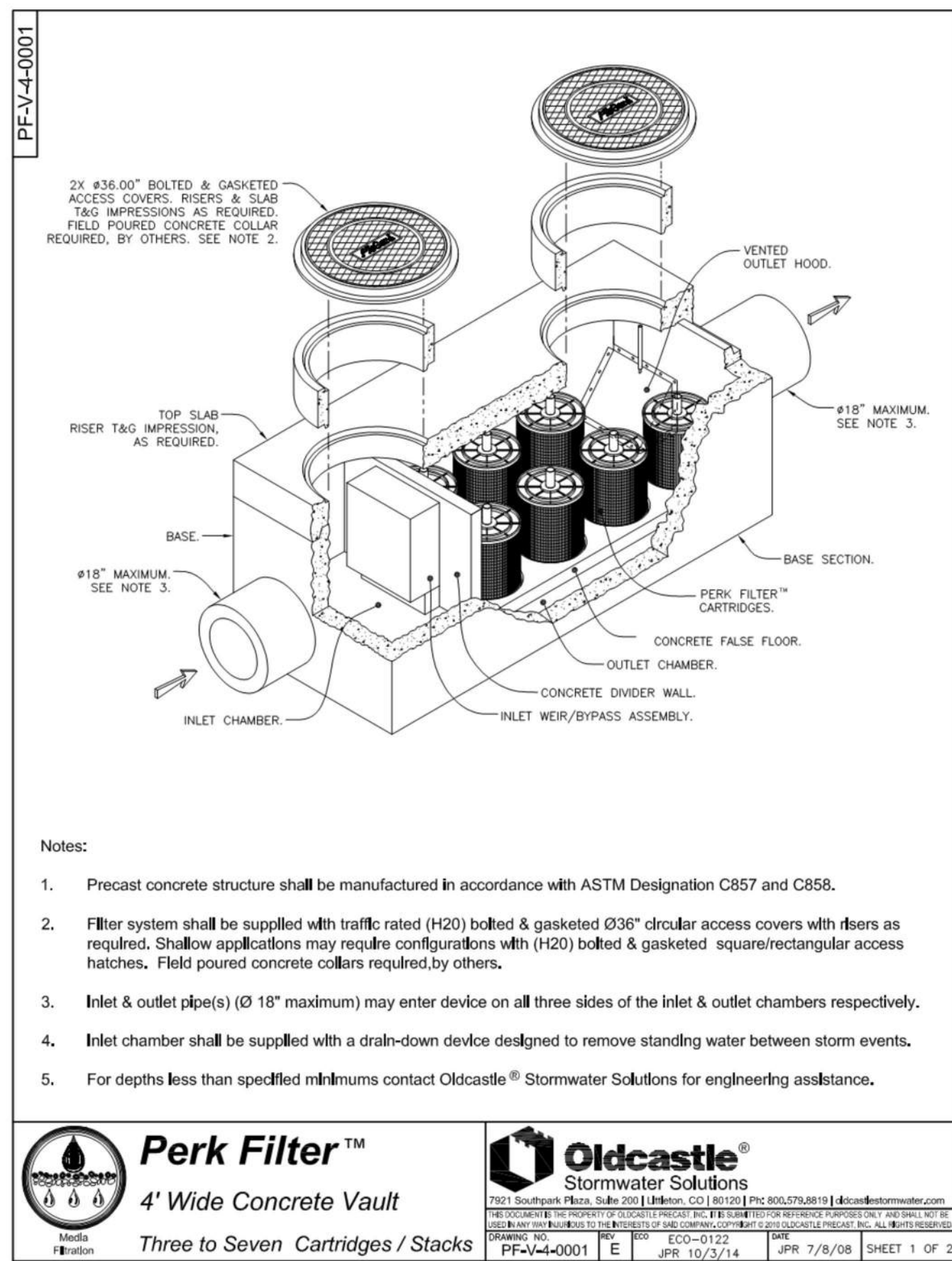
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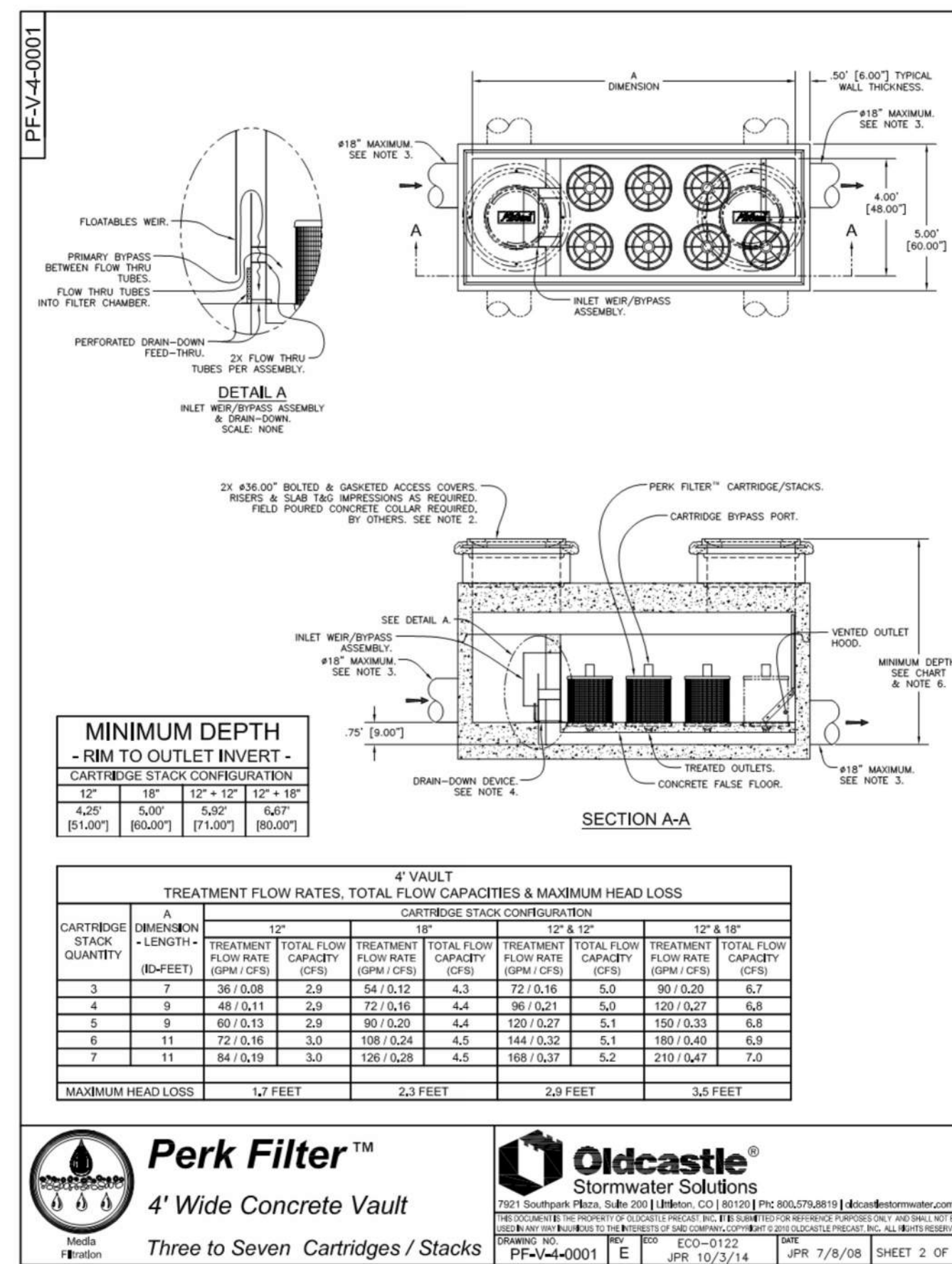
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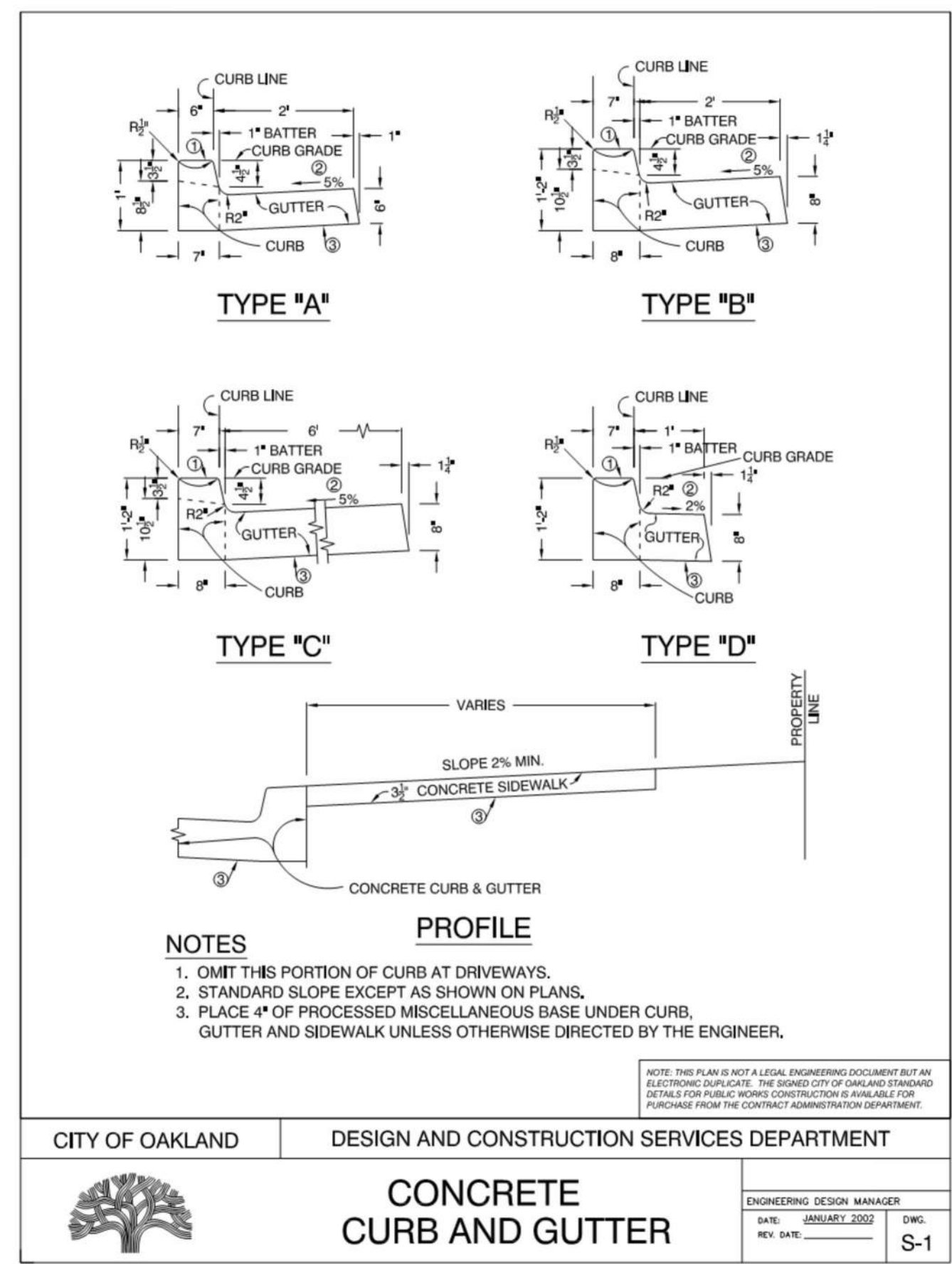
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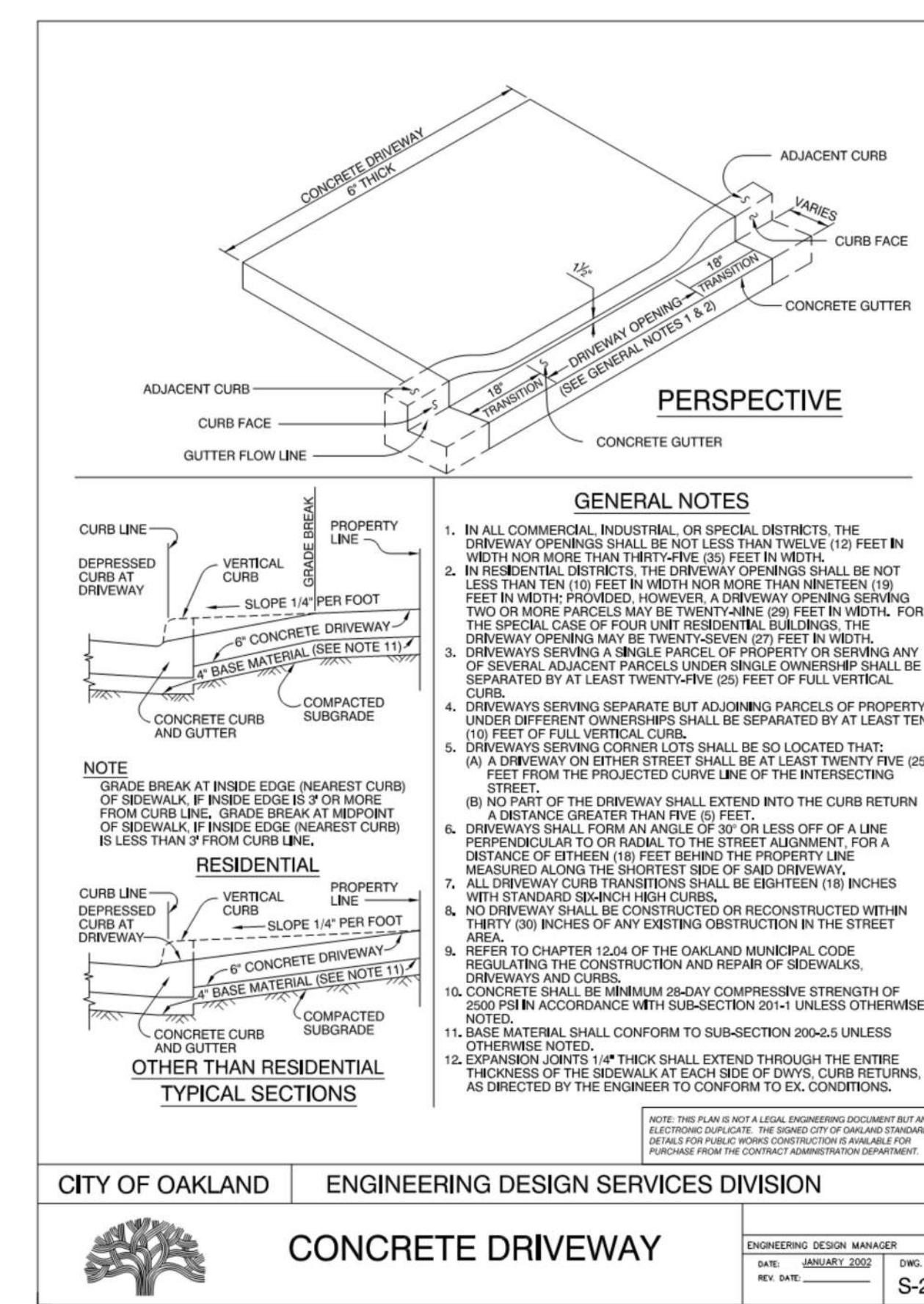
1 STORMWATER MEDIA FILTER VAULT (1 OF 2)  
NTS



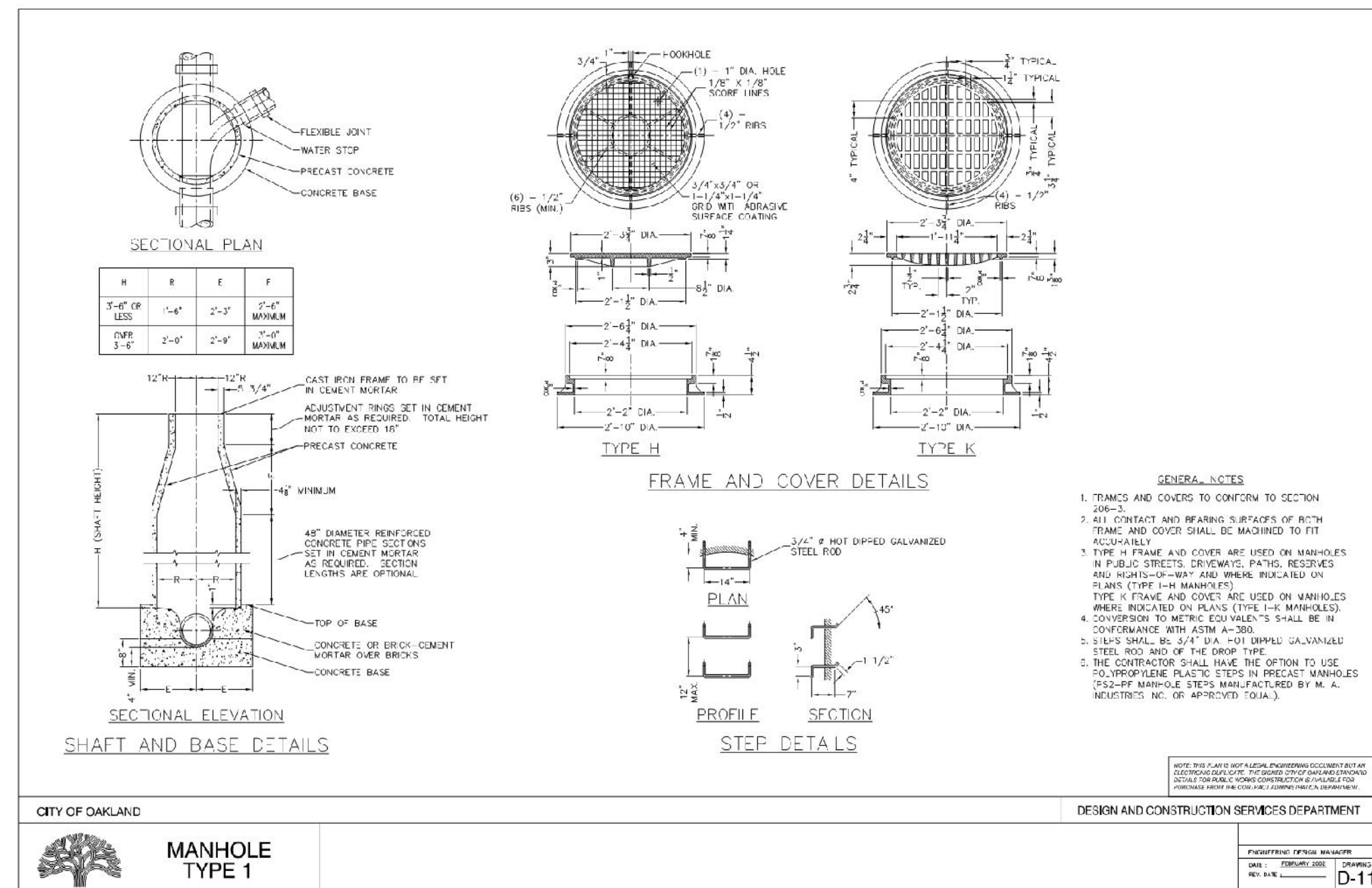
2 STORMWATER MEDIA FILTER VAULT (2 OF 2)  
NTS



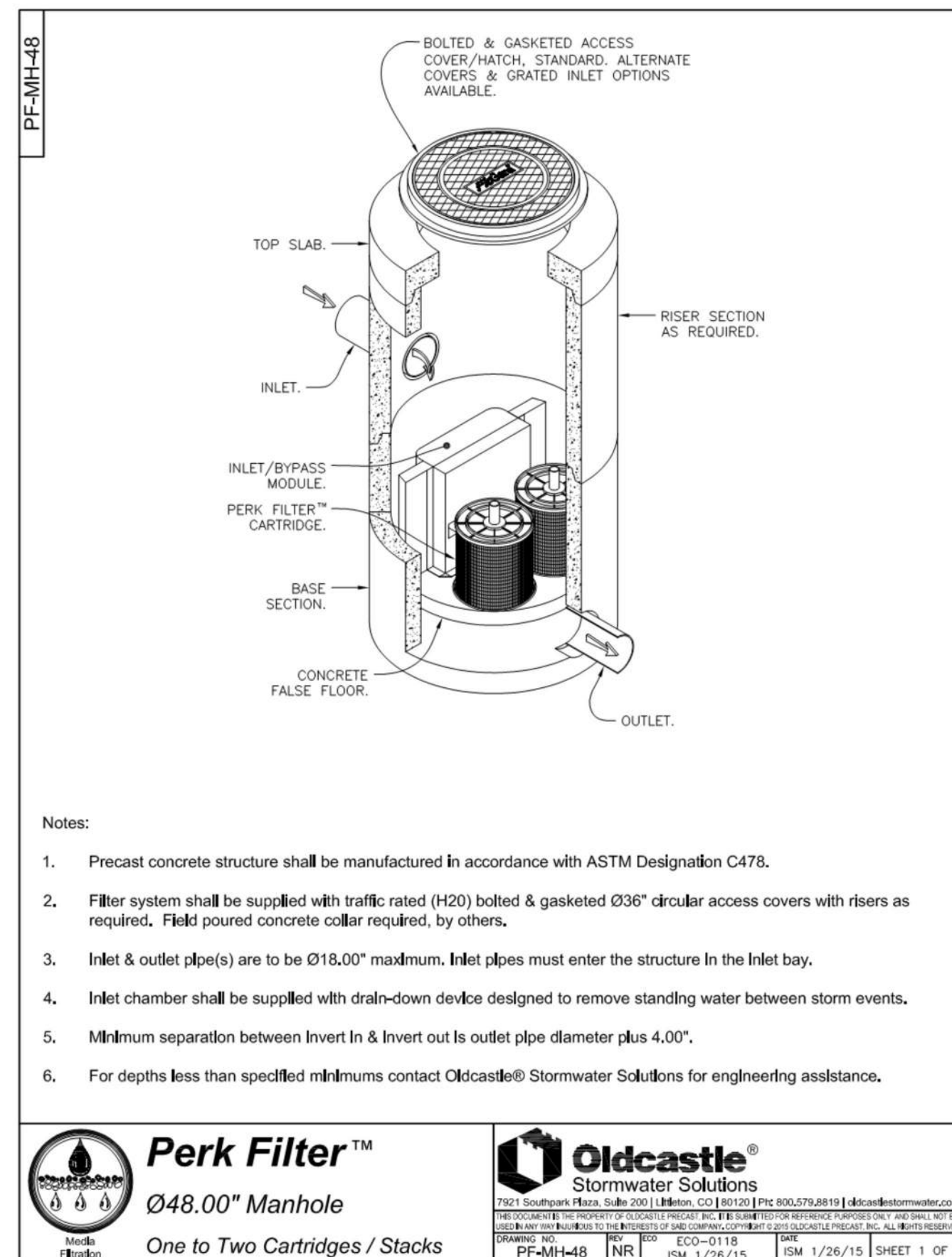
3 CONCRETE CURB & GUTTER  
NTS



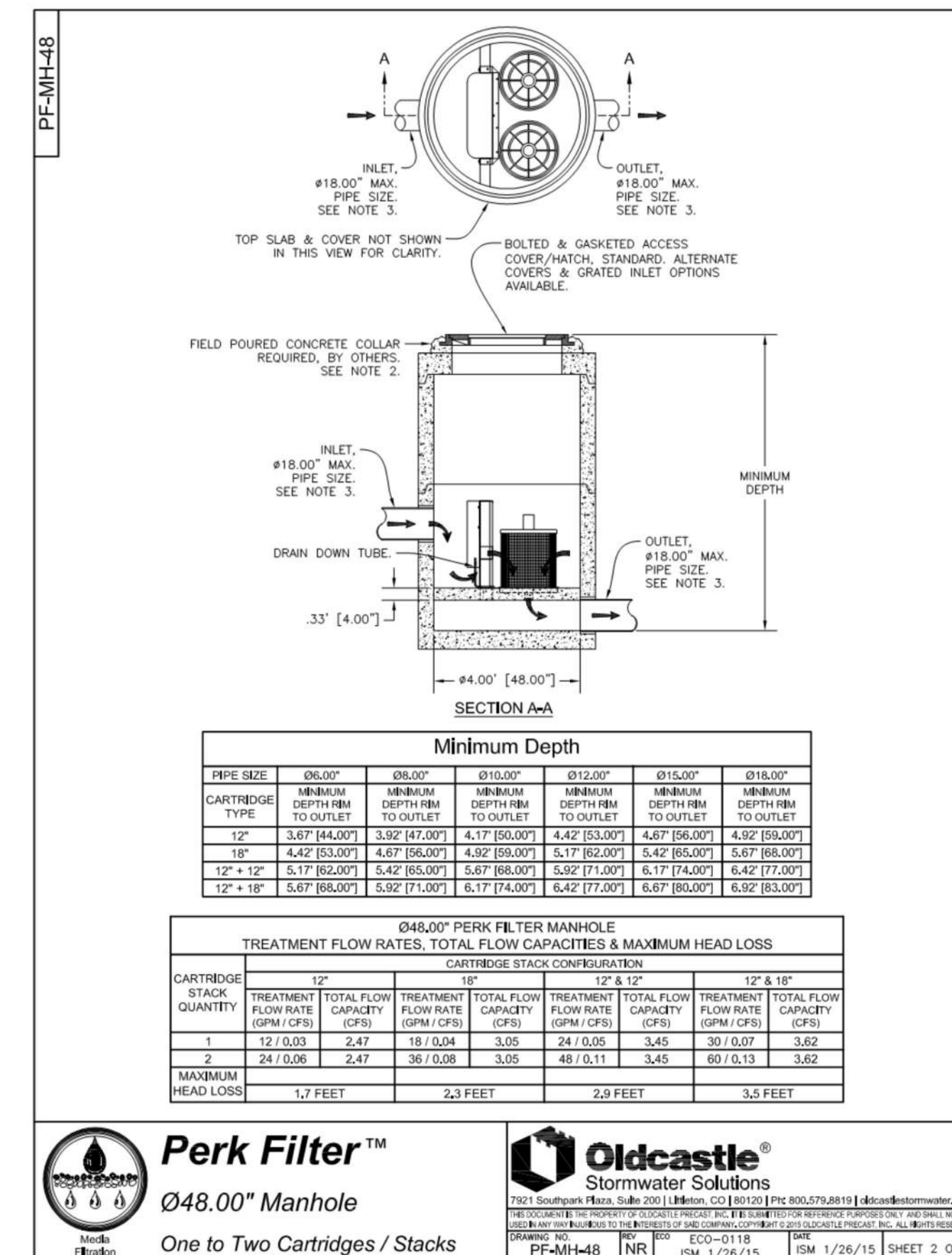
4 CONCRETE DRIVEWAY  
NTS



5 MANHOLE (TYPE 1)  
NTS



6 STORMWATER MEDIA FILTER MANHOLE (1 OF 2)  
NTS



7 STORMWATER MEDIA FILTER MANHOLE (2 OF 2)  
NTS



STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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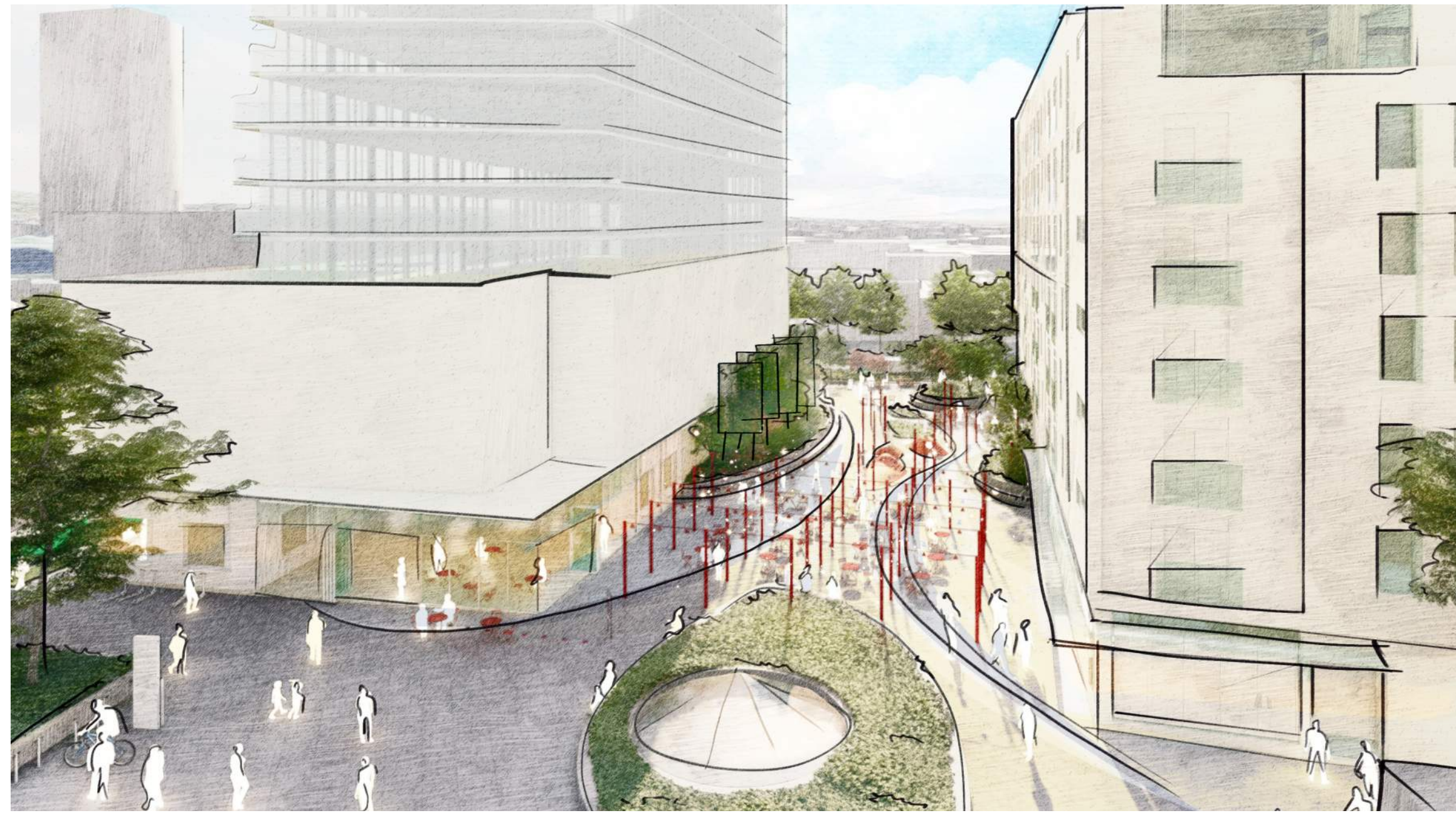
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ISSUE DATE: xxx/xxx/xx

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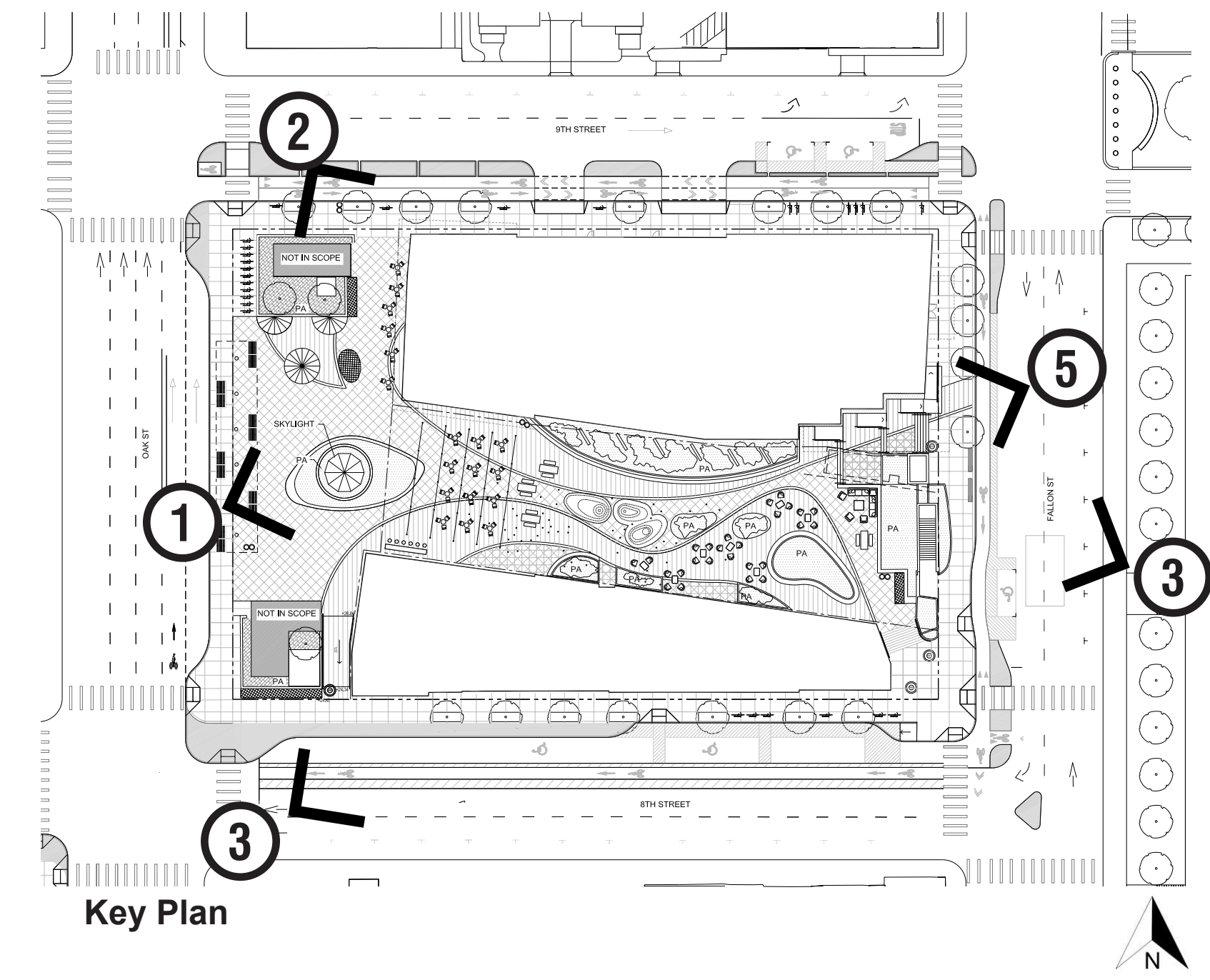
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**RENDERED VIEWS**

SHEET:

**L4.00**



1 Paseo from Oak St.



2 9th St. looking in towards Building A and Paseo



3 8th St. looking towards Building B

1. ALL ELEMENTS WILL COMPLY WITH BFS STANDARDS.
2. ALL PLANT MATERIAL ABOVE THE BART TUNNEL WILL BE HOSE WATERED.
3. SEE MATERIALS PLANS FOR DETAILED SPECIFICATION OF MATERIAL COLOR, FINISH, AND MANUFACTURER.
4. SEE FURNITURE PLANS FOR DETAILED SPECIFICATIONS OF FURNITURE COLOR, FINISH, AND MANUFACTURER.
5. SEE LIGHTING PLANS FOR DETAILED SPECIFICATION OF LIGHTS.
6. SEE PLANTING PLANS FOR DETAILED SPECIFICATION OF PLANTS AND IRRIGATION. DRIP IRRIGATION TO BE USED IN ALL LANDSCAPE AREAS ON GROUND.
7. SPECIES SELECTIONS MAY CHANGE BASED ON AVAILABILITY, DISEASE, OR OVERALL COORDINATION OF PLANTING PALETTE, AND BART OPERATIONAL IMPACTS.

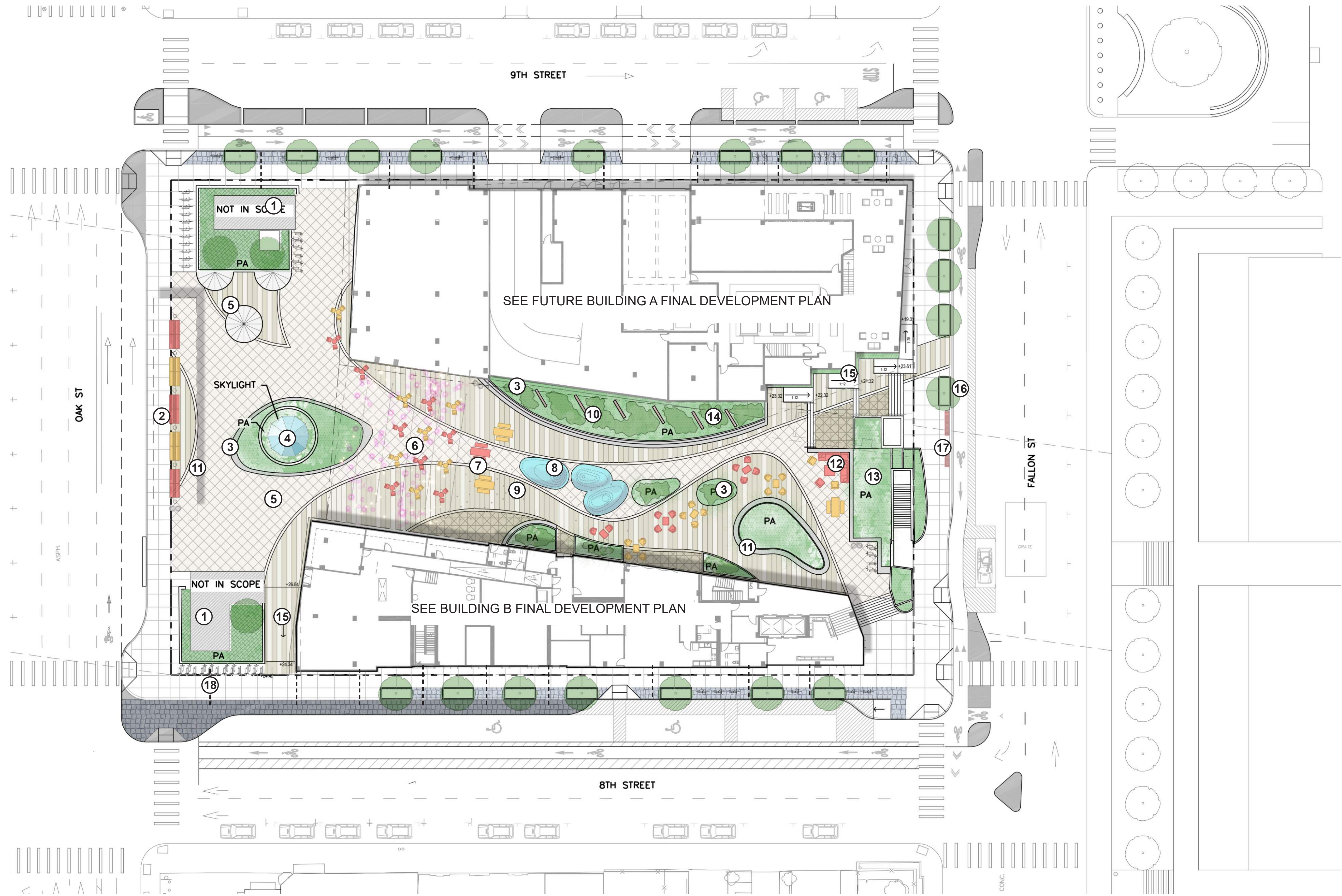


4 Paseo from Fallon St.



5 Fallon St. looking at Building A lobby and Paseo

PRECEDENT IMAGERY



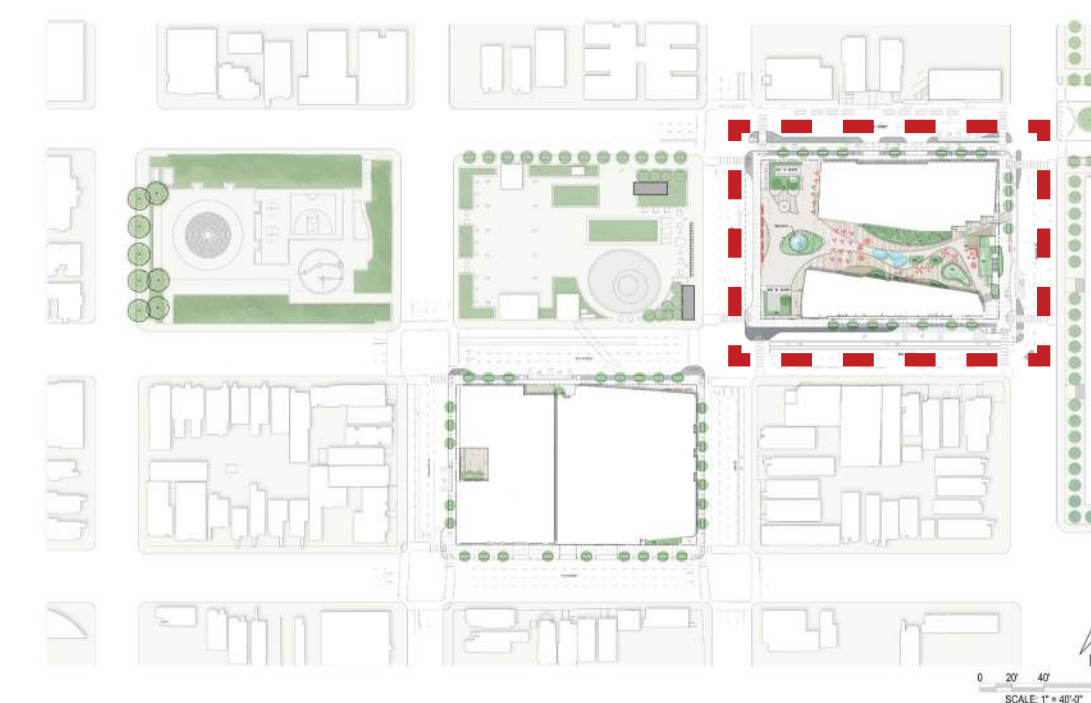
SITE LEGEND

- ① BART HEAD HOUSE
- ② EXISTING CANOPY
- ③ RAISED PLANTER
- ④ EXISTING SKYLIGHT
- ⑤ POURED IN PLACE COLORED CONCRETE PAVEMENT WITH DETAILED PATTERNS
- ⑥ POLE LIGHTS WITH STRUNG CATENARY LANTERNS DEVELOPED WITH ARTIST OR ARTISTS
- ⑦ COVERED VALLEY GUTTER WITH TRECH DRAIN LID
- ⑧ RESILIENT SURFACING WITH INTERACTIVE SCULPTURAL ELEMENTS
- ⑨ LIGHT WANDS
- ⑩ VINE STRUCTURES
- ⑪ SEATING
- ⑫ LOUNGE SEATING
- ⑬ EVERGREEN PLANTING
- ⑭ FLOWERING GROVE
- ⑮ ADA RAMP
- ⑯ STREET TREE
- ⑰ BENCH
- ⑱ LANTERN CABLES

GENERAL NOTES:

1. DRIP IRRIGATION TO BE USED IN ALL LANDSCAPE AREAS ON GROUND.
  2. ALL PLANT MATERIAL ABOVE THE BART TUNNEL WILL BE HOSE WATERED.
  3. SEE MATERIALS PLANS FOR DETAILED SPECIFICATION OF MATERIAL COLOR, FINISH, AND MANUFACTURER.
  4. SEE FURNITURE PLANS FOR DETAILED SPECIFICATIONS OF FURNITURE COLOR, FINISH, AND MANUFACTURER.
  5. SEE LIGHTING PLANS FOR DETAILED SPECIFICATION OF LIGHTS.
  6. SEE PLANTING PLANS FOR DETAILED SPECIFICATION OF PLANTS AND IRRIGATION.
- SPECIES SELECTIONS MAY CHANGE BASED ON AVAILABILITY, DISEASE, OR OVERALL COORDINATION OF PLANTING PALETTE, AND BART OPERATIONAL IMPACTS.

KEY PLAN



LUMA

LAKE MERRITT BART HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE	
50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE		
NO.	ISSUE	DATE

JOB NUMBER:	1808
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CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx

SCALE:	As indicated
PHASE:	1
BLOCK:	1

ILLUSTRATIVE MATERIALS PHASE 1.2

SHEET:

L4.01

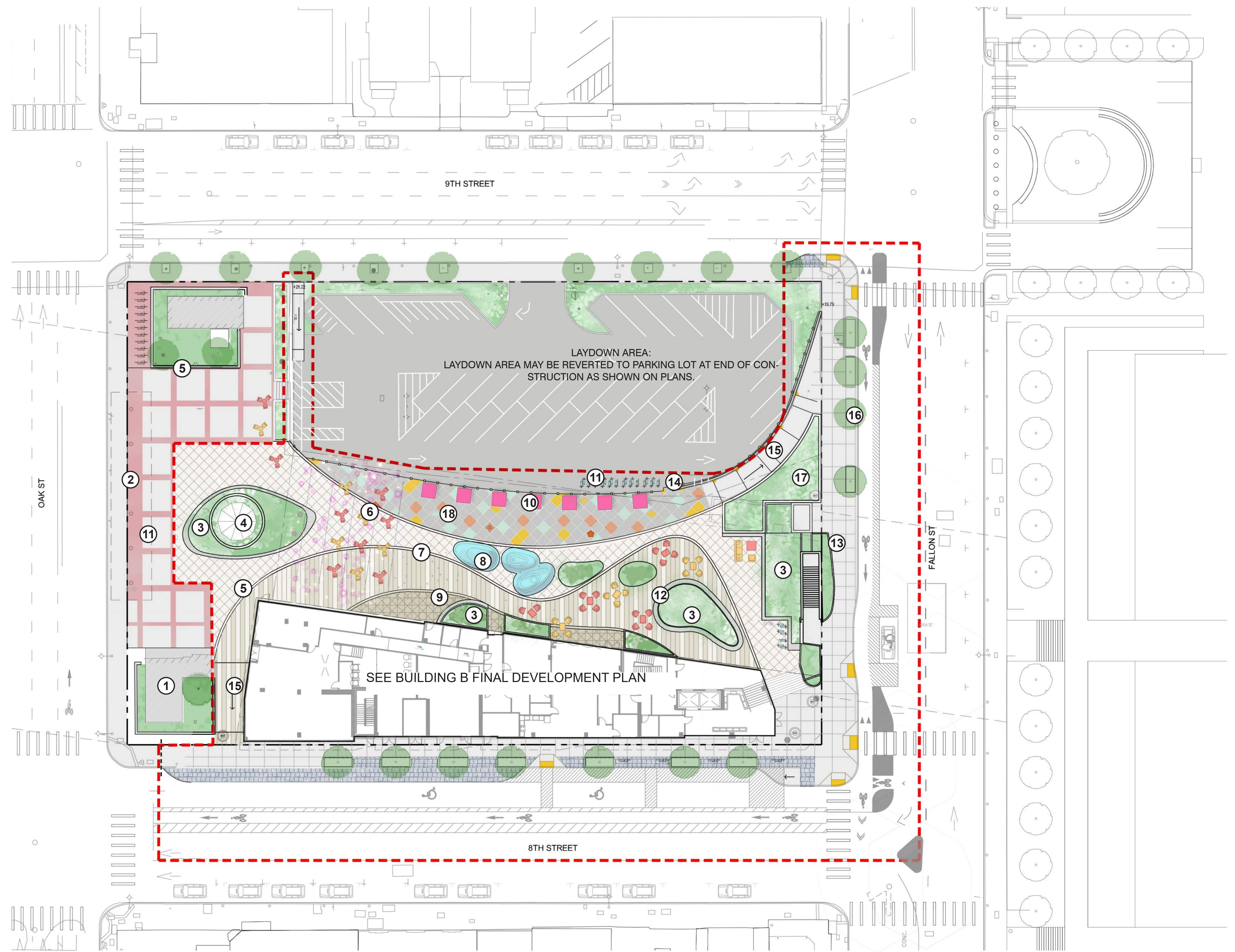
# PHASE 1.1



PHASE 1.1 VIEW DOWN FALLON ST.



PHASE 1.1 CONNECTION TO EAST AT FALLON ST.



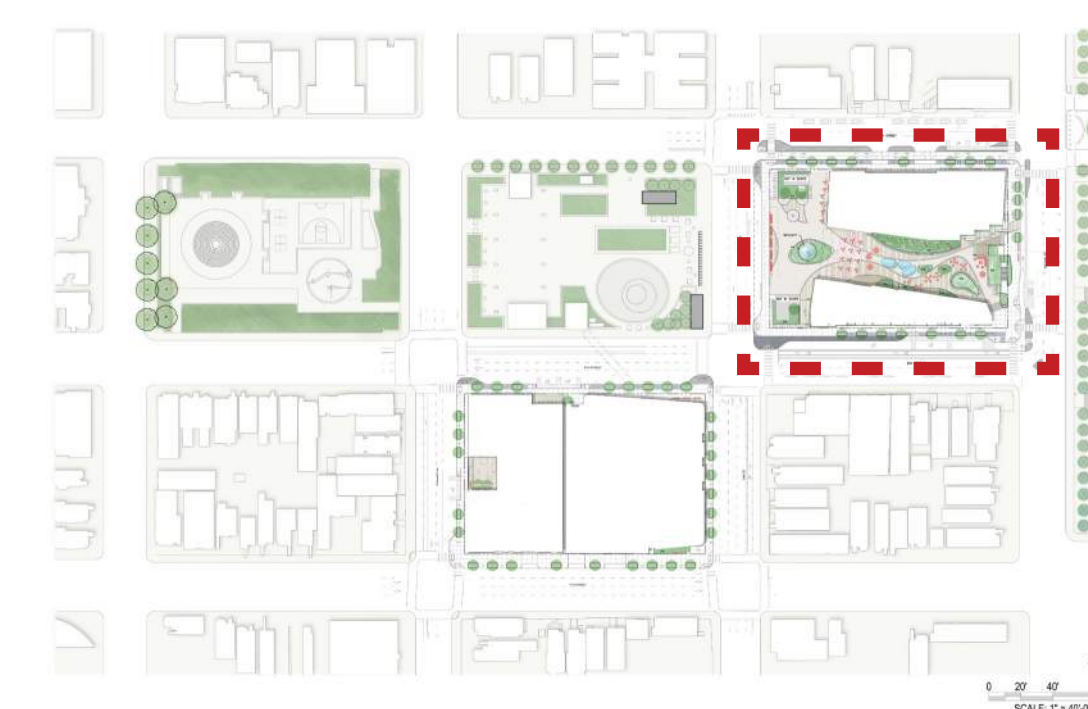
## PHASE 1.1 SITE LEGEND

- |  |  |   |
|--|--|---|
| ① BART HEAD HOUSE  | ⑧ RESILIENT SURFACING WITH INTERACTIVE SCULPTURAL ELEMENTS | ⑮ TEMPORARY ADA RAMP                            |
| ② EXISTING CANOPY  | ⑨ LIGHT WANDS  | ⑯ STREET TREE                                   |
| ③ RAISED PLANTER   | ⑩ RELOCATED BIKE LOCKERS WITH NEW GRAPHIC PAINT            | ⑰ TEMPORARY PLANTER                             |
| ④ EXISTING SKYLIGHT  | ⑪ SCOOTER CORRAL   | ⑱ TEMPORARY ASPHALT WITH GRAPHIC PAINT STENCILS |
| ⑤ POURED IN PLACE COLORED CONCRETE PAVEMENT WITH DETAILED PATTERNS           | ⑫ LOUNGE SEATING   |   |
| ⑥ POLE LIGHTS WITH STRUNG CATENARY LANTERNS DEVELOPED WITH ARTIST OR ARTISTS | ⑬ EVERGREEN PLANTING                                       |   |
| ⑦ COVERED VALLEY GUTTER WITH TRECH DRAIN LID                                 | ⑭ TEMPORARY FENCE WITH COLORFUL POSTS                      |   |

## GENERAL NOTES:

- DRIP IRRIGATION TO BE USED IN ALL LANDSCAPE AREAS ON GROUND.
- ALL PLANT MATERIAL ABOVE THE BART TUNNEL WILL BE HOSE WATERED.
- SEE MATERIALS PLANS FOR DETAILED SPECIFICATION OF MATERIAL COLOR, FINISH, AND MANUFACTURER.
- SEE FURNITURE PLANS FOR DETAILED SPECIFICATIONS OF FURNITURE COLOR, FINISH, AND MANUFACTURER.
- SEE LIGHTING PLANS FOR DETAILED SPECIFICATION OF LIGHTS.
- SEE PLANTING PLANS FOR DETAILED SPECIFICATION OF PLANTS AND IRRIGATION.
- SPECIES SELECTIONS MAY CHANGE BASED ON AVAILABILITY, DISEASE, OR OVERALL COORDINATION OF PLANTING PALETTE, AND BART OPERATIONAL IMPACTS.

## KEY PLAN



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE	NO.	ISSUE	DATE
50 SD - HORIZONTAL FDP	10292021		
100 SD - HORIZONTAL FDP	12172021		
100 SD - HORIZONTAL FDP	05022022		
100 SD - HORIZONTAL FDP2	06162022		

REVISION SCHEDULE	NO.	ISSUE	DATE

JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx

SCALE: As indicated  
PHASE: 1  
BLOCK: 1

TITLE:  
**ILLUSTRATIVE MATERIALS  
PHASE 1.1**

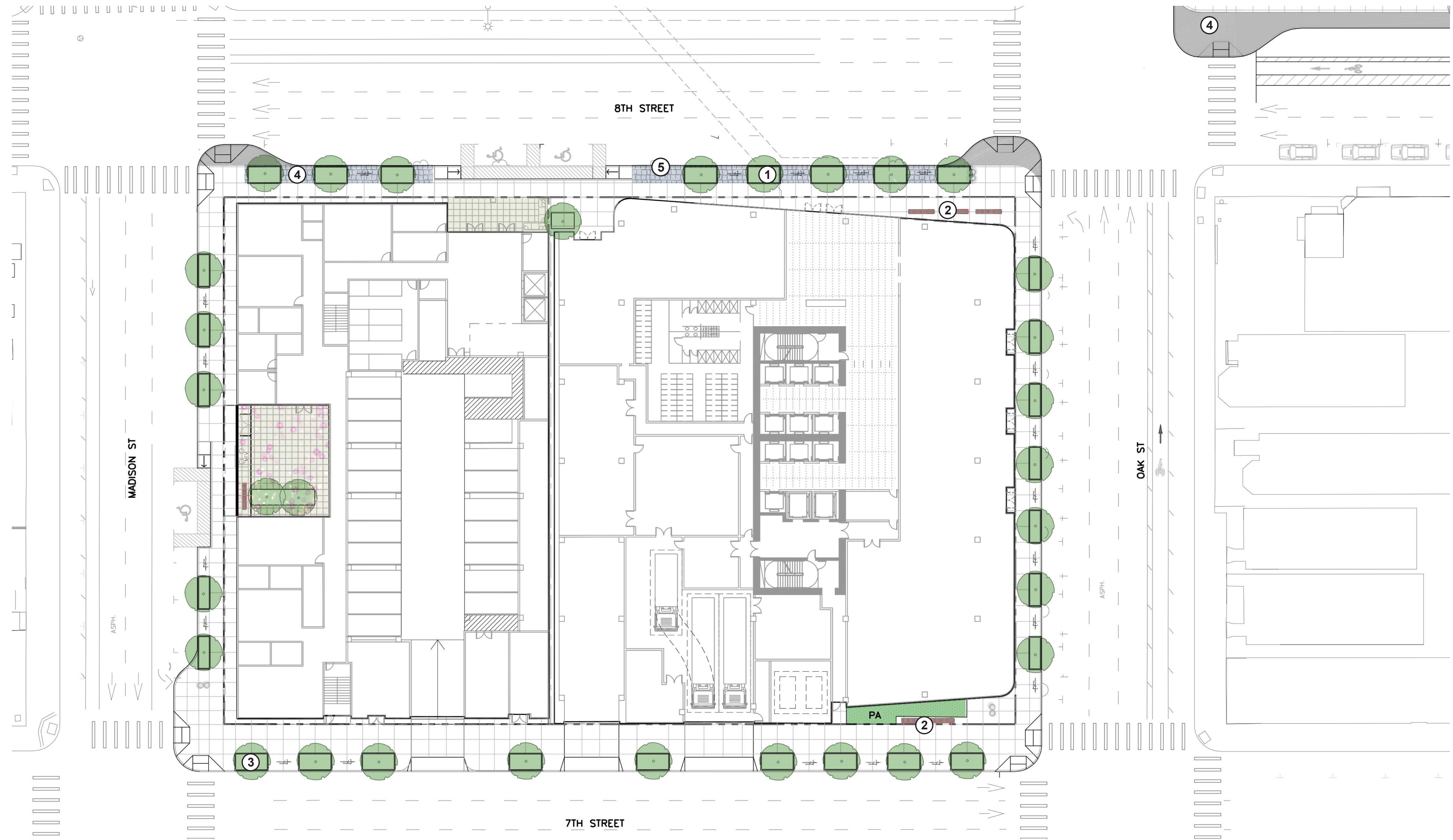
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**L4.01B**

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PRECEDENT IMAGERY



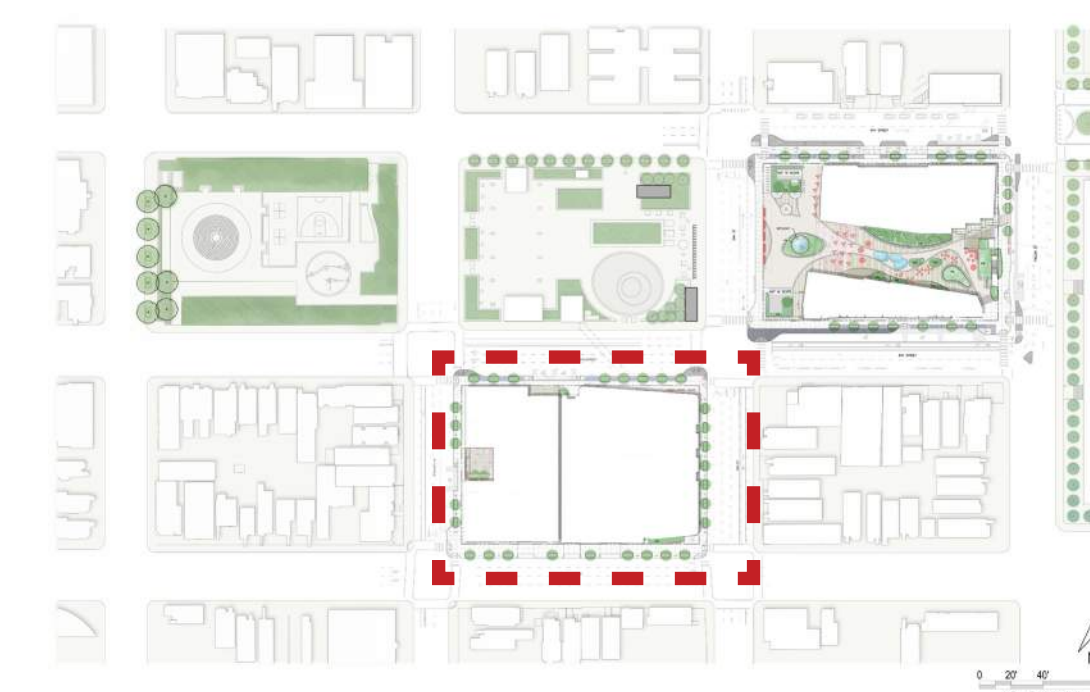
SITE LEGEND

- ① STREET TREE
- ② BENCH
- ③ SIDEWALK PLANTING
- ④ SPECIAL PAVING AT 8th AND 9th
- ⑤ BIKE RACK

GENERAL NOTES:

1. DRIP IRRIGATION TO BE USED IN ALL LANDSCAPE AREAS ON GROUND.
2. ALL PLANT MATERIAL ABOVE THE BART TUNNEL WILL BE HOSE WATERED.
3. SEE MATERIALS PLANS FOR DETAILED SPECIFICATION OF MATERIAL COLOR, FINISH, AND MANUFACTURER.
4. SEE FURNITURE PLANS FOR DETAILED SPECIFICATIONS OF FURNITURE COLOR, FINISH, AND MANUFACTURER.
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6. SEE PLANTING PLANS FOR DETAILED SPECIFICATION OF PLANTS AND IRRIGATION. SPECIES SELECTIONS MY CHANGE BASED ON AVAILABILITY, DISEASE, OR OVERALL COORDINATION OF PLANTING PALETTE, AND BART OPE RATIONAL IMPACTS.

KEY PLAN



LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
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100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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JOB NUMBER:	1808
DRAWN BY:	Author
CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx

SCALE:	As indicated
PHASE:	1
BLOCK:	2

ILLUSTRATIVE MATERIALS  
PHASE 2.2

SHEET:

L4.02



Specification:  
Lithocrete Concrete (500  
Kirkham)

1 SPECIALTY CONCRETE PAVEMENT



4 BIKE RACK / SCOOTER RACK



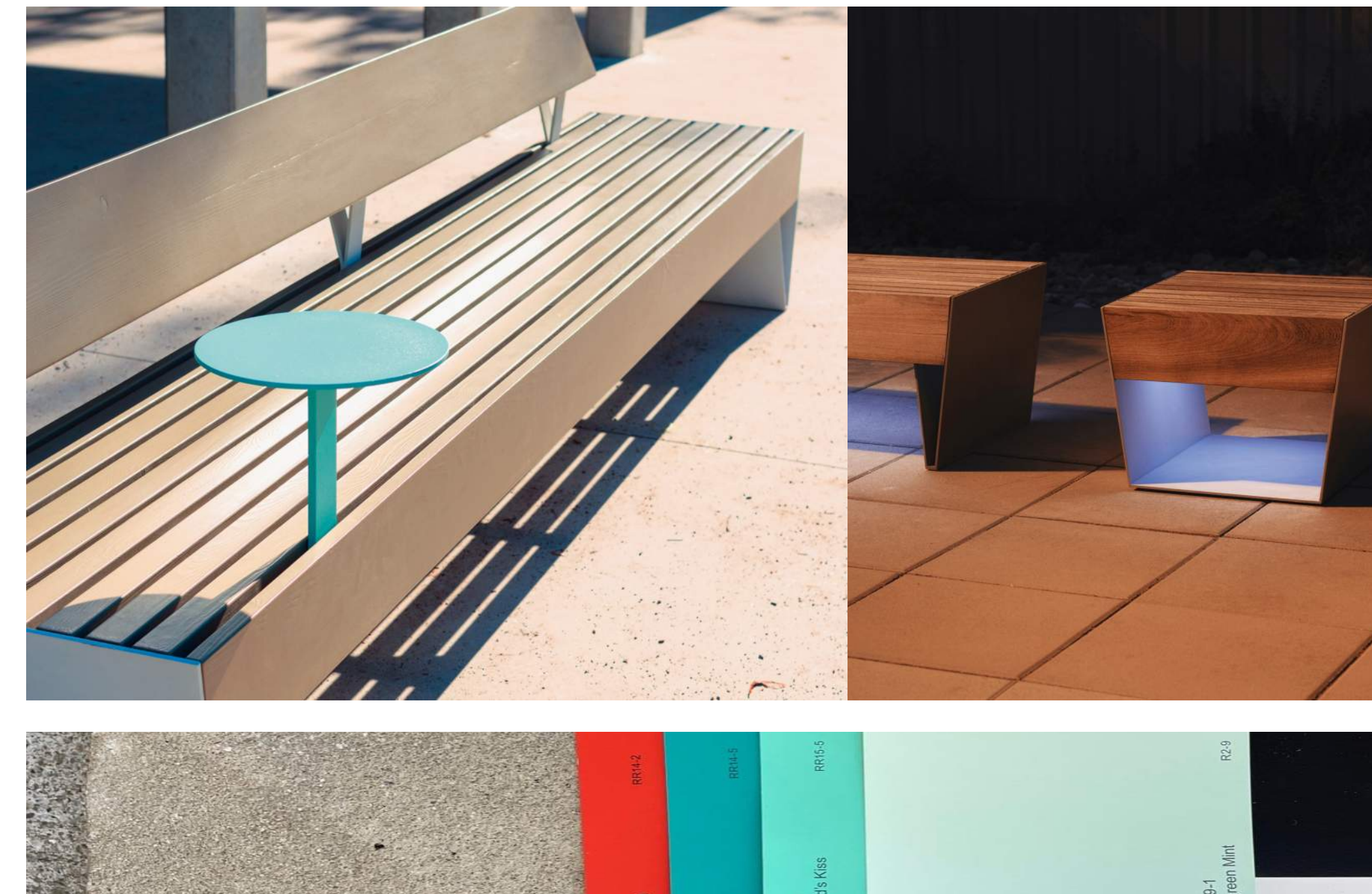
4 PICNIC TABLES



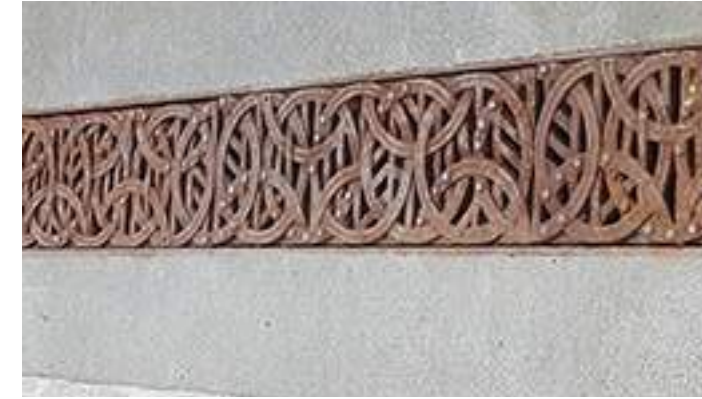
2 PLANTERS: CONCRETE AND BROKEN CERAMICS



5 DECORATIVE EDGE AT 8TH AND 9TH STREET



5 BENCHES



1 TRENCH DRAINS



RESILIENT SURFACING

6 RESILIENT SURFACING



1 MOVEABLE FURNITURE

EINWILLERKUEHL  
LANDSCAPE ARCHITECTURE  
318 HARRISON STREET SUITE 301  
OAKLAND CALIFORNIA 94607  
(510) 891-1696  
EAST BAY ASIAN LOCAL  
DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94621  
STRADA INVESTMENT GROUP  
101 MISSION ST. #20  
SAN FRANCISCO, CA 94105  
BKF ENGINEERS  
255 SHORELINE DR, SUITE 200  
REDWOOD CITY, CA 94065

LUMA LIGHTING DESIGN  
48 GOLDEN GATE AVENUE  
SAN FRANCISCO, CA 94102

LUMA

LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

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100 SD - HORIZONTAL FDP#2	06/16/2022

REVISION SCHEDULE	
NO.   ISSUE	DATE

JOB NUMBER: 1808  
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CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx  
SCALE: As indicated  
PHASE: 1  
BLOCK: 1 AND 2  
TITLE:  
ILLUSTRATIVE MATERIALS

SHEET:  
L4.03



# LUMA

## LAKE MERRITT BART HORIZONTAL FDP Oakland, CA 94607

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ISSUE SCHEDULE		
50 SD - HORIZONTAL FDP	10/29/2021	
100 SD - HORIZONTAL FDP	12/17/2021	
100 SD - HORIZONTAL FDP	05/02/2022	
100 SD - HORIZONTAL FDP2	06/16/2022	

REVISION SCHEDULE	
NO.	ISSUE / DATE

JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx

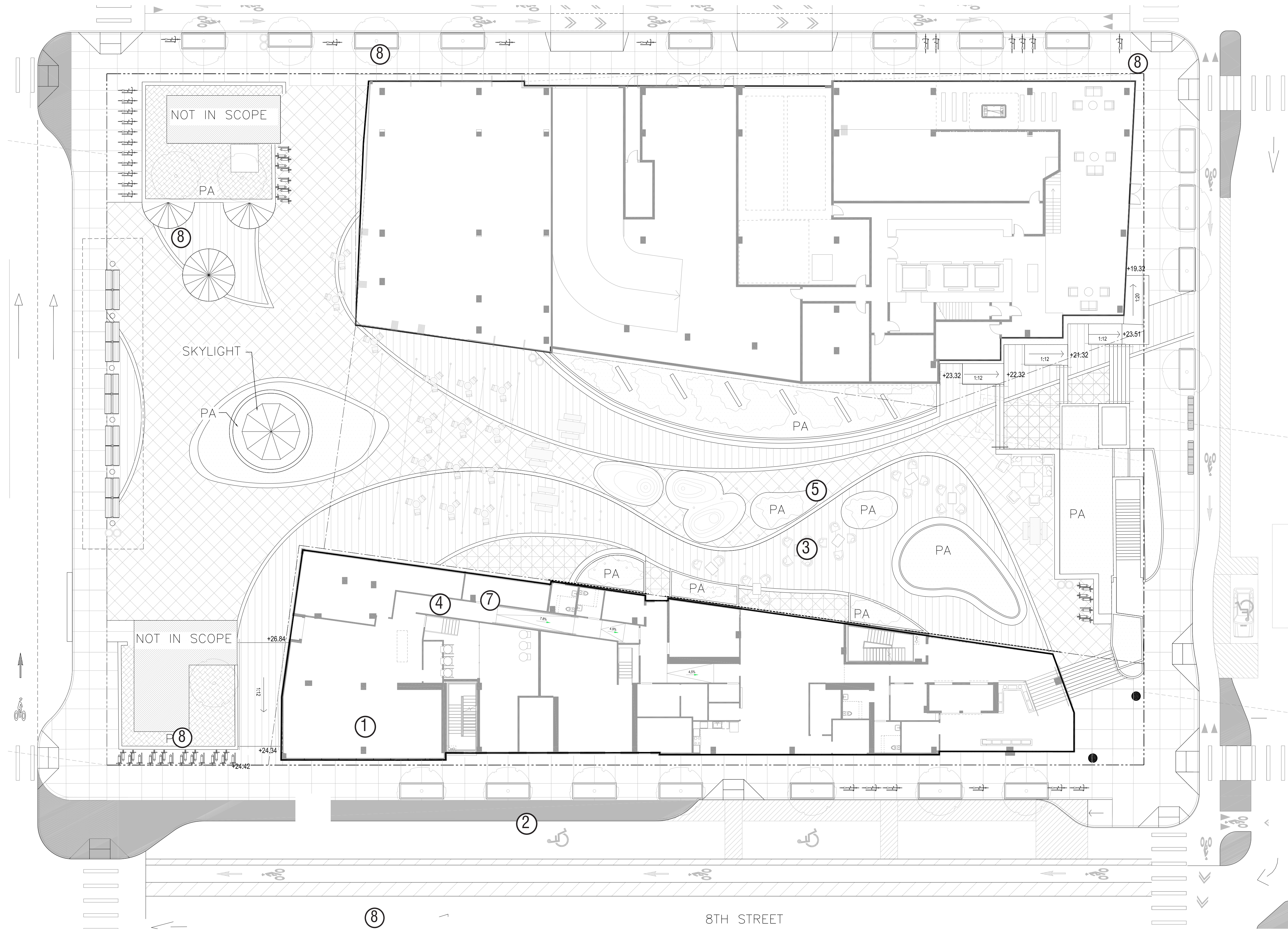
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BLOCK: 1

TITLE:  
**LANDSCAPE PLAN**

SHEET:

# L4.1

PRELIMINARY - Not for Construction -

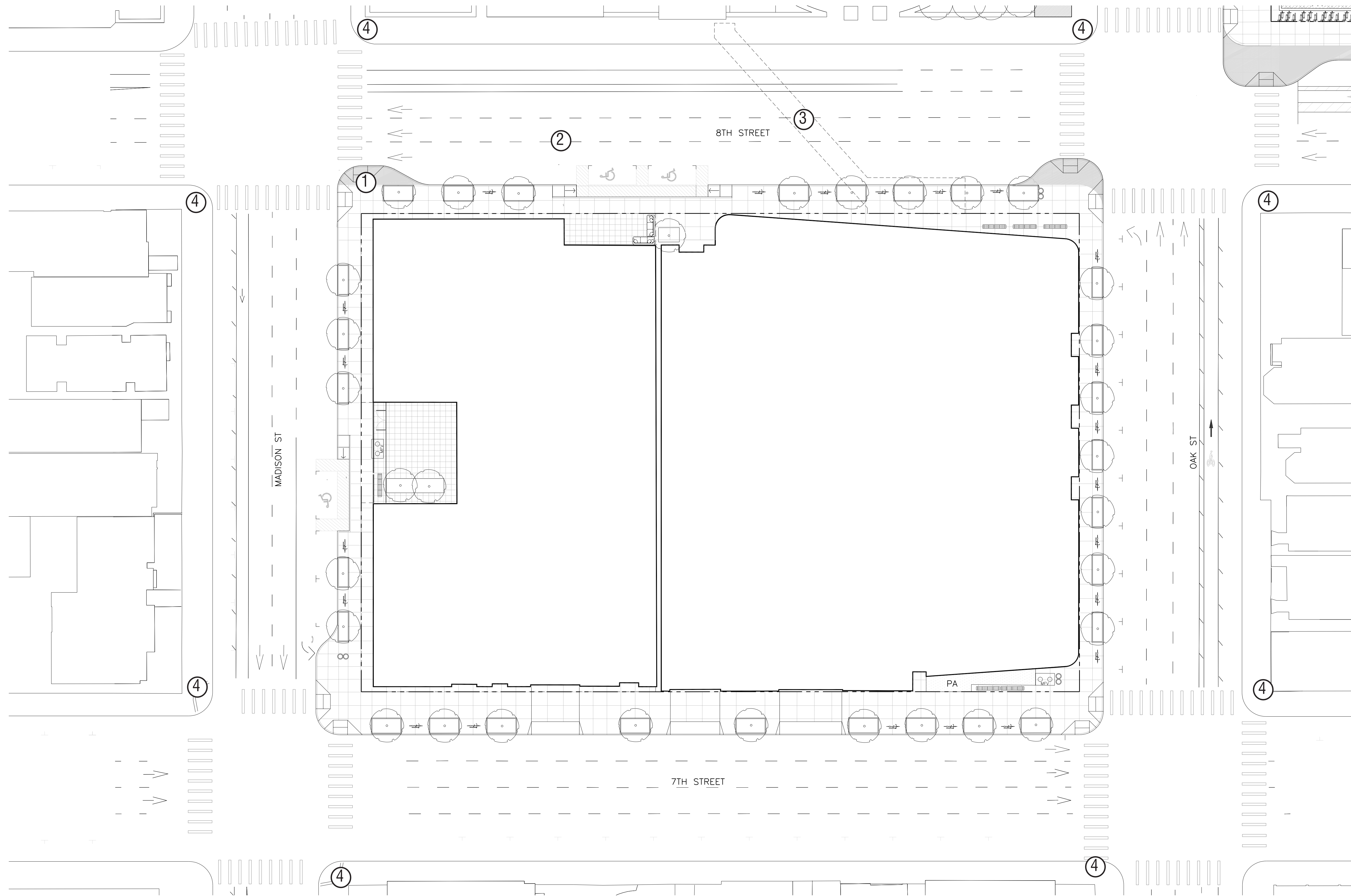


- LEGEND
- BULBOUT
  - PAINT STRIPING
  - ADA PARKING OR LOADING
  - SCOOTER CORRAL

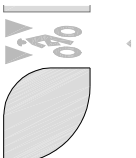


- NOTES:
- ① See Civil Drawings for limit of project sidewalk improvements.
  - ② Street access plan striping shown for planning purposes only.
  - ③ Paseo design under study. Furniture is movable. Typical conditions shown.
  - ④ All open space elements to be maintained by BART will be designed to BFS standards.
  - ⑤ The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads. Plant areas shown over the tunnel structure must comply with BFS standards.
  - ⑥ BART security cameras to be included as agreed to with BART.
  - ⑦ Property lines between the headhouse and building frontage of Building A and Building B will be called out in the paving with physical markings. See Civil Drawings for Property Lines
  - ⑧ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping and crosswalks as necessary.

NOTE:  
"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval" and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."





**LEGEND**

-  BULBOUT
-  PAINT STRIPING
-  ADA PARKING OR LOADING

**NOTES:**

- ① See Civil Drawings for limit of project sidewalk improvements.
- ② Street access plan striping shown for planning purposes only.
- ③ Sub surface tunnel (approximate location based On As-Built Drawings from Bart Project #17hc-110).
- ④ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping as necessary.

**NOTE:**

"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

**STAMP:**

**ISSUE SCHEDULE**

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

**REVISION SCHEDULE**

NO.	ISSUE	DATE
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JOB NUMBER: 1808  
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CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx

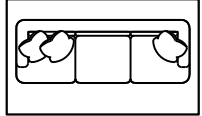
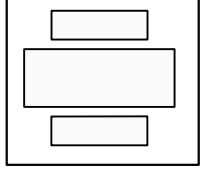
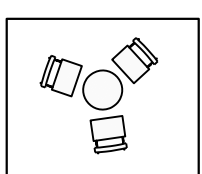
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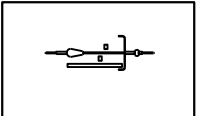
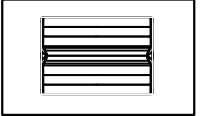

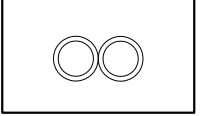
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**LANDSCAPE PLAN**

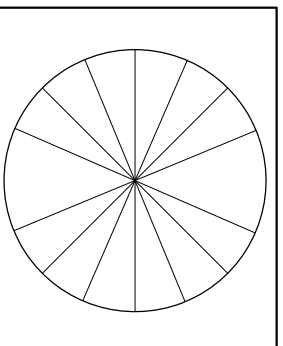
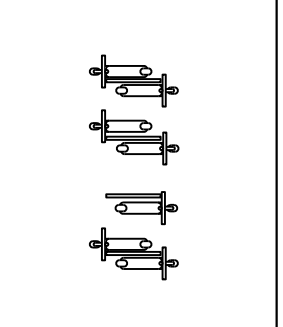
SHEET:  
**L4.2**

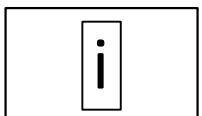
PRELIMINARY - Not for Construction -

LEGEND

-  SECTIONAL LOUNGE COUCH  
-TBD
-  ACCESSIBLE PICNIC TABLE  
-FFE
-  CAFE TABLE AND CHAIRS  
-FFE

-  BIKE RACK  
-BELSON ALUMINUM LOOP BIKE RACK  
-#DF6575
-  PLAZA BENCH  
-LANDSCAPE FORMS GENERATION 50  
-CUSTOM BACK TO BACK  
-WOOD: THERMALLY MODIFIED ASH  
-STEEL: POWDER COATED CUSTOM GREY
-  PROMENADE BENCH  
-CAST IN PLACE CONCRETE WITH BROKEN CERAMIC  
-MOOSAIC SURFACING
-  TRASH AND RECYCLING BINS  
-LANDSCAPE FORMS  
-SELECT RECYCLING SYSTEM  
-DOUBLE UNIT  
-COLOR: OBSIDIAN

-  BIKE LOCKER  
- EXISTING TO BE RELOCATED
-  - SCOOTER RACK  
-BELSON ALUMINUM LOOP BIKE RACK  
-#DF6575

-  INFO KIOSK  
-MMCITE  
-IF520-INFO

NOTES:  
 1. SEE CIVIL PLANS FOR EXTENTS OF BULB OUT PAVING AND ROAD WAY IMPROVEMENTS.  
 2. SEE LIGHTING PLANS FOR LIGHTING LAYOUT.

**EINWILLERKUEHL**  
 LANDSCAPE ARCHITECTURE  
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 OAKLAND CALIFORNIA 94607  
 (510) 891-1696

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
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STRADA INVESTMENT GROUP  
 101 MISSION ST. #20  
 SAN FRANCISCO, CA 94105

BKF ENGINEERS  
 255 SHORELINE DR, SUITE 200  
 REDWOOD CITY, CA 94065

LUMA LIGHTING DESIGN  
 48 GOLDEN GATE AVENUE  
 SAN FRANCISCO, CA 94102

# LAKE MERRITT BART HORIZONTAL FDP

Oakland, CA 94607

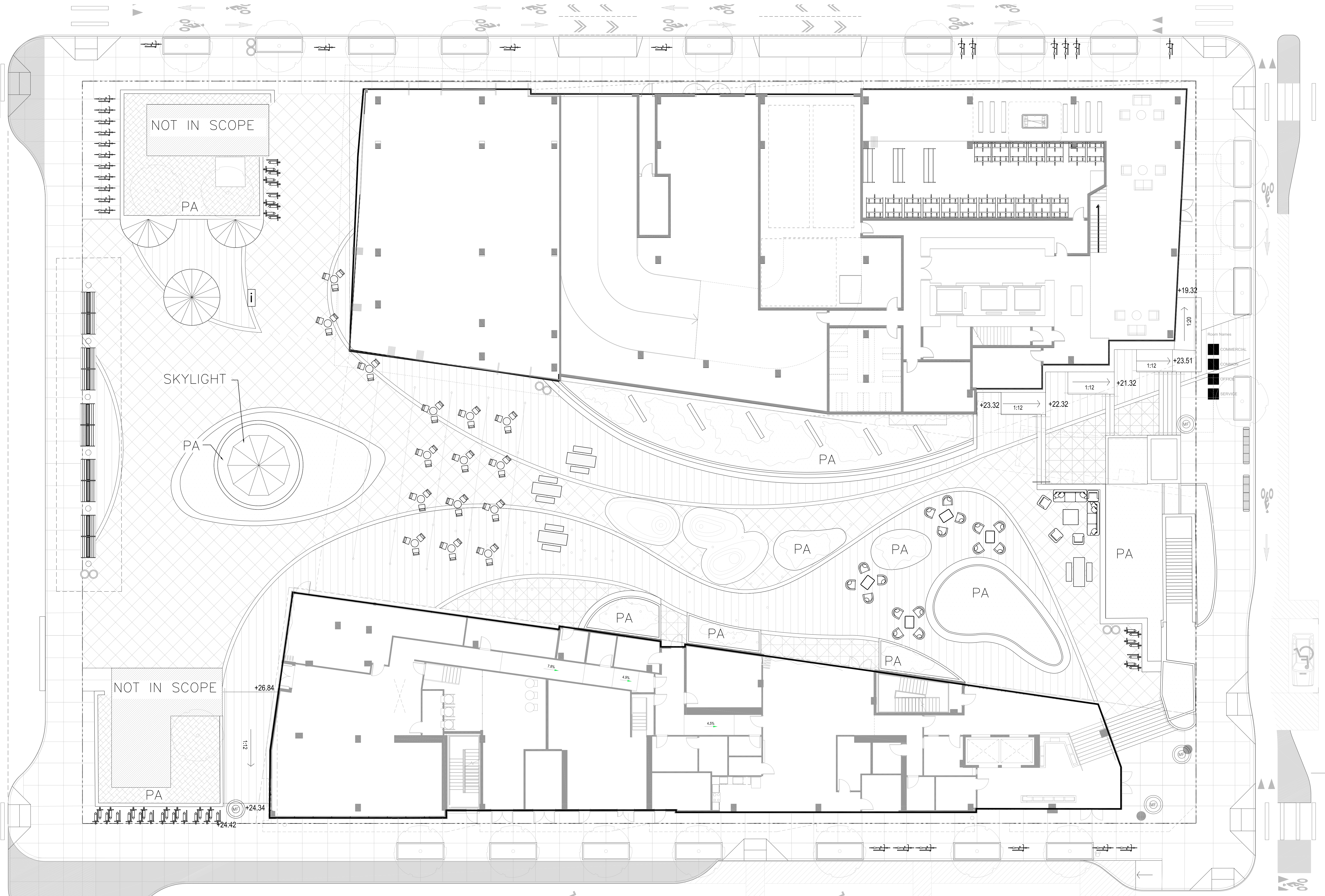
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ISSUE SCHEDULE		
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100 SD - HORIZONTAL FDP	12/17/2021	
100 SD - HORIZONTAL FDP	05/02/2022	
100 SD - HORIZONTAL FDP2	06/16/2022	

REVISION SCHEDULE	
NO.	DATE

JOB NUMBER: 1808  
 DRAWN BY: Author  
 CHECKED BY: Checker  
 ISSUE DATE: xx/xx/xxxx  
 SCALE: As indicated  
 PHASE: 1  
 BLOCK: 1  
 TITLE: FURNITURE PLAN

SHEET: **L5.1**  
 PRELIMINARY - Not for Construction -



8/24/2021 6:59:42 PM

LEGEND

STREET POLE TYPE S1  
-SEE L11.4 FOR DATA

FESTOON LIGHTING S2  
-SEE L11.4 FOR DATA

SECTIONAL LOUNGE COUCH  
-TBD

BIKE RACK  
-BELSON (SEE L5.1 FOR DATA)

COMMUNAL BENCH  
-LBO110 BLOCQ PARK BENCH BY MMCITE  
-WOOD: THERMALLY MODIFIED ASH  
-METAL FINISH: POWDER-COATED

TRASH AND RECYCLING BINS  
-LANDSCAPE FORMS (SEE L5.1 FOR DATA)

NOTES:  
1. SEE CIVIL PLANS FOR EXTENTS OF BULB OUT PAVING AND ROAD WAY IMPROVEMENTS.



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
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100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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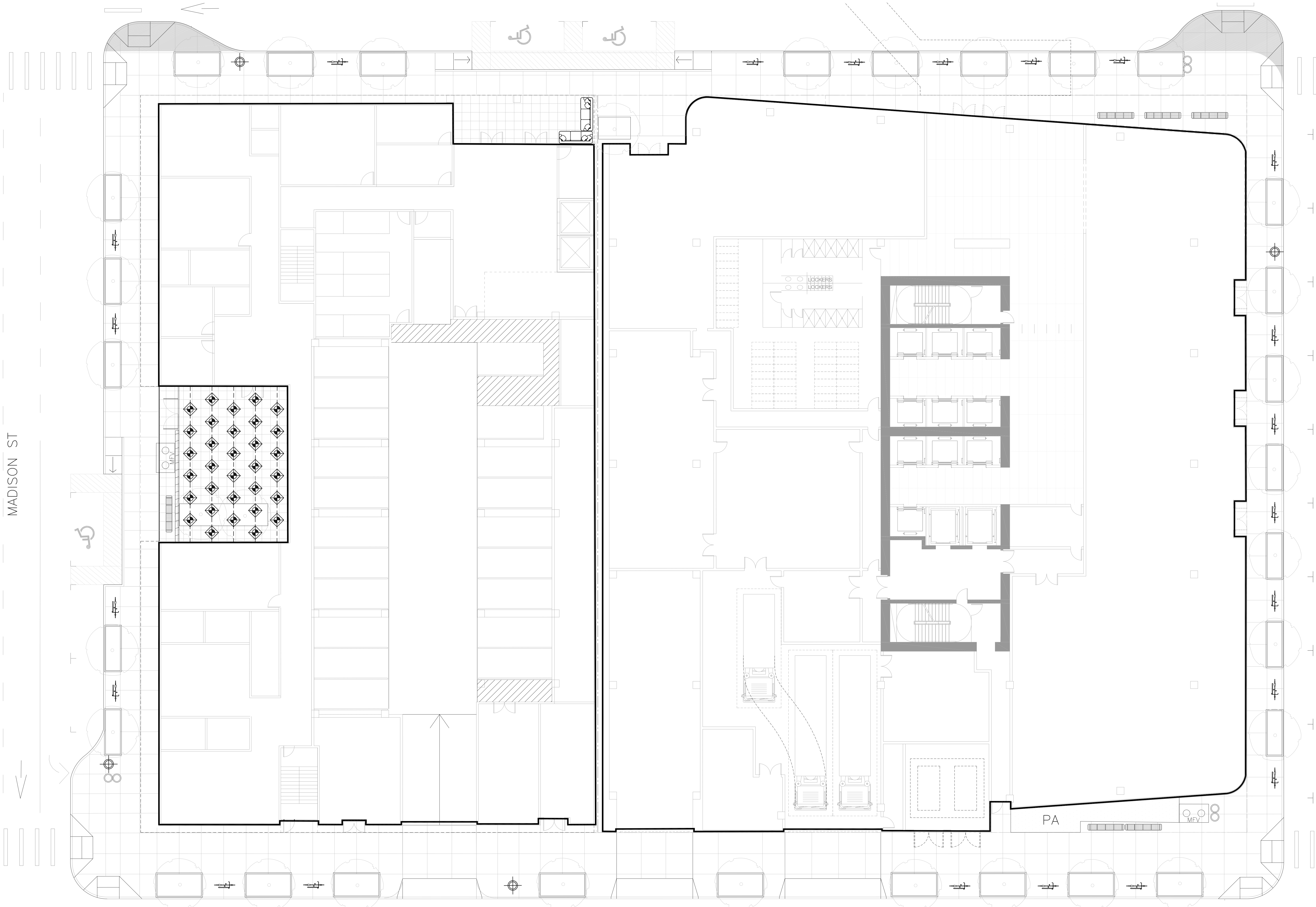
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ISSUE DATE: xx/xx/xxxx

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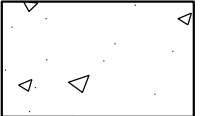

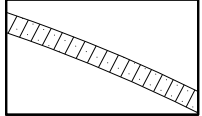

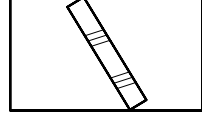
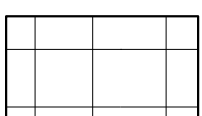
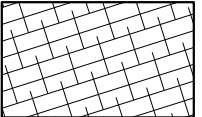
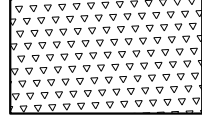
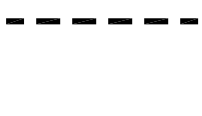
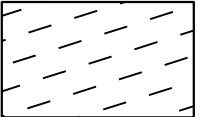

TITLE:  
**LIGHT AND FURNITURE PLAN**

SHEET:  
**L5.2**

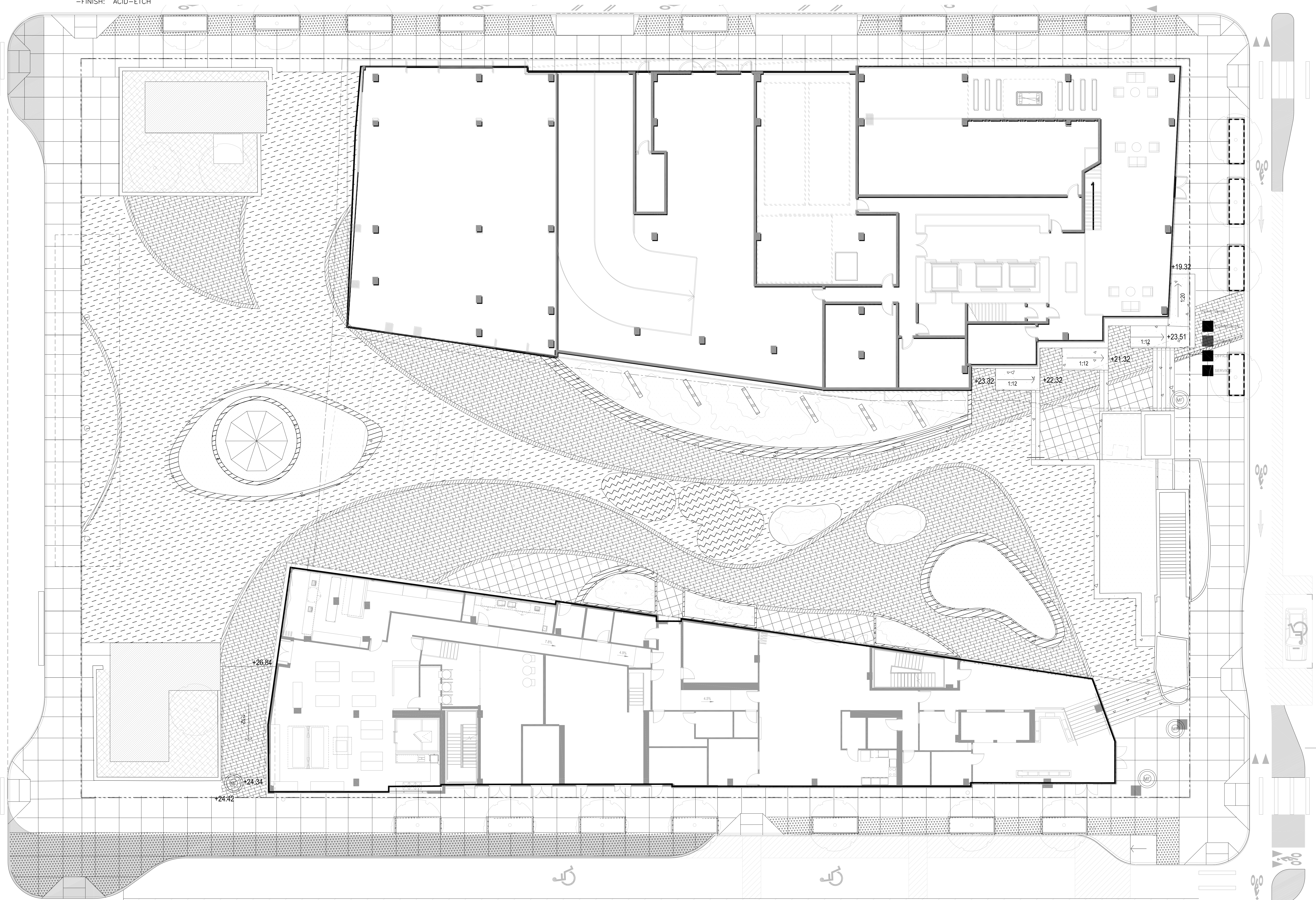
PRELIMINARY - Not for Construction -



LEGEND

- |   |  |   |   |   |   |
|---|--|---|---|---|---|
|   | CONCRETE STAIRS AND WALLS<br>-SCOFIELD WINTER BEIGE                            |   | COLORED CONCRETE 3<br>-LITHOCRETE<br>-COLOR: PEWTER<br>-FINISH: ACID-ETCH   |   | TRENCH DRAIN<br>-URBAN ACCESSORIES, CUSTOM PATTERN<br>-CAST IRON                                |
|  | -CAST IN PLACE CONCRETE<br>WITH BROKEN CERAMIC MOSAIC<br>SURFACING             |  | STEEL FRAME FOR VINE LATTICE<br>-COLOR: DARK GREY<br>-FINISH: POWDER COATED |  | SIDEWALK<br>-COLOR: 10% LAMP BLACK<br>-FINISH: BROOM FINISH<br>-OAKLAND CITY STANDARD           |
|  | COLORED CONCRETE 1<br>-LITHOCRETE<br>-COLOR: CHARCOAL<br>-FINISH: ACID-ETCH    |  | COBBLE STONE<br>-COLOR: DARK GREY<br>-SEE L6.2                              |  | STEEL RAISED EDGING<br>-1/4" THICK CORTEN OR POWDER COATED<br>GALVANIZED STEEL<br>-COLOR: BLACK |
|  | COLORED CONCRETE 2<br>-LITHOCRETE<br>-COLOR: FRENCH GREY<br>-FINISH: ACID-ETCH |  | RESILIENT SURFACING<br>-COLORS: TBD   |   |   |

NOTES:  
 1. SEE CIVIL PLANS FOR EXTENTS OF BULB OUT PAVING AND ROAD WAY IMPROVEMENTS.  
 2. CONCRETE COLOR AND PATTERN TO BE COMPLEX, RICH TONES, AND SPECIAL INSTALL.



**EINWILLERKUEHL**  
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 318 HARRISON STREET SUITE 301  
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EAST BAY ASIAN LOCAL  
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 1825 SAN PABLO AVE. #200  
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STRADA INVESTMENT GROUP  
 101 MISSION ST. #420  
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BKF ENGINEERS  
 255 SHORELINE DR, SUITE 200  
 REDWOOD CITY, CA 94065

LUMA LIGHTING DESIGN  
 48 GOLDEN GATE AVENUE  
 SAN FRANCISCO, CA 94102

**LAKE MERRITT BART  
 HORIZONTAL FDP**  
 Oakland, CA 94607

STAMP:

ISSUE SCHEDULE	NO.	ISSUE	DATE
50 SD - HORIZONTAL FDP	10092021		
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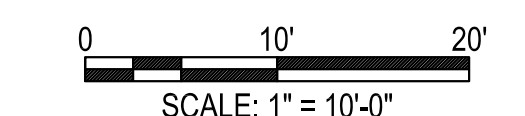
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JOB NUMBER: 1808  
 DRAWN BY: Author  
 CHECKED BY: Checker  
 ISSUE DATE: xx/xx/xxxx  
 SCALE: As indicated  
 PHASE: 1  
 BLOCK: 1

TITLE: **MATERIALS PLAN**  
 SHEET: **L6.1**

PRELIMINARY - Not for Construction -  
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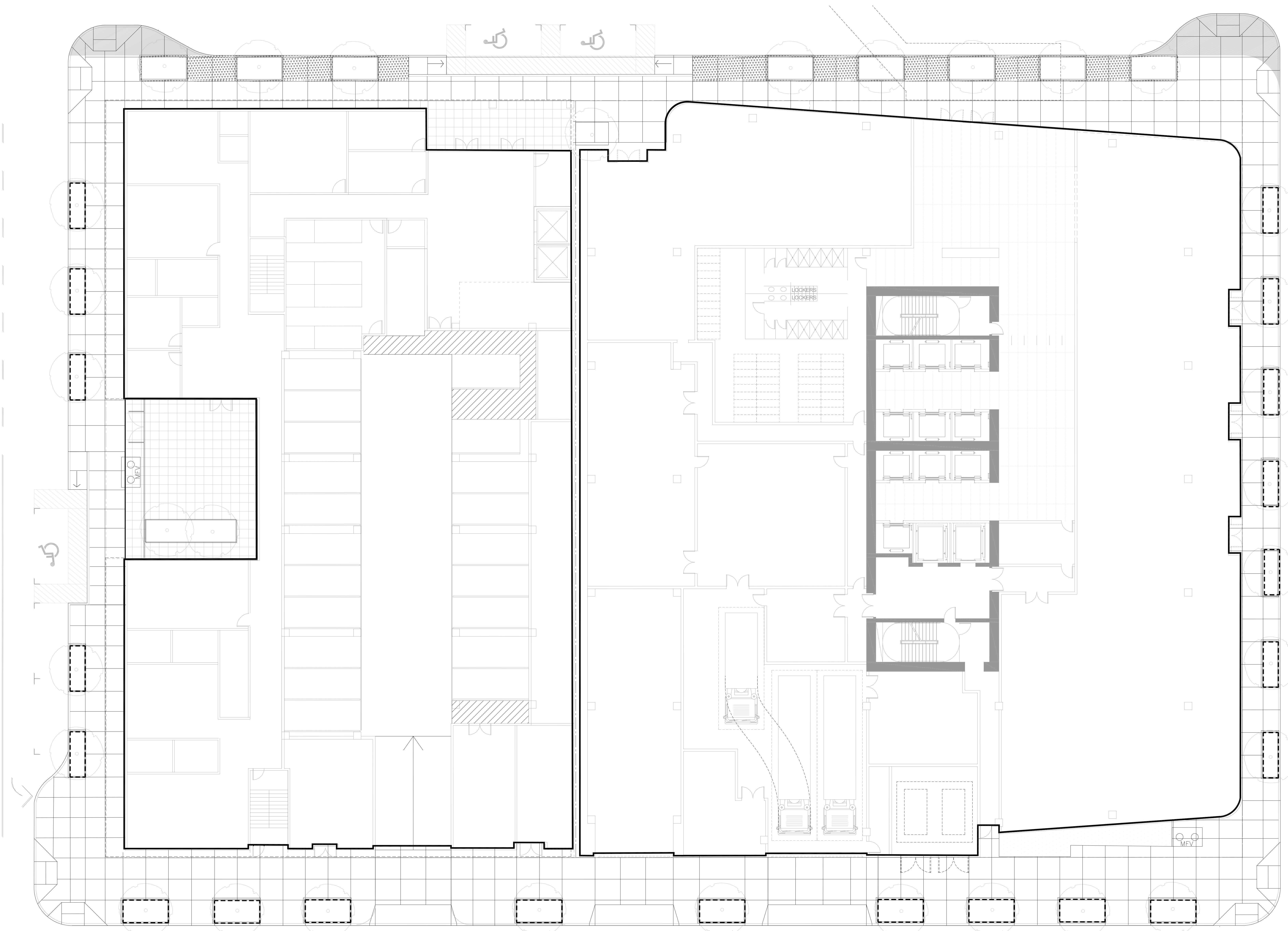
LEGEND

- OPTION 1: STAMPED CONCRETE
  - SCOFIELD CHARCOAL
  - OPTION 2: GRANITE COBBLE
  - SAWN AND FLAMED TOP X SPLIT SIDES
  - 3"X3"X3", SANDESET
- SIDEWALK
  - COLOR: 10% LAMP BLACK
  - FINISH: BROOM FINISH
  - OAKLAND CITY STANDARD

- CONCRETE UNIT PAVER
  - STEPSTONE INC.
  - NARROW MODULAR PAVERS 3"X18"
  - COLOR: AGAVE
- CORTEN STEEL RAISED EDGING

NOTES:

1. SEE CIVIL PLANS FOR EXTENTS OF BULB OUT PAVING AND ROAD WAY IMPROVEMENTS.
2. CONCRETE COLOR AND PATTERN TO BE COMPLEX, RICH TONES, AND SPECIAL INSTALL.



MADISON ST



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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JOB NUMBER:	1808
DRAWN BY:	Author
CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx

SCALE:	As indicated
PHASE:	1
BLOCK:	2

TITLE: MATERIALS PLAN

SHEET:

**L6.2**

LEGEND

- 40% POLYSTICHUM MUNITUM  
40% ACACIA COGNATA 'COUSIN ITT'  
20% AEGONIUM SP2  
- SIZE: 1 GAL CONTAINER  
- SPACING: 24" OC TRIANGULAR
- 50% POLYSTICHUM MUNITUM  
50% EUPHORBIA CHARACIAS  
- SIZE: 1 GAL CONTAINER  
- SPACING: 24" OC TRIANGULAR
- 50% DIANELLA TASMANICA 'TINY TAZ'  
50% EUPHORBIA CHARACIAS  
- SIZE: 1 GAL CONTAINER  
- SPACING: 24" OC TRIANGULAR
- 30% EUPHORBIA CHARACIAS  
70% SESLERIA NITIDA  
- SIZE: 1 GAL CONTAINER  
- SPACING: 24" OC TRIANGULAR
- 70% LIRIOPE MUSCARI  
30% SESLERIA NITIDA  
- SIZE: 4" CONTAINER  
- SPACING: 18" OC TRIANGULAR

- 8TH ST + 9TH ST TREES BLOCK 1  
- QUERCUS SUBER  
- SIZE: 36" BOX
- OAK ST BLOCK 2 AND FALLON ST BLOCK 1 TREES  
- AESCULUS CARNEA 'BRIOTI'  
- SIZE: 36" BOX
- PASEO TREES  
- AFROCARPUS GRACILIOR  
- SIZE: 36" BOX
- PASEO AND HEADHOUSE TREES  
- PRUNUS KWANZAAN  
- SIZE: 36" BOX
- PASEO SHRUB  
- CORNUS FLORIDA  
- SIZE: 24" BOX

- PASEO TREES  
- AFROCARPUS GRACILIOR  
- SIZE: 36" BOX
- PASEO AND HEADHOUSE TREES  
- ACER PALMATUM  
- SIZE: 36" BOX
- HEADHOUSE TREES  
- PRUNUS KWANZAAN  
- SIZE: 36" BOX
- PASEO SHRUB  
- CORNUS FLORIDA  
- SIZE: 24" BOX

- PASEO SHRUB  
- CORNUS 'RUTGAN'  
- SIZE: 24" BOX
- PASEO VINE  
- PASSIFLORA CAERULEA  
- SIZE: 1 GAL

NOTES:  
1. SEE CIVIL PLANS FOR EXTENTS OF BULB OUT PAVING AND ROAD WAY IMPROVEMENTS.  
2. SPECIES SELECTIONS MY CHANGE BASED ON AVAILABILITY, DISEASE, OR OVERALL COORDINATION OF PLANTING PALETTE, AND BART OPERATIONAL IMPACTS.

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LUMA LIGHTING DESIGN  
48 GOLDEN GATE AVENUE  
SAN FRANCISCO, CA 94102

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101 MISSION ST. #20  
SAN FRANCISCO, CA 94105

**STRADA**  
INVESTMENT GROUP

BKF ENGINEERS  
255 SHORELINE DR, SUITE 200  
REDWOOD CITY, CA 94065

**BKF** BKF ENGINEERS

LUMA LIGHTING DESIGN  
48 GOLDEN GATE AVENUE  
SAN FRANCISCO, CA 94102

**LUMA**

**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE

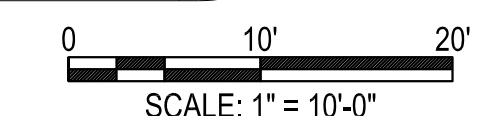
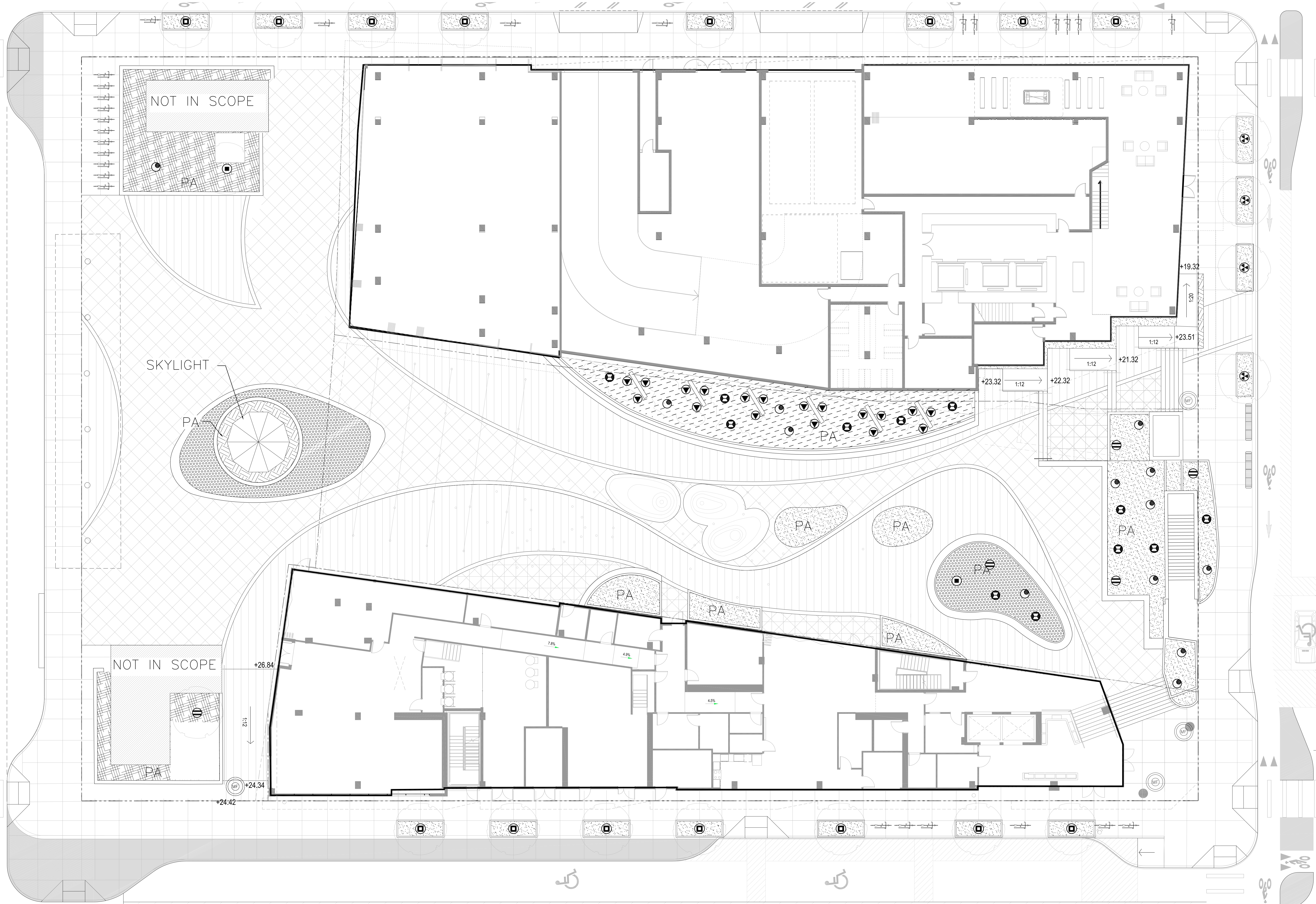
NO.	ISSUE	DATE
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JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx  
SCALE: As indicated  
PHASE: 1  
BLOCK: 1  
TITLE: **PLANTING PLAN**

SHEET:  
**L7.1**

PRELIMINARY - Not for Construction -

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LEGEND


- 50% DIANELLA TASMANICA 'TINY TAZ'
  - 50% EUPHORBIA CHARACIAS
  - SIZE: 1 GAL CONTAINER
  - SPACING: 24" OC TRIANGULAR
- 30% EUPHORBIA CHARACIAS
  - 70% SESLERIA NITIDA
  - SIZE: 1 GAL CONTAINER
  - SPACING: 24" OC TRIANGULAR

- 8TH ST TREES BLOCK 2
  - MELALEUCA QUINQUENARIA
  - SIZE: 36" BOX
- OAK ST BLOCK 2 AND FALLON ST BLOCK 1 TREES
  - AESCULUS CARNEA 'BRIOTI'
  - SIZE: 36" BOX
- 7TH ST TREES BLOCK 2
  - LOPHOSTEMON CONFERTUS
  - SIZE: 36" BOX

NOTES:  
 1. SEE CIVIL PLANS FOR EXTENTS OF BULB OUT PAVING AND ROAD WAY IMPROVEMENTS.  
 2. SPECIES SELECTIONS MY CHANGE BASED ON AVAILABILITY, DISEASE, OR OVERALL COORDINATION OF PLANTING PALETTE, AND BART OPERATIONAL IMPACTS.

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 LANDSCAPE ARCHITECTURE  
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STRADA INVESTMENT GROUP  
 101 MISSION ST. #420  
 SAN FRANCISCO, CA 94105



BKF ENGINEERS  
 255 SHORELINE DR, SUITE 200  
 REDWOOD CITY, CA 94065



LUMA LIGHTING DESIGN  
 48 GOLDEN GATE AVENUE  
 SAN FRANCISCO, CA 94102

**LUMA**

**LAKE MERRITT BART  
 HORIZONTAL FDP**  
 Oakland, CA 94607

STAMP:

ISSUE SCHEDULE	
50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE		
NO.	ISSUE	DATE

JOB NUMBER: 1808  
 DRAWN BY: Author  
 CHECKED BY: Checker  
 ISSUE DATE: xx/xx/xxxx

SCALE: As indicated  
 PHASE: 1  
 BLOCK: 2

TITLE:  
**PLANTING PLAN**

SHEET:  
**L7.2**

PRELIMINARY - Not for Construction -

© 2021 EINWILLERKUEHL LANDSCAPE ARCHITECTURE & URBAN DESIGN



MADISON ST

8/24/2021 6:59:42 PM

0 10' 20'  
 SCALE: 1" = 10'-0"



Key Plan

NOTE:

1. The project shall demonstrate compliance with the City of Oakland's Landscape Ordinance prior to building permit final. Project will be responsible to carry out any changes that may result from compliance with the Landscape Ordinance, which may include, but not limited to, revised plans, additional technical information, and/or additional planning applications.

2. Species selections may change based on availability, disease, or overall coordination of planting palette, and BART operational impacts.

3. The proposed planting type, character, and water use is in accordance to the City of Oakland's master street tree list. The species selection will develop as we work in conjunction with TOF and Madison Square Park.

4. This is intended to work as a 3 block open space, and the continuity of plant species will be consistent between project blocks.

PLANTING CONCEPT STATEMENT

Greening and providing healthy plant material with rich textures is an important part of the design for this site adjacent to chinatown's heavily built environment. The planting design showcases durable urban plants that have been proven to be successful in Oakland. Conceptually, the planting design highlights the lush, shade tolerant plants that provide a sense of tranquility on the site. The design of the public open space melds with the streetscape and provides variety throughout the site. Microclimates inform the species selection and the opportunity to showcase dry shade and understory plants. In all areas, low water use was a key consideration in selection - thus, all plants selected are wucols rated moderate to low water use. As the planters are on structure, soil depth is shallow, with foam used to add topographic variation. All grasses will be planted from 4" plugs, and herbaceous perennials from 1 gallon pots, 2' on center.

IRRIGATION CONCEPT STATEMENT

The irrigation design for the site shall comply with the state of California model water efficient landscape ordinance (TITLE 23 - DIVISION 2-CHAPTER 2.7) And the city of Oakland water efficient landscape standards. The irrigation systems will be automatically controlled by an et irrigation controller capable of multiple programming and independent timing of individual irrigation systems. The controller will have a 24-hour clock to allow multiple start times and repeat cycles to adjust for soil percolation rates. The irrigation systems will consist primarily of low volume, low flow bubblers for trees, point source drip irrigation for shrubs and groundcovers, and low flow irrigation for turf plantings. Plants will be grouped onto separate valves according to sun exposure and water use to allow for irrigation application by hydrozone. The irrigation scheduling will reflect the regional evapo-transpiration rates. The entire site will be designed to run during nighttime hours when irrigation is most efficient.

PROPOSED TREE SPECIES

3 Block Open Space Frame - 75 Trees



Block 1 & 2: Cork Oak, *Quercus suber*  
Medium-sized, evergreen tree, with open spreading form. Interesting bark texture.  
Non native, Low water use



Enlargement of Cork Oak

The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads.

Plant areas shown over the tunnel structure must comply with BES standards.

LEGEND AND WUCOLS RATING

- Pollinator
- Water Use Low
- Water Use Regular
- Water Use High
- Oakland Master Street Tree List

PROPOSED TREE SPECIES BY STREET

Oak Street - 7 Trees



Red Horsechestnut, *Aesculus carnea 'Brioti'*  
Upright-oval rounded form, shade tree and ornamental tree. Showy flowers.  
Non native, moderate water use, pollinator

Madison Street - 5 Trees



Brisbane Box, *Ixopstemon corlettus*  
Large, upright, evergreen tree. Effective screen or wind break.  
Water regular, non-native, pollinator

7th Street - 8 Trees



Brisbane Box, *Ixopstemon corlettus*  
Large, upright, evergreen tree. Effective screen or wind break.  
Water regular, non-native, pollinator

9th & 8th Opposite Open Space Frame



Paperbark Tree, *Melaleuca quinquenervia*  
Multi trunk. Erect, spreading form with low canopy. Evergreen. Showy flowers in Summer or Fall. Paper like bark.  
Drought tolerant, non-native, pollinator

**EINWILLERKUEHL**  
LANDSCAPE ARCHITECTURE

318 HARRISON STREET SUITE 301  
OAKLAND CALIFORNIA 94607  
(510) 891-1696

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION

1825 SAN PABLO AVE. #200  
OAKLAND, CA 94621

STRADA INVESTMENT GROUP

101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**STRADA**  
INVESTMENT GROUP

BKF ENGINEERS

255 SHORELINE DR, SUITE 200  
REDWOOD CITY, CA 94065

**BKF** BKF ENGINEERS

LUMA LIGHTING DESIGN

48 GOLDEN GATE AVENUE  
SAN FRANCISCO, CA 94102

**LUMA**

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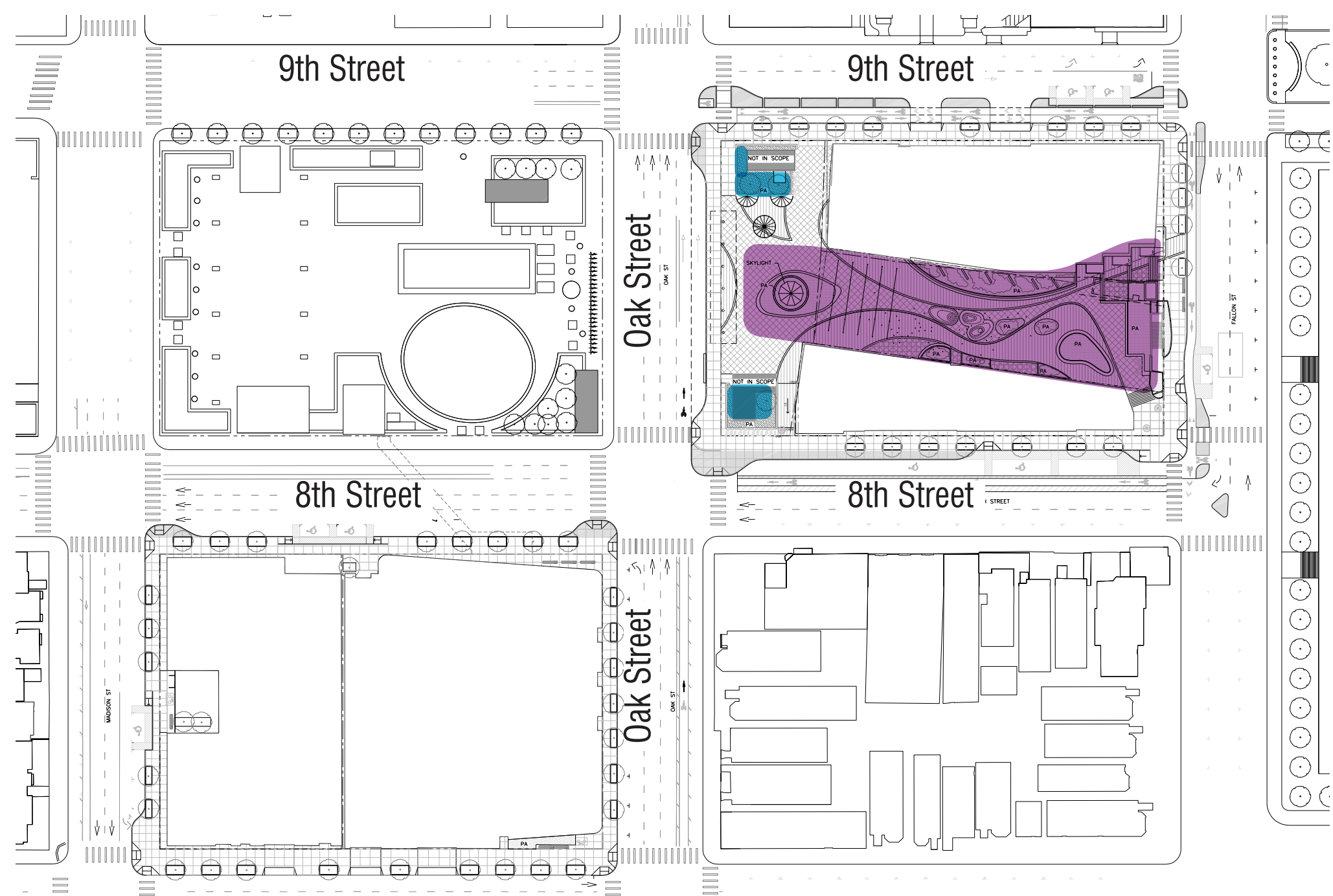
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SCALE:	As indicated
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BLOCK:	1 AND 2
TITLE:	

PLANTING PALETTE

SHEET:  
**L7.3**





Key Plan

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  2. Species selections may change based on availability, disease, or overall coordination of planting palette, and BART operational impacts.
  3. We're proposing planting type, character, and water use in accordance to the City of Oakland's approved street tree list. The species selection will develop as we work in conjunction with TOF and Madison Square Park.
  4. Species selected are sourced from Oakland's approved street tree species list.

LEGEND AND WUCOLS RATING

- Pollinator
- Water Use Low
- Water Use Regular
- Water Use High
- Oakland Master Street Tree List

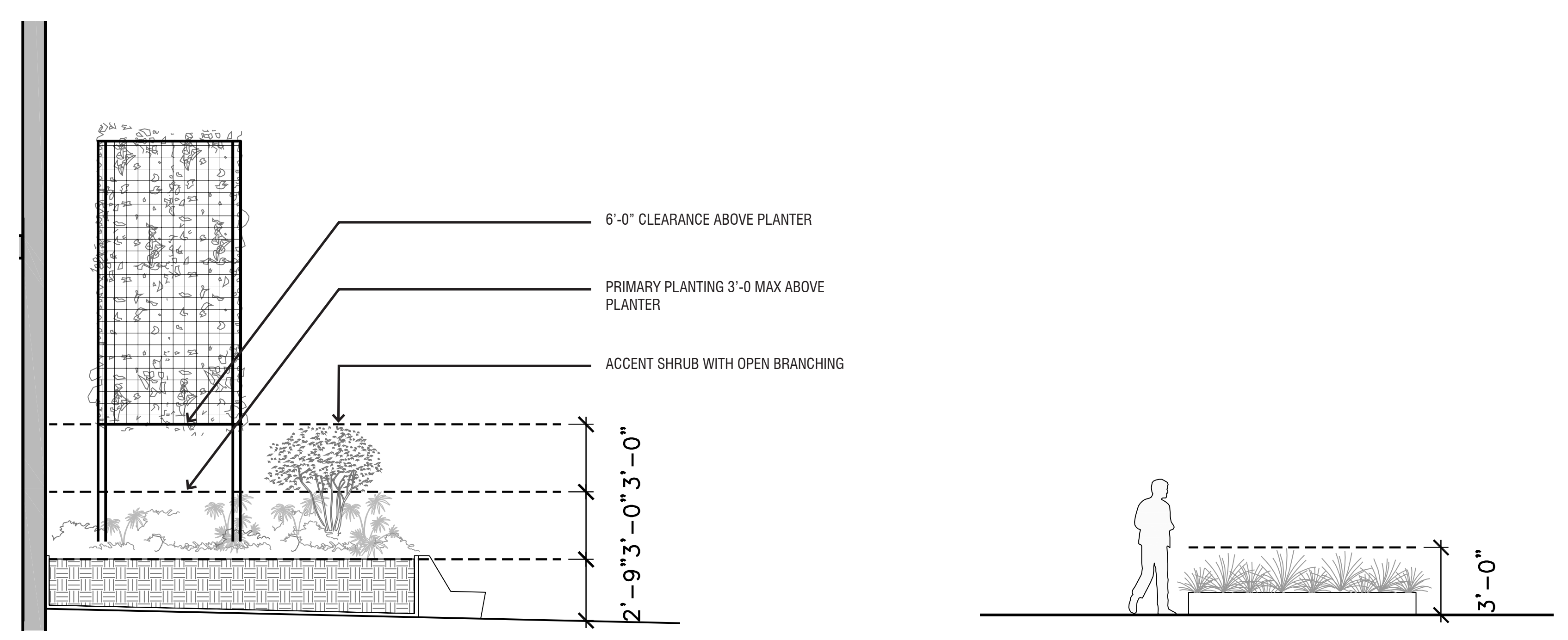
Proposed Skylight and Paseo Planting

Paseo Planter: <i>Sesleria nitida</i> mix with <i>euphorbia</i> Regular water use, non natives	Vine on parking garage: <i>Passiflora caerulea</i> , Low water use, non native	Enlargement: <i>Accacia cognata</i> 'Cousin It' and <i>Aceuilums</i> mix. Low water use, non native	Paseo Planter: Western Sword Fern. Regular water, native	Paseo Planter: Sweet Box, <i>Sarcococca confusa</i> Regular water, non native, pollinator	Tree Choice: Fern Pine, <i>Afrocarpus Gracilior</i> Non native, regular to low water	Tree Choice: Flowering Dogwood, <i>Cornus florida</i> Non native, regular water	Tree Choice: Stellar Pink Dogwood, <i>Cornus 'rutgan' Stellar Pink</i> Non native, regular to low water use

Proposed Headhouse Planting

Monkey Grass, Mondo grass Non native, regular to low water use	Green Leaf Japanese Maple, <i>Acer Platanum</i> , with Mondo grass Non native, regular water	Tree Choice: Fern Pine, <i>Afrocarpus Gracilior</i> Non native, regular to low water	Tree Choice: Flowering Cherry Tree, <i>Prunus 'Kwanzan'</i> Non native, regular water use

Visibility and Planting Height Diagrams



Planting Adjacent to Building A

Typical Planter

EINWILLERKUEHL  
LANDSCAPE ARCHITECTURE

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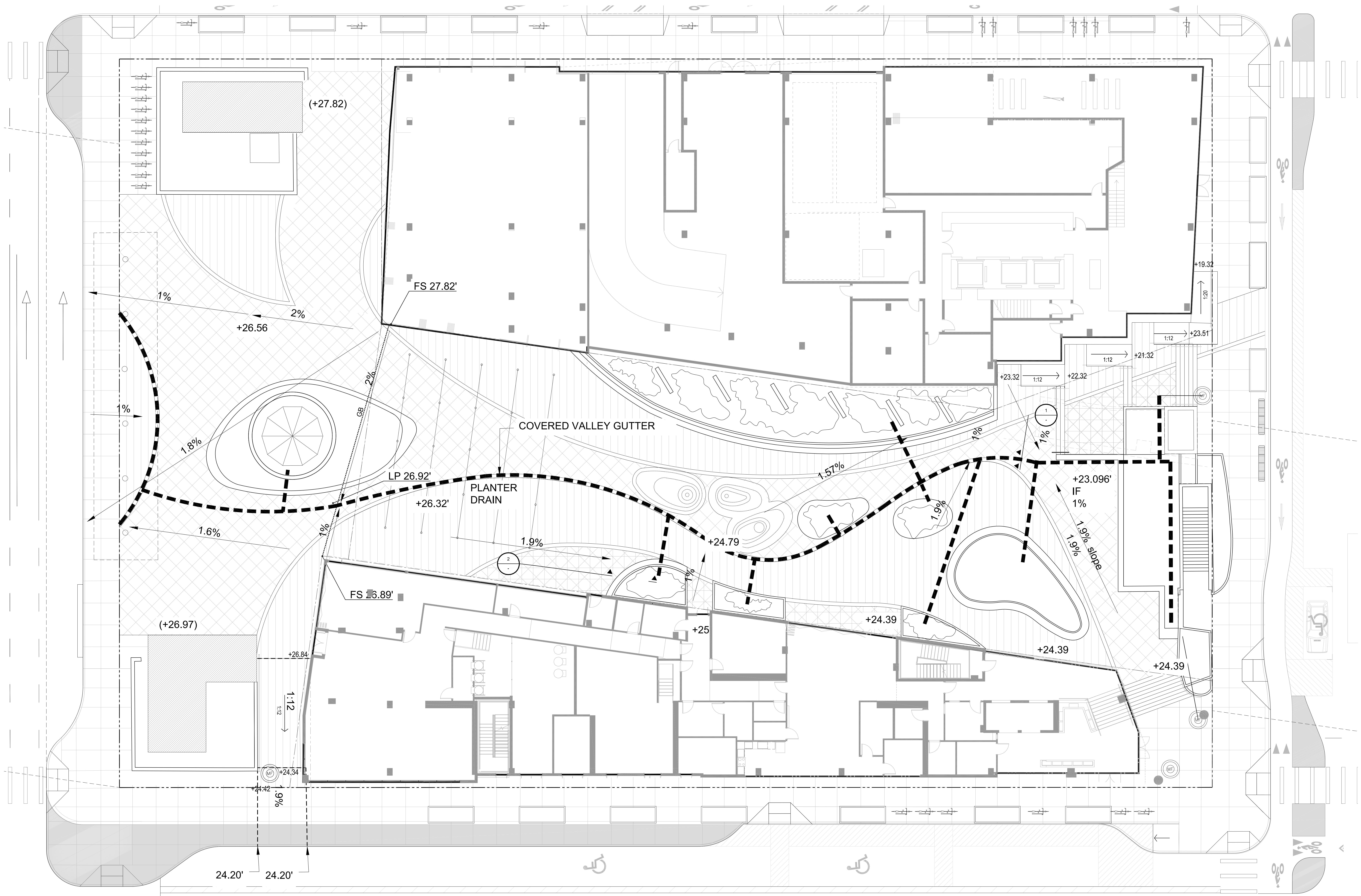
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SCALE:	As indicated
PHASE:	1
BLOCK:	1

PLANTING PALETTE

SHEET:  
**L7.4**

- NOTES:
1. All planting and irrigation above tunnel will be designed to meet BFS standards.
  2. Waterproofing design to be determined. Final design will comply with BFS standards.
  3. Existing drainage (see Civil C2.1 and C2.2) to be replaced with new drainage trench drain system. 2. Waterproofing design to be determined. Final design will comply with BFS standards.
  5. See civil plans for extents of bulb out paving and road way improvements
  6. See civil plans for sidewalk and road grades



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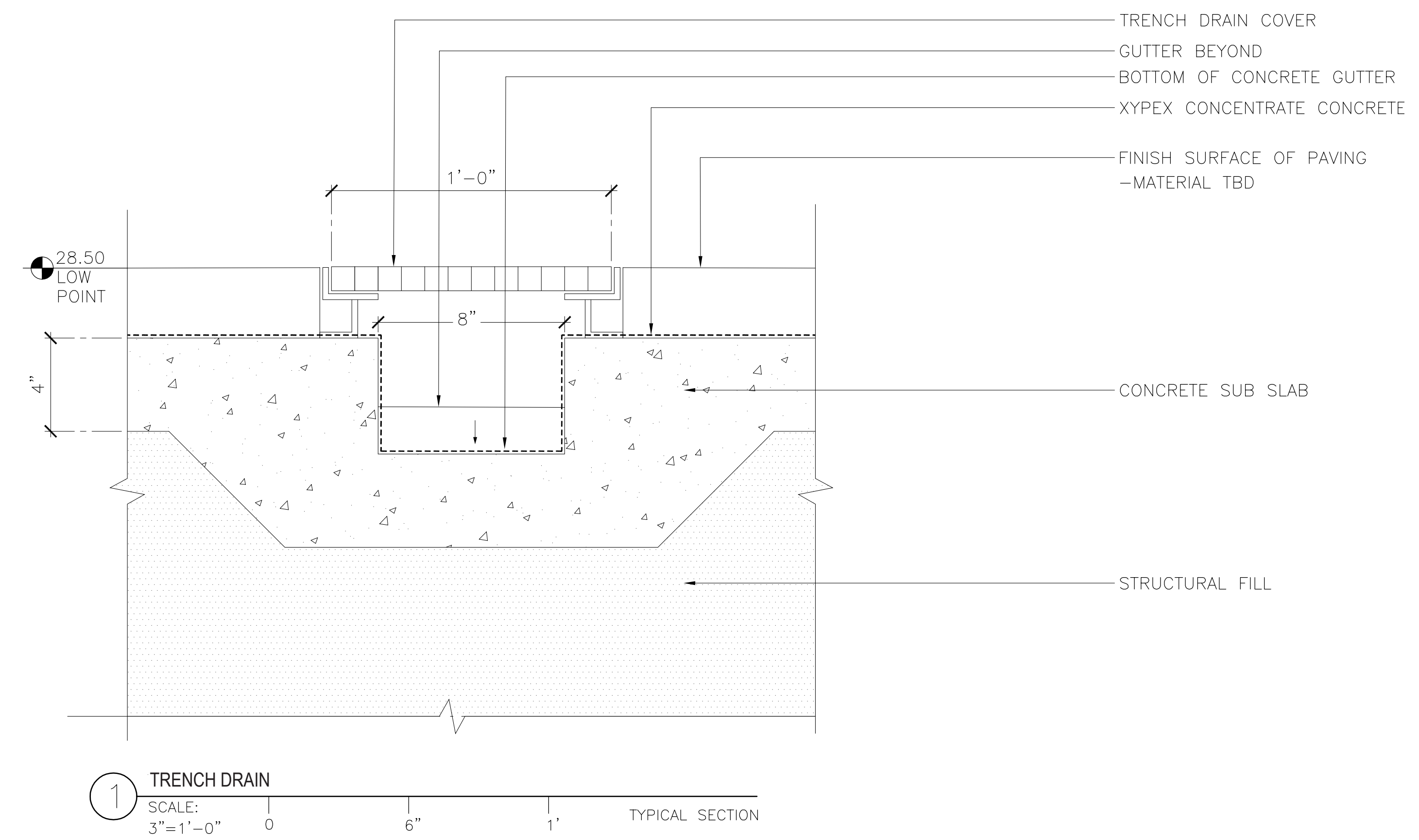
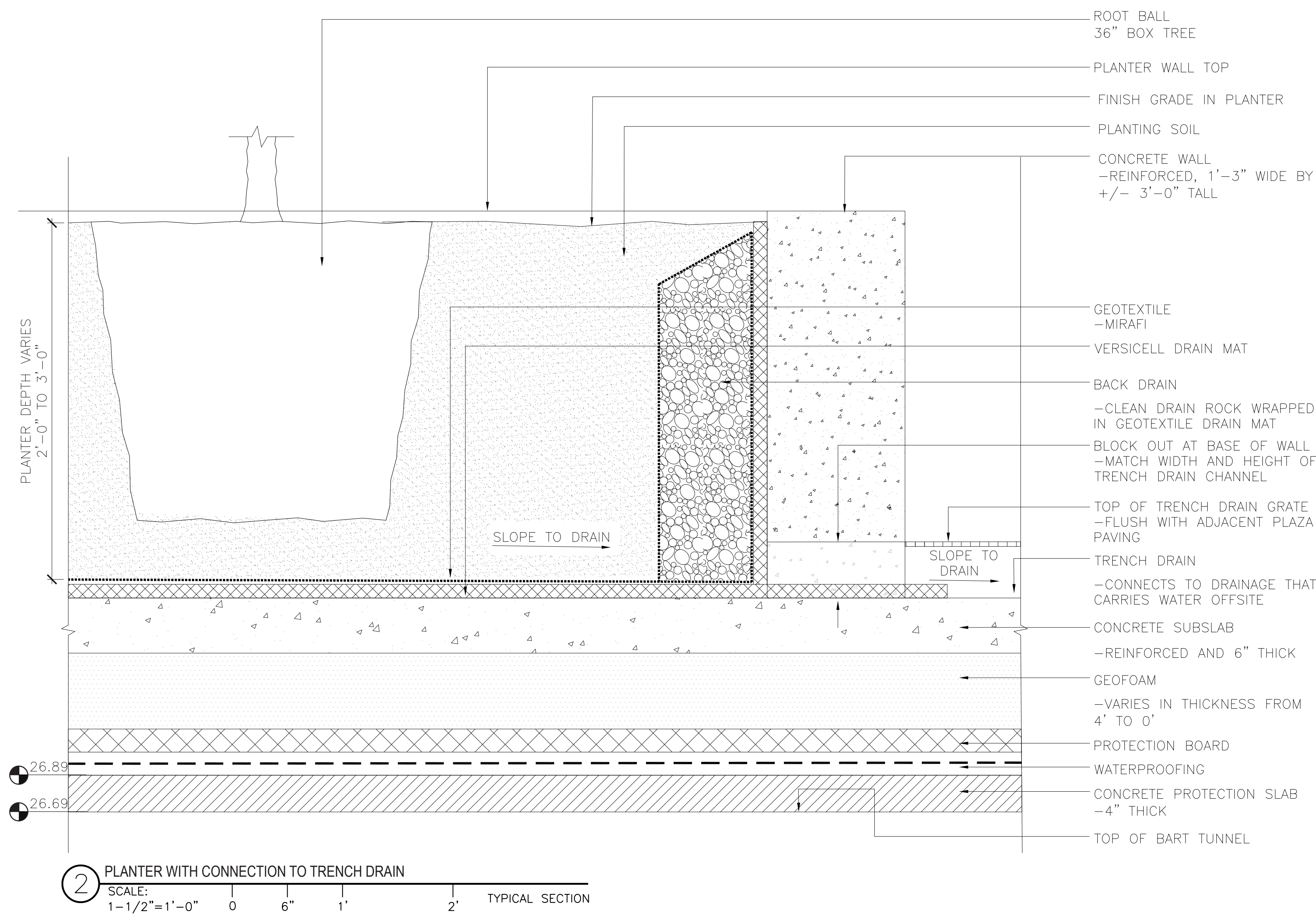
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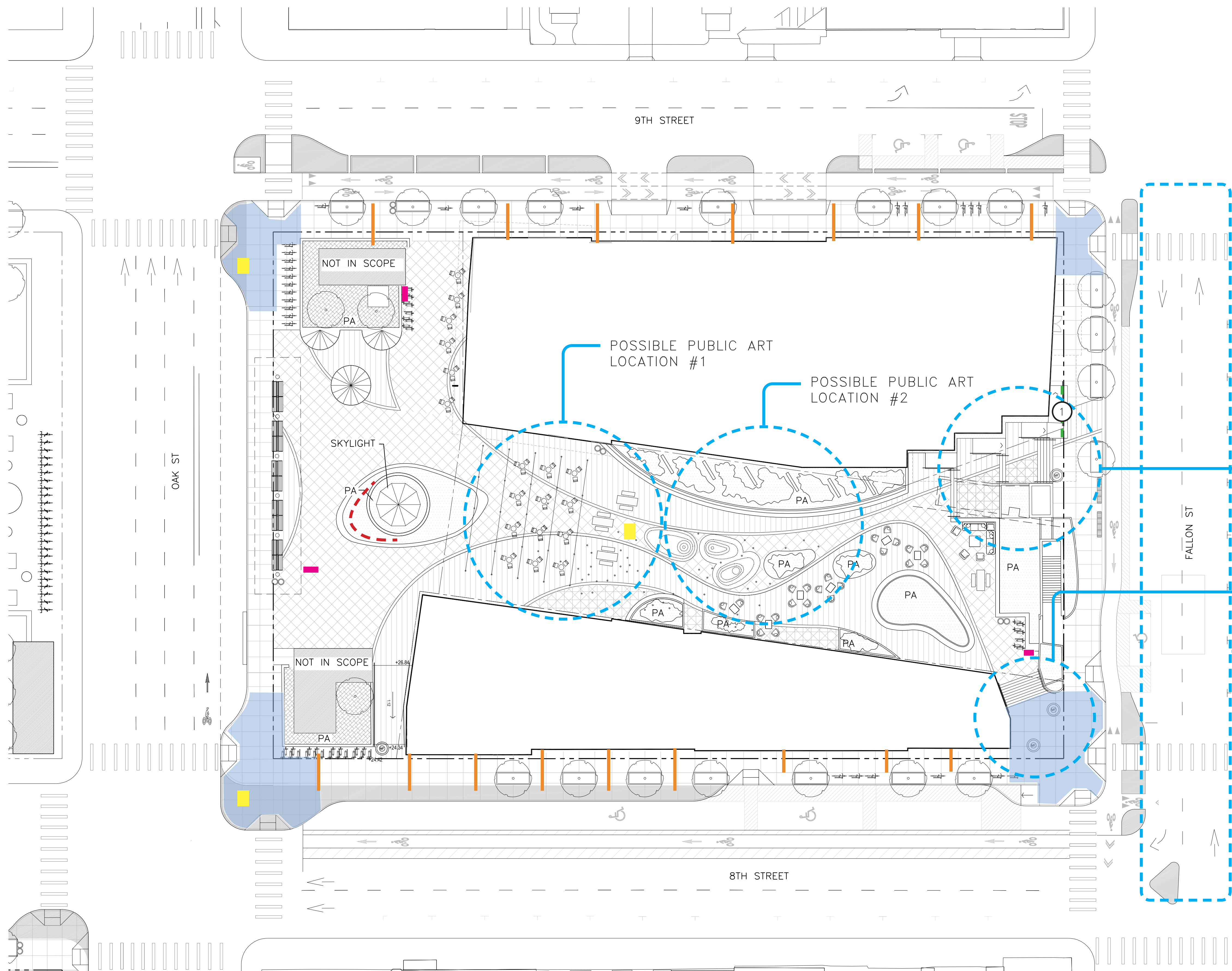
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**GRADING PLAN**

SHEET:  
**L8.0**

PRELIMINARY - Not for Construction -

- NOTES:
1. ALL PLANTING AND IRRIGATION ABOVE TUNNEL WILL BE DESIGNED TO MEET BFS STANDARDS.
  2. WATERPROOFING DESIGN TO BE DETERMINED. FINAL DESIGN WILL COMPLY WITH BFS STANDARDS.
  3. EXISTING DRAINAGE (SEE CIVIL C2.1 AND C2.2) TO BE REPLACED WITH NEW DRAINAGE TRENCH DRAIN SYSTEM.





- LEGEND**
- INFORMATIONAL KIOSK
  - BANNER POLES  
8TH + 9TH STG
  - HISTORICAL SIGNAGE
  - SIDEWALK POEMS
  - MONUMENT SIGN
  - FLUSH MONUMENT SIGN
  - POSSIBLE PUBLIC ART LOCATIONS



PUBLIC ART LOCATION OPTION #4  
DESCRIPTION: STREET GRAPHIC PAINT



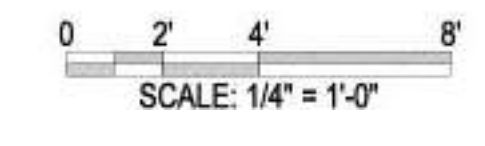
PUBLIC ART LOCATION OPTION #2  
DESCRIPTION: INTERACTIVE PLAY ELEMENT



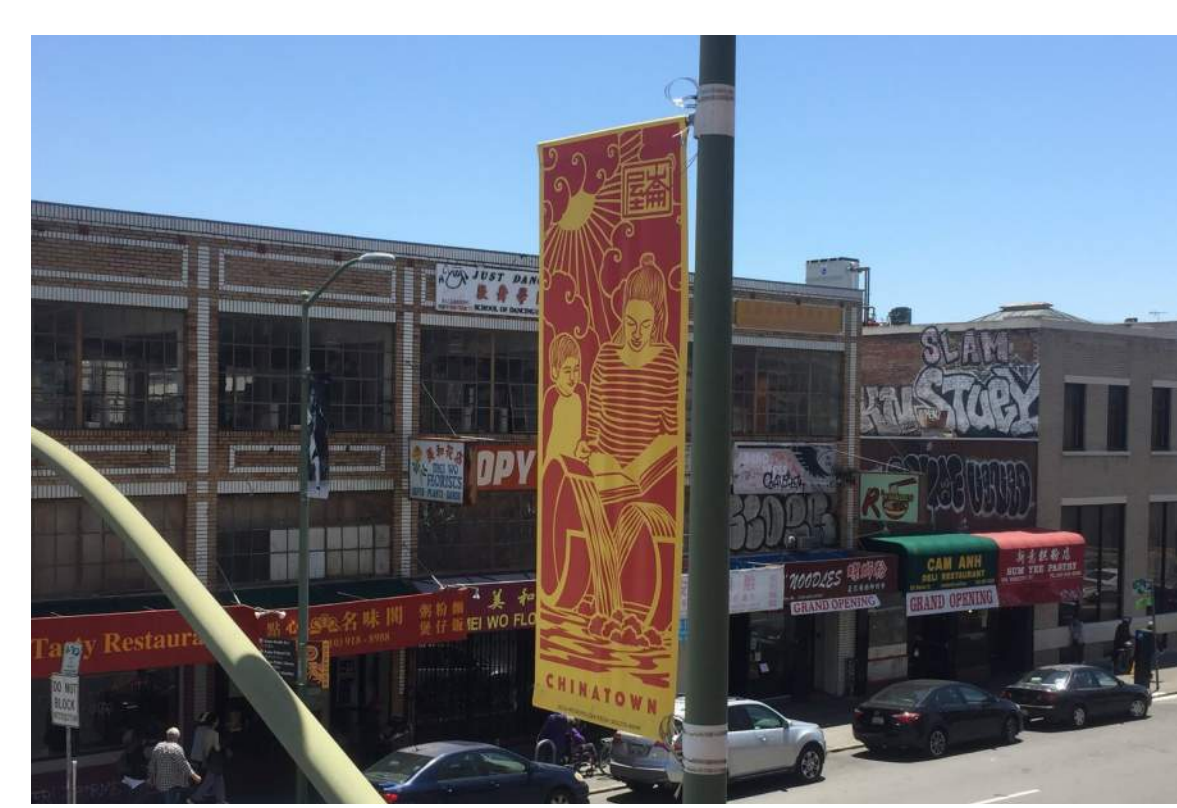
PUBLIC ART LOCATION OPTION #1  
DESCRIPTION: LANTERNS



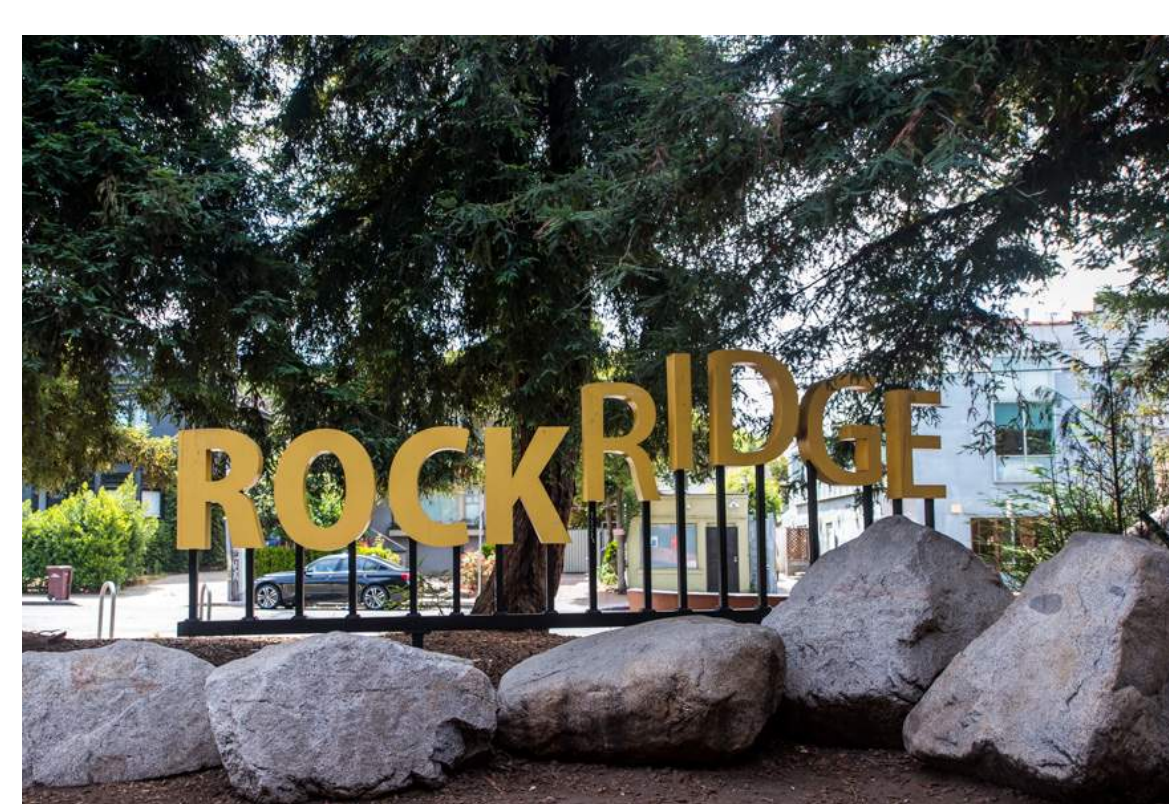
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FLUSH MONUMENT SIGN



- NOTE:**
1. BUS STOP, STREET SIGNS, AND TRANSIT RELATED SIGNS NOT INCLUDED IN THIS DIAGRAM
  2. SIGNAGE WILL COPMLY WITH B.F.S. STANDARDS



BANNER POLES



MONUMENT SIGN AT ROCKRIDGE BART



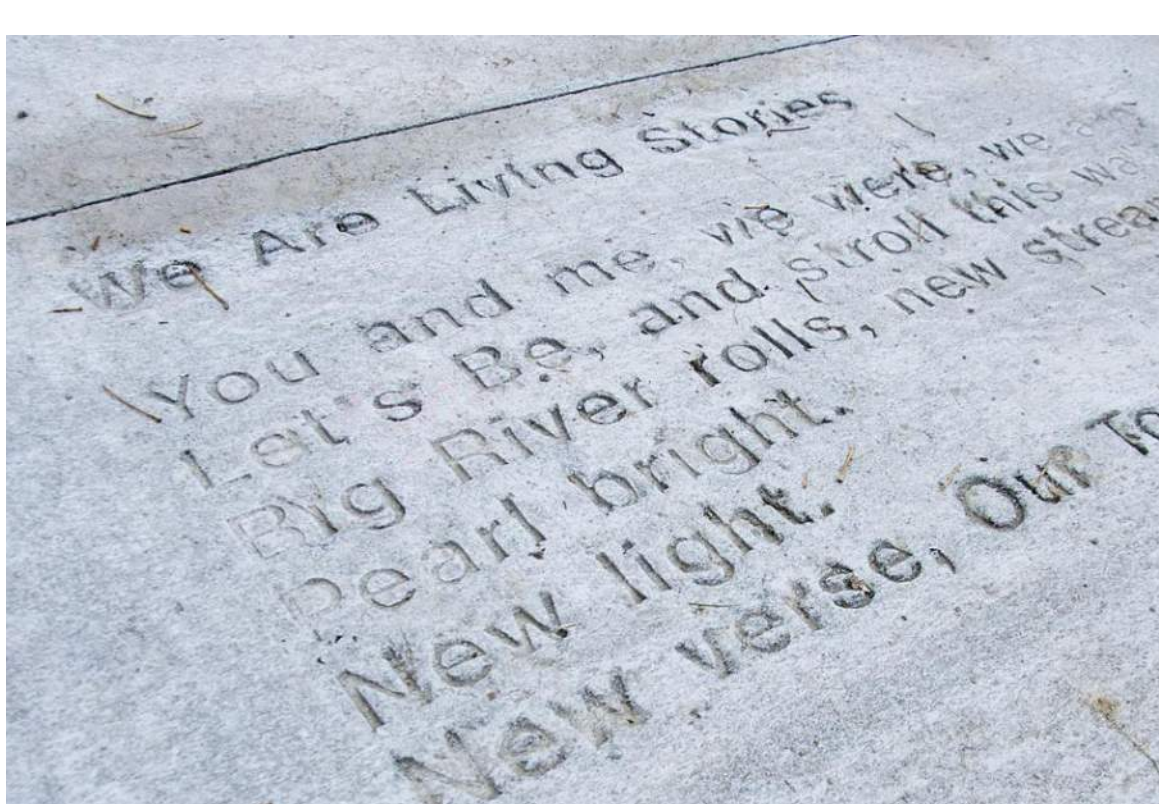
FLUSH MONUMENT SIGN



HISTORICAL SIGNAGE



INFOMATIONAL KIOSK



SIDEWALK POEMS



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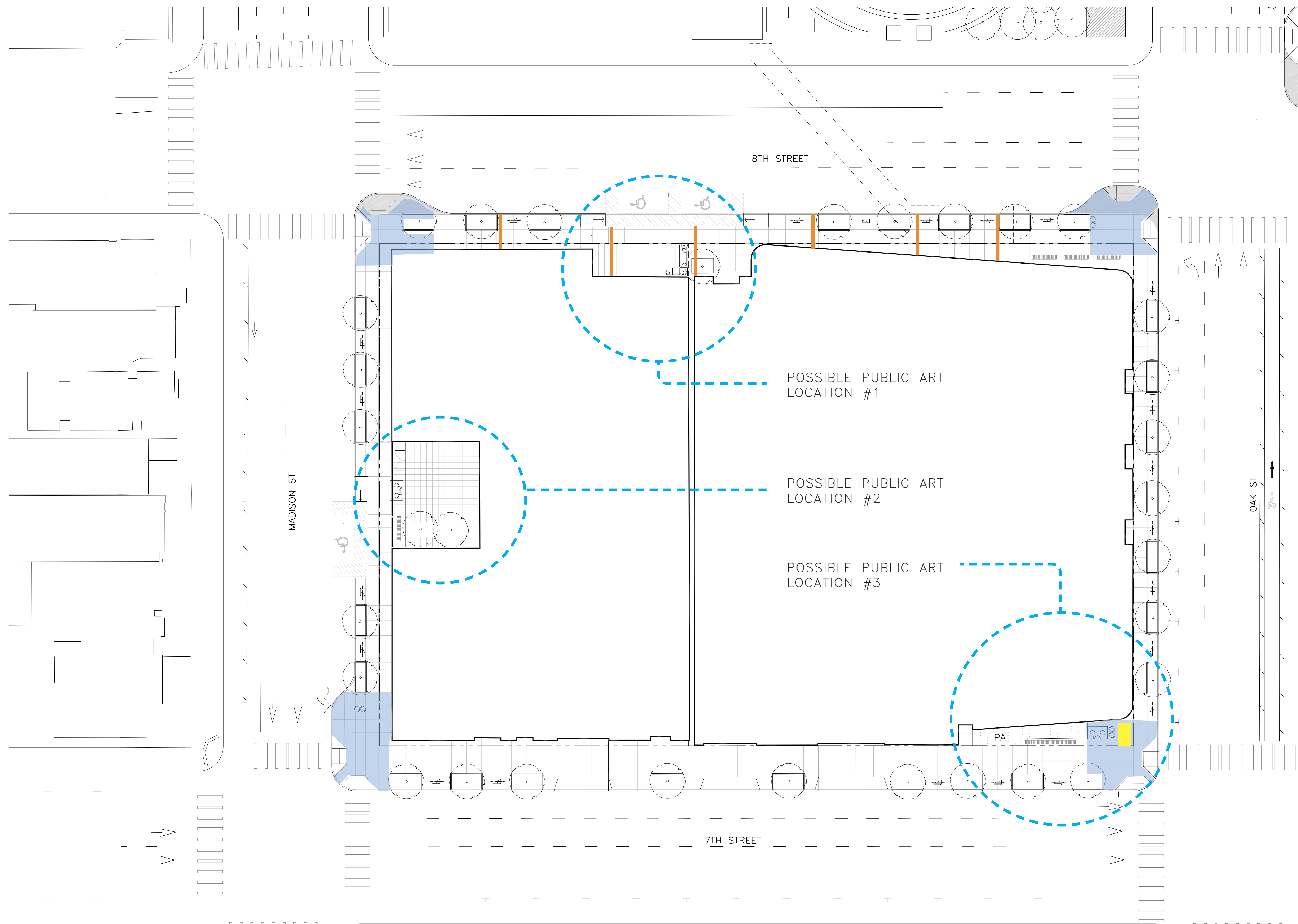
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BLOCK:	1
TITLE:	<b>SIGN PLAN</b>

SHEET:  
**L9.1**



LEGEND

- BANNER POLES  
8TH + 9TH STG
- HISTORICAL SIGNAGE
- SIDEWALK POEMS
- POSSIBLE PUBLIC ART  
LOCATIONS

NOTE:

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SIGN PLAN

SHEET:

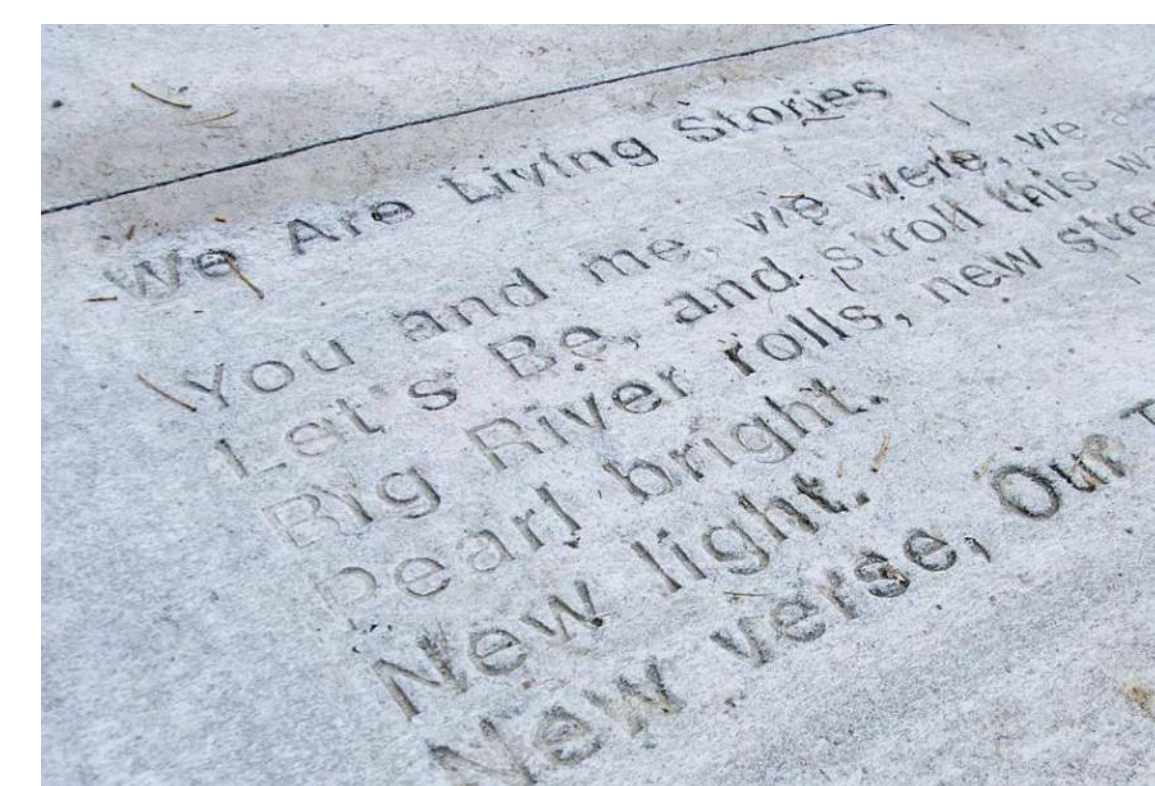
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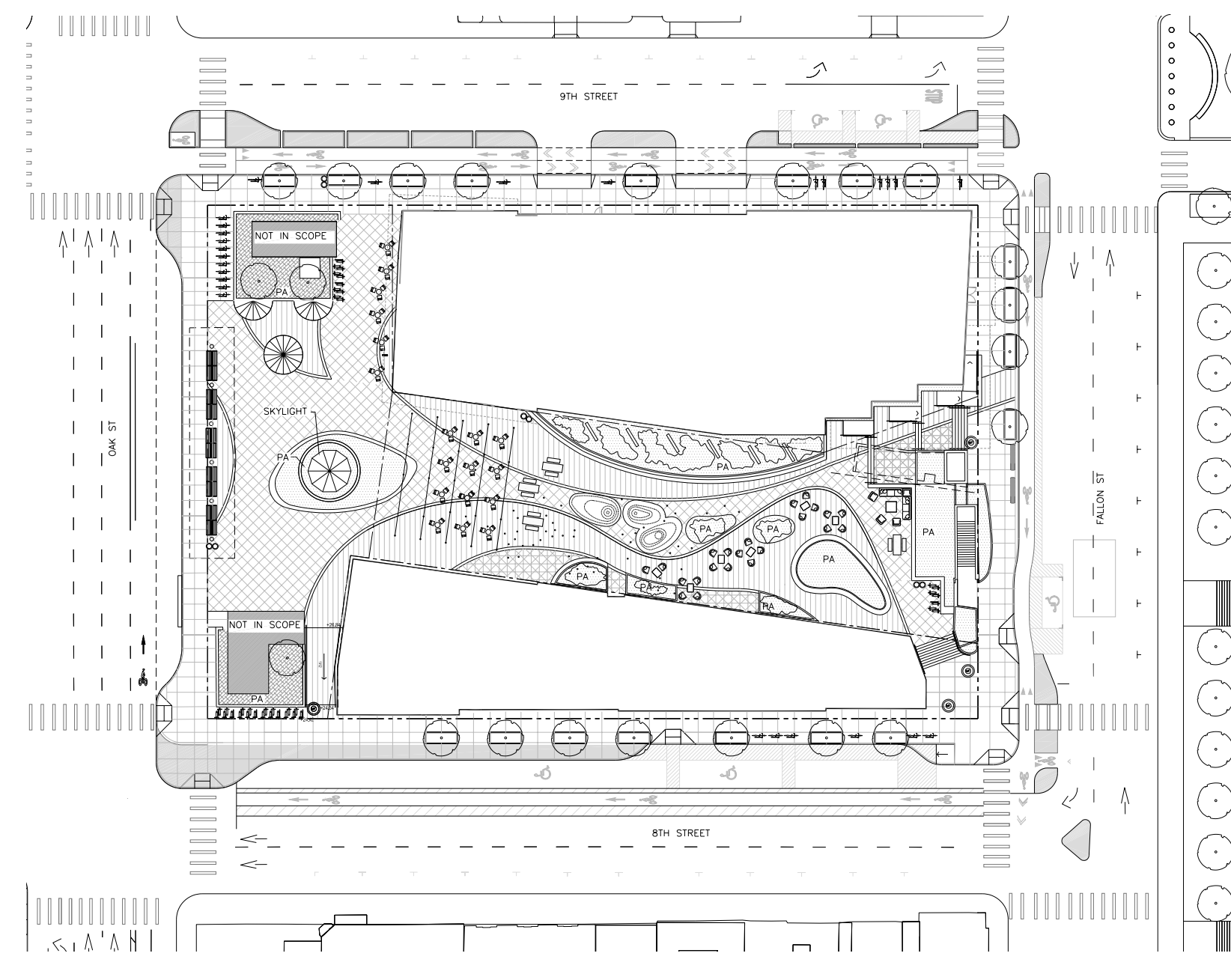
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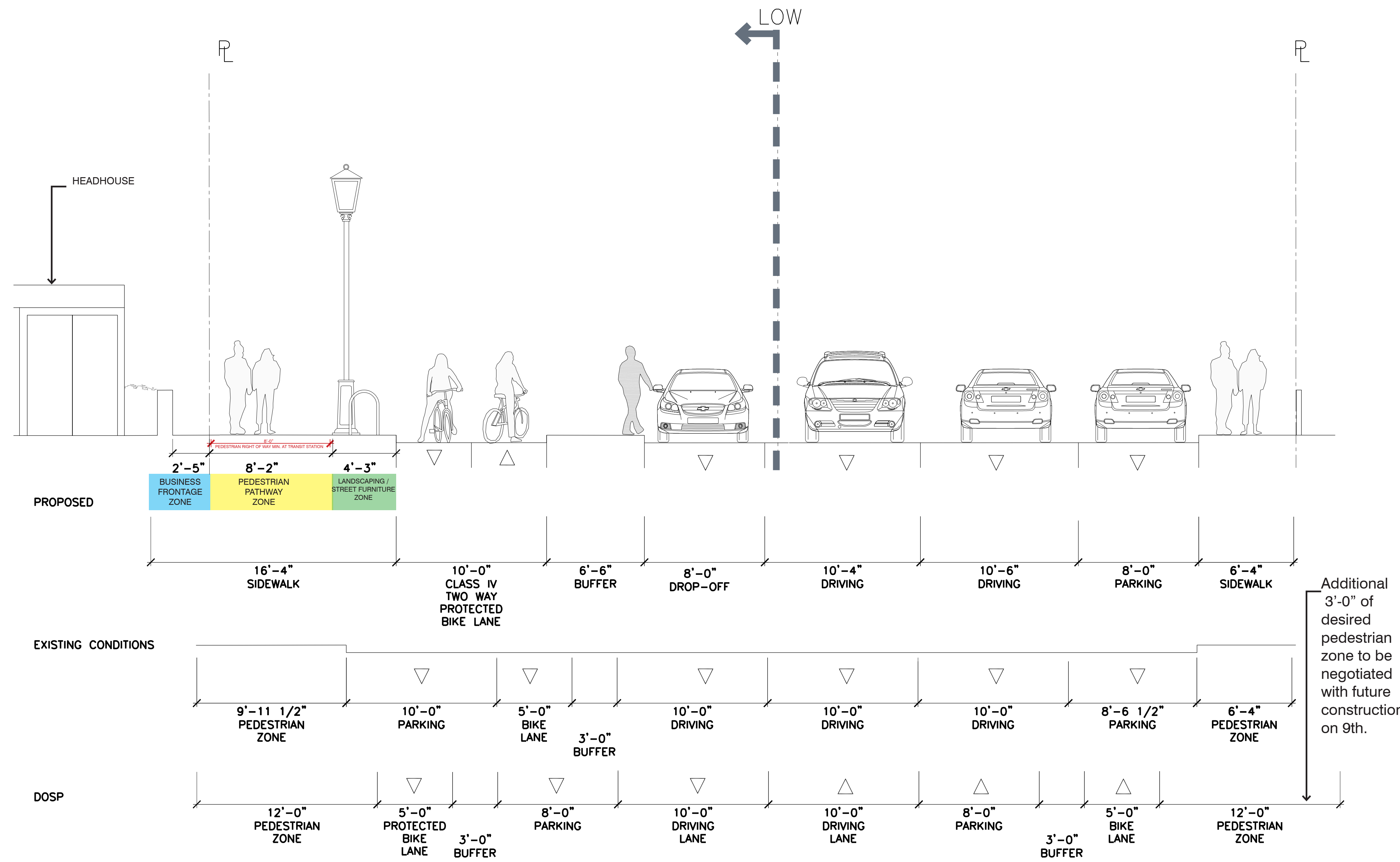
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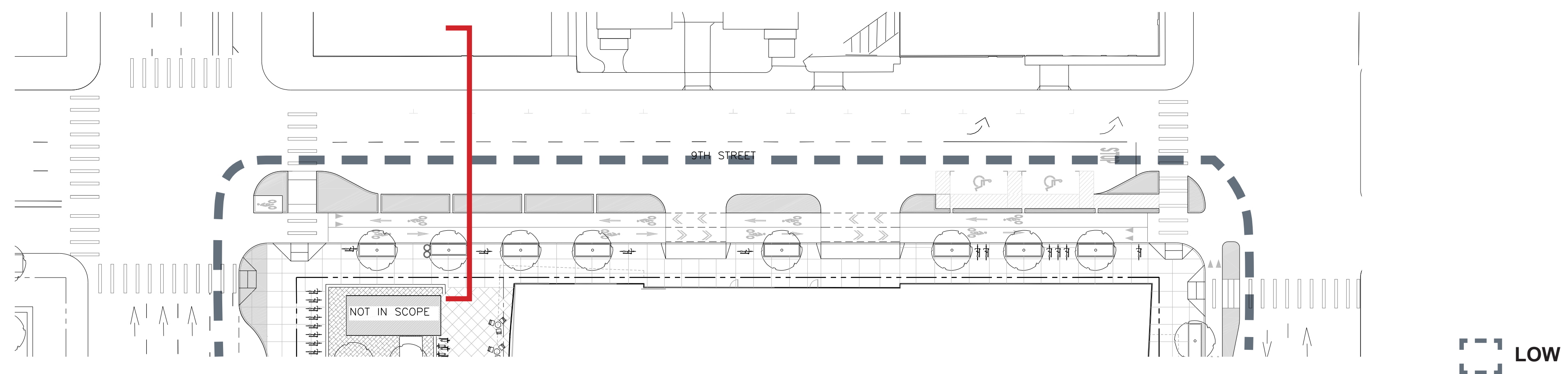
SIDEWALK POEMS



Key Plan



Typical Cross Section



Plan

Note:  
This Street Design Framework was developed to allow the project to progress with building locations and program functions that connect the on site improvements to offsite improvements. Current planning documents are not consistent in their recommendations, so the strategy of the framework was to insure flexibility. The proposed option 1 is a potential working solution, but the final street design and dimensions will be the result of a process involving multiple stakeholders and transportation experts. The design team reserves the right to modify and refine these sections in light of new information and the evolving BART Transit Operations Facility (TOF) in the block bordered by 8th St., Madison St., 9th St., and Oak Street. These plans represent a best effort at capturing all information available today in a physical design solution which meets the maximum number of stated criteria for this important public realm. The final design will be ADA compliant.



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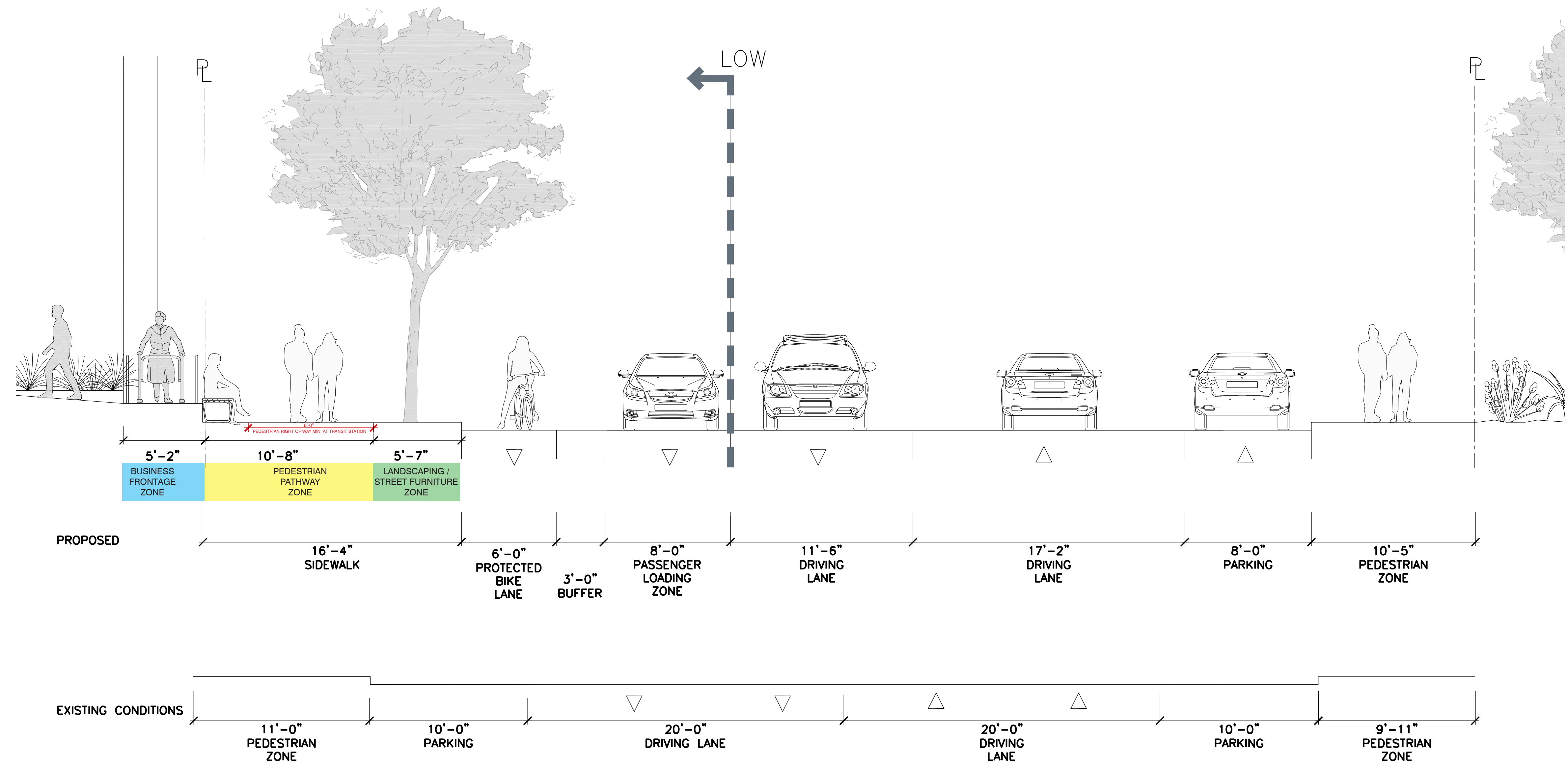
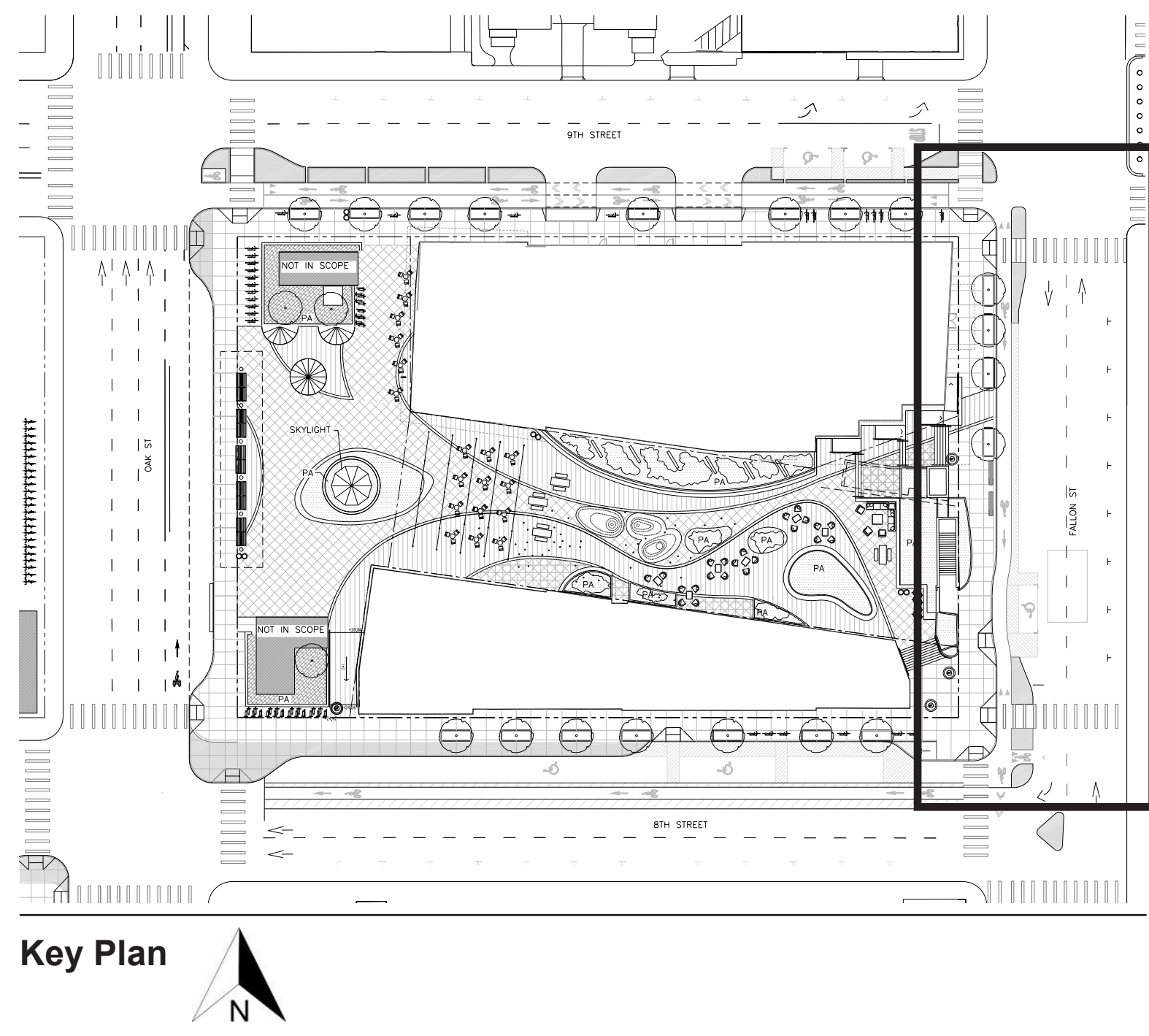
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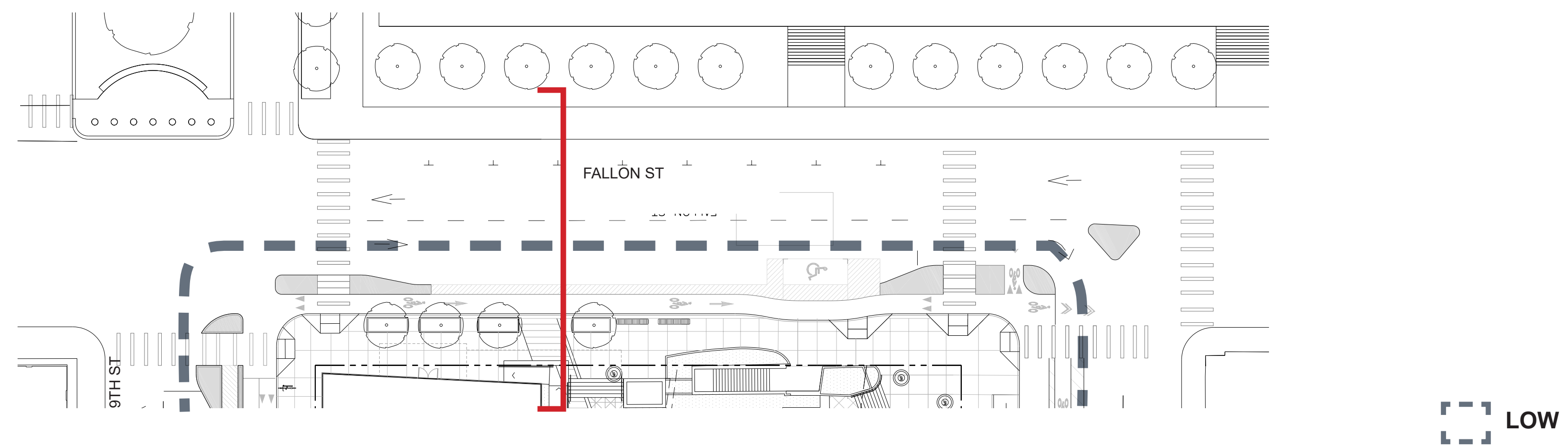
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TITLE:  
**9TH ST SECTION**

SHEET:  
**L10.1**



DOSP N/A  
Typical Cross Section



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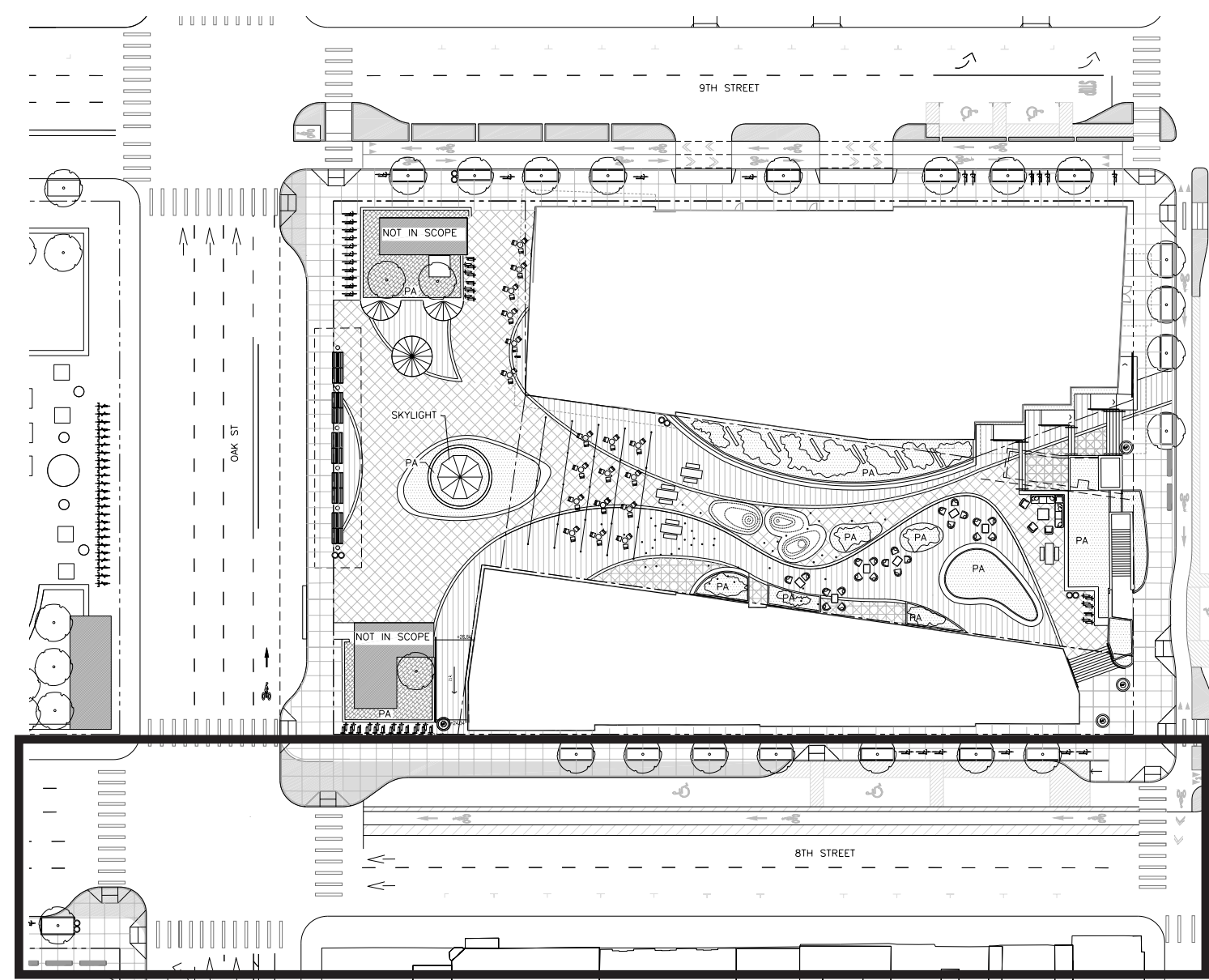
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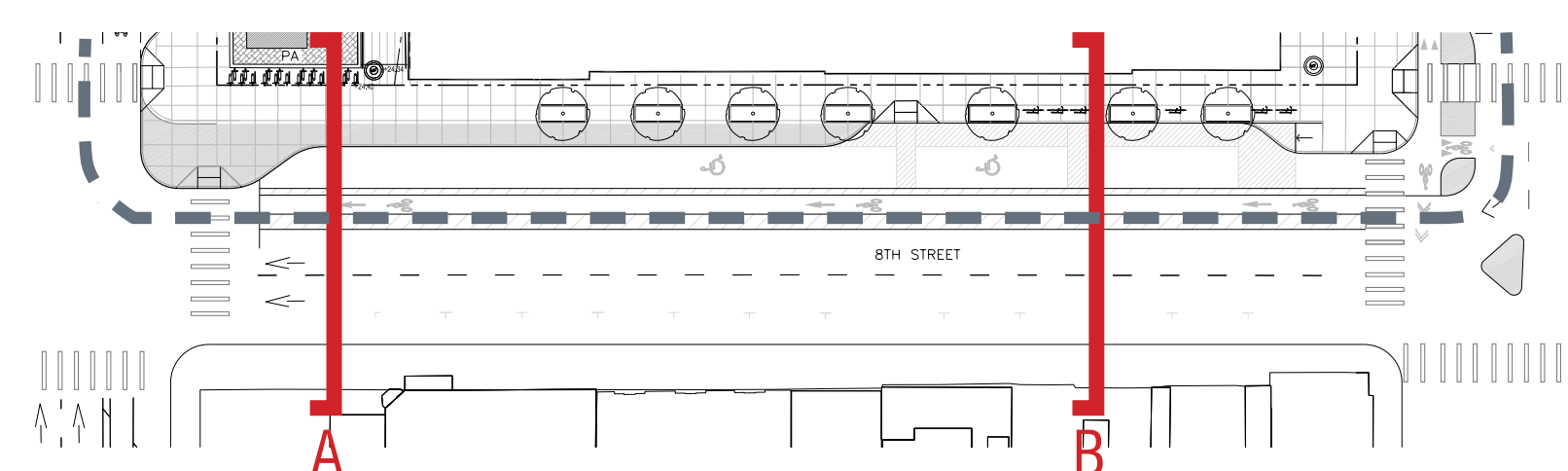
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**L10.2**

PRELIMINARY - Not for Construction -



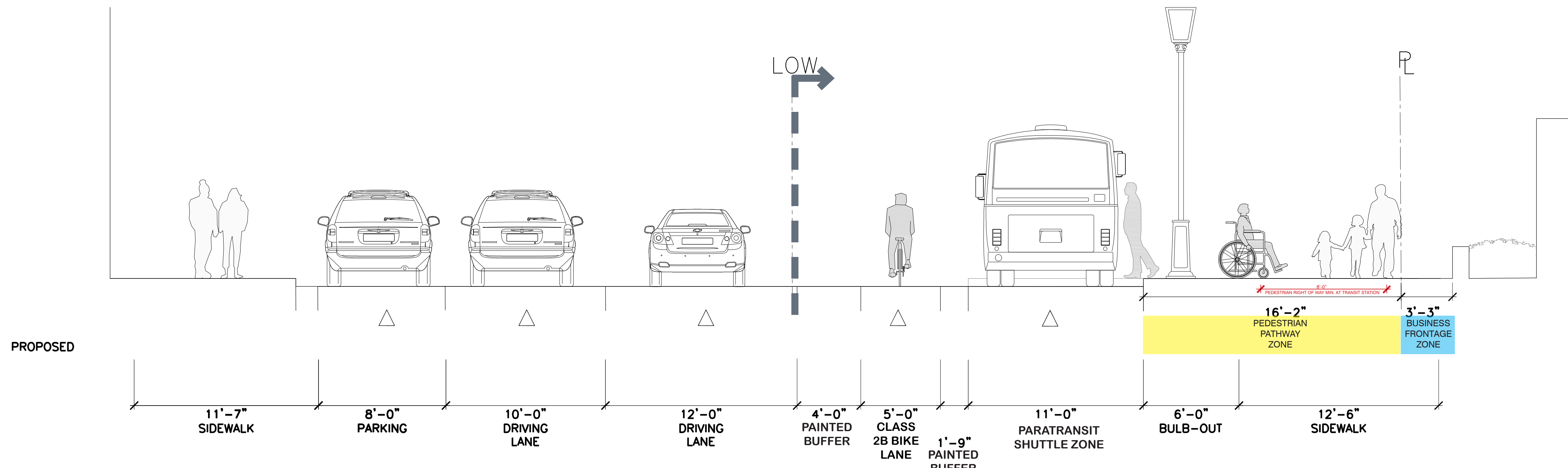
Key Plan



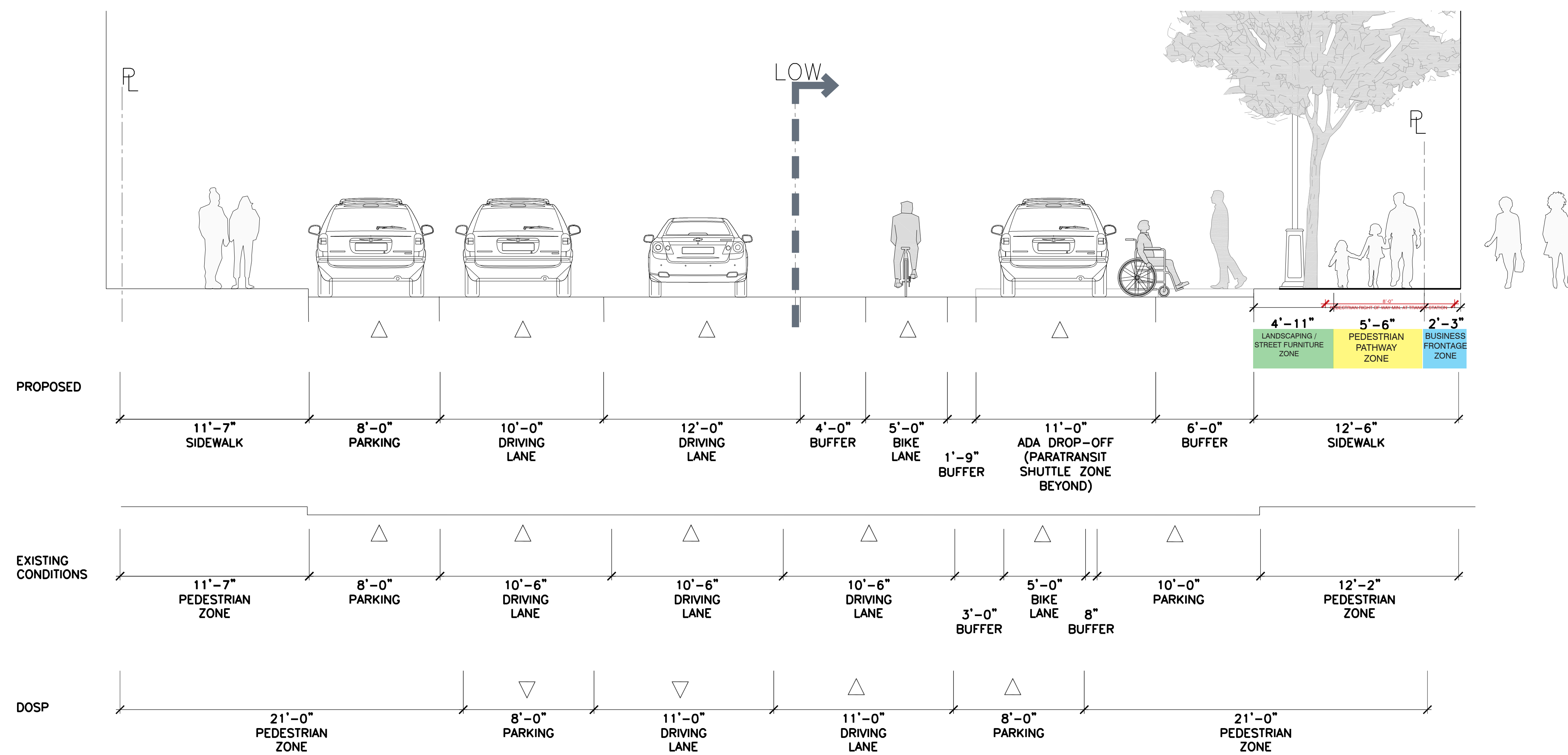
Plan



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Typical Cross Section: A Shuttle Zone



Typical Cross Section: B Not official BART Loading



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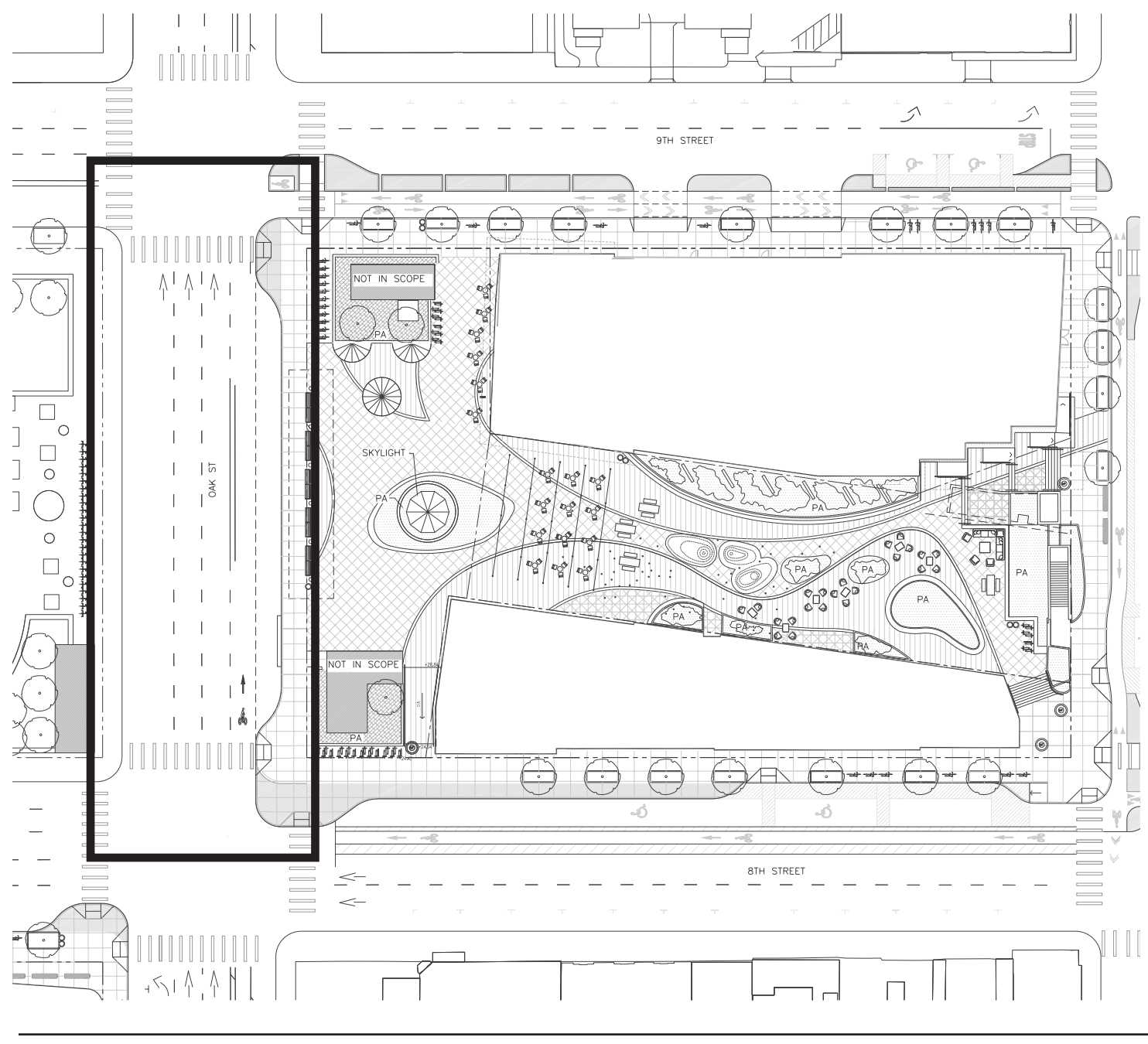
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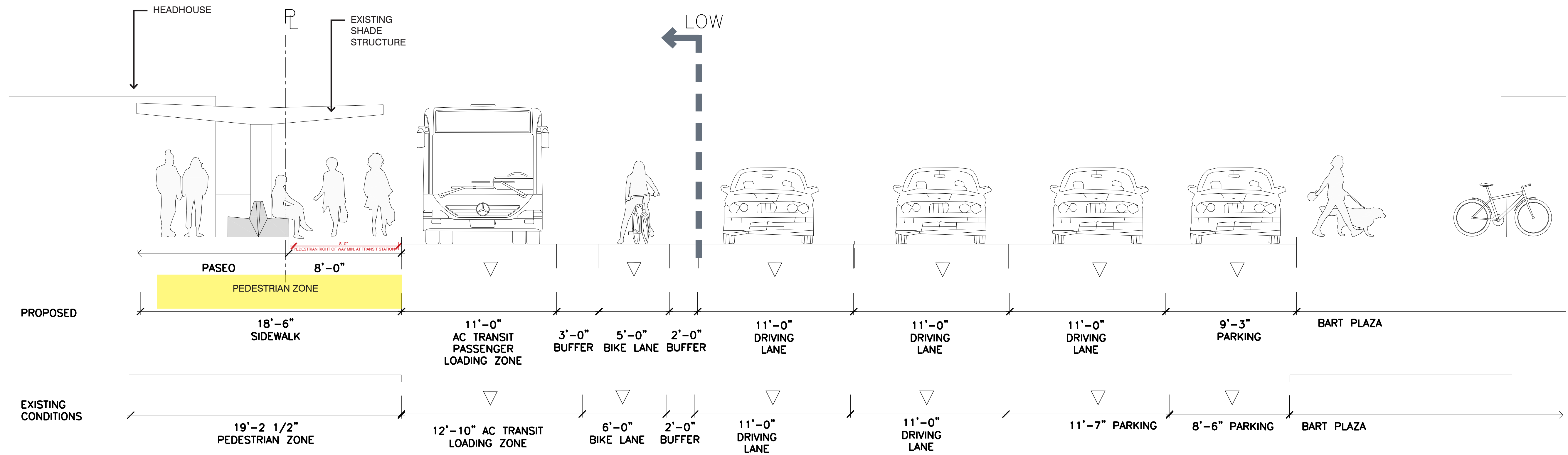
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SHEET: **L10.3**

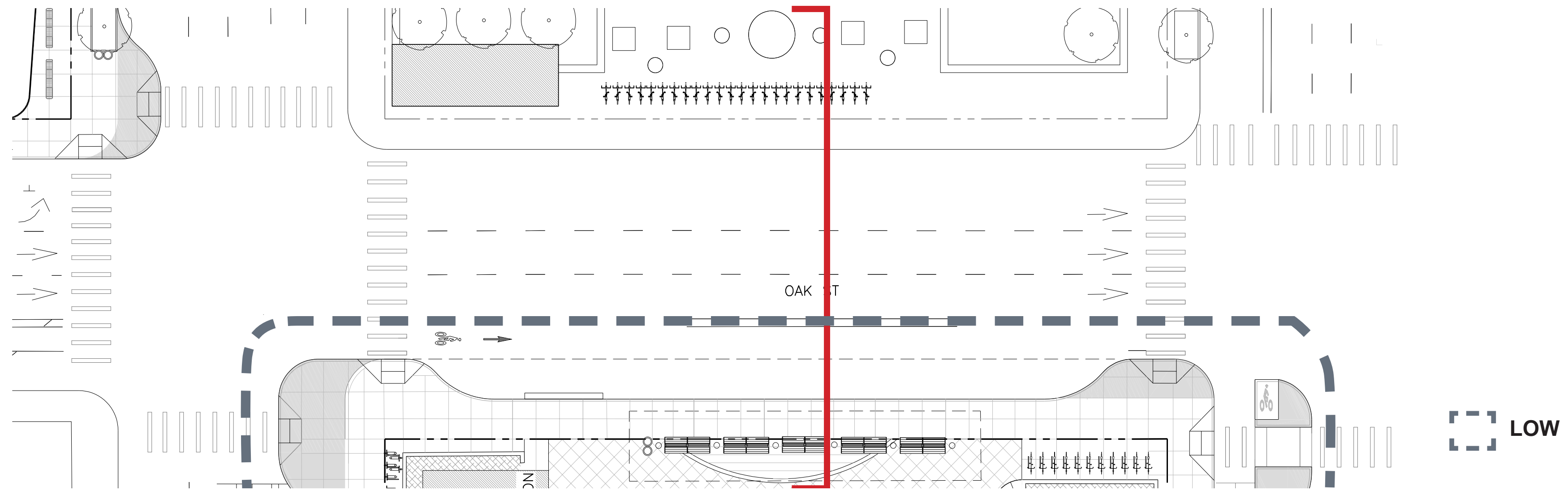




Key Plan



Typical Cross Section



Plan

Note:  
Alameda CTC is proposing to construct a two-way bike lane on the west side of Oak, between 3rd and 9th Streets as part of the Oakland-Alameda Access Project. This facility has been planned in conjunction with City of Oakland Planning and OakDot.



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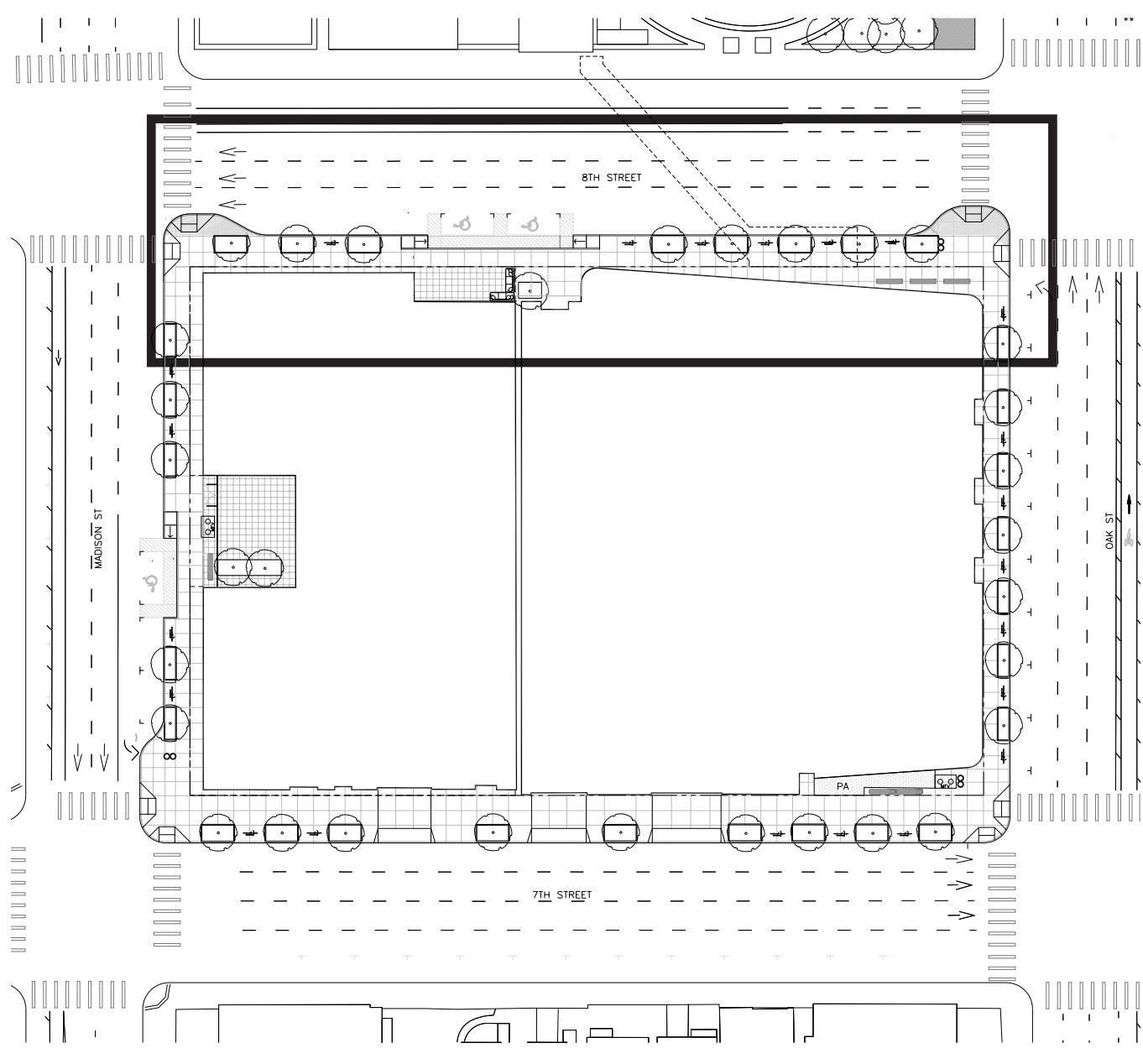
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OAK ST SECTION

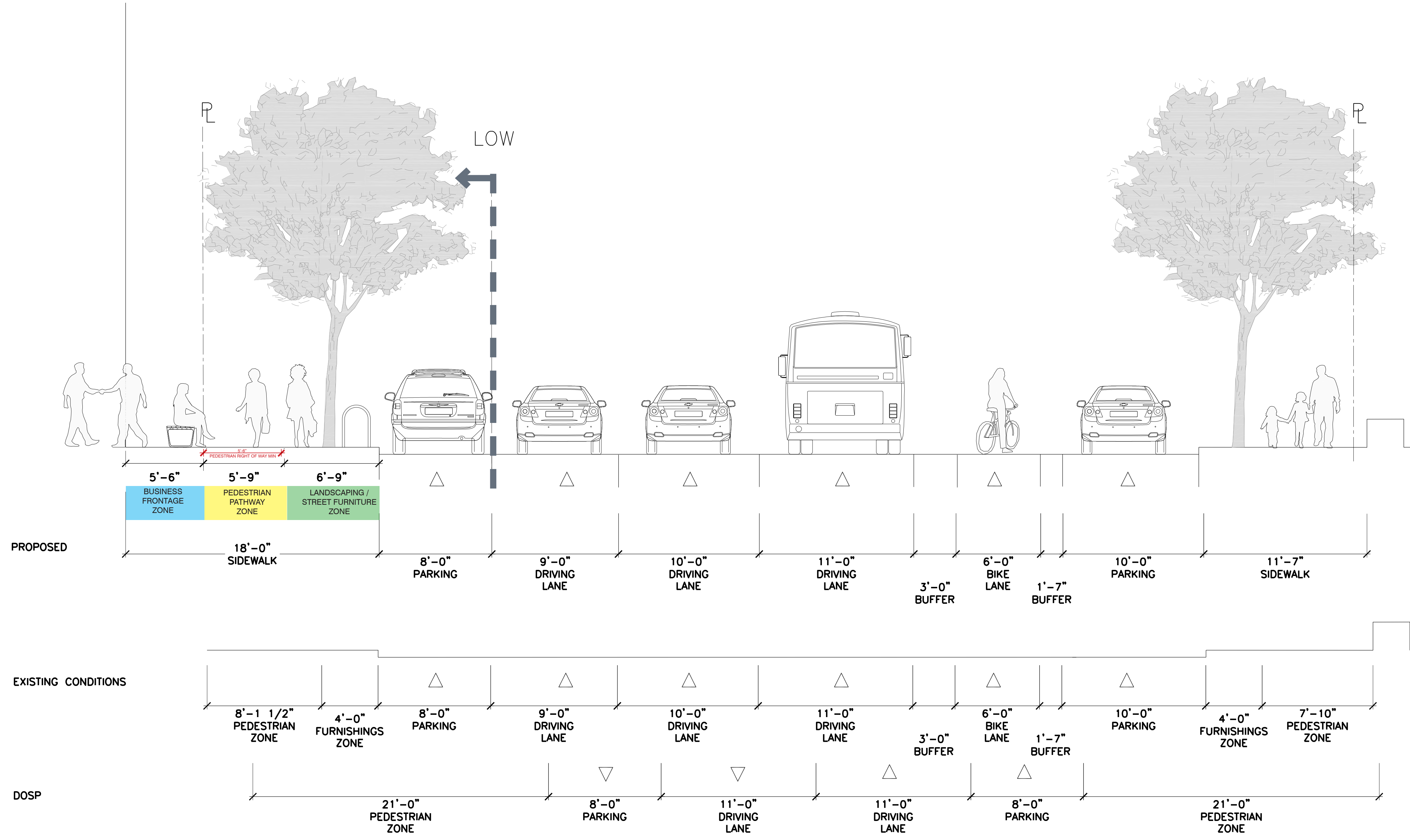
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L10.4

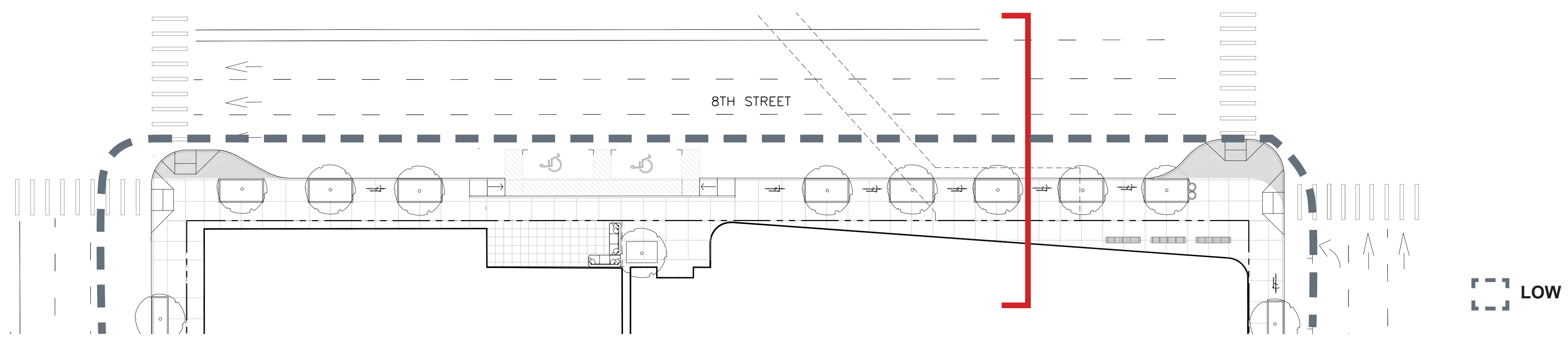
PRELIMINARY - Not for Construction -



Key Plan



Typical Cross Section



Plan



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

NO.	DESCRIPTION	DATE
50	SD - HORIZONTAL FDP	10/29/2021
100	SD - HORIZONTAL FDP	12/17/2021
100	SD - HORIZONTAL FDP	05/02/2022
100	SD - HORIZONTAL FDP#2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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JOB NUMBER: 1808  
DRAWN BY: Author  
CHECKED BY: Checker  
ISSUE DATE: xx/xx/xxxx

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PHASE: 1

BLOCK: 2

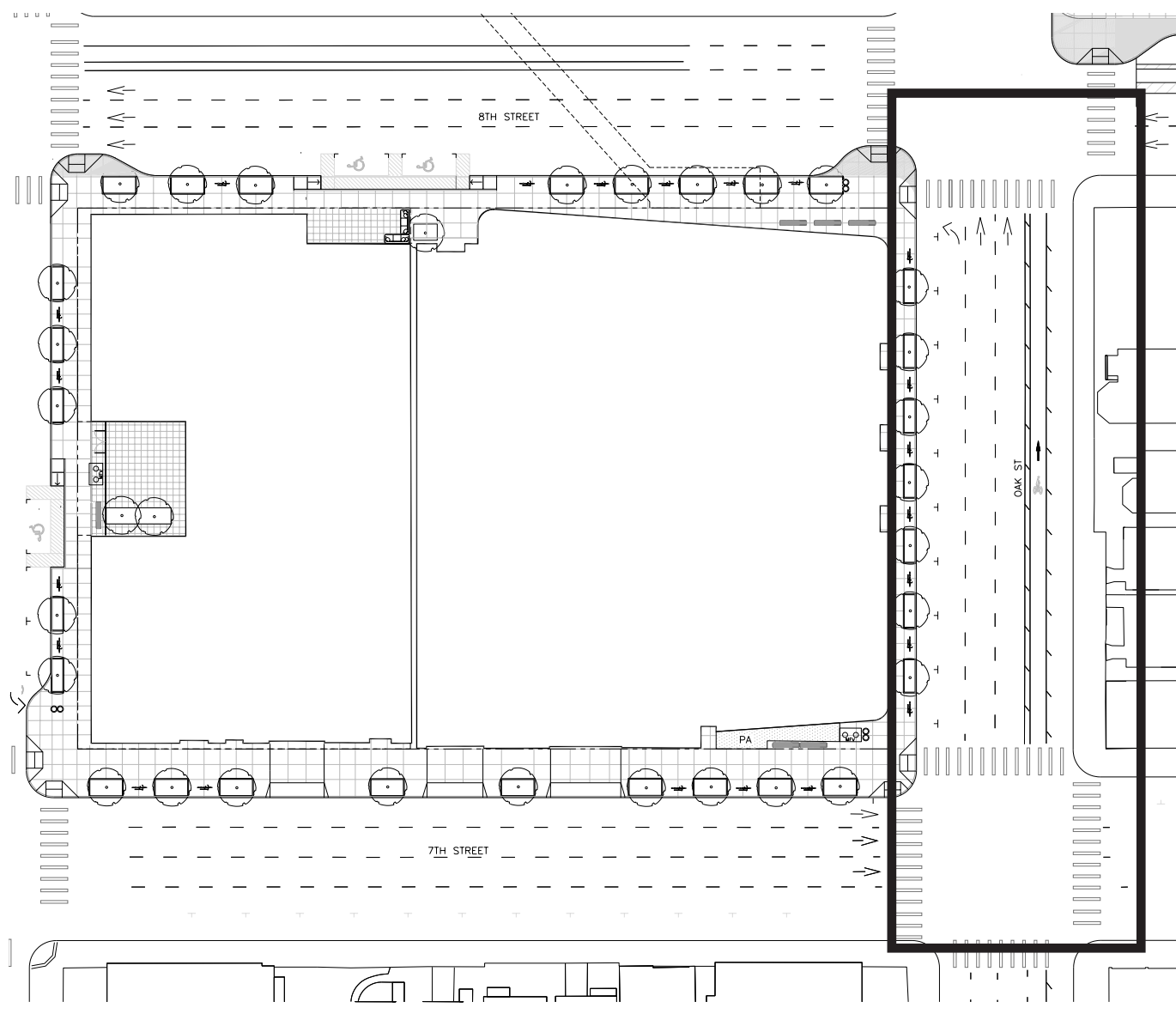
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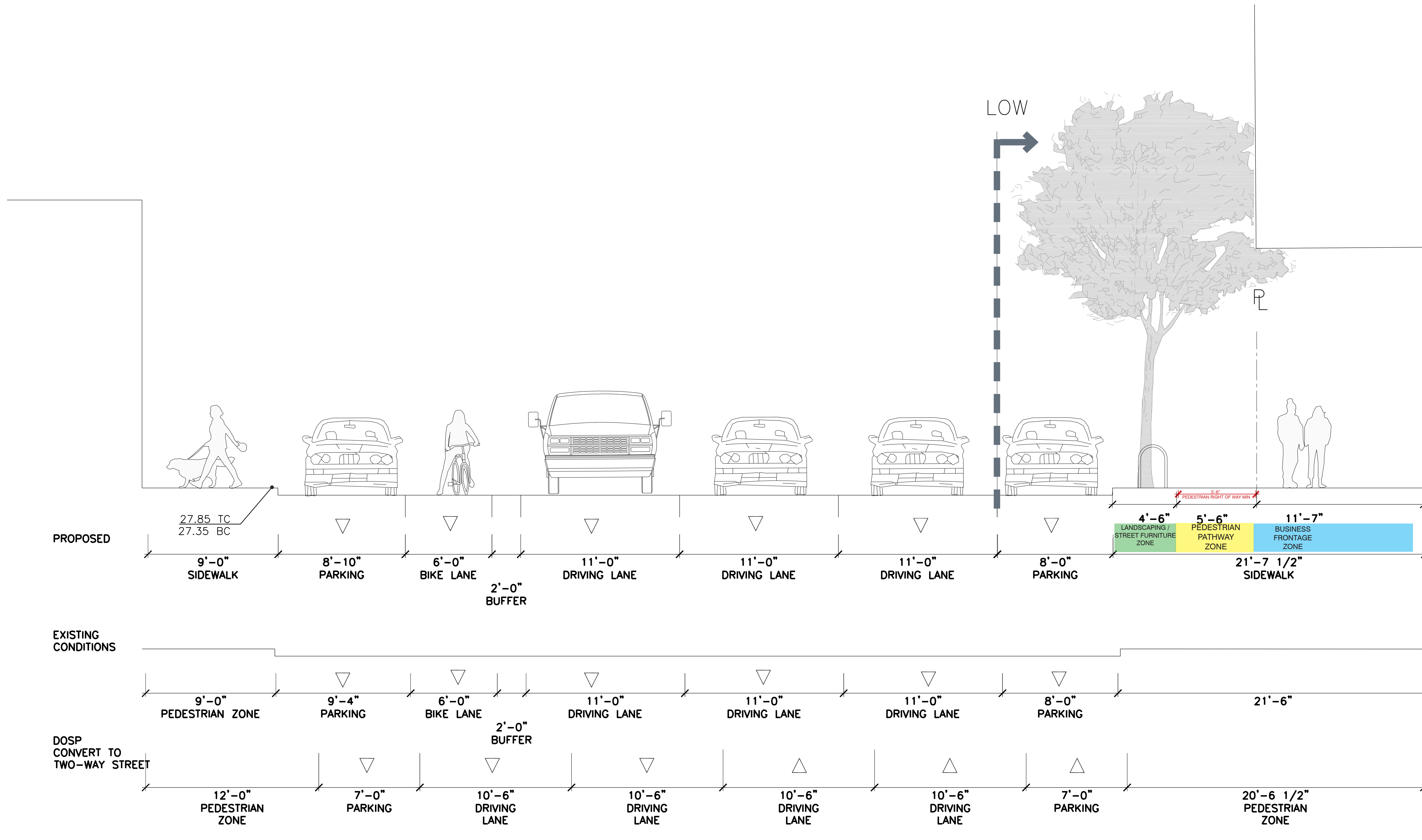
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**L10.5**

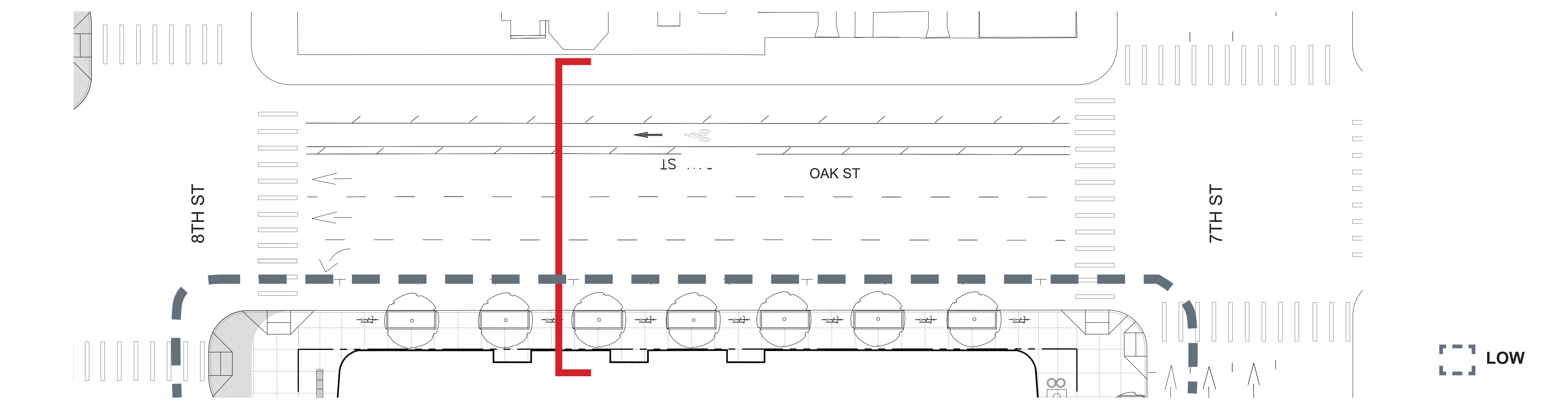
PRELIMINARY - Not for Construction -



Key Plan



Typical Cross Section



Plan



STAMP:

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50 SD - HORIZONTAL FDP	10/29/2021
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REVISION SCHEDULE

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JOB NUMBER:	1808
DRAWN BY:	Author
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ISSUE DATE:	xx/xx/xxxx

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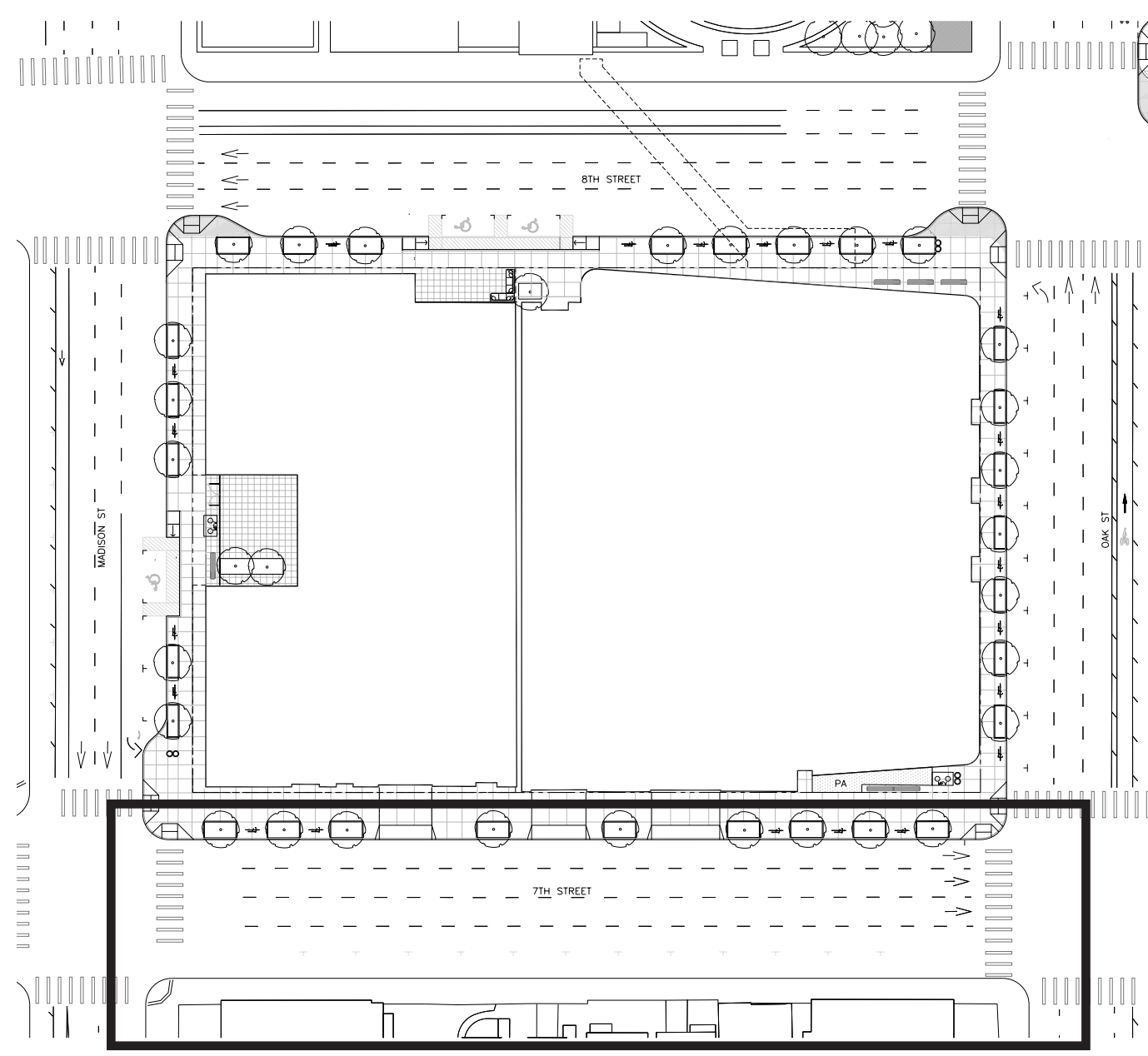
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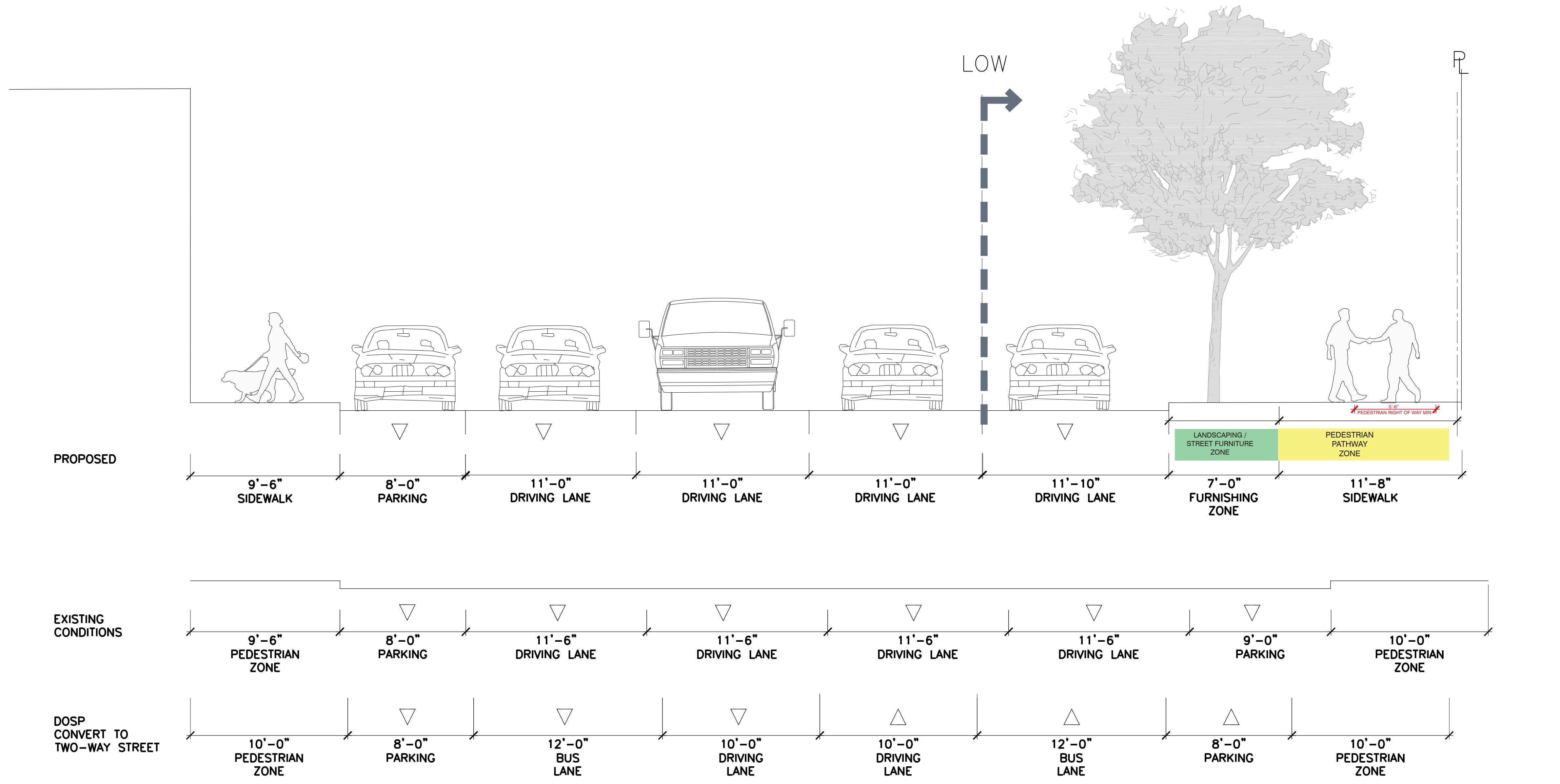
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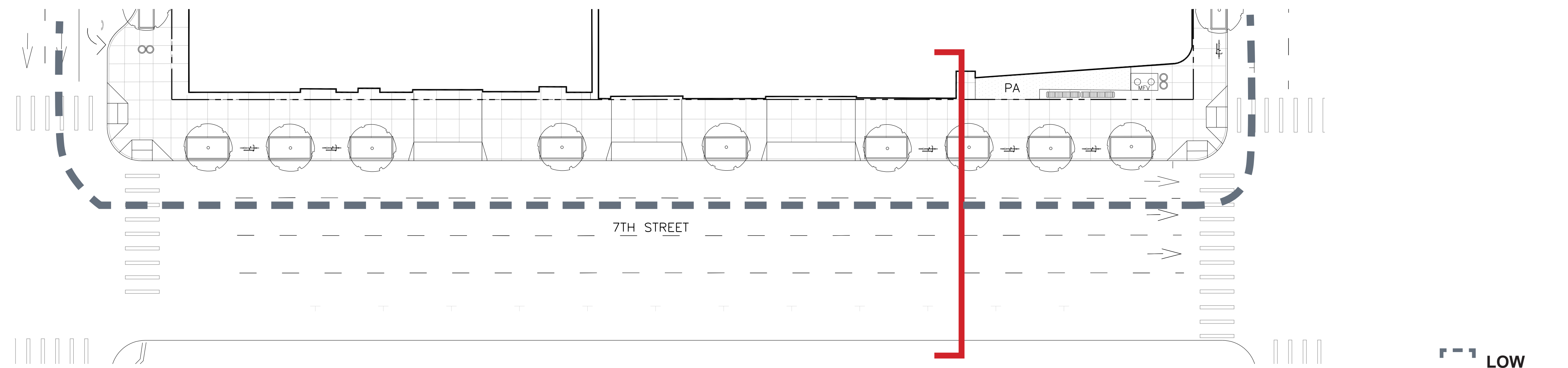
PRELIMINARY - Not for Construction -



Key Plan



Typical Cross Section



Plan



STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
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JOB NUMBER: 1808  
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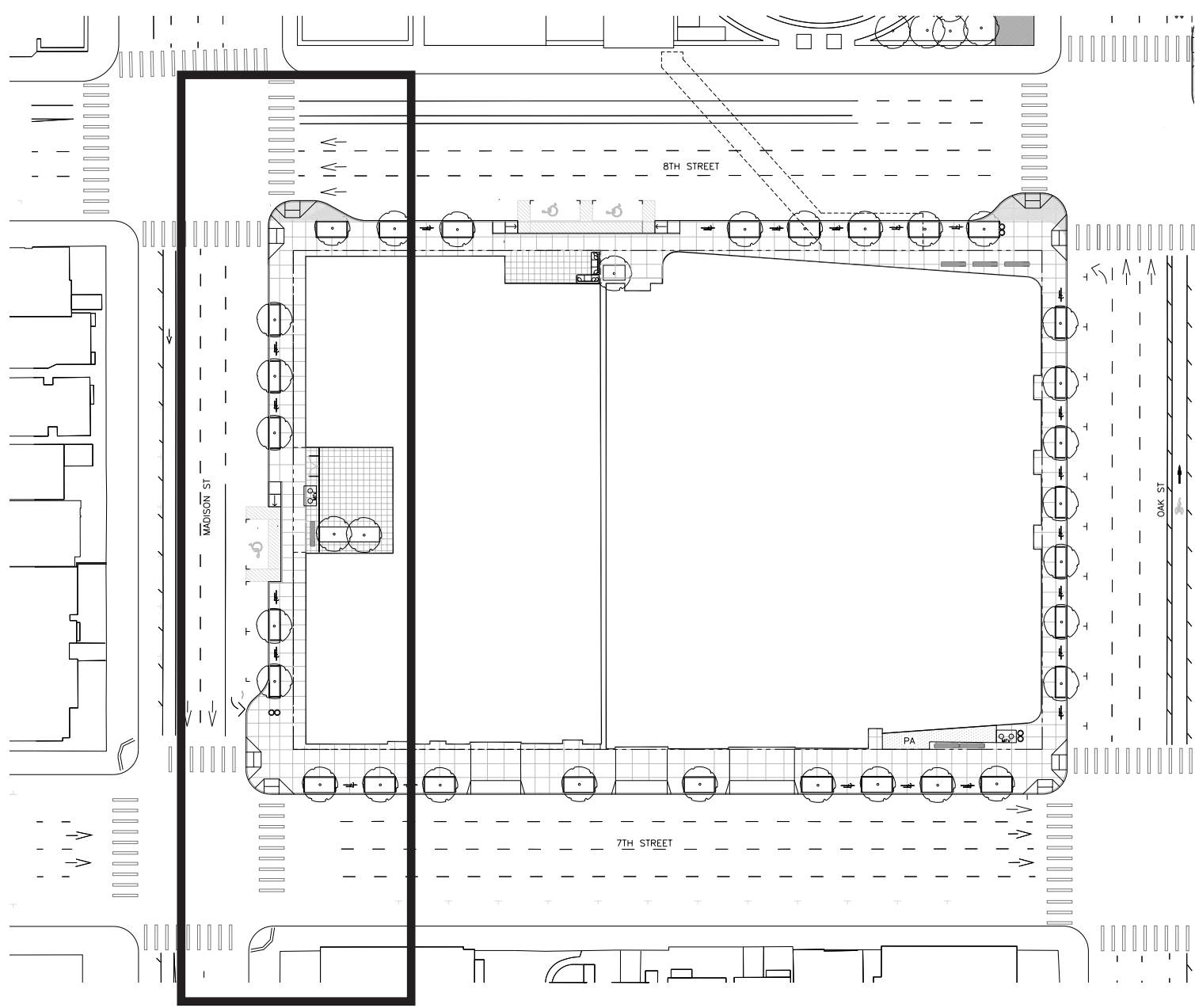
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SHEET:

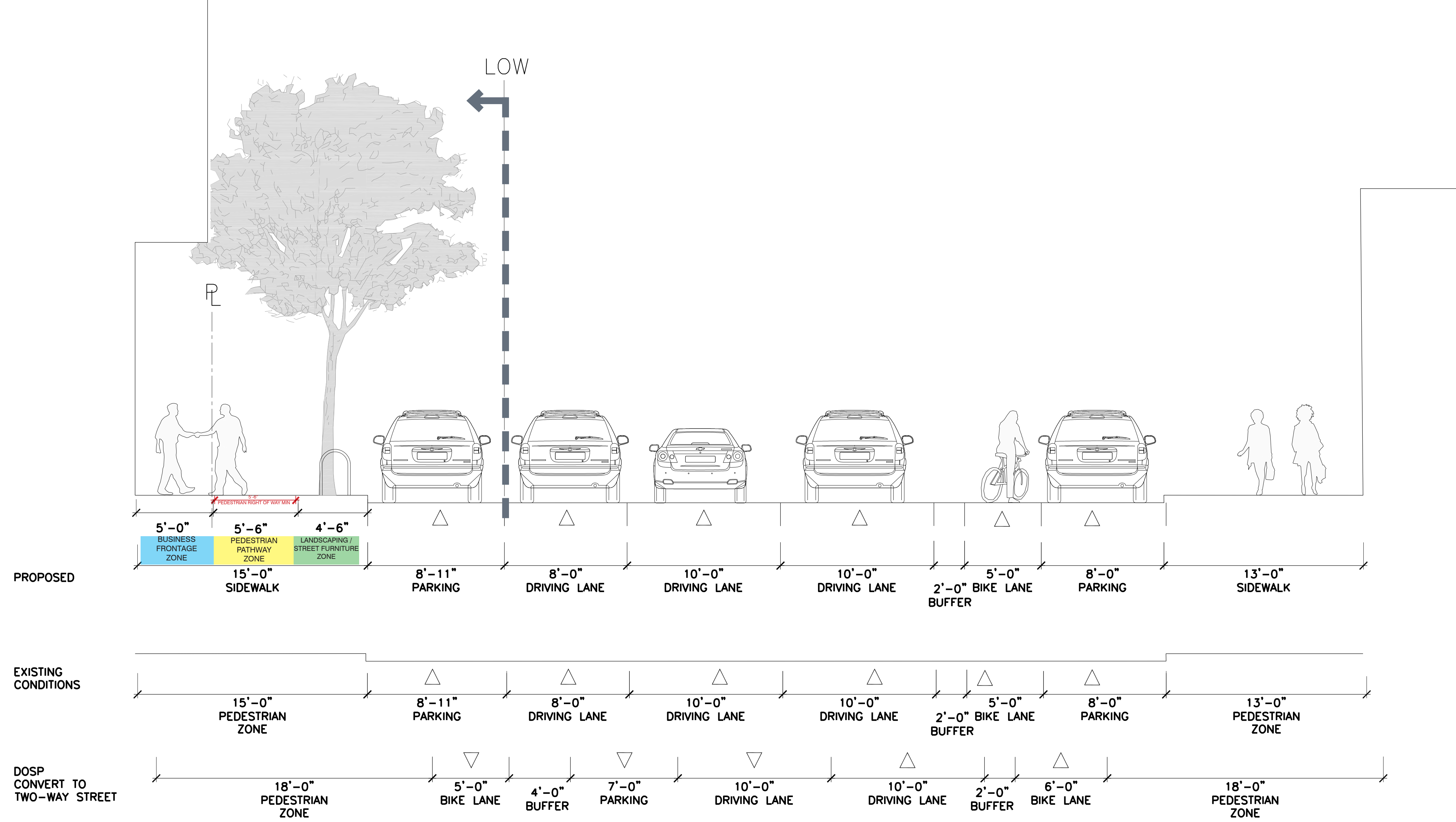
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PRELIMINARY - Not for Construction -

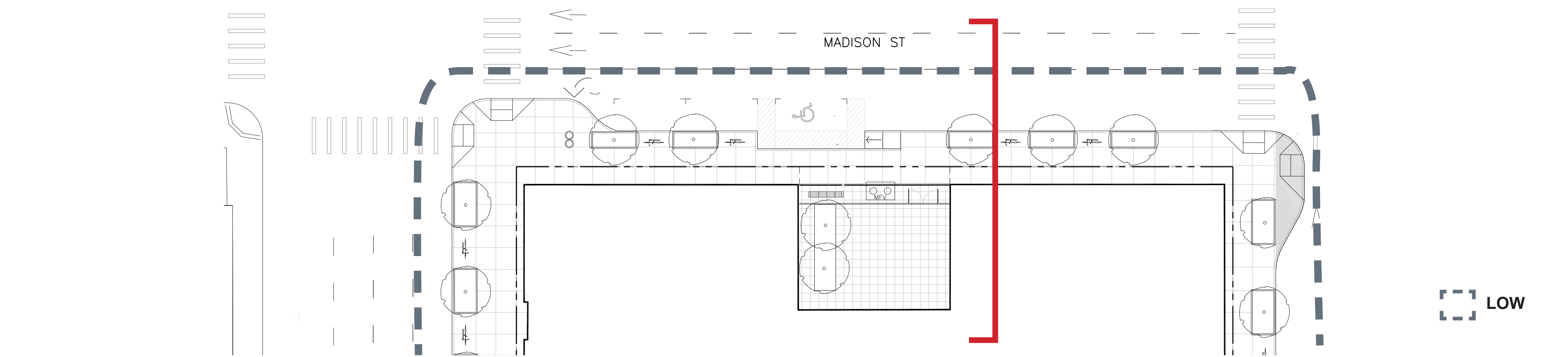
© 2021 EINWILLERKUEHL LANDSCAPE ARCHITECTURE & URBAN DESIGN



Key Plan



Typical Cross Section



Plan



LAKE MERRITT BART  
HORIZONTAL FDP  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

50 SD - HORIZONTAL FDP	10/29/2021
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100 SD - HORIZONTAL FDP#2	06/16/2022

REVISION SCHEDULE

NO.	ISSUE	DATE
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ISSUE DATE:	xx/xx/xxxx

SCALE:	As indicated
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TITLE:  
**MADISON ST SECTION**

SHEET:

**L10.8**

PRELIMINARY - Not for Construction -



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

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100 SD - HORIZONTAL FDP2	06/16/2022

REVISION SCHEDULE		
NO.	ISSUE	DATE

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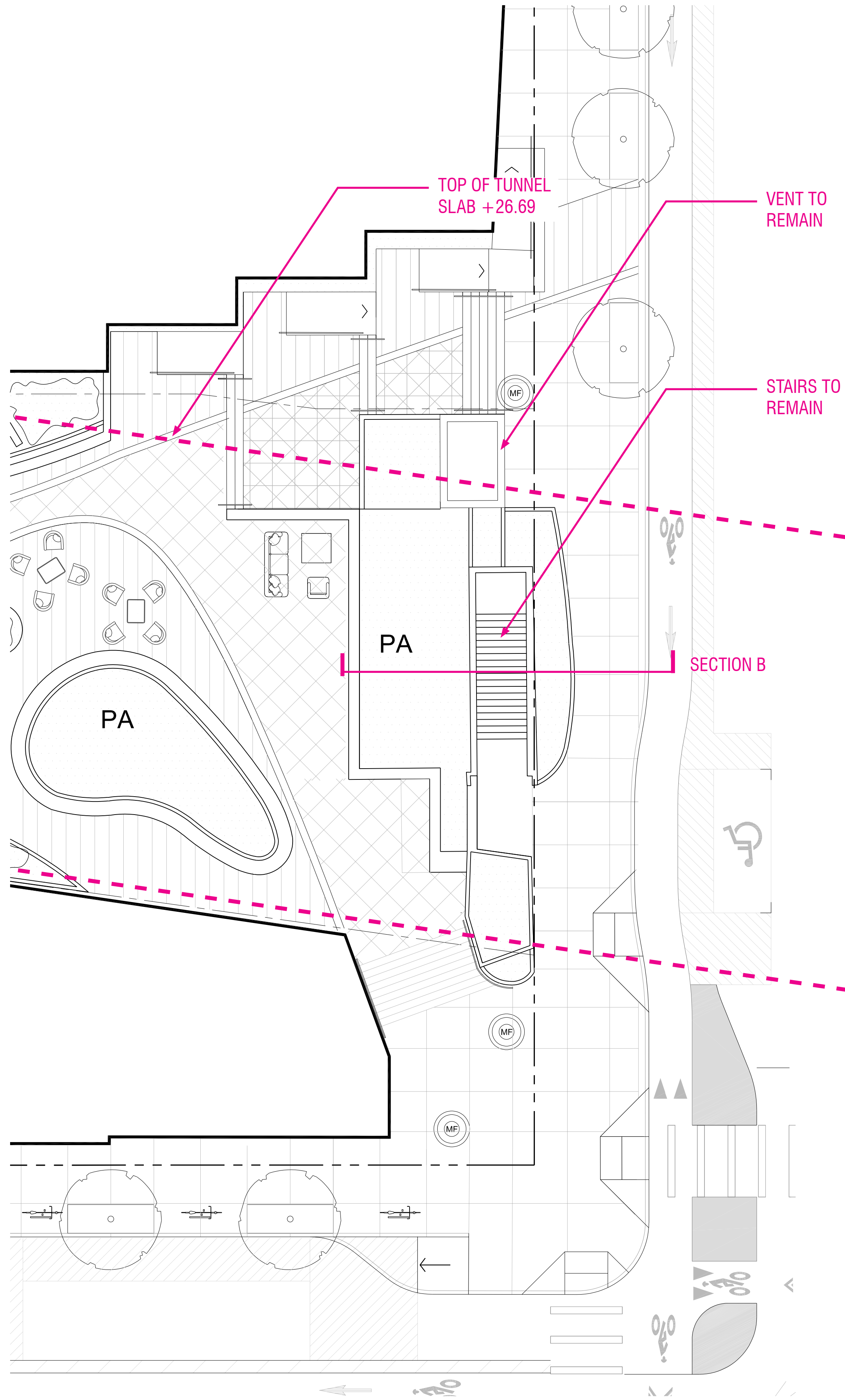
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**BART INFRASTRUCTURE**

SHEET:  
**L10.9**

PRELIMINARY - Not for Construction -



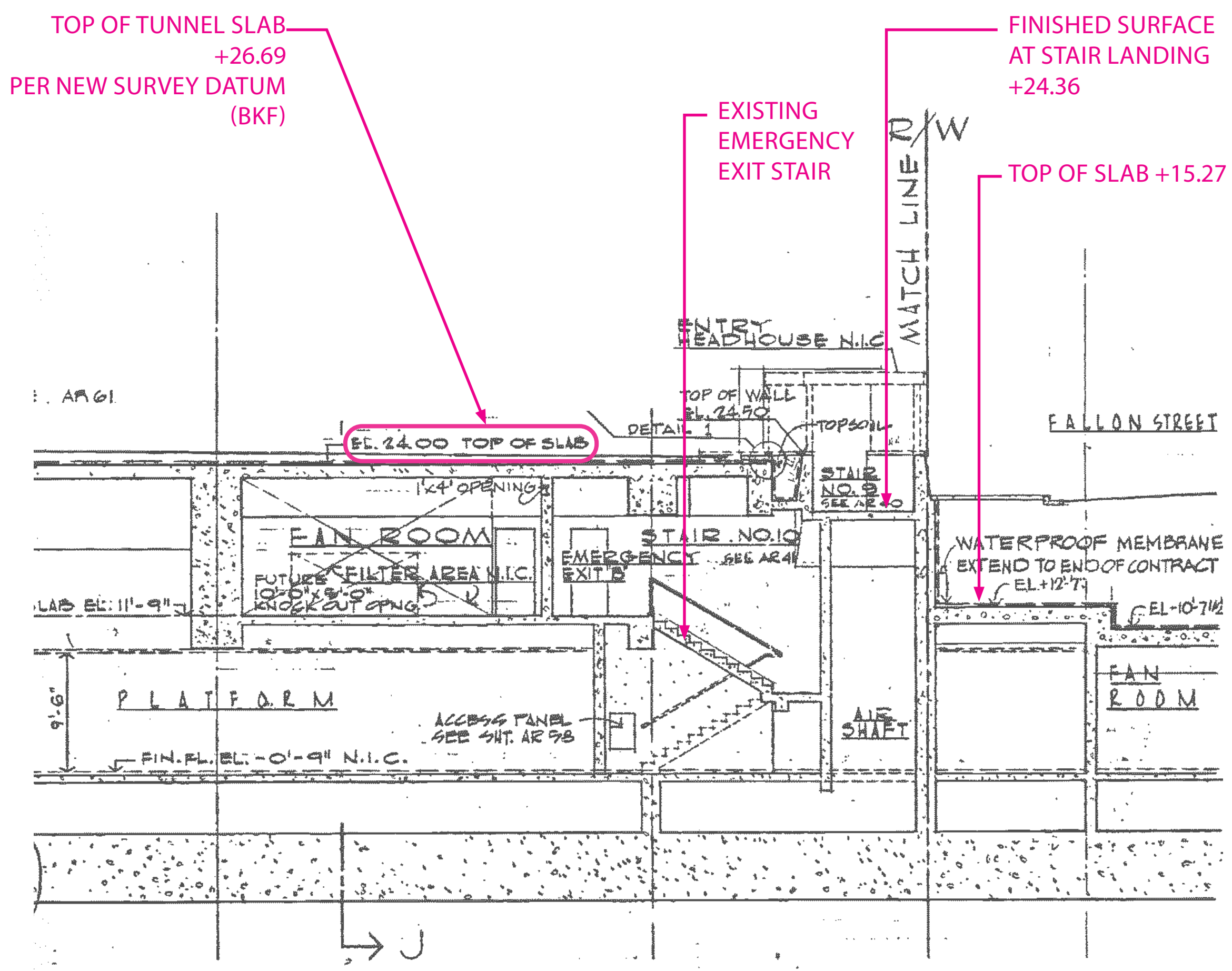
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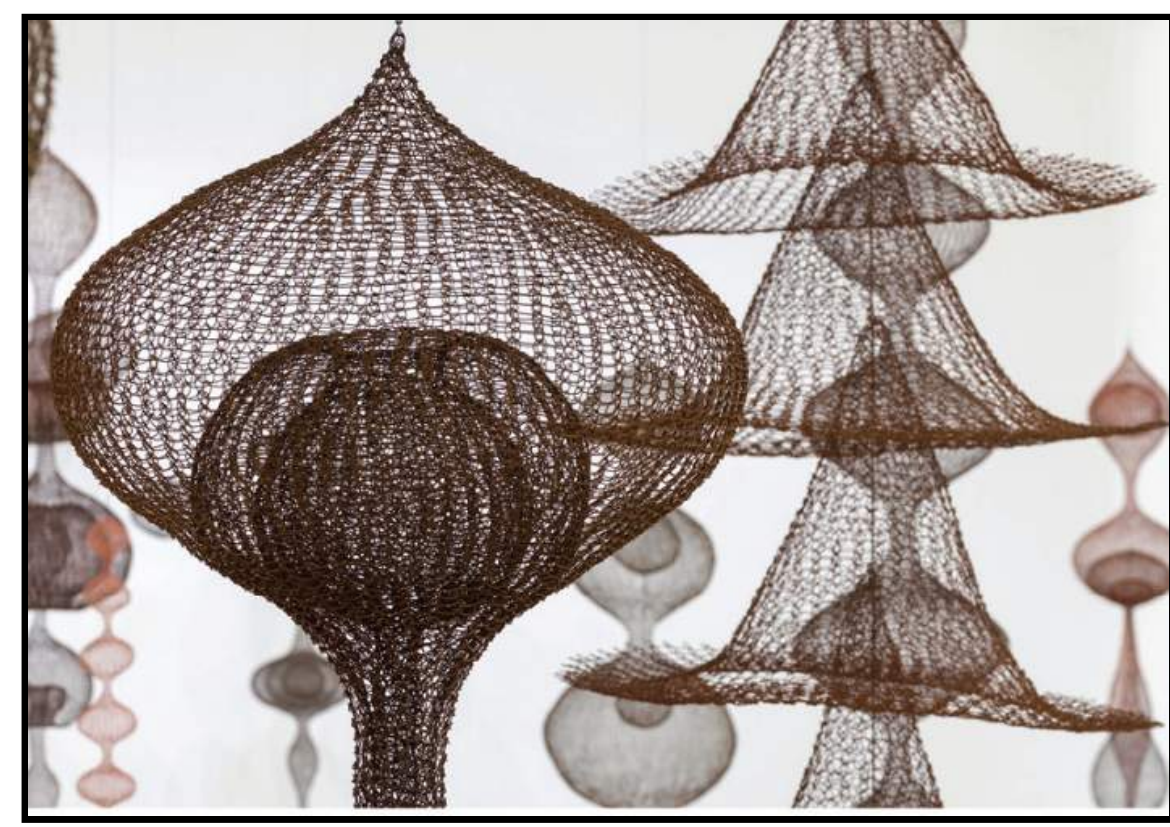
VENT: TO REMAIN  
FINAL DESIGN TREATMENT TO BE DEVELOPED WITH BART. OPPORTUNITY FOR ART.



STAIR: TO REMAIN  
FINAL DESIGN TREATMENT TO BE DEVELOPED WITH BART. OPPORTUNITY FOR ART.



SECTION B  
STAIR: TO REMAIN AS-BUILT OVERLAY AT EMERGENCY EXIT STAIR



Decorative lanterns



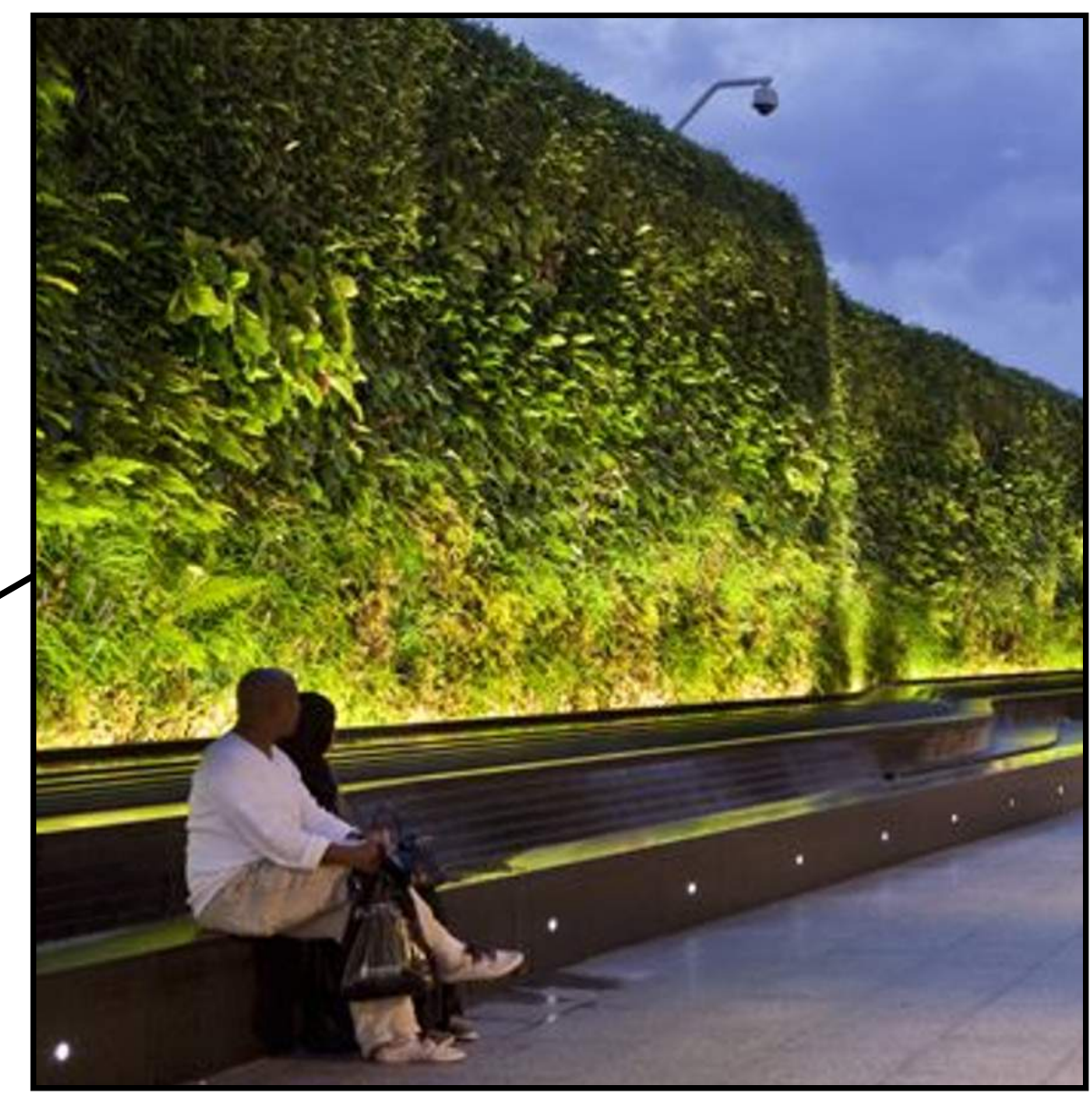
Festoon lighting



General lighting - Multi-headed pole



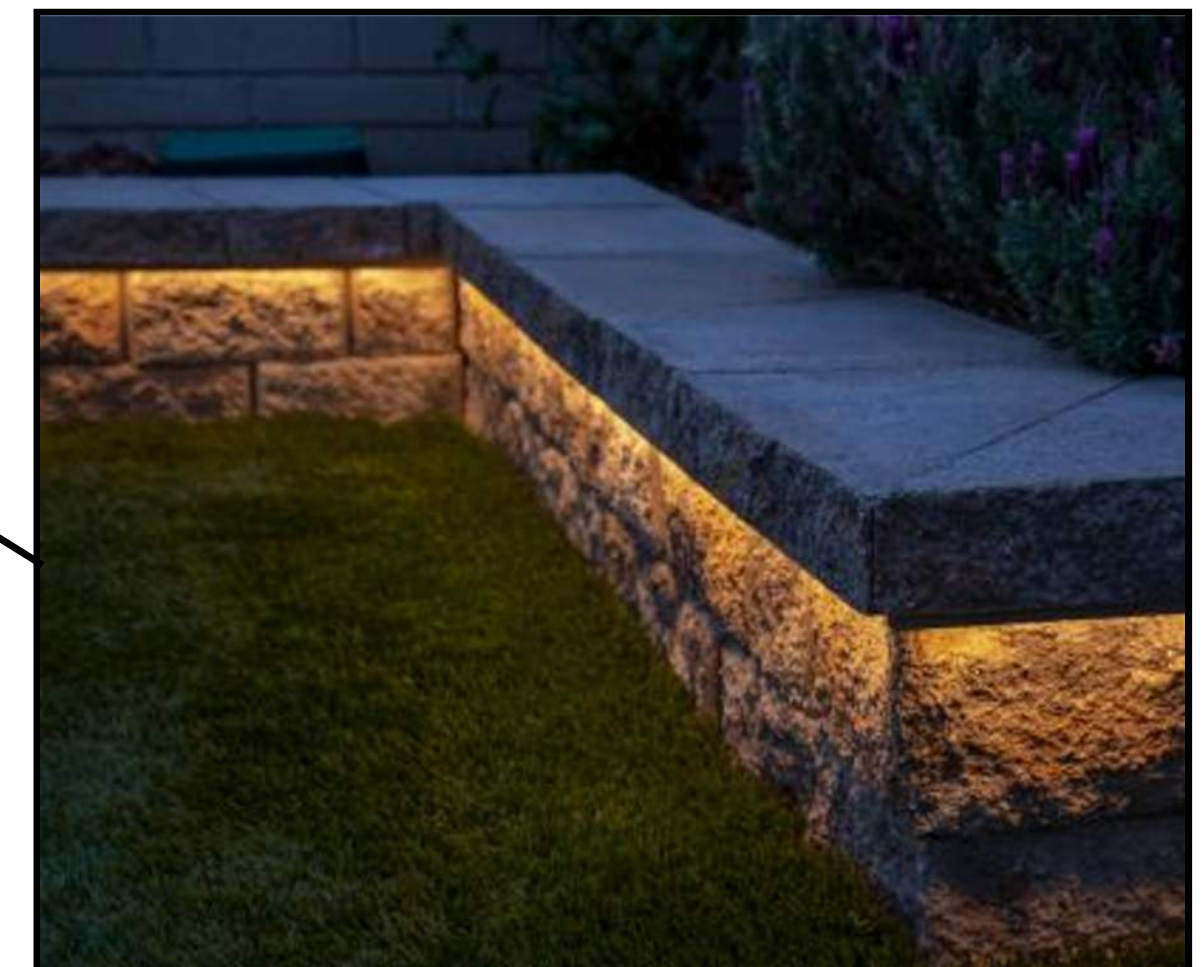
Tree uplighting



Plant wall uplight



Embedded plant wall light



Integrated bench lighting



'Light Wands' - Decorative light installation



Street Poles



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

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**PRELIMINARY LIGHTING  
CONCEPTS**

SHEET:

**L11.1**

PRELIMINARY - Not for Construction -

NOTE:

"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the the project as presened is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

Baseline design and construction in the West Plaza is fully funded by the developer.

Existing bus shelter, headhouses, and skylight to remain.



**LAKE MERRITT BART  
HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE

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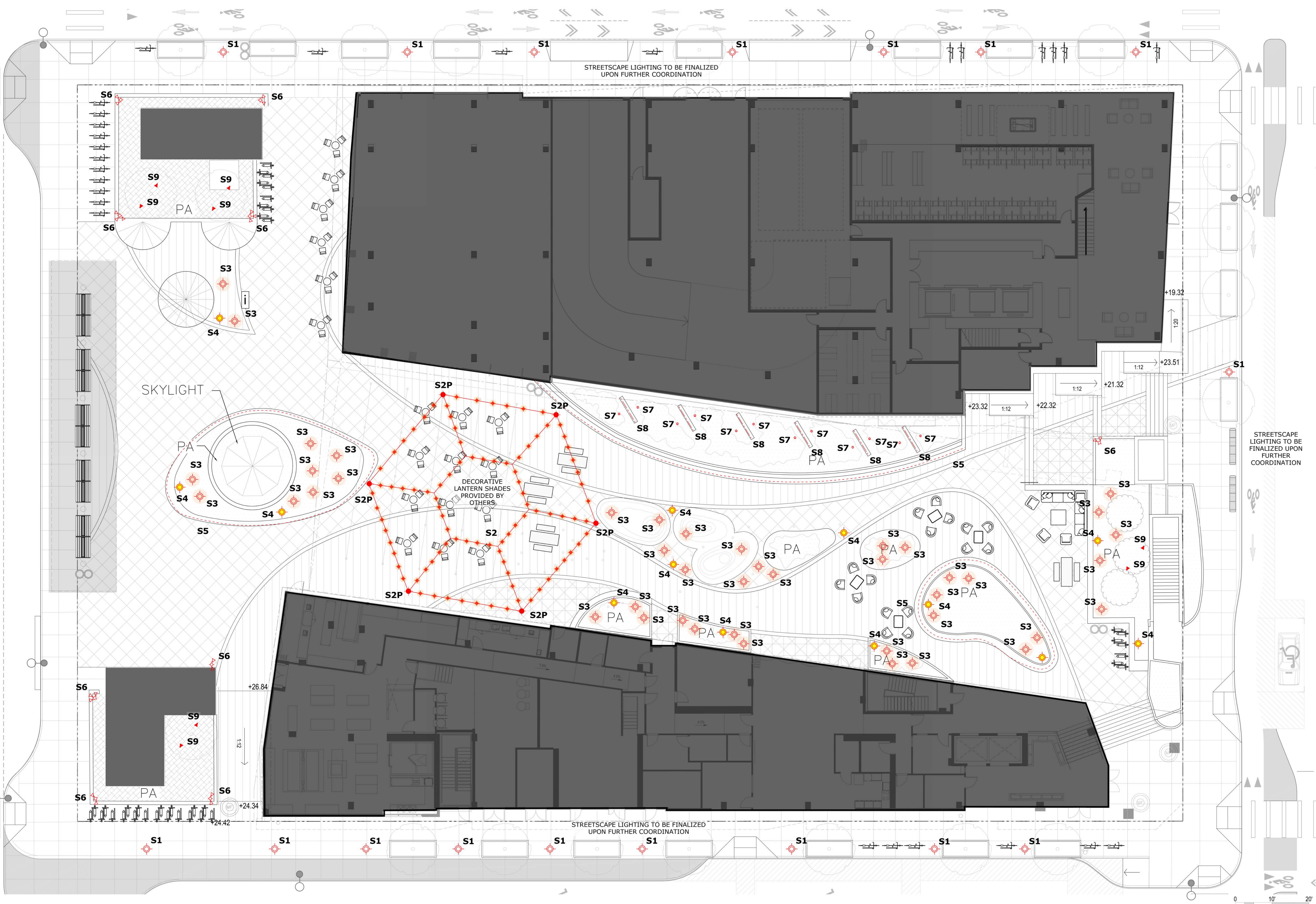
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TITLE:  
**PRELIMINARY LIGHTING  
LAYOUT**

SHEET:

**L11.2**

PRELIMINARY - Not for Construction -



- S1: Street Pole
- S2: Festoon Lighting
- S2P: Mounting Pole
- S3: Light Wand
- S4: Pedestrian Pole
- S5: Integrated bench
- S6: Multi-head Pole
- S7: Uplighting
- S8: Embedded Plant Wall Light
- S9: Stake-mounted Tree Uplight
- Existing Roadway Pole

NOTE:  
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**PRELIMINARY PHOTOMETRIC  
CALCULATION**

SHEET:

**L11.3**

PRELIMINARY - Not for Construction -

STREETSCAPE LIGHTING TO BE FINALIZED  
UPON FURTHER COORDINATION

STREETSCAPE  
LIGHTING TO BE  
FINALIZED UPON  
FURTHER  
COORDINATION

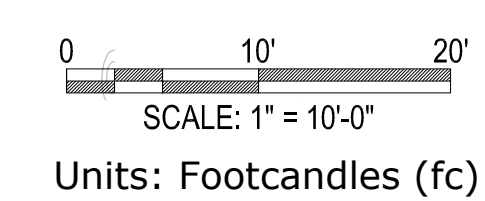
STREETSCAPE LIGHTING TO BE FINALIZED  
UPON FURTHER COORDINATION

**NOTE:**  
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Existing bus shelter, headhouses, and skylight to remain.

- S1:** Street Pole
- S3:** Light Wand
- S6:** Multi-head Pole
- S9:** Stake-mounted Tree Uplight
- S2:** Festoon Lighting
- S4:** Pedestrian Pole
- S7:** Uplighting
- S8:** Embedded Plant Wall Light
- S2P:** Mounting Pole
- S5:** Integrated bench




**S1: Street Pole**

Date: \_\_\_\_\_ Customer: \_\_\_\_\_ **selux**

Project: \_\_\_\_\_

Type: \_\_\_\_\_ Qty: \_\_\_\_\_

**Modular MTR Column LED**



Order Code: \_\_\_\_\_

MMTR Series	MMTR Modular MTR Column LED
Nominal Ht. of Lit Section	2 3 4
Light Engine	1830 2830 3830 4830
CCT	27 30 35 40
Finish	WH BK BL BZ SV SP
Voltage	120 208 240 277 347 480
Options	DM HL50


Product Modifications: \_\_\_\_\_ Approvals: \_\_\_\_\_

Date: \_\_\_\_\_

Selux Corporation © 2018. T 845-834-1400, 800-735-8927, F 845-834-1401, www.selux.us

**S2: Festoon Lighting**

**tivoli** 12V Litesphere™ 2.0

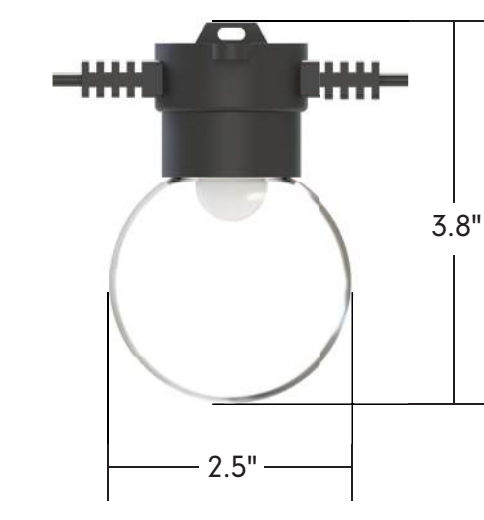


Project: \_\_\_\_\_ Type: \_\_\_\_\_

**Product Features**

- Tivoli's next evolution of Litesphere delivers a robust specification-grade strand with factory molded standard spacing for consistent quality from start to finish
- Litesphere 2.0 design provides optional suspended mounting or a twist-off cap for surface applications
- 12V DC Low voltage system for long runs
- IP67
- cULus
- 3 Year warranty

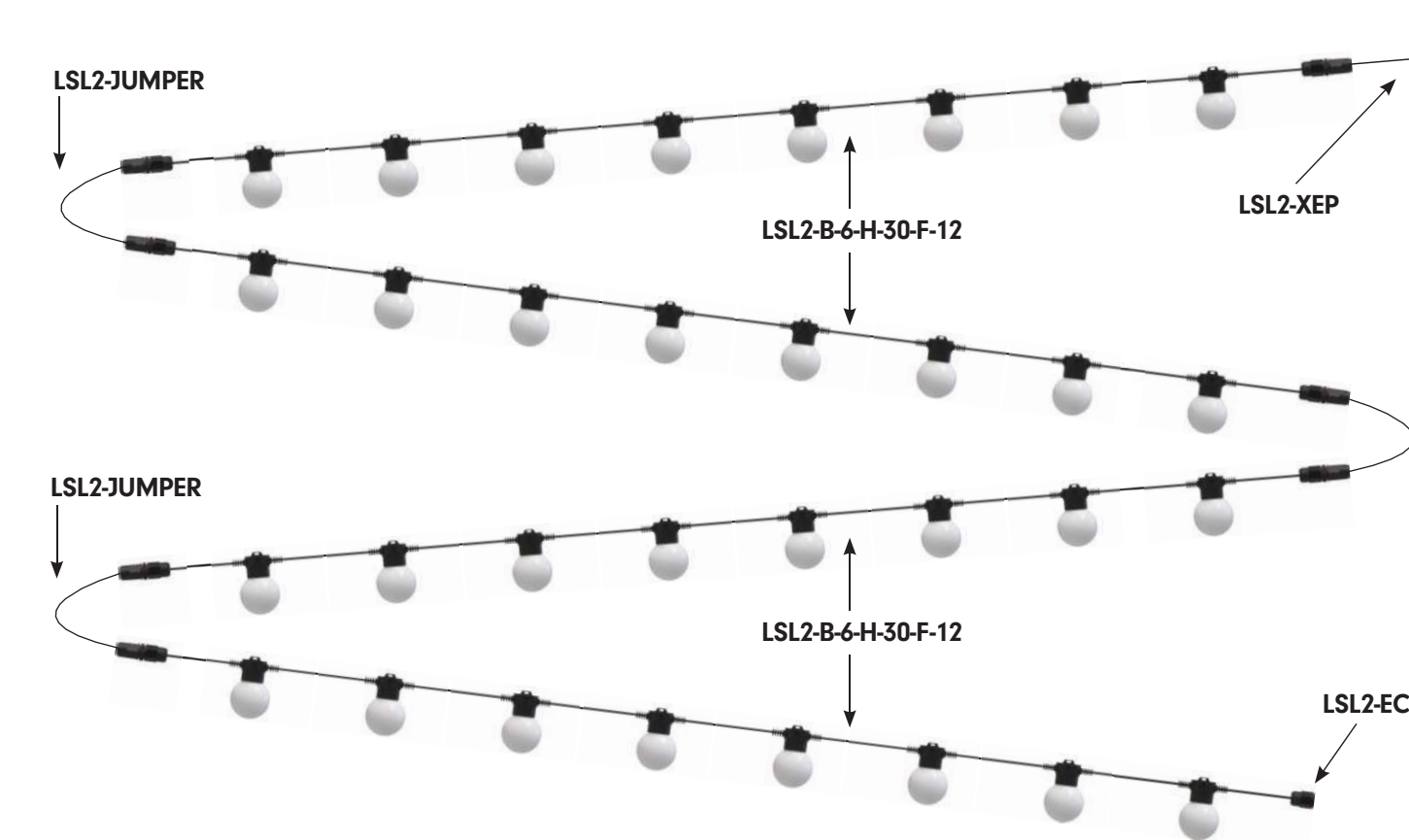
**Dimensions**



Tivoli, LLC reserves the right to modify this specification without prior notice.

**tivoli** 12V Litesphere™ 2.0

**System Configuration Example**



**Strand Order Guide**

Product	Wire	Spacing	LED Type	LED Color	Globe	Voltage
Litesphere 2.0	B Black W White	06 6" OC 12 12" OC 18 18" OC 24 24" OC	V Very High Output H High Output S Standard Output	19 1900K 27 2700K 30 3000K 35 3500K 40 4000K 50 5000K*	C Clear F Frosted O Opal	12 12V DC

Tivoli, LLC reserves the right to modify this specification without prior notice.

**S2P: Mounting Pole, 13'-6"**

**valmont** STRUCTURES **ROUND TAPERED STEEL DS200 3-Bolt Anchor Base**

Job Name: \_\_\_\_\_ Client Name: \_\_\_\_\_

Job Location - City: \_\_\_\_\_ State: \_\_\_\_\_ Created By: \_\_\_\_\_ Date: \_\_\_\_\_

Product: \_\_\_\_\_ Quarter: \_\_\_\_\_ Customer Approval: \_\_\_\_\_ Date: \_\_\_\_\_

**SPECIFICATIONS**

**Plain Top** Pole - The pole shaft conforms to ASTM A595 Grade A with a constant linear taper of 0.14 in/ft.

**Pole Cap** Pole Top - The pole top is held to a 3.00" OD (±0.13") for external top mount luminaire and/or bracket. For internal top mount luminaire please consult factory. A removable pole cap is provided for poles receiving drilling patterns for side-mount luminaire arm assemblies.

**Anchor Base** - The anchor base (base plate) conforms to ASTM A36.

**Anchor Bolts** - Anchor bolts conform to ASTM F1554 Grade 55 and are provided with one hex nut, one acorn nut and two flat washers. Bolts have an "L" bend on one end and are galvanized a minimum of 12" on the threaded end.

**Hardware** - All structural fasteners are galvanized high strength carbon steel. All non-structural fasteners are galvanized or zinc-plated carbon steel or stainless steel.

**Finish** - Standard finishes are galvanized, prime painted or any of Valmont's V-PRO™ Protection Systems. Additional finish options available upon request.

**Design Criteria** - Please reference Design Criteria Specification for appropriate design conditions.

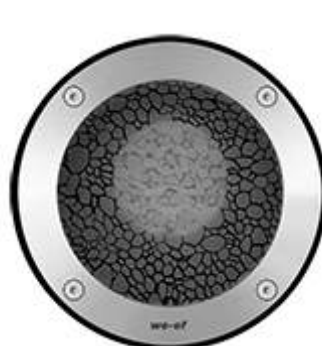
**RTS / DS200** **3-Bolt Anchor Base**

VALMONT INDUSTRIES, INC. 28809 10A STREET, PO BOX 358 - VALLEY, NE 68064 USA 800.825.6668 VALMONTSTRUCTURES.COM

**S3: Light Wand, Integrated**

**ETC120-GB LED ASC** Inground Luminaires **we-ef**

1/8



**Description**

IP68, Class I, K10, Stainless steel construction, PCS hardware, Silicone (CSiO) Controlled Compression Gasket, Safety glass lens, max load 5.5 tons (11,000 lbs). Luminaire can be driven over at low speed. Cable gland with spiral cable bending protection. Factory-sealed termination chamber complete with cable gland and 3 ft. of flexible PVC free, gel impregnated anti-wicking cable. Factory-installed circuit board with High Efficiency LEDs. Gimbal mounted, 20° tilt and 355° rotatable. Suitable for flush installation in concrete or earth. The concrete pour installation breakout is supplied as standard with luminaire. Specify product with 7 Digit product code - Finish Color. Accessories, such as mounting, optical, and electrical, must be specified separately. Example: XXX-XXXX-8004 (Black) + XXX-XXXX (Accessory 1)

WE-EF LIGHTING USA LLC 4100 Kaysville Drive | Warminster PA 18980 | U.S.A. | Tel +1 724 742 8030 | Fax +1 724 742 8035 | info@we-ef.com | www.we-ef.com | 30-11-2021 16:35 Technical modifications and errors accepted

**ETC120-GB LED ASC** Inground Luminaires **we-ef**

3/8

**Choices**

Light Distributions	Nominal Lumen	Nominal Watt	Colour Temperatures	Colours
symmetric, medium beam (M)	738	6	3000 K	Stainless Steel
symmetric, very narrow beam (VN)			4000 K	
symmetric, very narrow beam, 'sharp cut-off' (VNS)				

WE-EF LIGHTING USA LLC 4100 Kaysville Drive | Warminster PA 18980 | U.S.A. | Tel +1 724 742 8030 | Fax +1 724 742 8035 | info@we-ef.com | www.we-ef.com | 30-11-2021 16:35 Technical modifications and errors accepted



**LAKE MERRITT BART HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

**ISSUE SCHEDULE**

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100	SD - HORIZONTAL FDP	05/02/2022
100	SD - HORIZONTAL FDP2	06/16/2022

**REVISION SCHEDULE**

NO.	ISSUE	DATE
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TITLE: **FIXTURE CUT SHEET**

SHEET: **L11.4**

**S4: Pedestrian Pole**

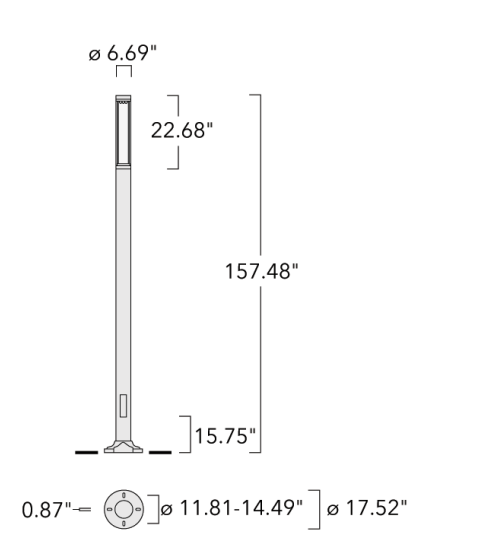
**LTM444 LED**  
Light Columns

1/5



**we-ef**

**Description**  
IP66, Class I, IK09, Marine-grade, die-cast aluminum alloy. SCE superior corrosion protection including PCS hardware. Silicone COB Controlled Compression Gasket. Polycarbonate outer lens. Sensor door with fused cable connecting box. CAD-optimized optics for superior illumination and glare control. 0-10V Dimming comes standard with luminaire. Specify product with 7 Digit product code - Finish Color. Accessories, such as mounting, optical and electrical, must be specified separately. Example: XXX-XXXX-9004 (Black) + XXX-XXXX (Accessory 1)



Technical specifications and contact information for we-ef Lighting USA LLC.

**LTM444 LED**  
Light Columns

3/5

**we-ef**

**Choices**

Light Distributions	Nominal Lumen	Nominal Watt	Colour Temperatures	Colours
[S6] Type III Medium	2097	18	2700 K	RAL9004 Black
[R6] Type IV Medium	2213	27	3000 K	RALB07 Grey Metallic
	3230		4000 K	RALB016 White
	3516			RALB019 Dark Bronze
	4563			

Technical specifications and contact information for we-ef Lighting USA LLC.

**S5: Integrated bench**

**Kurba Horizontal** Flexible LED strip - 24 VDC

**Features**  
Kurba Horizontal is a direct view, small profile, energy efficient and flexible LED strip suitable for wet locations capable of horizontal bends up to 2.5" radius. Built with robust factory made power feeds or joiners, Kurba Horizontal mounts to a specifically designed aluminum extruded channel.

**Applications**  
Outdoor wet location

**Operating Voltage**  
24VDC

**Operating Temperature**  
-4°F to 113°F (-20°C to 45°C)

**Average Life**  
50,000 hours

**Dimming**  
Low voltage dimmer and class 2 dimming power supply required

**Approvals**  
Class 2 wet listed, IP67

**Warranty**  
3 years

**CAUTION!** Reference diagram below on Can and Cannots when installing the Kurba Horizontal

**CAN** Bend LED in these positions only to avoid damage

**CANNOT** Twisting or bending LED in these positions will cause damage to the LED

**Technical Information**

Model	KBM-XX	CCT INFO / LUMEN MULTIPLIER
LEDs/ft	22	Color Multiplier (reference 3000K)
Light Output at 3000K	122 lm/ft	2300K 0.98
Average Power Consumption at 4'	3.4 W/ft	2400K 1.00
Ordering Increment	3.28"	2700K 1.03
Maximum Run Length (in Series)	26'	3000K 1.00
		3500K 1.09
		4000K 1.10

Ordering Code and contact information for opic arts by lumini.

**S6: Pole-Mounted Adjustable Downlight**

**UOD-50001**  
Odessa 1 Small Floodlight

**LIGMAN LIGHTING USA**

**Construction**  
Aluminum Casting. Lens: 2.5" thick copper coated - Marine Grade 6060 extruded & LMG Aluminum High Pressure die casting provides excellent mechanical strength - clean detailed product lines and excellent heat dissipation.

**Finish**  
8 step degrease and phosphate process that includes descaling and etching as well as a zinc and nickel phosphoric process before product painting.

**Memory Retentive - Silicon Gasket**  
Provided with special injection molded "fit for purpose" long life high temperature memory retentive silicon gaskets. Maintains the gaskets exact profile and seal over years of use and compression.

**Thermal management**  
LMG Aluminum is used for its excellent mechanical strength and thermal dissipation properties in low and high ambient temperatures. The superior thermal heat sink design by Ligman used in conjunction with the driver, controls thermal below critical temperature range to ensure maximum luminous flux output, as well as providing long LED service life and ensuring less than 10% lumen depreciation at 50,000 hours.

**Surge Suppression**  
Standard 10kV surge suppressor provided with all fixtures.

**BUC Rating**  
B2 - UO - G0

**Finishing**  
All Ligman products go through an extensive finishing process that includes fettling to improve paint adherence.

**Paint**  
UV Stabilized 4.9Mil thick powder coat paint and baked at 200 Deg. C. This process ensures that Ligman products can withstand harsh environments. Rated for use in marine environments.

**Hardware**  
Provided Hardware is Marine grade 316 Stainless steel.

**Anti Seize Screw Holes**  
Tapped holes are infused with a special anti seize compound designed to prevent seizure of threaded connections, due to electrolysis from heat, corrosive atmospheres and moisture.

**Crystal Clear Low Iron Glass Lens**  
Provided with tempered, impact resistant crystal clear low iron glass ensuring no green glass tinge.

**Optics & LED**  
Precision optic design provides exceptional light control and precise distribution of light. LED CRI > 80

**Lumen - Maintenance Life**  
LED @100 at 50,000 hours (This means that at least 90% of the LED still achieve 80% of their original flux)

Technical specifications and contact information for Ligman Lighting USA.

**UOD-50001**  
Odessa 1 Small Floodlight

**LIGMAN LIGHTING USA**

**PROJECT**  **DATE**

**QUANTITY**  **TYPE**  **NOTE**

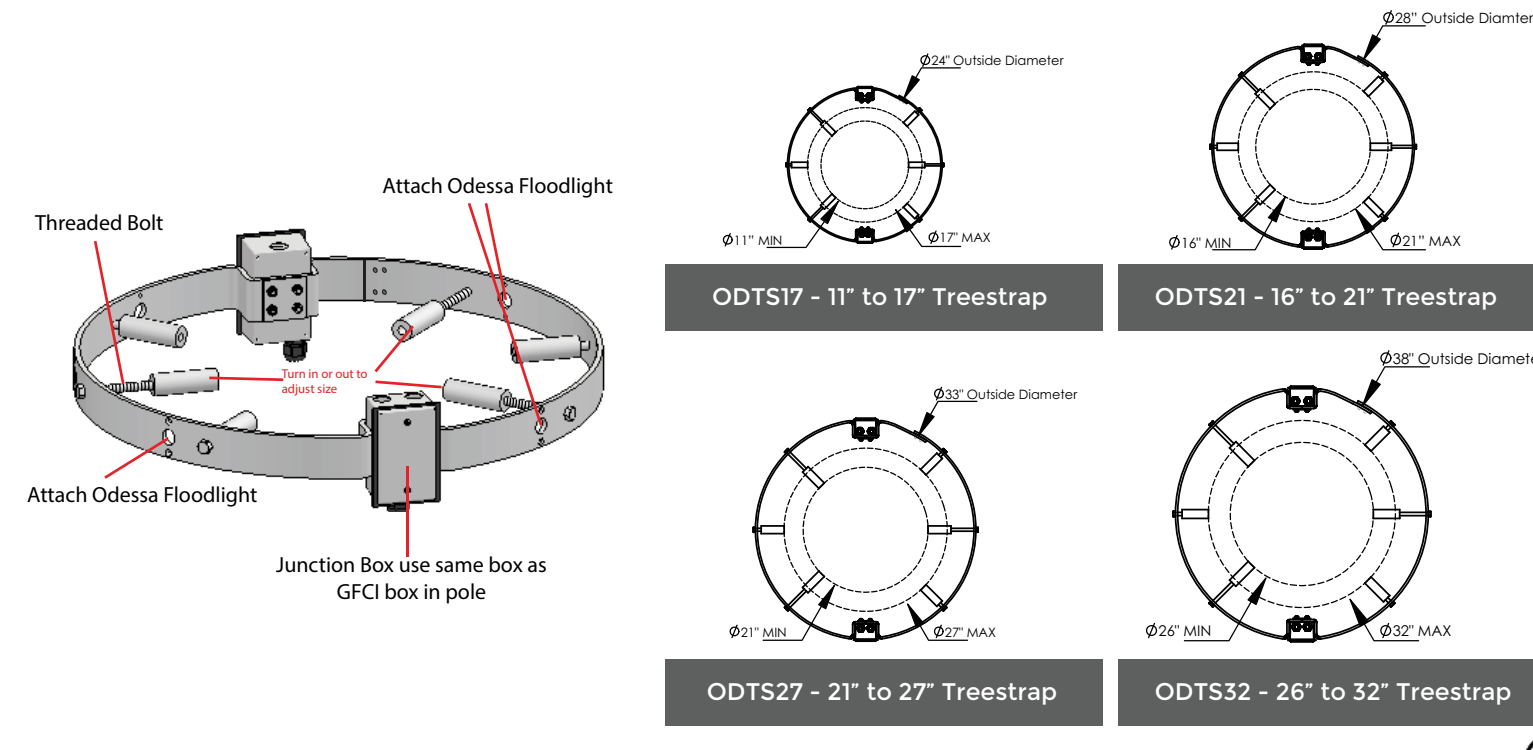
**ORDERING EXAMPLE || UOD-50001 - 21w - M - W30 - 03 - 120/277V**

LAMP	BEAM	LED COLOR	FINISH COLOR	VOLTAGE
21w COB 1675 Lumens	M - Medium 33" W - Wide 32" VW - Very Wide 47"	W27 - 2700K W30 - 3000K W35 - 3500K W40 - 4000K	01 - BLACK RAL 9011 03 - DARK GREY RAL 7043 05 - WHITE RAL 9003 06 - METALLIC SILVER RAL 9006 07 - MATT SILVER RAL 9008 04 - LIGMAN BRONZE 07 - CUSTOM RAL	120/277V Other - Specify

**ADDITIONAL OPTIONS**

OPTION	DESCRIPTION
DM - 0-10v dimming	AS021 - HomogymB Louvre
NAY - Natriumium Rated	AS044 - Ground Spike
AR031 - Anti Glare Visor	AS018 - Surface Pedestal 24" Extended Arm
AR028 - Linear Spread Lens	AS028 - 1/2" Threaded 24" Extended Arm
	DICHROIC COLOR FILTERS
	RD - Red Lens
	BL - Blue Lens
	AM - Amber Lens
	GR - Green Lens
	TREE STRAP
	ODT517 - 11" to 17" DIA.
	ODT521 - 16" to 21" DIA.
	ODT527 - 21" to 27" DIA.
	ODT532 - 26" to 32" DIA.
	POLE CLAMP
	PCS4 - 4.5" Clamp
	PCS5 - 5" Clamp
	PCS6 - 6.5" Clamp

**ODTS - Odessa Treestrap Options**



Technical specifications and contact information for Ligman Lighting USA.

**S6P: Mounting Pole, 30'-0"**

**valmont STRUCTURES**

**ROUND TAPERED STEEL DS200**  
3-Bolt Anchor Base

**Job Name:**  **Client Name:**

**Job Location - City:**  **State:**  **Created By:**  **Date:**

**Product:**  **Quote:**  **Customer Approval:**  **Date:**

**SPECIFICATIONS**

**Plain Top**  
Pole - The pole shaft conforms to ASTM A595 Grade A with a constant linear taper of 0.14 in/ft.

**Pole Cap**  
Pole Top - The pole top is held to a 3.00" OD (±0.13") for external top mount luminaire and/or bracket. For internal top mount luminaire please consult factory. A removable pole cap is provided for poles receiving drilling patterns for side-mount luminaire arm assemblies.

**Cross Section**  
Round

**Handhole**  
Handhole - A covered handhole and grounding provision with hardware is provided.

**Anchor Base**  
Anchor Base - The anchor base (base plate) conforms to ASTM A36.

**Anchor Bolts**  
Anchor bolts conform to ASTM F1554 Grade 55 and are provided with one hex nut, one acorn nut and two flat washers. Bolts have an "L" bend on one end and are galvanized a minimum of 12" on the threaded end.

**Hardware**  
All structural fasteners are galvanized high strength carbon steel. All non-structural fasteners are galvanized or zinc-plated carbon steel or stainless steel.

**Finish**  
Standard finishes are galvanized, prime painted or any of Valmont's V-PRO™ Protection Systems. Additional finish options available upon request.

**Design Criteria**  
Please reference Design Criteria Specification for appropriate design conditions.

**ACORN NUTS**

VALMONT INDUSTRIES, INC. 28800 IDA STREET, PO BOX 358 - VALLEY, NE 68664 USA 800.625.6888 VALMONTSTRUCTURES.COM



**LUMA**

**LAKE MERRITT BART HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

**ISSUE SCHEDULE**

50 SD - HORIZONTAL FDP	10/29/2021
100 SD - HORIZONTAL FDP	12/17/2021
100 SD - HORIZONTAL FDP	05/02/2022
100 SD - HORIZONTAL FDP2	06/16/2022

**REVISION SCHEDULE**

NO. / ISSUE	DATE
-------------	------

**JOB NUMBER:** 1808  
**DRAWN BY:** Author  
**CHECKED BY:** Checker  
**ISSUE DATE:** xx/xx/xxxx


**SCALE:** As indicated  
**PHASE:** 1  
**BLOCK:** 1

**FIXTURE CUT SHEET**

SHEET:

**L11.5**



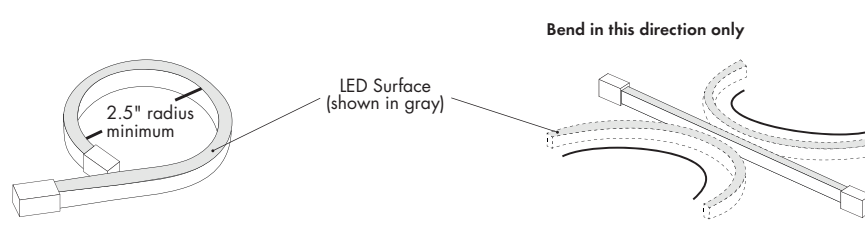
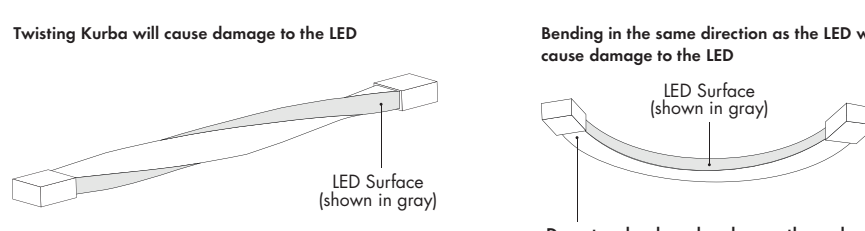
**S7: Plant Wall Stake-Mounted Uplight**

DELTA STAR LED			IP66 RATED
DATE:	PROJECT:	TYPE:	
CATALOG NUMBER LOGIC:			
			
<p><i>*Designed for use with LED transformer. Requires magnetic low voltage dimmer.</i></p> <p><i>**Please see Adjust-a-Lume photometry to determine desired intensity.</i></p> <p><i>***The 360SL coat is already included in the price of LPM, LPM dual, and Power Canopy.</i></p>			
<b>CATALOG NUMBER LOGIC</b>			
Example: B - DS - LED - e64 - SP - A7 - BZP - 12 - 11 - A - 360SL			
<b>MATERIAL</b>			
(Blank) - Aluminum B - Brass S - Stainless Steel			
<b>SERIES</b>			
DS - Delta Star			
<b>SOURCE</b>			
LED - with Integral Dimming Driver (25W min. load when dimmed)*			
<b>LED TYPE</b>			
e64 - 7W LED/2700K		e66 - 7W LED/4000K	
e65 - 7W LED/3000K		e74 - 7W LED/Amber	
<b>OPTICS</b>			
NSP - Narrow Spot (13°)		MFL - Medium Flood (23°)	
SP - Spot (16°)		WFL - Wide Flood (31°)	
<b>ADJUST-E-LUME™ OUTPUT INTENSITY**</b>			
A9 (Standard), A8, A7, A6, A5, A4, A3, A2, A1			
<b>FINISH (See page 2 for full-color swatches)</b>			
Standard Finishes (BZP, BZW, BLP, BLW, WHF, WHW, SAP, VER)			
Premium Finish (ABP, AMG, AQW, BCM, BGE, BPP, CAP, CMG, CRI, CRM, HUG, MDS, NBP, OCP, RMG, SDS, SMG, TXF, WCP, WIR)			
Also available in RAL Finishes			
Brass Finishes (MAC, POL, MIT)			
Stainless Steel Finishes (MAC, POL)			
<b>LENS TYPE</b>			
12 - Soft Focus		13 - Rectilinear	
<b>SHIELDING</b>			
11 - Honeycomb Baffle			
<b>CAP STYLE</b>			
A - 45°			
B - 90°			
C - Flush Lens			
D - 45° Less Weephole (Interior use only)			
E - 90° Less Weephole (Interior use only)			
F - 90° with Flush Lens			
<b>OPTIONS</b>			
360SL - Knuckle Mounting System***			


**B-K LIGHTING** MADE IN THE USA 559.438.5800 | INFO@BKLIGHTING.COM | BKLIGHTING.COM

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**S8: Integrated Plant Wall Light**

Kurba Horizontal	Flexible LED strip - 24 VDC	optic arts by luma
		
<b>Features</b>		
Kurba Horizontal is a direct view, small profile, energy efficient and flexible LED strip suitable for wet locations capable of horizontal bends up to 2.5' radius. Built with robust factory made power feeds or joiners. Kurba Horizontal mounts to a specifically designed aluminum extruded channel.		
<b>Applications</b>		
Outdoor wet location		
<b>Operating Voltage</b>		
24VDC		
<b>Operating Temperature</b>		
-4°F to 113°F (20°C to 45°C)		
<b>Average Life</b>		
50,000 hours		
<b>Dimming</b>		
Low voltage dimmer and class 2 dimming power supply required		
<b>Approvals</b>		
Class 2 wet listed, IP67		
<b>Warranty</b>		
3 years		
		
<b>CAUTION!</b> Reference diagram below on Can and Cannot when installing the Kurba Horizontal		
<b>CAN</b>	Bend LED in these positions only to avoid damage	
		
<b>CANNOT</b>	Twisting or bending LED in these positions will cause damage to the LED	
		
<b>Technical Information</b>		
<b>Model</b>	<b>KBM-XX</b>	<b>CCT INFO / LUMEN MULTIPLIER</b>
LEDs/ft	22	Color Temperature Multiplier (reference: 3000K)
Light Output at 3000K	122 lm/ft	2200K 0.98
Average Power Consumption at 4'	3.4 W/ft	2400K 1.00
Ordering Increment	3.28'	2700K 1.03
Maximum Run Length (In Series)	26'	3000K 1.00
		3500K 1.09
		4000K 1.10
<b>Ordering Code</b>		
MSRCL	CCT	SECTION START
KBM-H	22K	BFF
<p>EC - End power feed (1" wire lead)   EC* - End cap   EBF - End power feed (1" wire lead)   EBF* - Injection Moulding End power feed (1" wire lead)   EBF** - Injection Moulding End power feed (1" wire lead)   EBF*** - Injection Moulding End power feed (1" wire lead)   EBF**** - Injection Moulding End power feed (1" wire lead)   EBF***** - Injection Moulding End power feed (1" wire lead)   EBF***** - Injection Moulding End power feed (1" wire lead)</p> <p>EC - End cap   EBF - End power feed (1" wire lead)   EBF* - Injection Moulding End power feed (1" wire lead)   EBF** - Injection Moulding End power feed (1" wire lead)   EBF*** - Injection Moulding End power feed (1" wire lead)   EBF**** - Injection Moulding End power feed (1" wire lead)   EBF***** - Injection Moulding End power feed (1" wire lead)</p>		
<p>1.0.0 needs lead wire. Color matched/ordered orders are non-cancelable.</p> <p>REV: 0.1/22/2021 page 1 of 9 www.luma.com tel: 224.333.6033</p>		

**S9: Stake-Mounted Tree Uplight**

DELTA STAR LED			IP66 RATED
DATE:	PROJECT:	TYPE:	
CATALOG NUMBER LOGIC:			
			
<p><i>*Designed for use with LED transformer. Requires magnetic low voltage dimmer.</i></p> <p><i>**Please see Adjust-a-Lume photometry to determine desired intensity.</i></p> <p><i>***The 360SL coat is already included in the price of LPM, LPM dual, and Power Canopy.</i></p>			
<b>CATALOG NUMBER LOGIC</b>			
Example: B - DS - LED - e64 - SP - A7 - BZP - 12 - 11 - A - 360SL			
<b>MATERIAL</b>			
(Blank) - Aluminum B - Brass S - Stainless Steel			
<b>SERIES</b>			
DS - Delta Star			
<b>SOURCE</b>			
LED - with Integral Dimming Driver (25W min. load when dimmed)*			
<b>LED TYPE</b>			
e64 - 7W LED/2700K		e66 - 7W LED/4000K	
e65 - 7W LED/3000K		e74 - 7W LED/Amber	
<b>OPTICS</b>			
NSP - Narrow Spot (13°)		MFL - Medium Flood (23°)	
SP - Spot (16°)		WFL - Wide Flood (31°)	
<b>ADJUST-E-LUME™ OUTPUT INTENSITY**</b>			
A9 (Standard), A8, A7, A6, A5, A4, A3, A2, A1			
<b>FINISH (See page 2 for full-color swatches)</b>			
Standard Finishes (BZP, BZW, BLP, BLW, WHF, WHW, SAP, VER)			
Premium Finish (ABP, AMG, AQW, BCM, BGE, BPP, CAP, CMG, CRI, CRM, HUG, MDS, NBP, OCP, RMG, SDS, SMG, TXF, WCP, WIR)			
Also available in RAL Finishes			
Brass Finishes (MAC, POL, MIT)			
Stainless Steel Finishes (MAC, POL)			
<b>LENS TYPE</b>			
12 - Soft Focus		13 - Rectilinear	
<b>SHIELDING</b>			
11 - Honeycomb Baffle			
<b>CAP STYLE</b>			
A - 45°			
B - 90°			
C - Flush Lens			
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E - 90° Less Weephole (Interior use only)			
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<b>OPTIONS</b>			
360SL - Knuckle Mounting System***			

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**LUMA**

**LAKE MERRITT BART HORIZONTAL FDP**  
Oakland, CA 94607

STAMP:

ISSUE SCHEDULE	NO.	DATE
50 SD - HORIZONTAL FDP	10292021	
100 SD - HORIZONTAL FDP	12172021	
100 SD - HORIZONTAL FDP	05022022	
100 SD - HORIZONTAL FDP2	06162022	

REVISION SCHEDULE NO. | ISSUE | DATE

JOB NUMBER:	1808
DRAWN BY:	Author
CHECKED BY:	Checker
ISSUE DATE:	xx/xx/xxxx
SCALE:	As indicated
PHASE:	1
BLOCK:	1
TITLE:	

**L11.6**

PRELIMINARY - Not for Construction -



**Lake Merritt BART  
Preliminary Development Plan**

05.03.21

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
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OAKLAND, CA 94612

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**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPK COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPK3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPK4 COMMENTS	03/19/2021

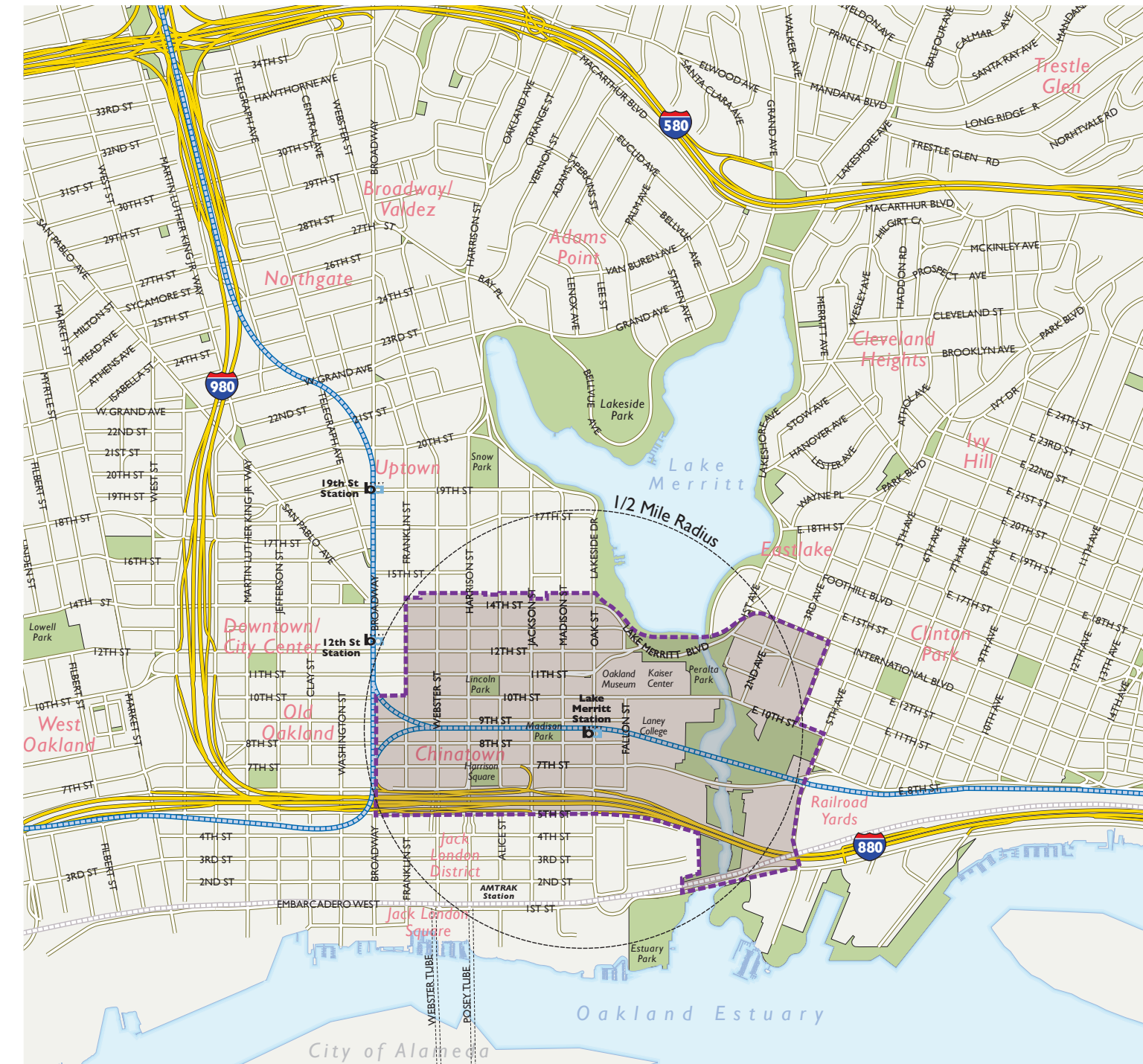
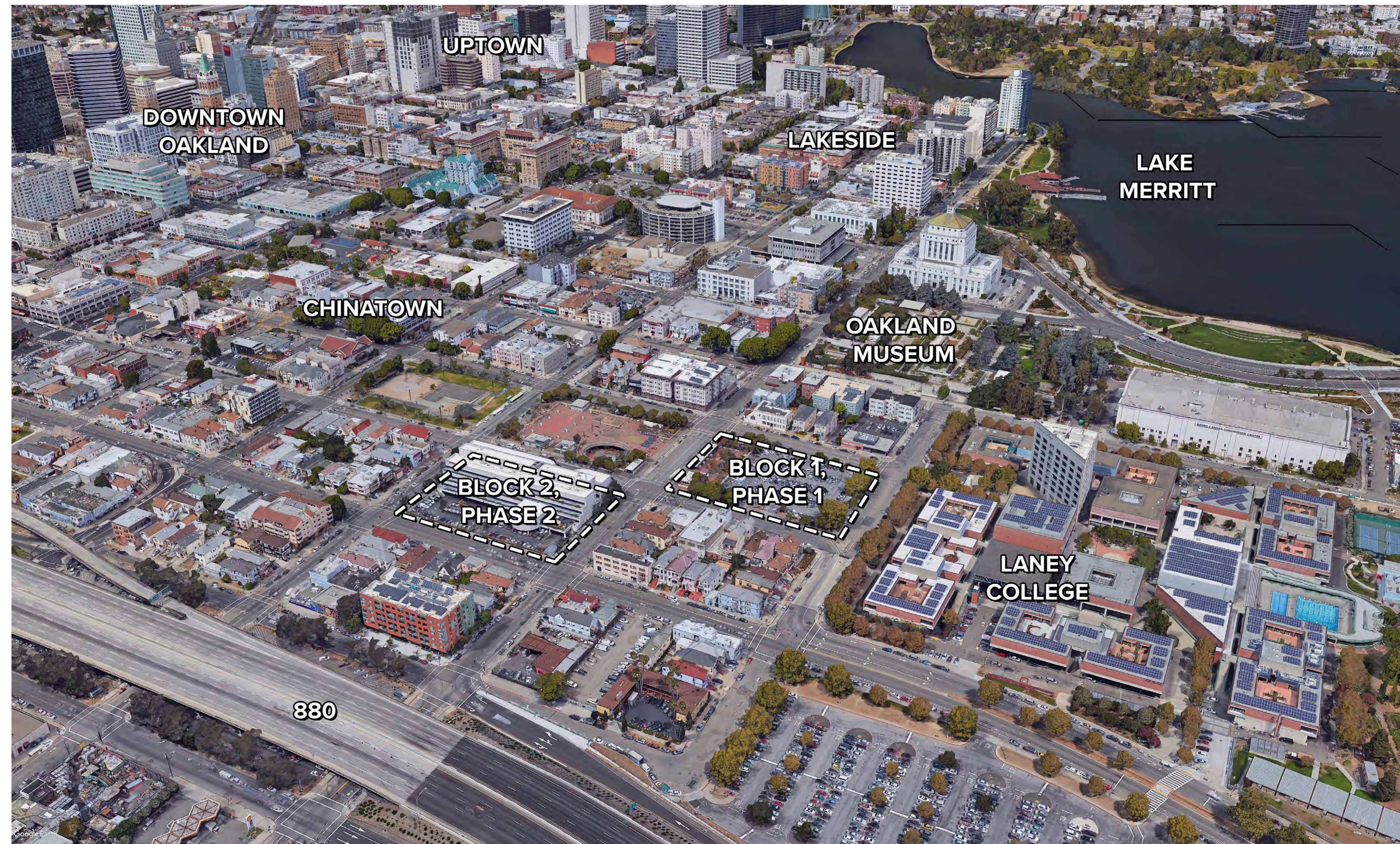
DATE:  
SCALE:  
**COVER SHEET**

**A0.0**

# Lake Merritt BART Oakland/ Chinatown Redevelopment PDP PACKAGE

## GENERAL NOTE:

BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval.” and “BART has completed an initial review of the project PDP and the the project as presened is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations. FDP Design must demonstrate per BART’s requirements that there are no structural impacts to the tunnel per BFS standards.



\*IMAGE FROM LAKE MERRITT SPECIFIC PLAN

**LOCAL CONTEXT OF THE PLANNING AREA**

- BART Station
- BART
- City Park
- Planning Area
- Railroad (Amtrak & Freight)

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## LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

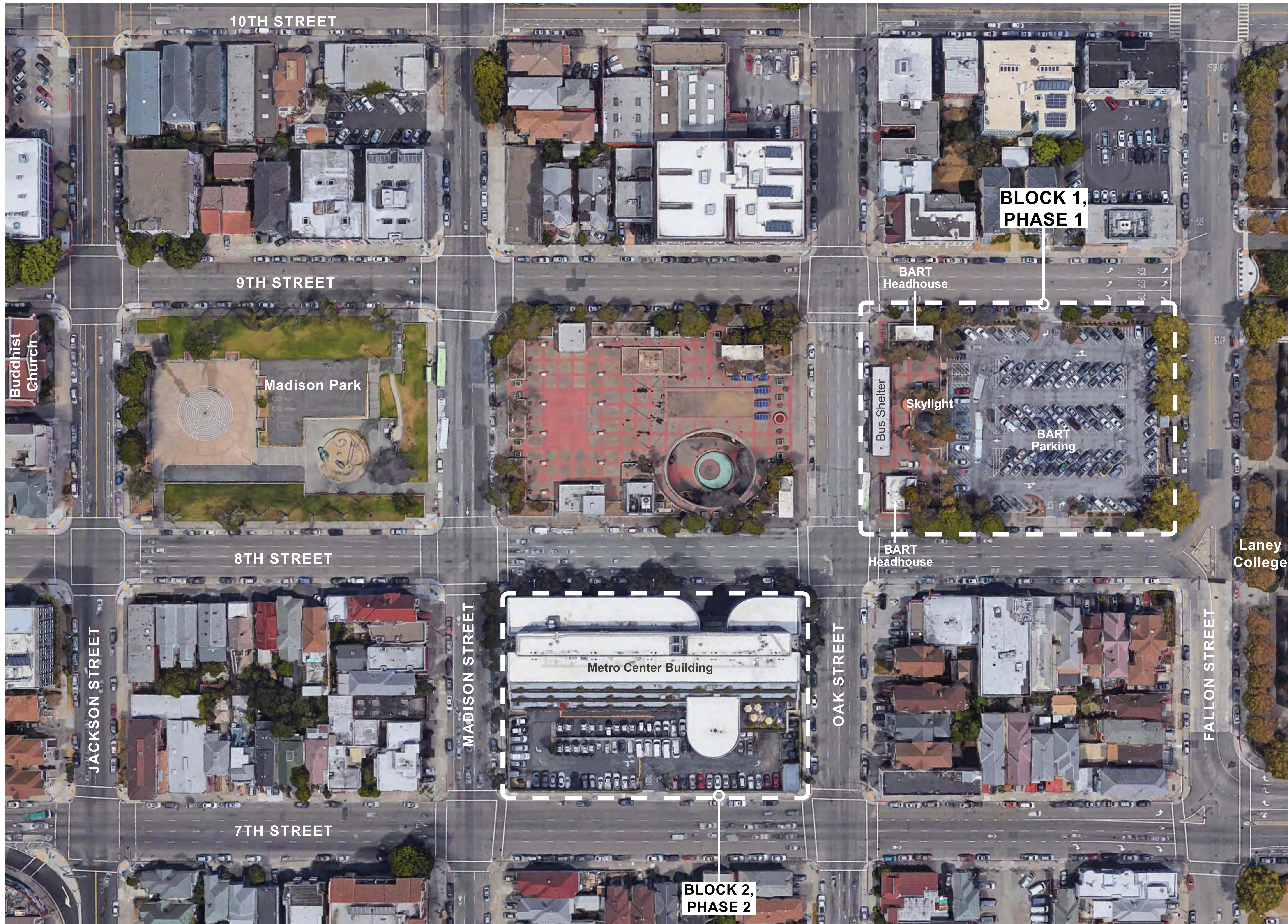
### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPRZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPRZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPRZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPRZ COMMENTS	03/19/2021

DATE:  
SCALE:  
**TITLE SHEET & DRAWING INDEX**

# A0.1



EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

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SAN FRANCISCO, CA 94105

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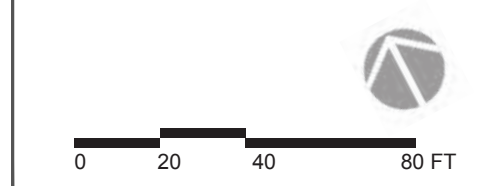
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Oakland, CA 94607

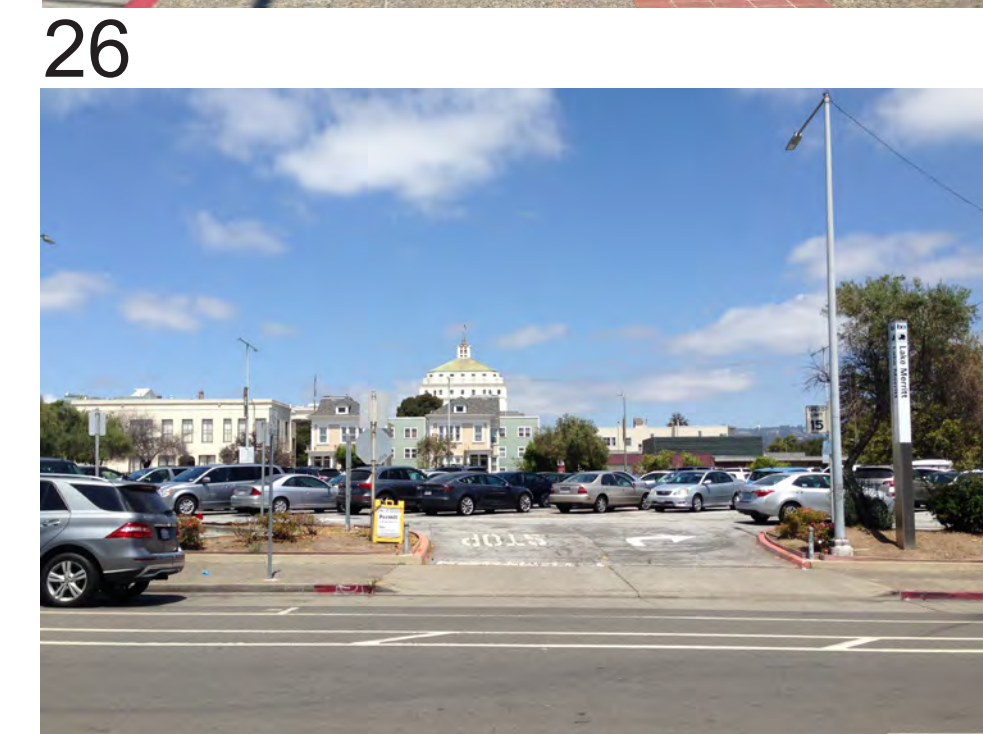
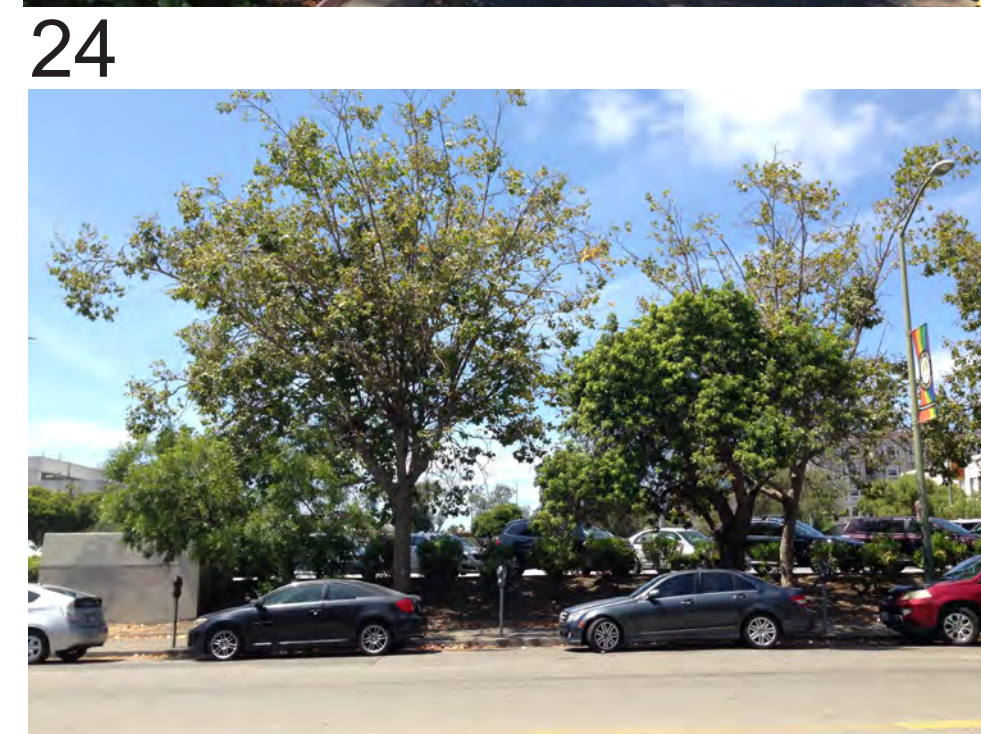
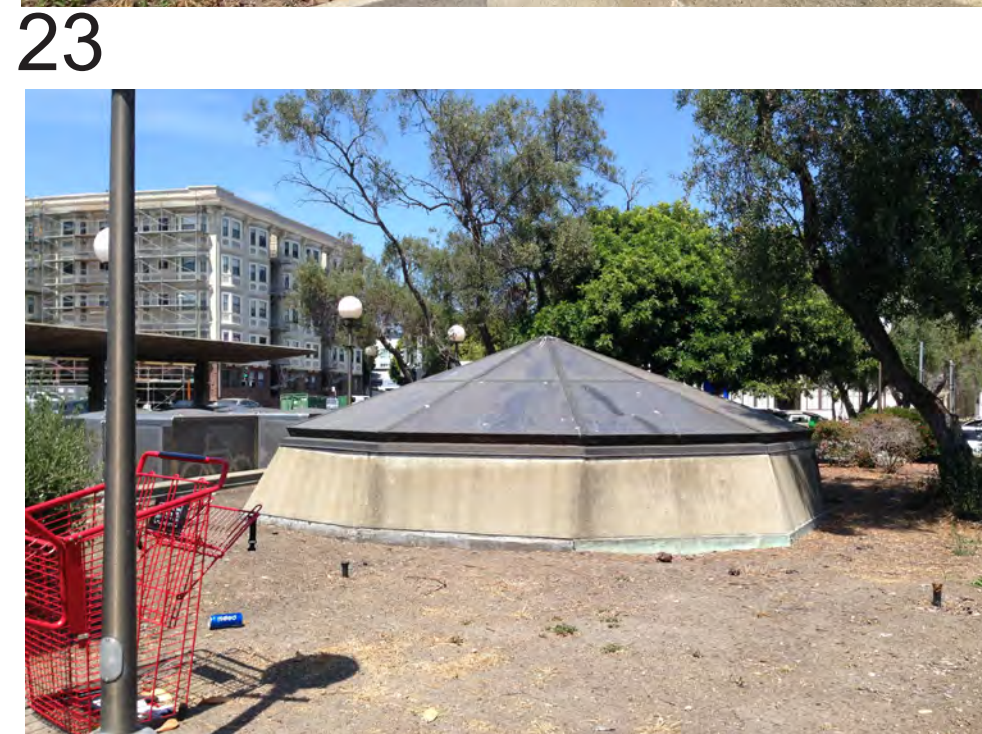
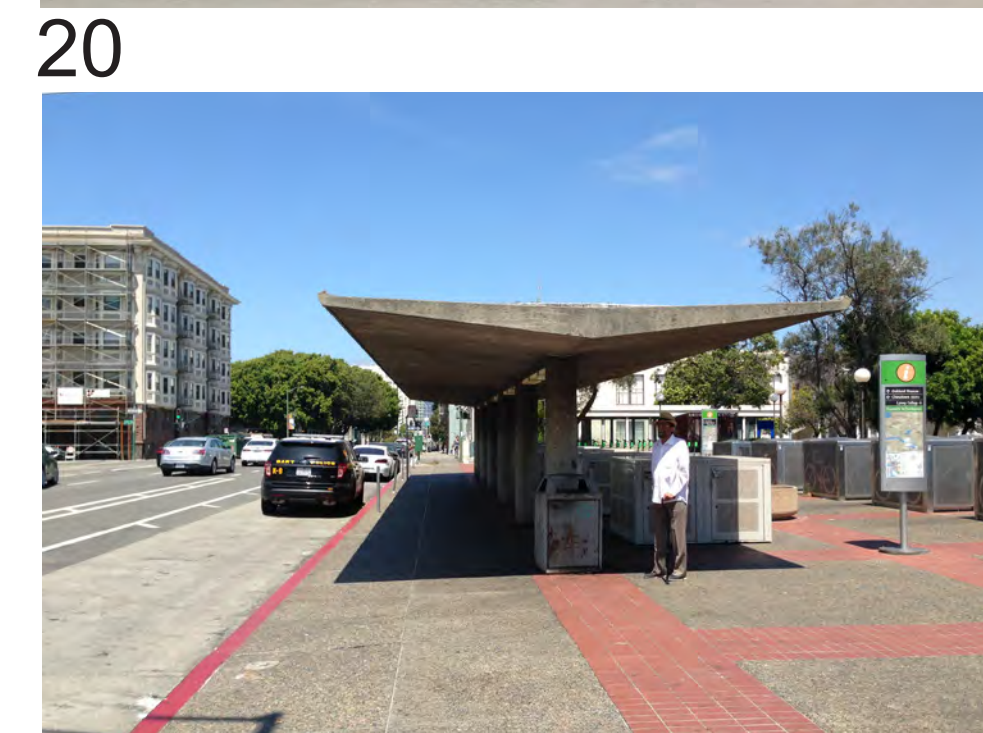
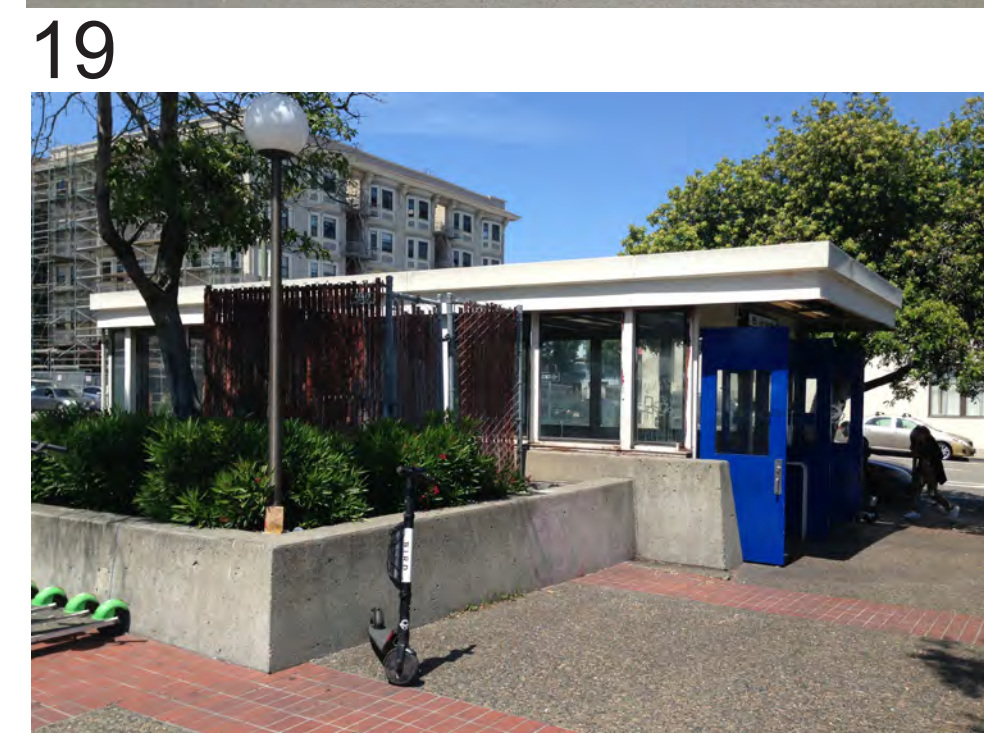
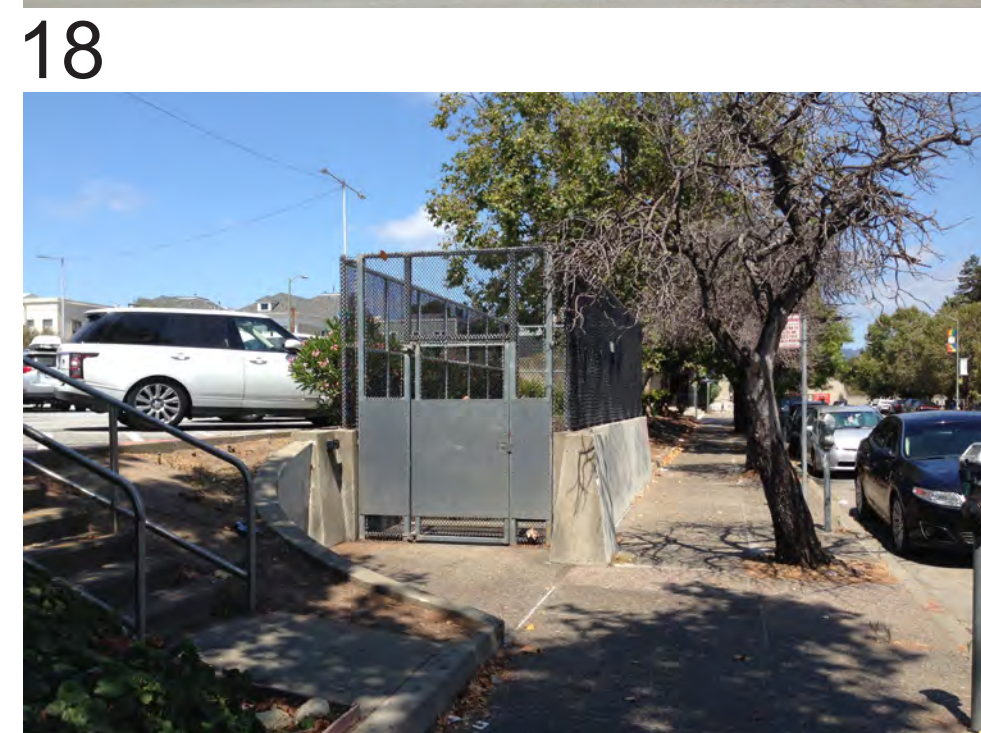
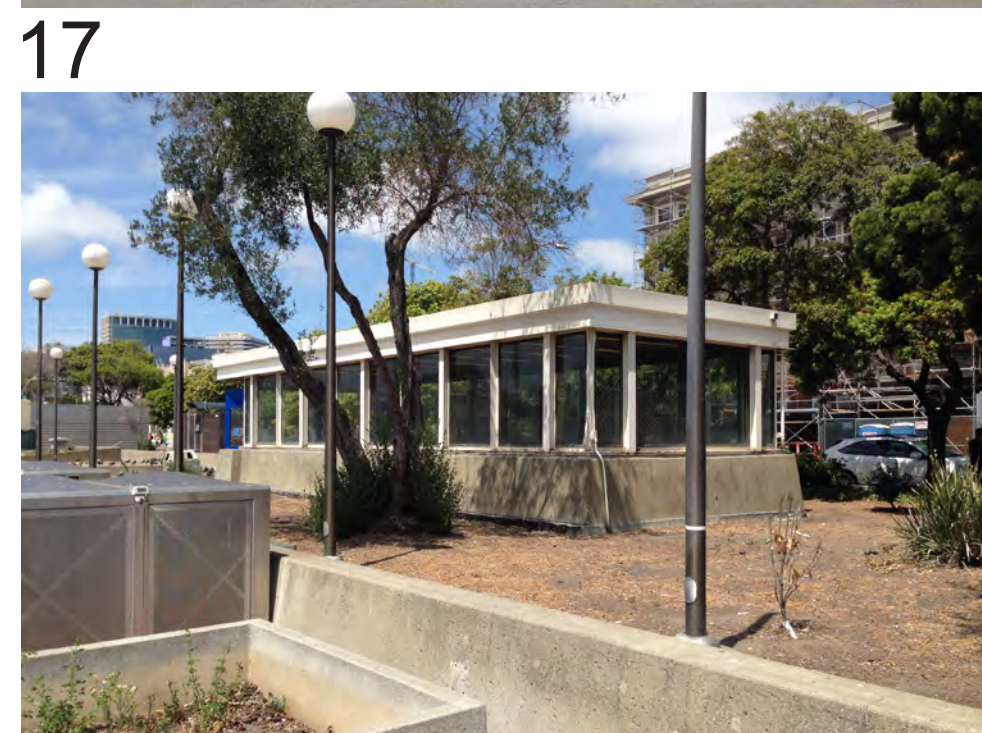
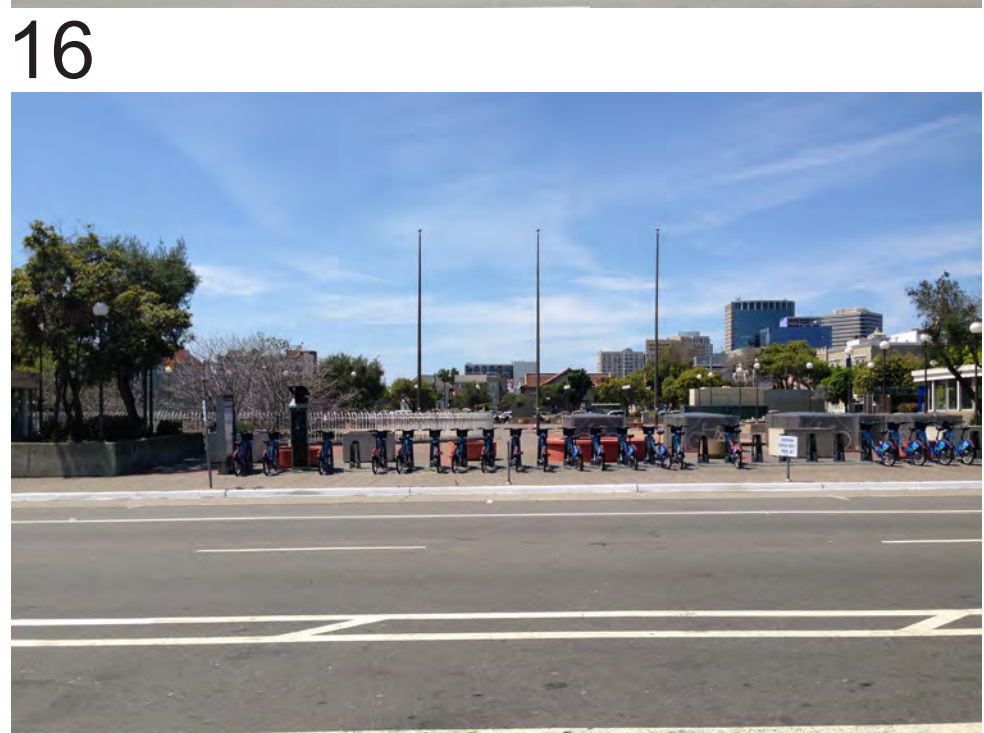
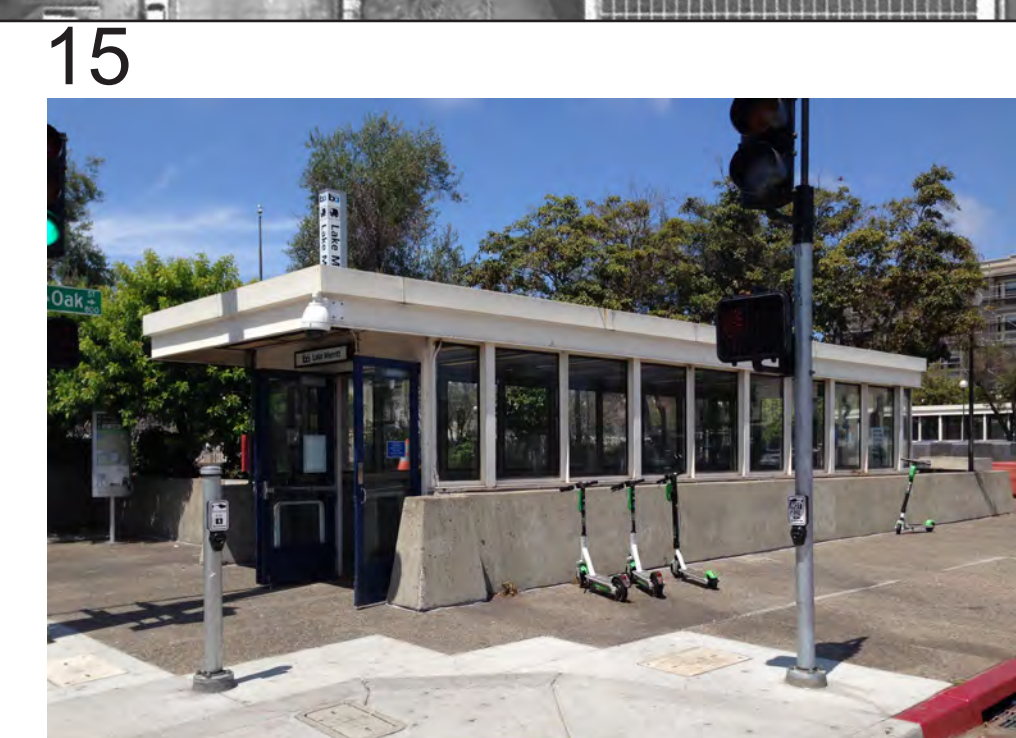
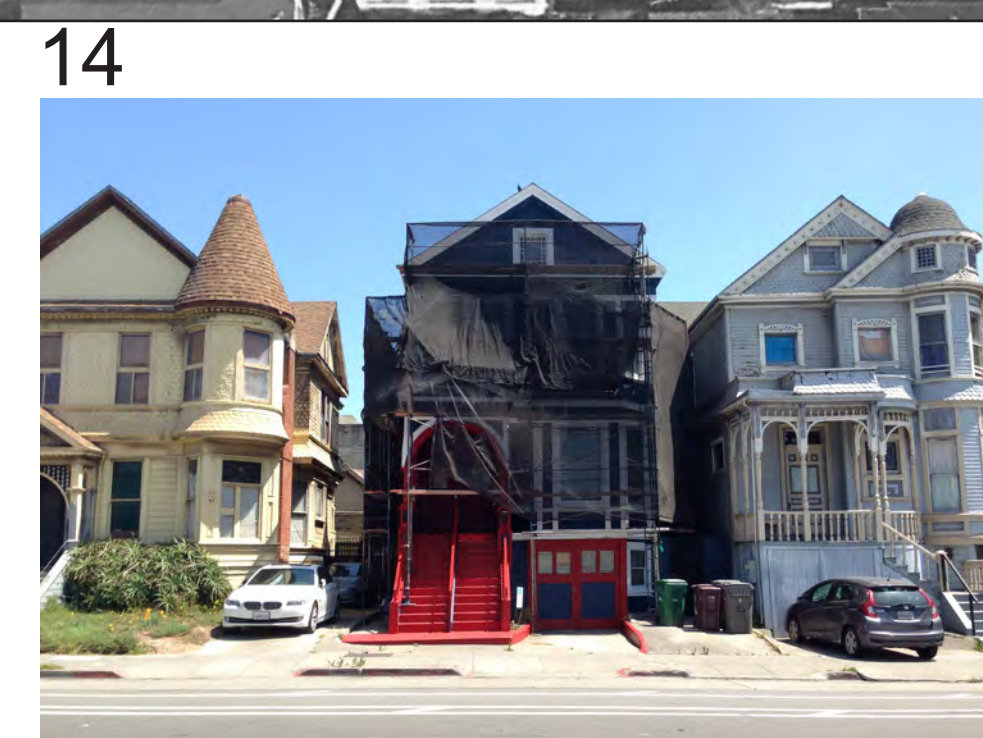
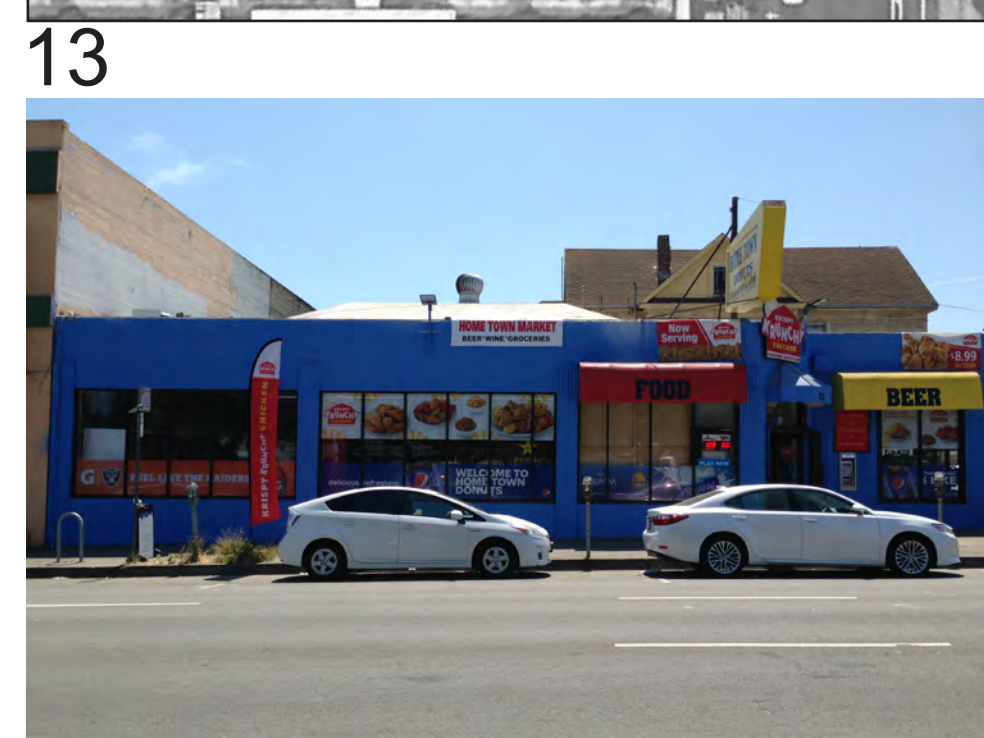
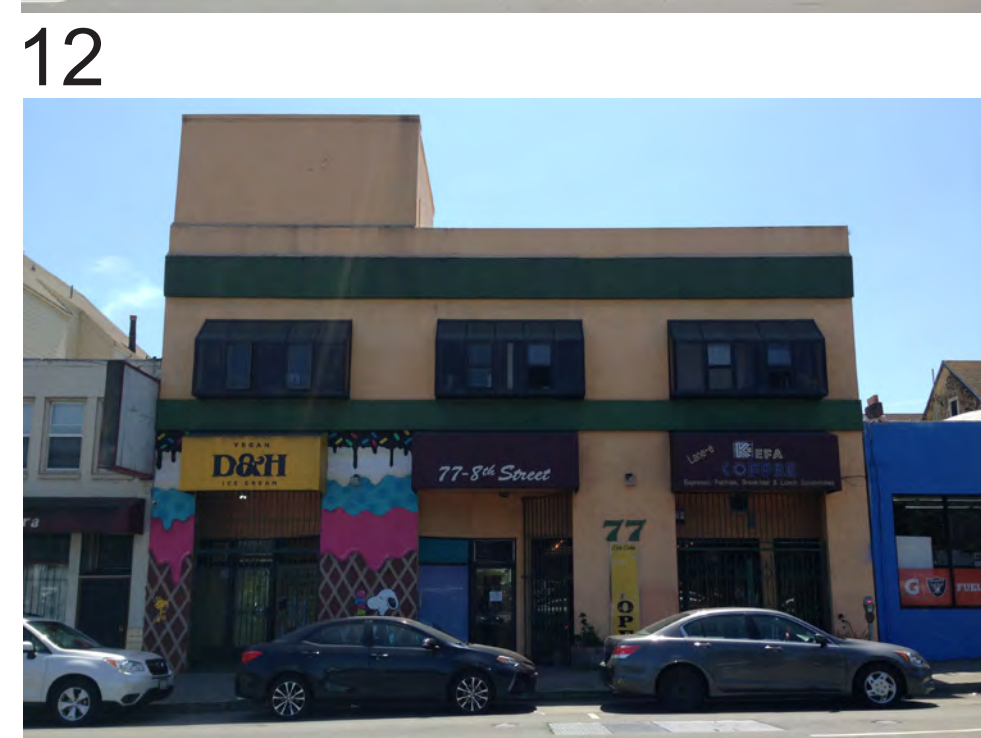
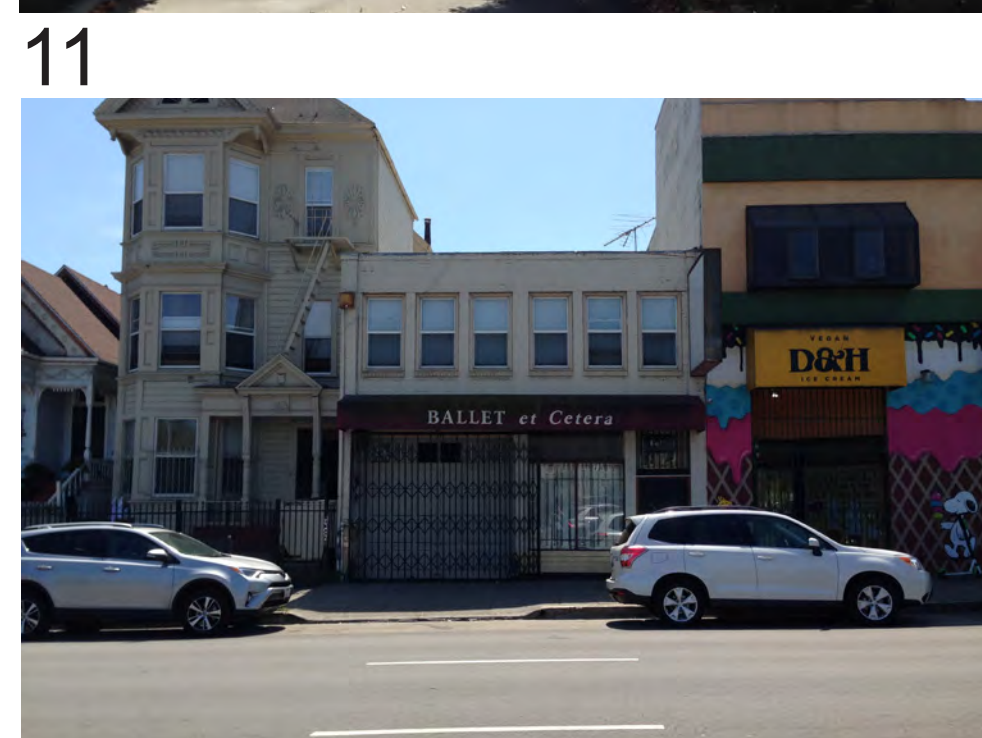
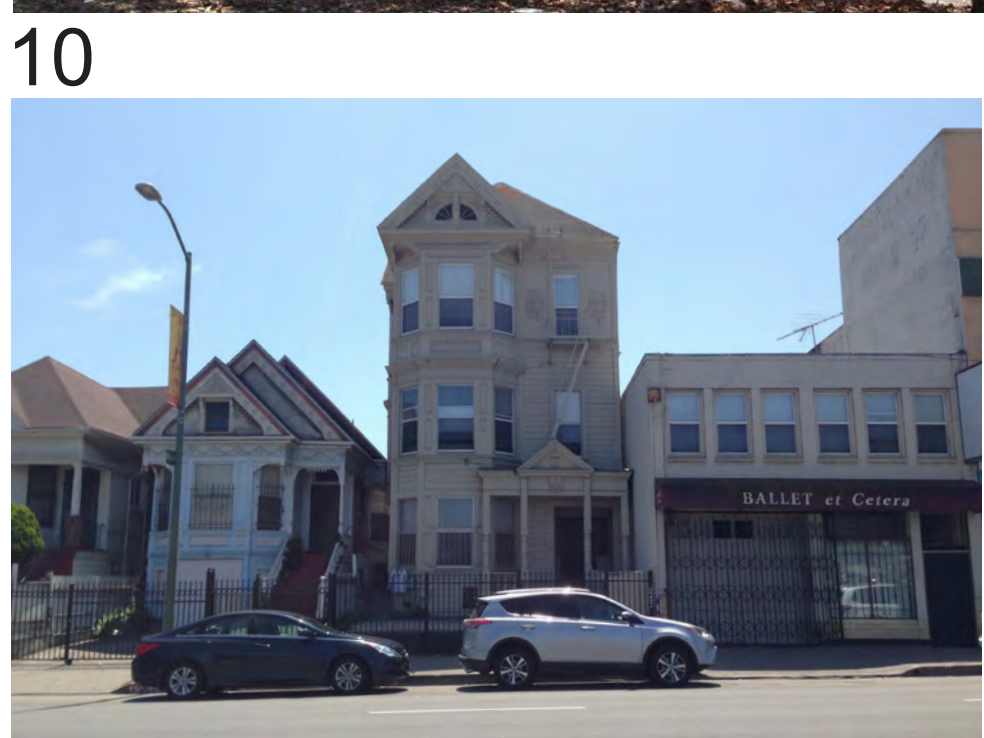
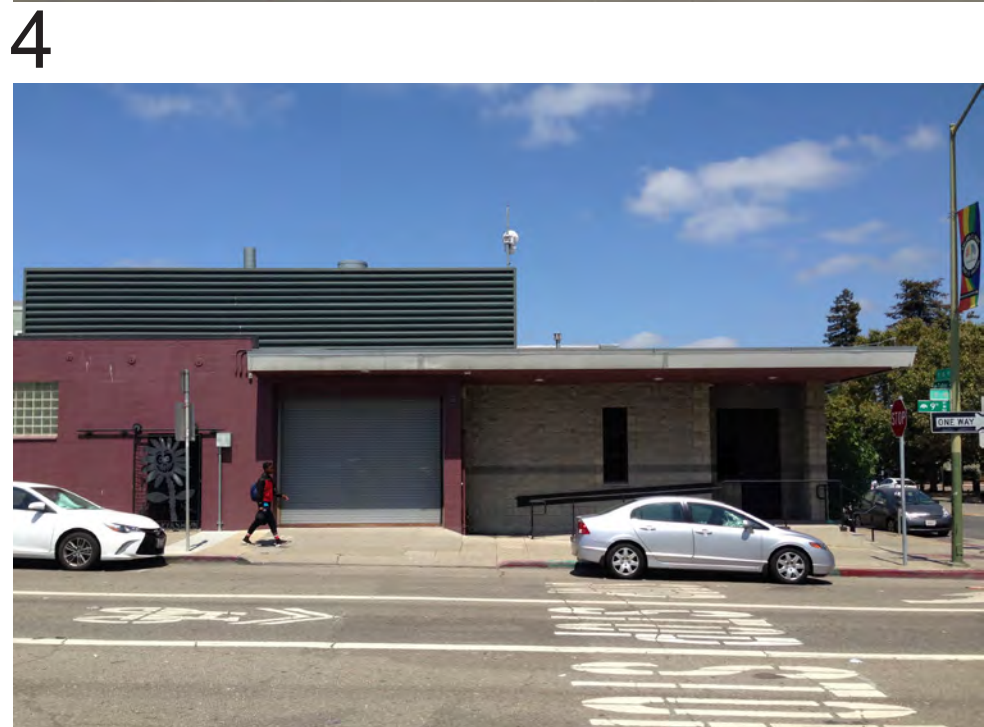
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
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5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/19/2021



DATE:  
SCALE:  
AERIAL VIEW OF SITE



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Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

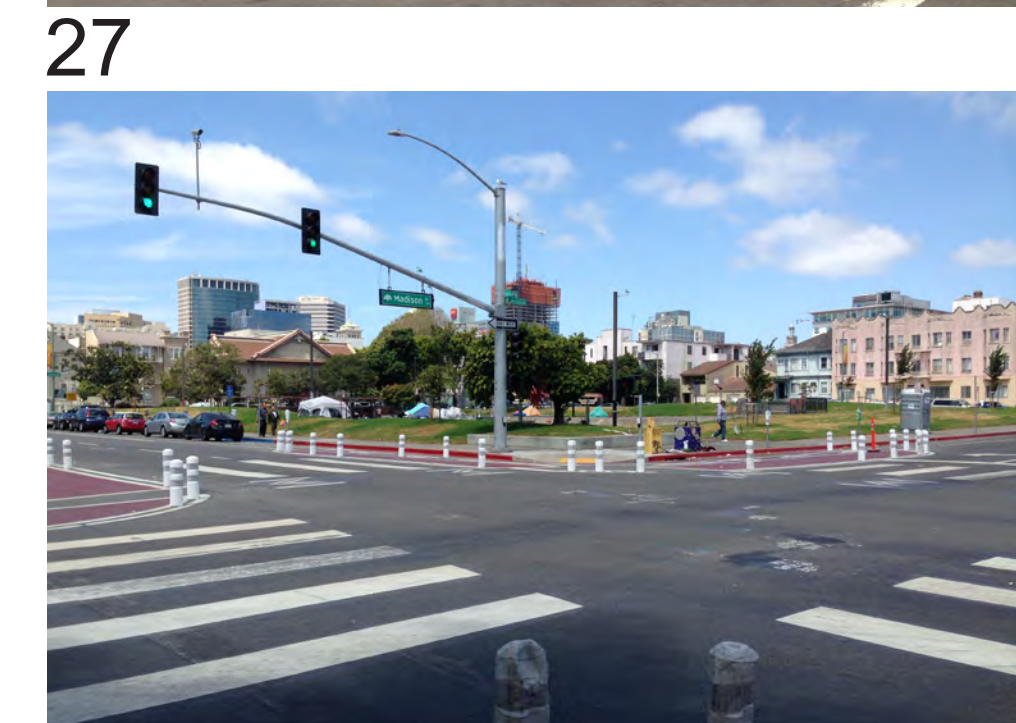
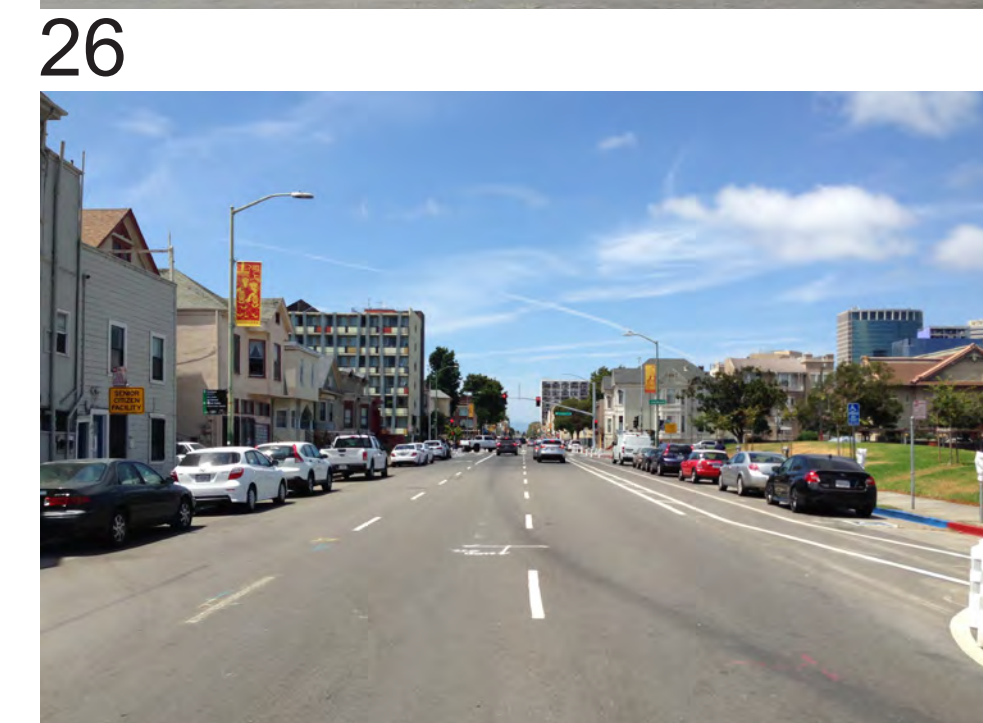
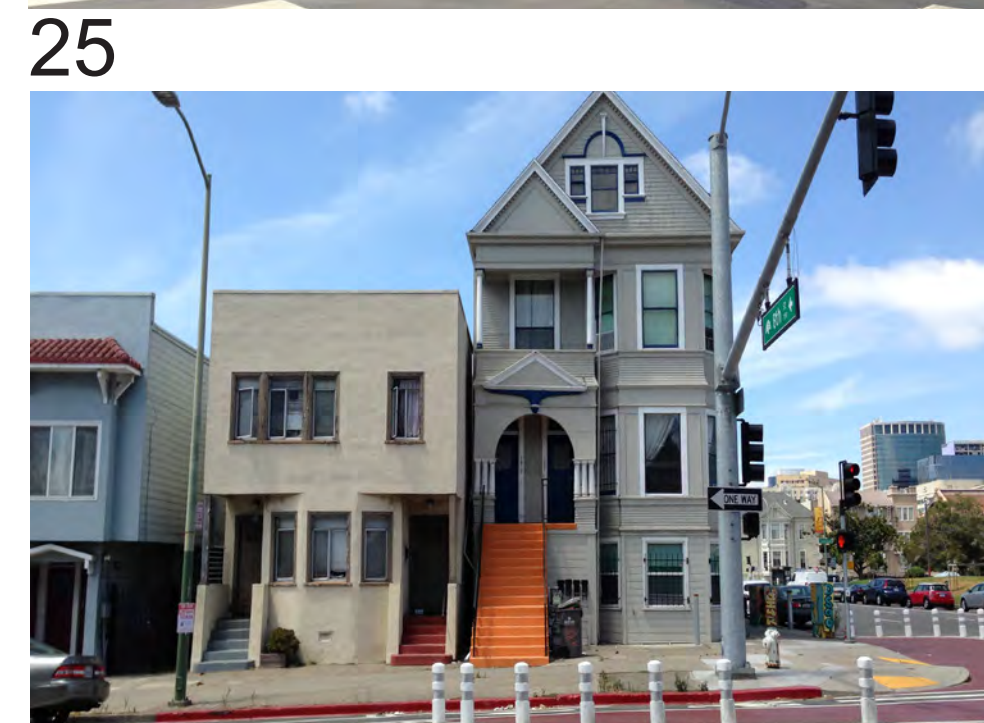
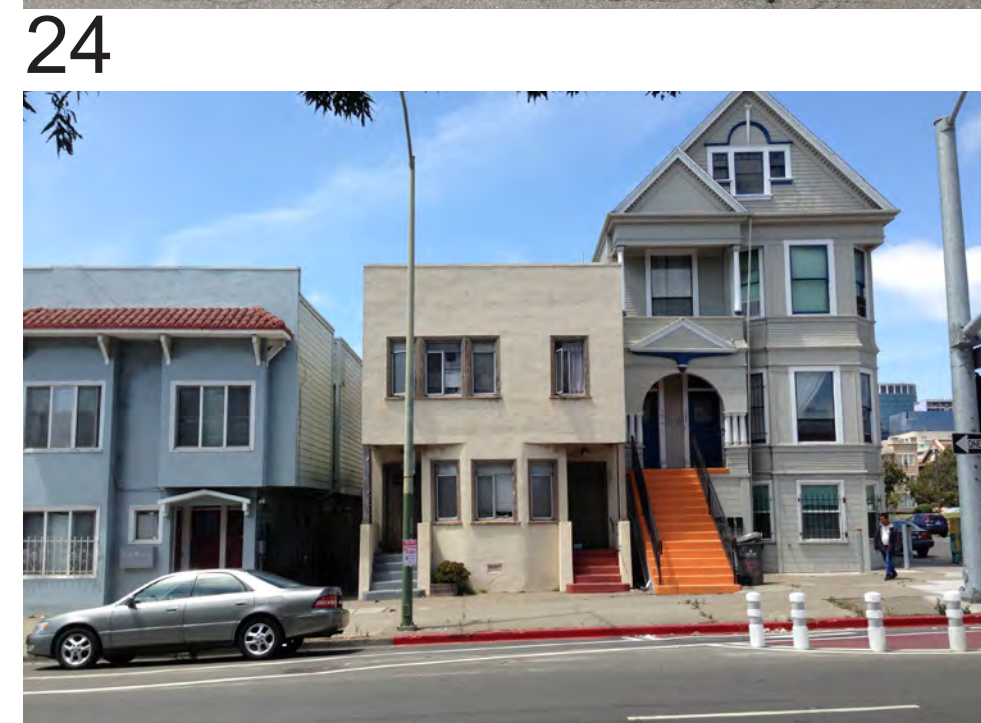
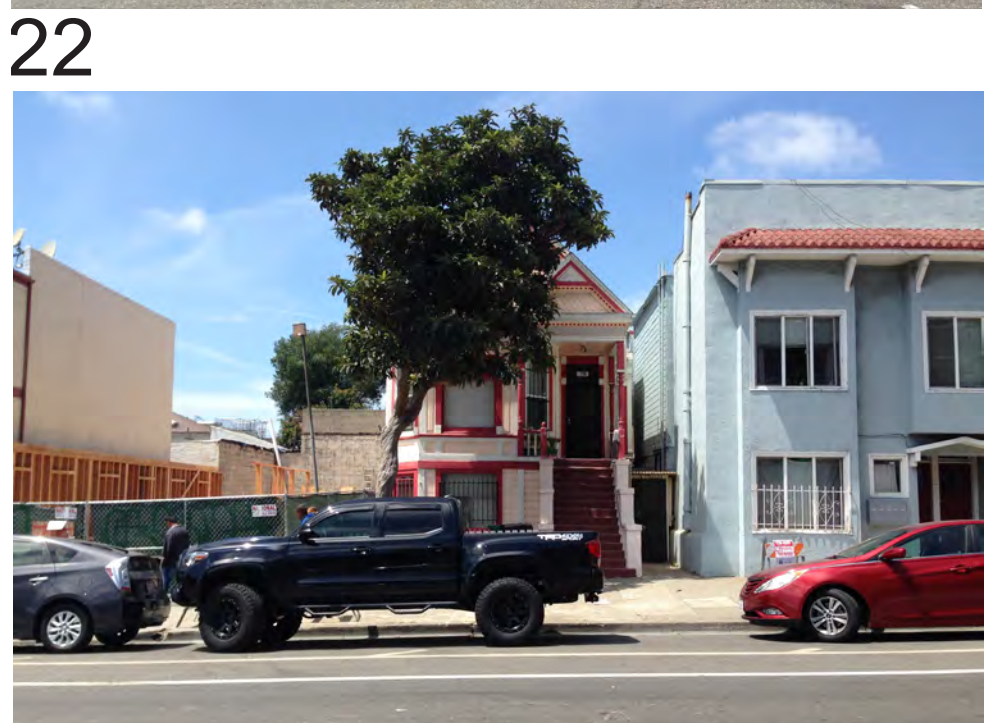
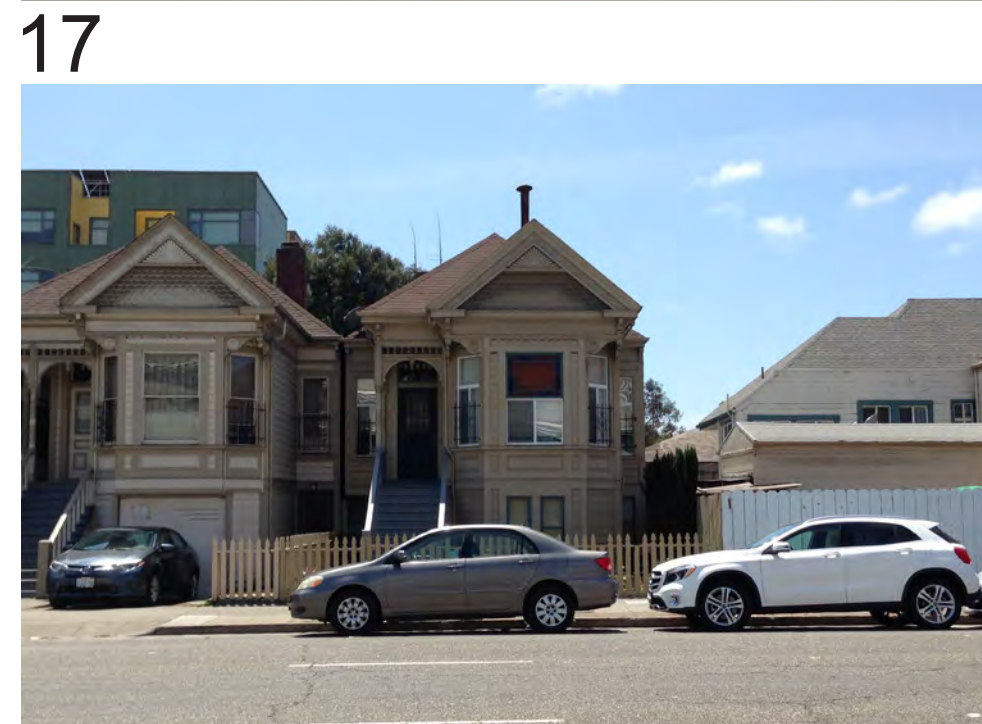
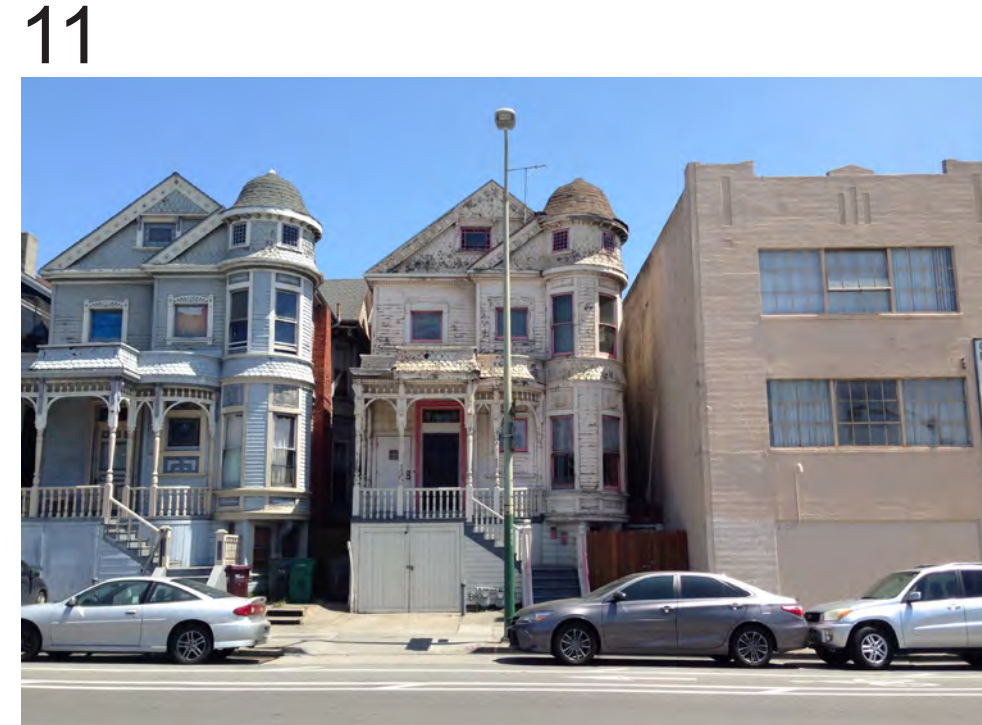
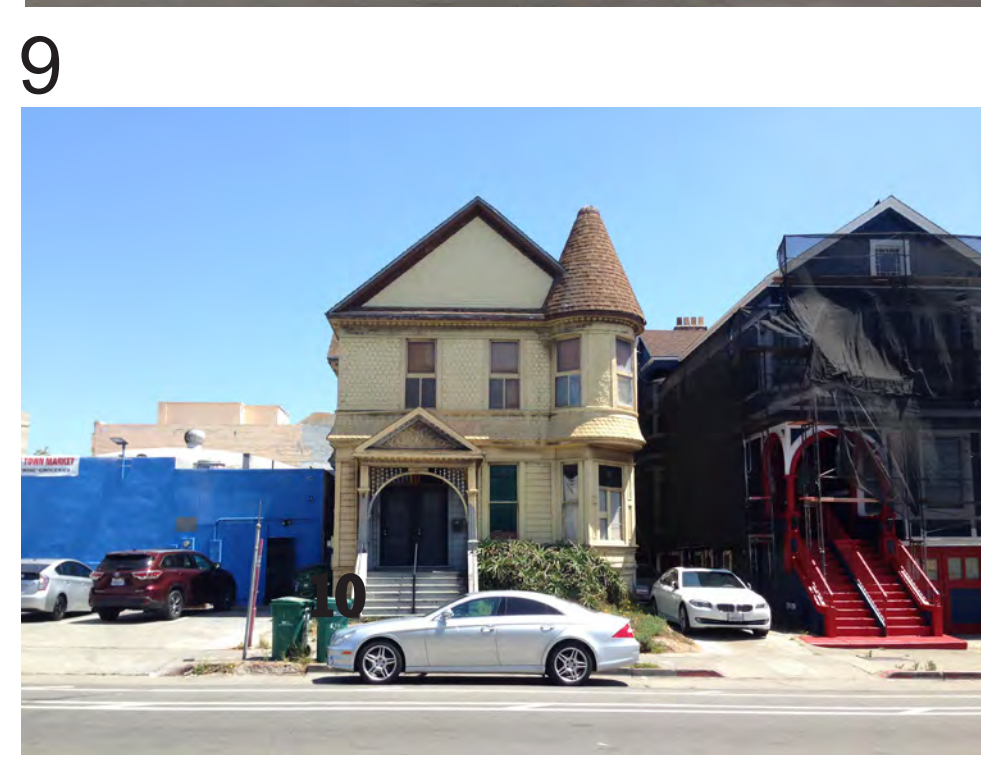
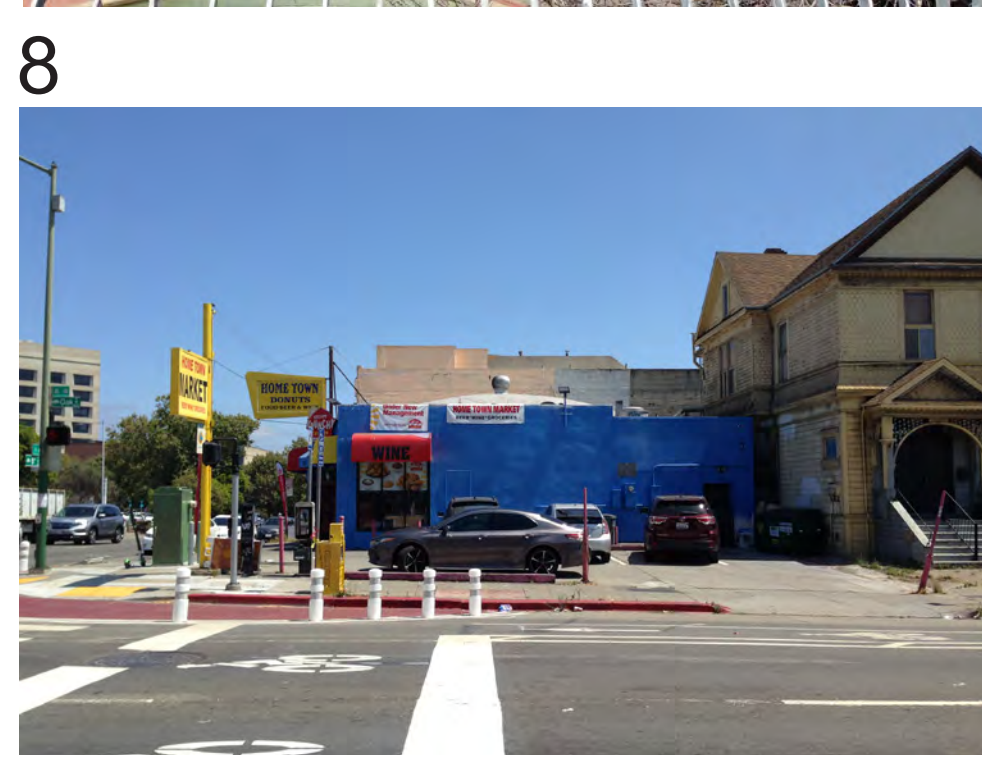
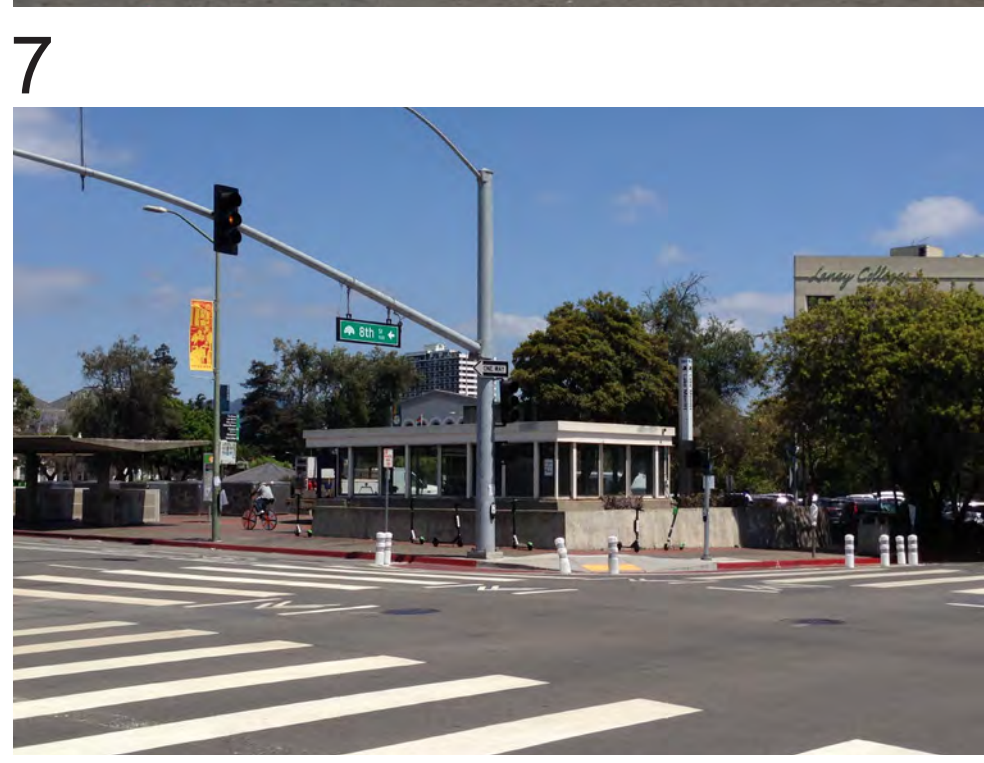
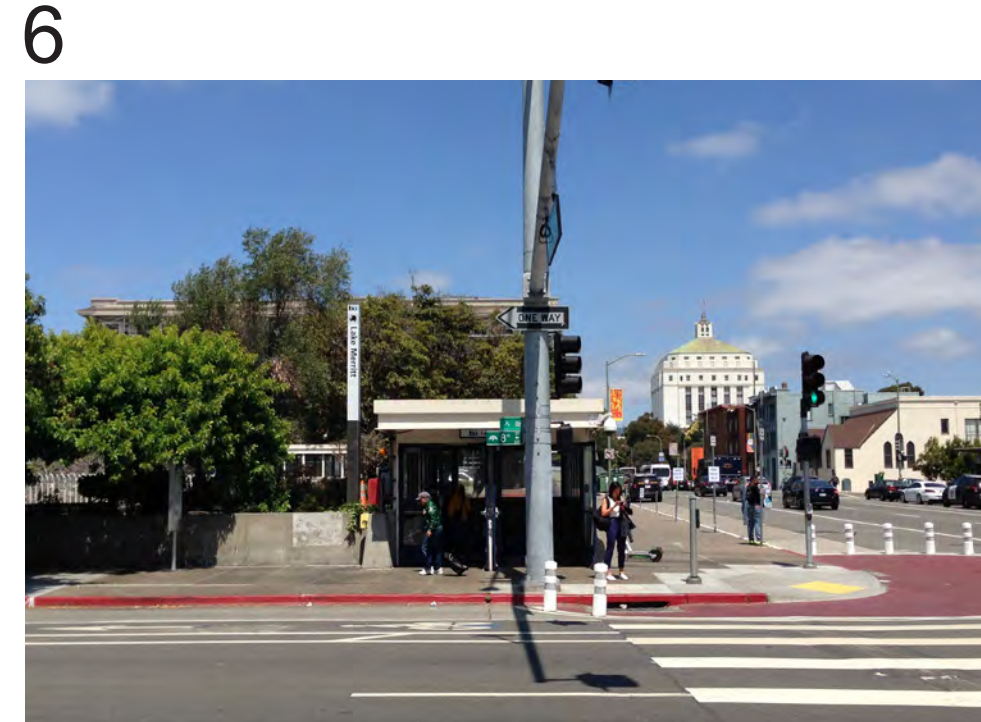
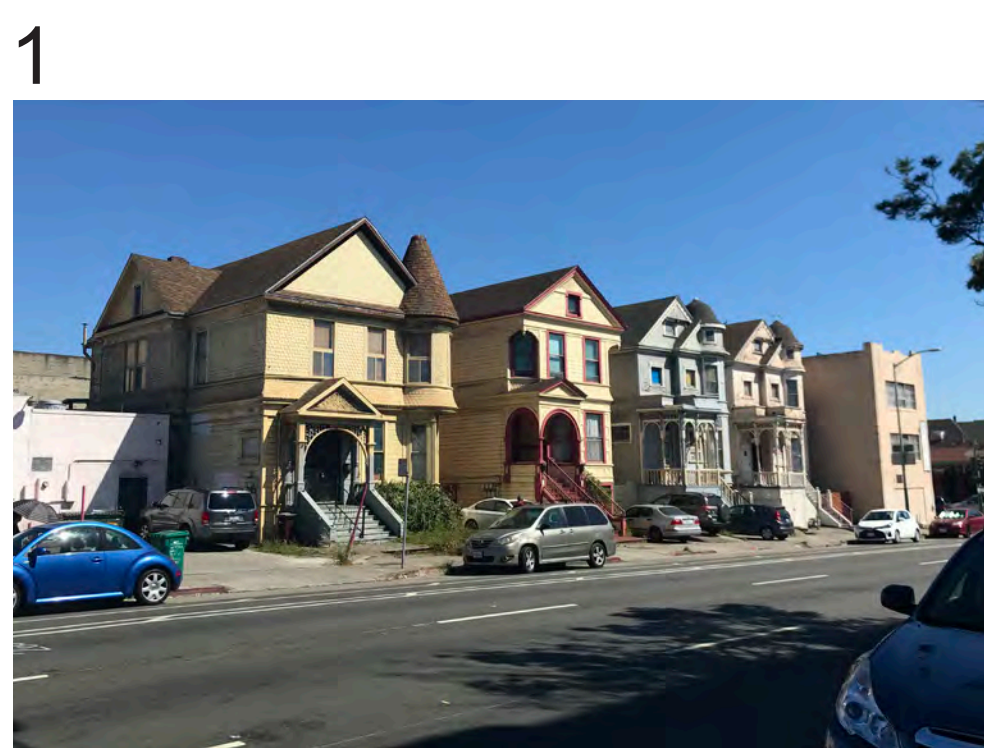
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DATE:  
SCALE:  
SITE PHOTOS

A0.3





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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

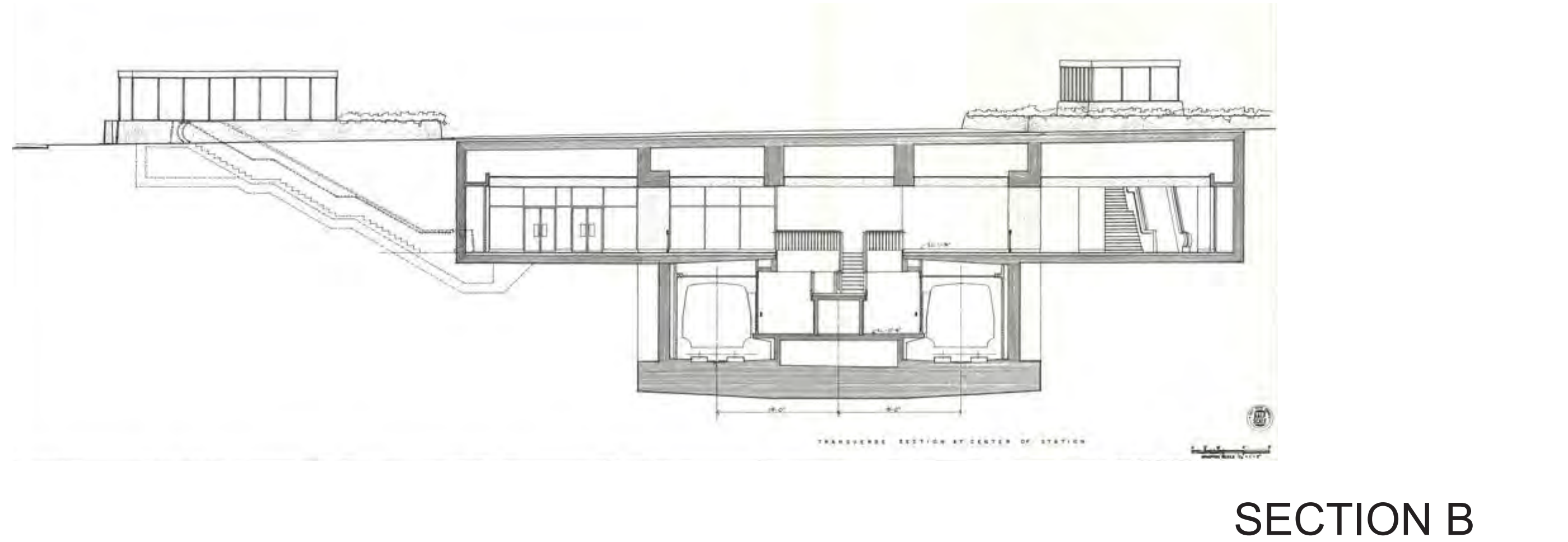
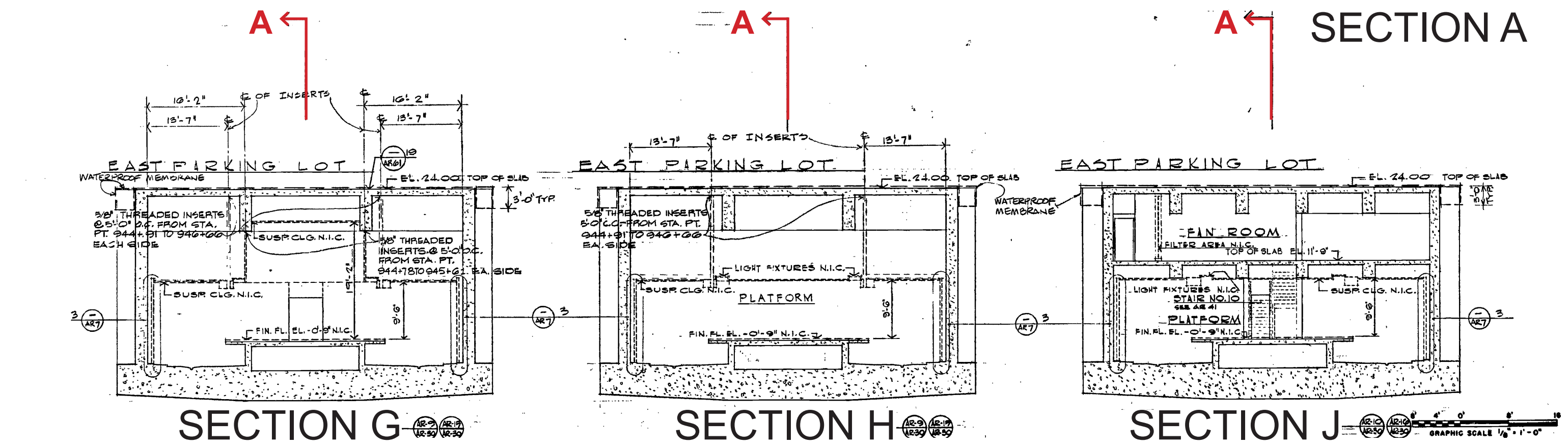
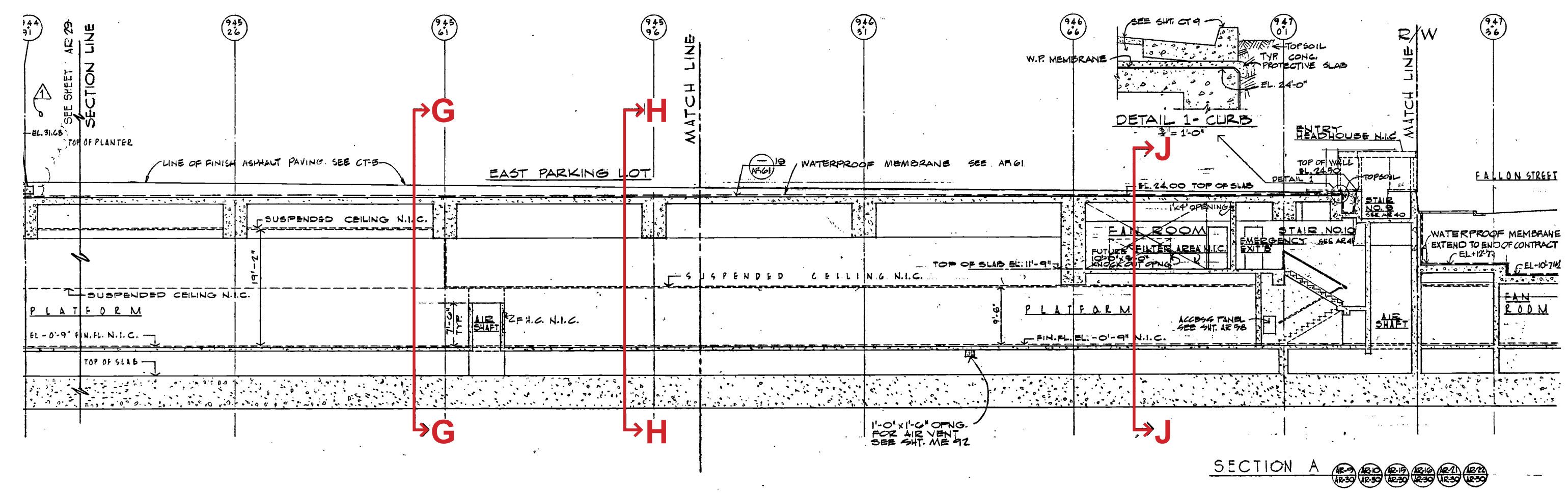
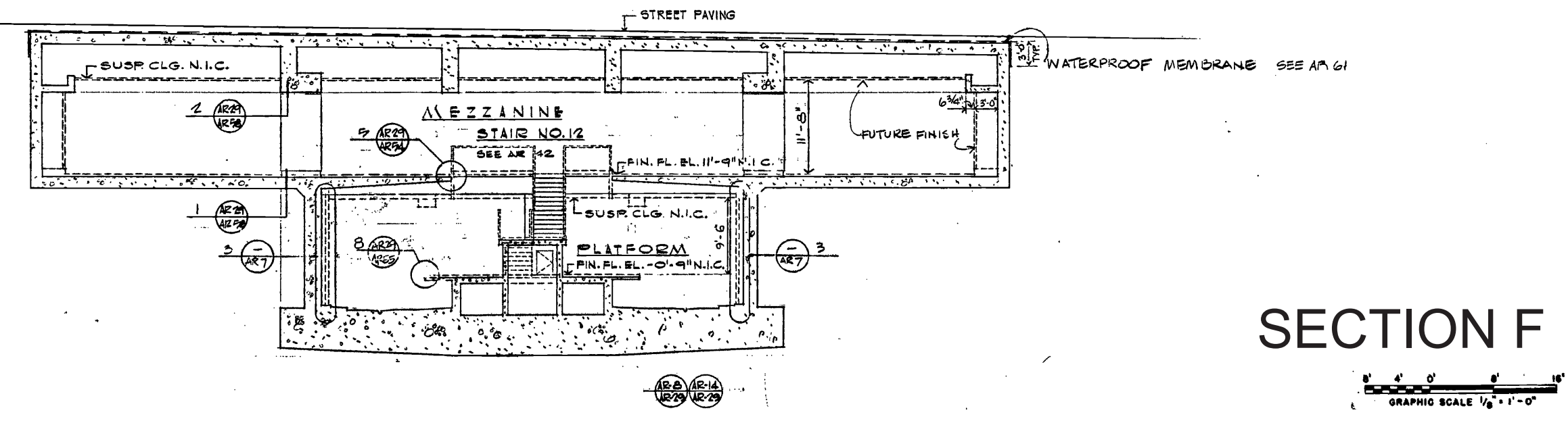
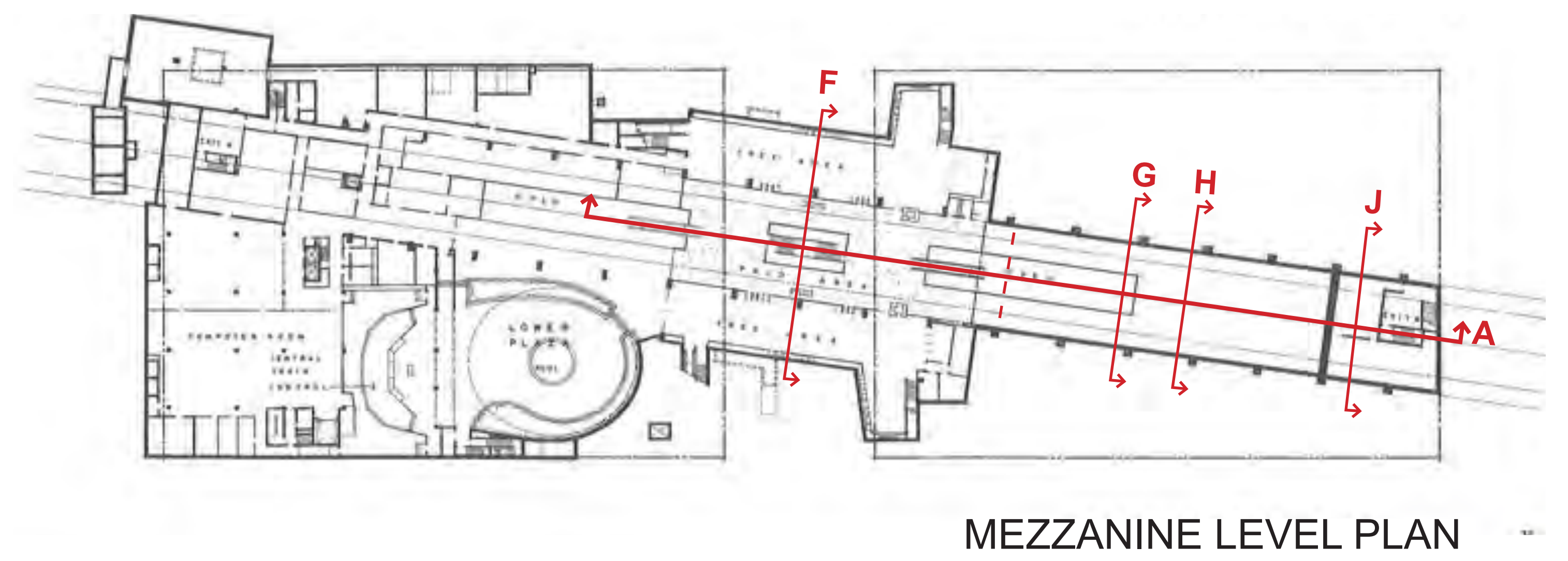
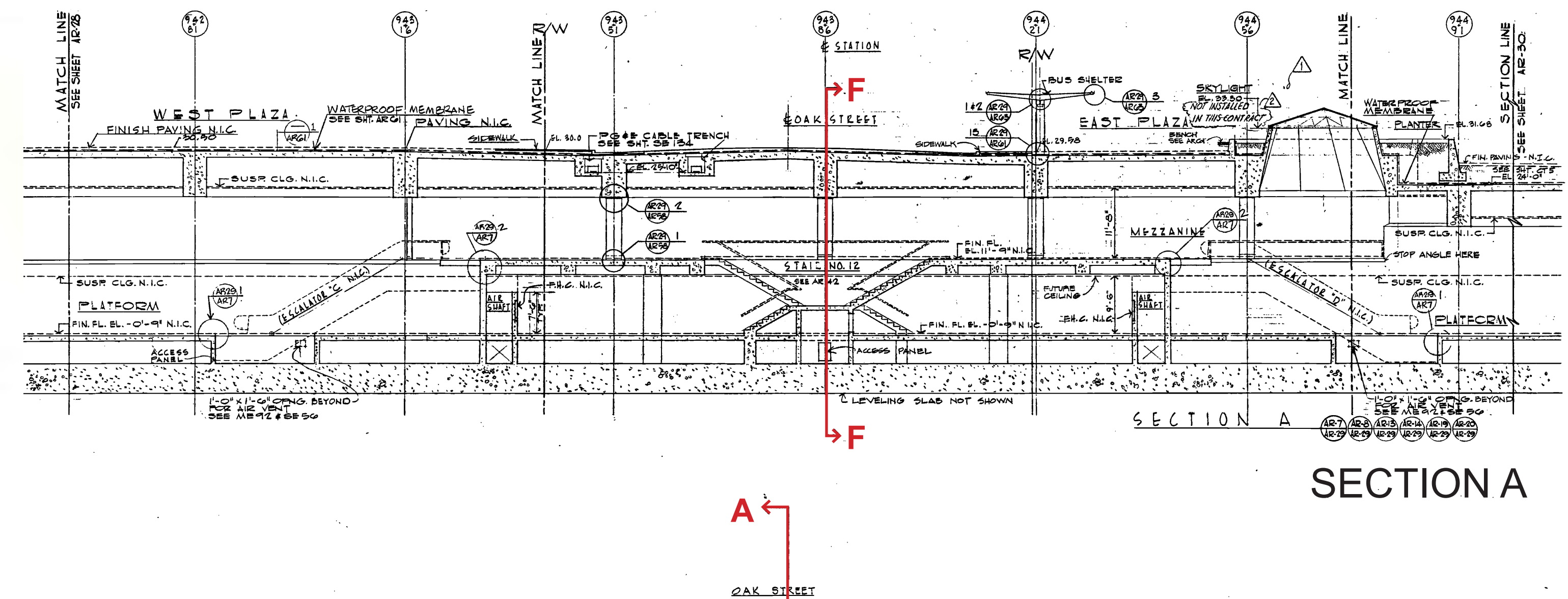
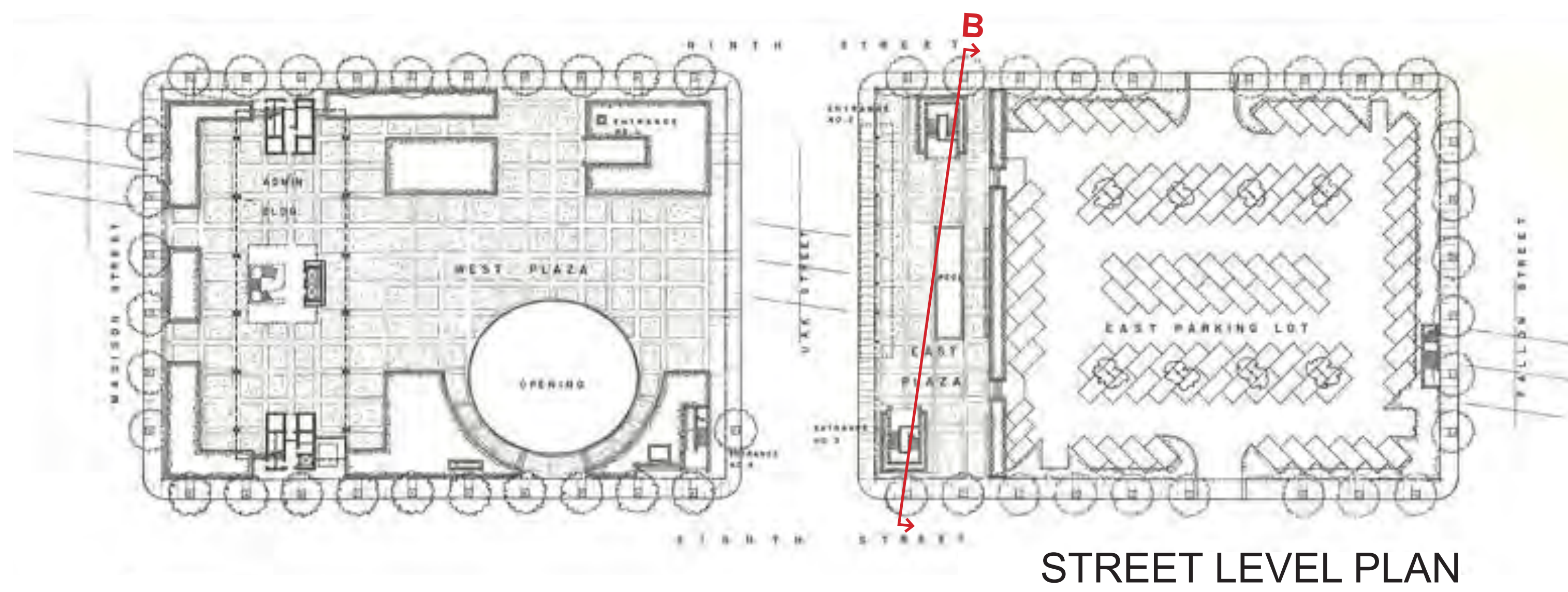
PRELIMINARY - NOT FOR CONSTRUCTION -

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6	REVISED RESPONSE TO POPPS COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPS COMMENTS	03/19/2021

DATE:  
SCALE:  
SITE PHOTOS

**A0.4**



NOTE: As-Built drawings to be replaced with updated survey information when available.

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6	REVISED RESPONSE TO POP'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP'S COMMENTS	03/19/2021

DATE:  
SCALE:  
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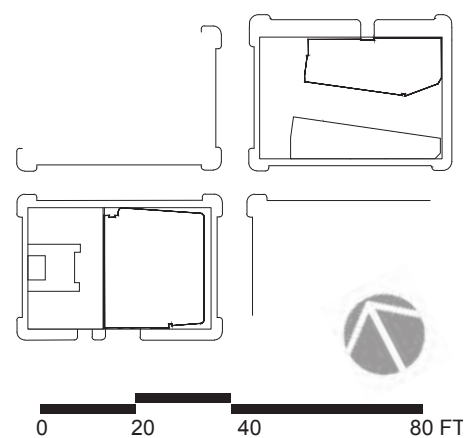
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Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

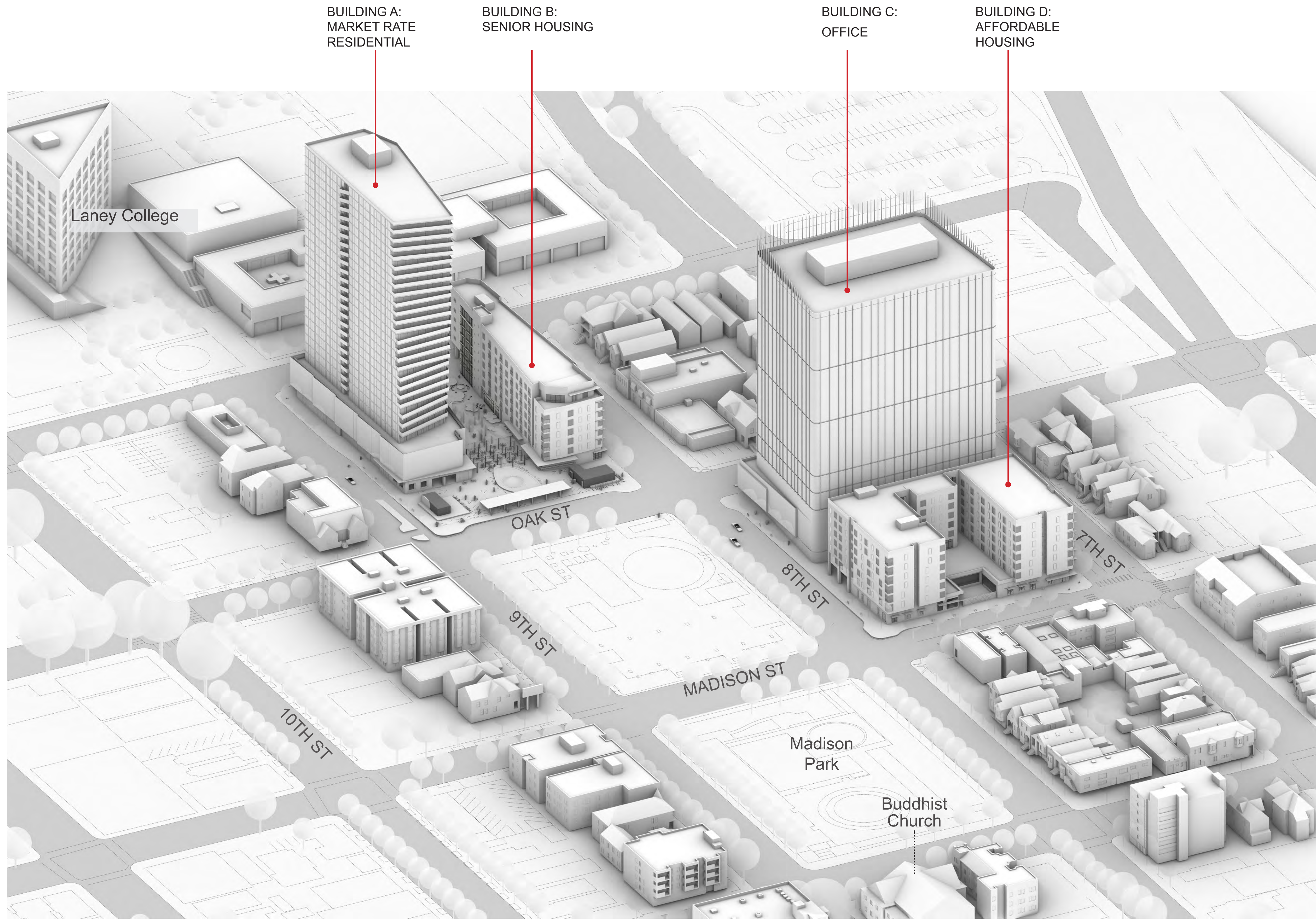
- PRELIMINARY - NOT FOR CONSTRUCTION -

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7	REVISED RESPONSE TO POPK4 COMMENTS	03/19/2021

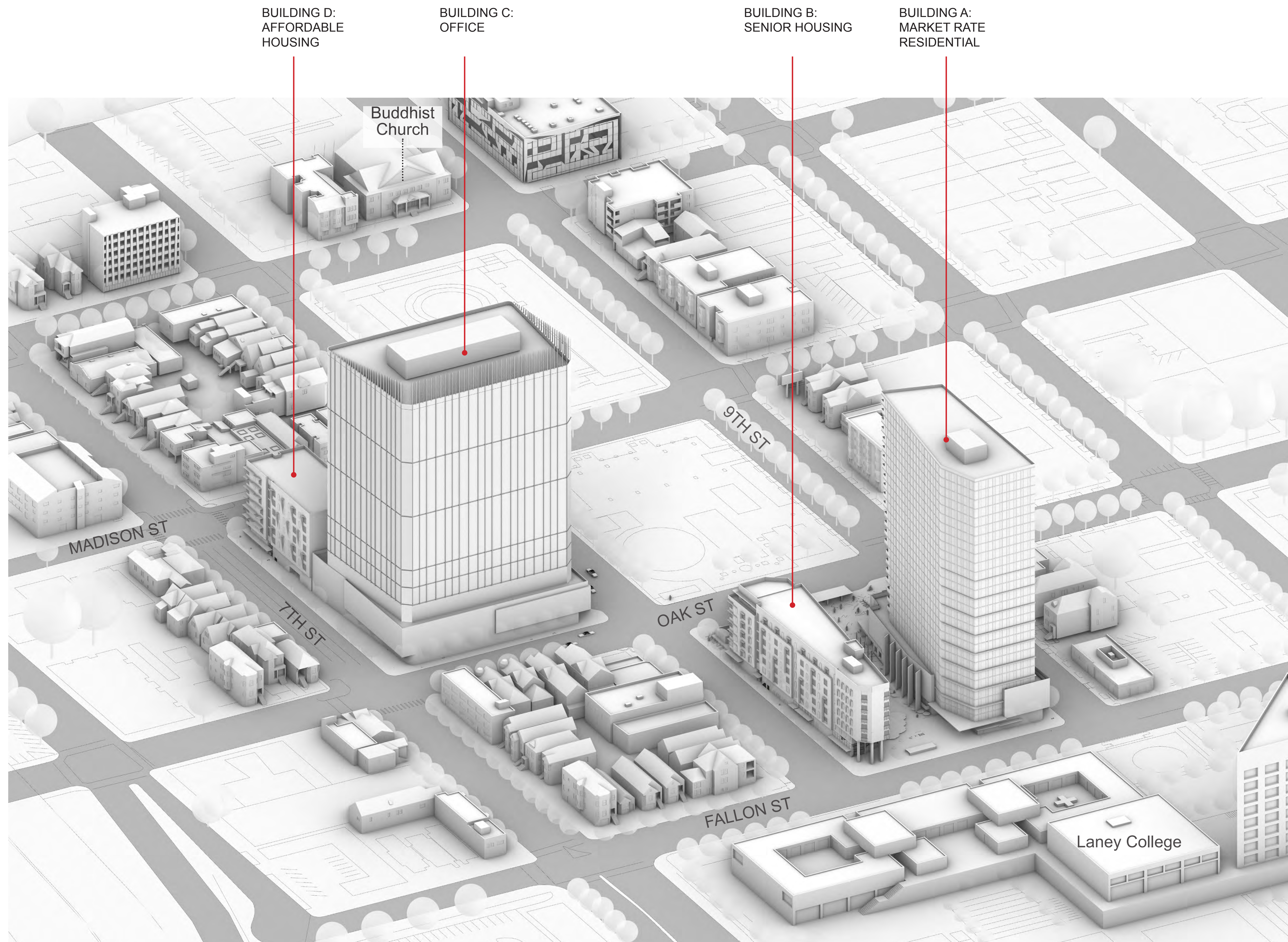


DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")

AXONOMETRIC VIEW OF SITE



**AXONOMETRIC VIEW LOOKING SOUTH-EAST**



BUILDING D:  
AFFORDABLE  
HOUSING

BUILDING C:  
OFFICE

BUILDING B:  
SENIOR HOUSING

BUILDING A:  
MARKET RATE  
RESIDENTIAL

Buddhist  
Church

Laney College

MADISON ST

7TH ST

OAK ST

9TH ST

FALLON ST

AXONOMETRIC VIEW LOOKING NORTH-WEST

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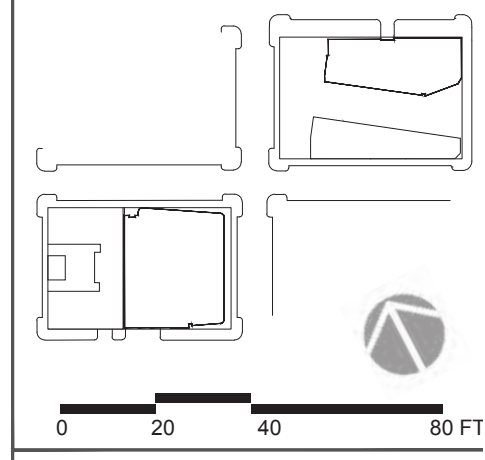
**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
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**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

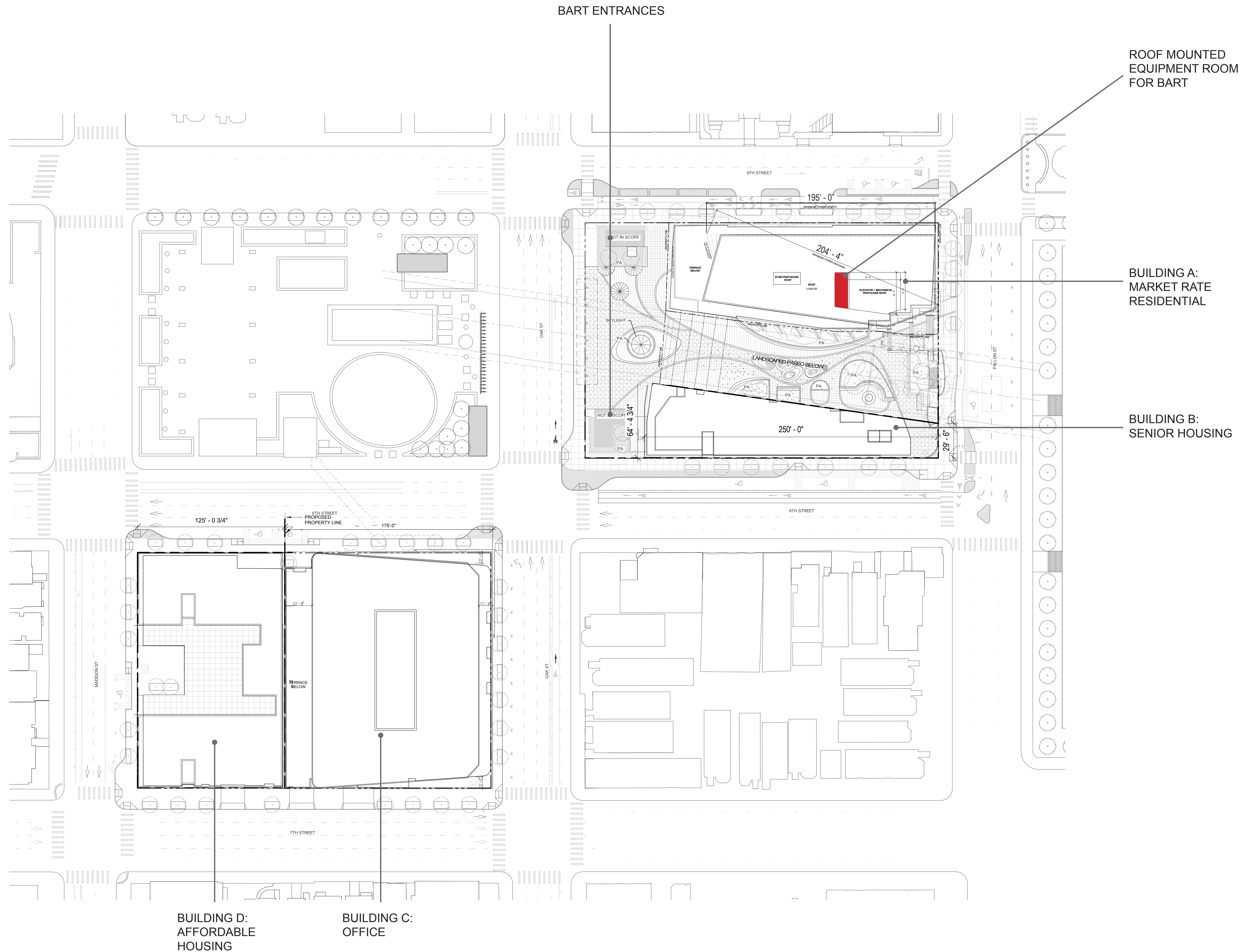
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7	REVISED RESPONSE TO POPPS COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
AXONOMETRIC  
VIEW OF SITE



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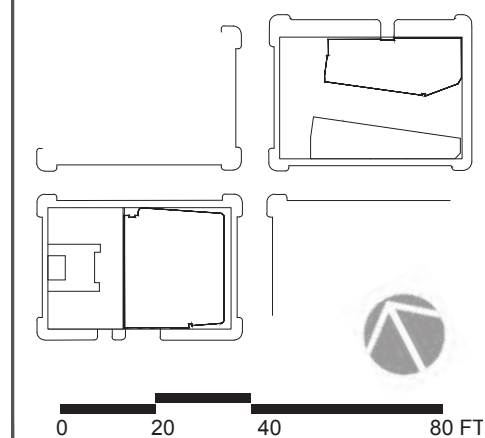
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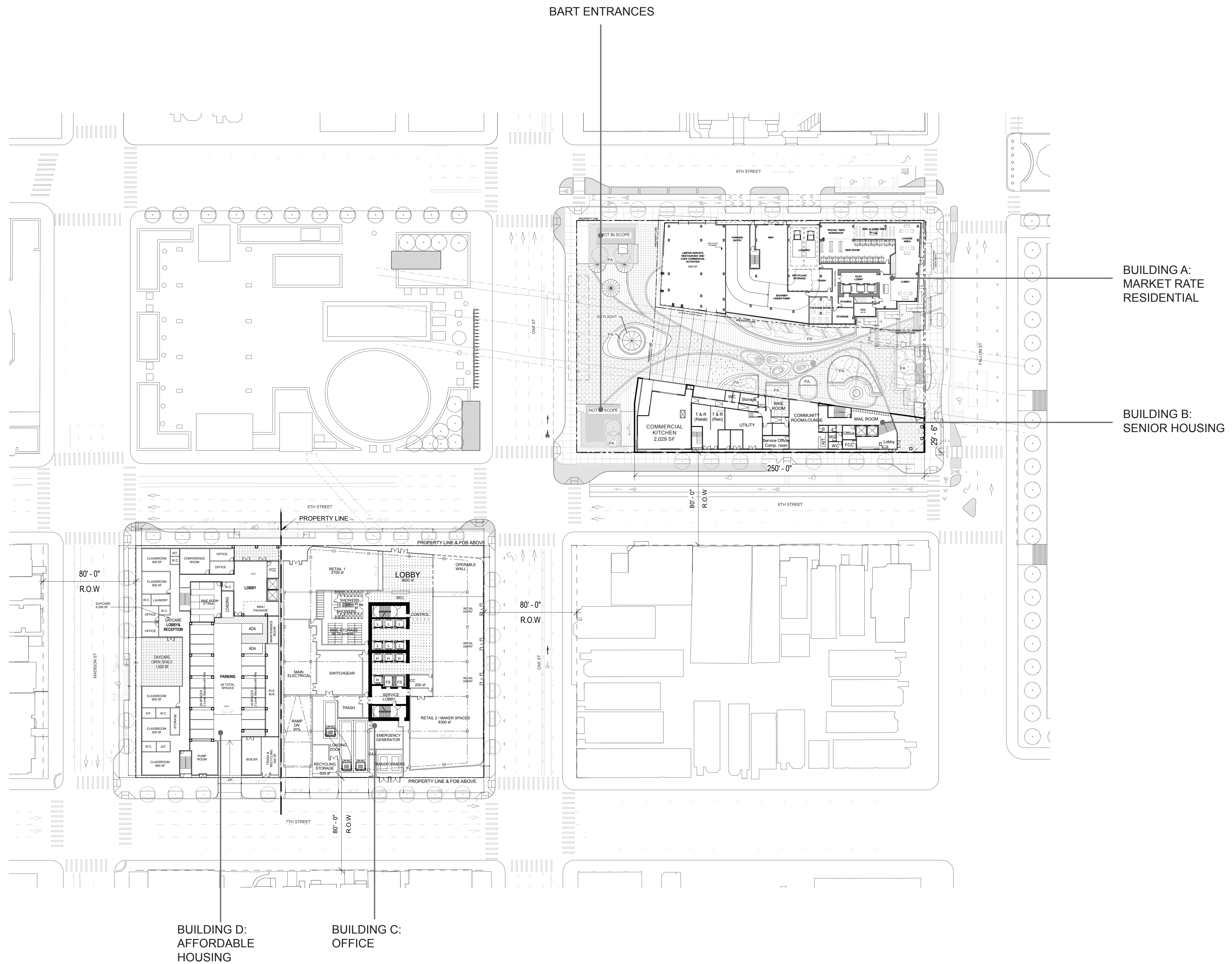
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7	REVISED RESPONSE TO POPZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
**OVERALL SITE PLAN BLOCK 1 & 2**



BUILDING A:  
MARKET RATE  
RESIDENTIAL

BUILDING B:  
SENIOR HOUSING

BUILDING D:  
AFFORDABLE  
HOUSING

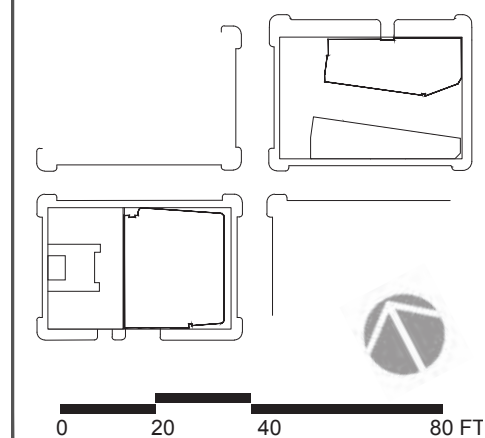
BUILDING C:  
OFFICE

**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

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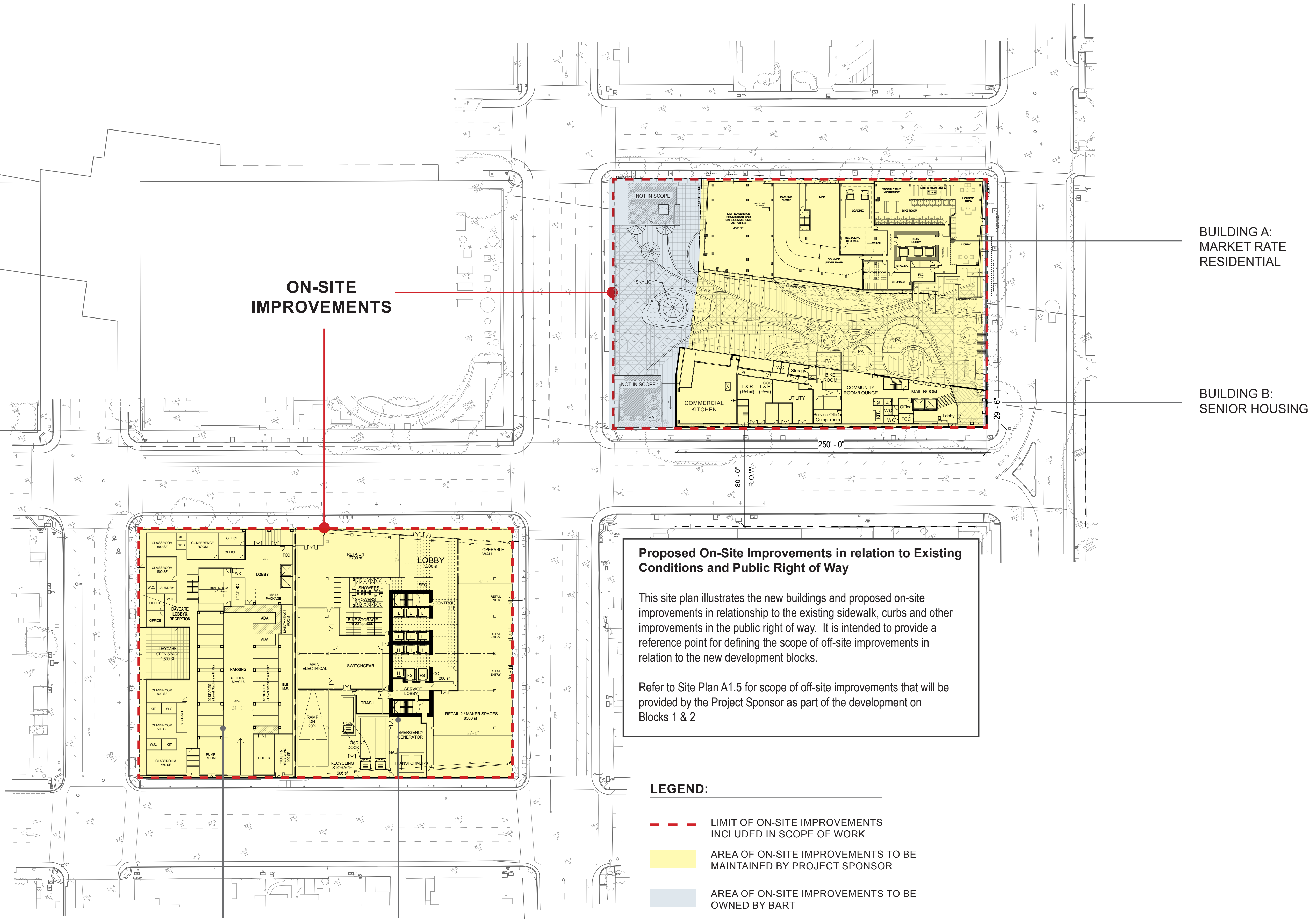
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DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")

**OVERALL  
L1 FLOOR PLAN  
BLOCK 1 & 2**

**A1.3**



**ON-SITE IMPROVEMENTS**

**BUILDING A:  
MARKET RATE  
RESIDENTIAL**

**BUILDING B:  
SENIOR HOUSING**

**BUILDING D:  
AFFORDABLE  
HOUSING**

**BUILDING C:  
OFFICE**

**Proposed On-Site Improvements in relation to Existing Conditions and Public Right of Way**

This site plan illustrates the new buildings and proposed on-site improvements in relationship to the existing sidewalk, curbs and other improvements in the public right of way. It is intended to provide a reference point for defining the scope of off-site improvements in relation to the new development blocks.

Refer to Site Plan A1.5 for scope of off-site improvements that will be provided by the Project Sponsor as part of the development on Blocks 1 & 2

**LEGEND:**

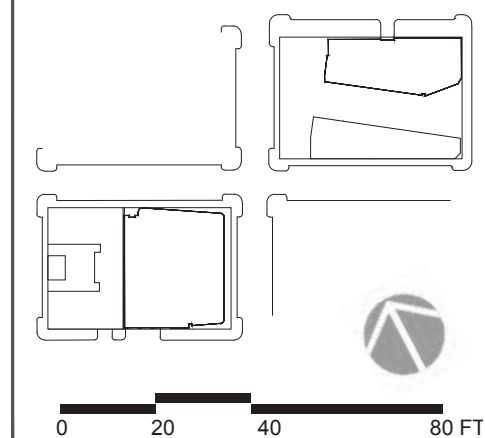
- - - LIMIT OF ON-SITE IMPROVEMENTS INCLUDED IN SCOPE OF WORK
- AREA OF ON-SITE IMPROVEMENTS TO BE MAINTAINED BY PROJECT SPONSOR
- AREA OF ON-SITE IMPROVEMENTS TO BE OWNED BY BART

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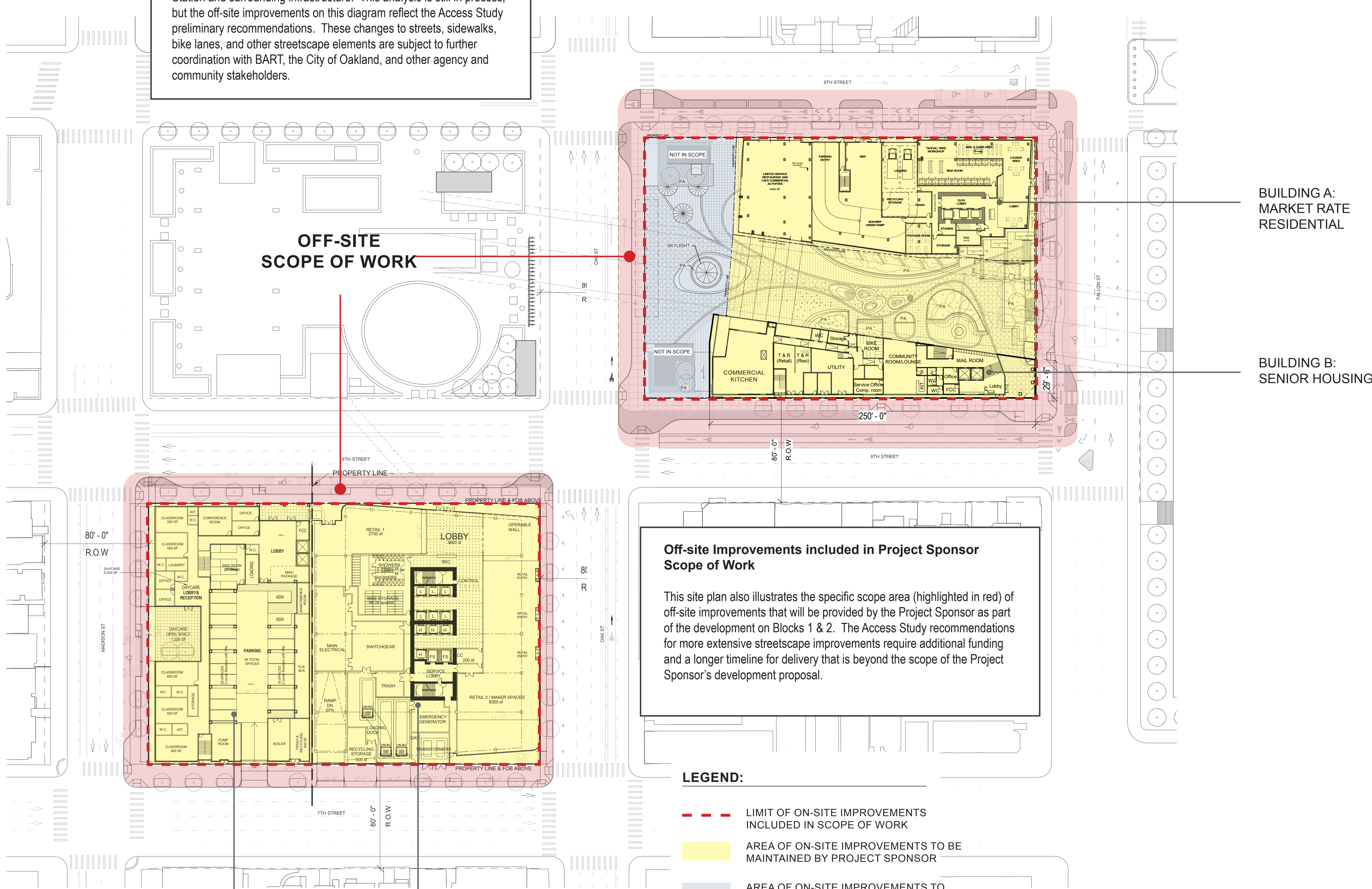
DATE:  
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**OVERALL L1 FLOOR PLAN BLOCK 1 & 2 ON-SITE IMPROVEMENT**

**A1.4**

**Preliminary BART Access Study Proposed Off-Site Improvements**

BART is conducting an analysis of access to the Lake Merritt BART Station and surrounding infrastructure. This analysis is still in process, but the off-site improvements on this diagram reflect the Access Study preliminary recommendations. These changes to streets, sidewalks, bike lanes, and other streetscape elements are subject to further coordination with BART, the City of Oakland, and other agency and community stakeholders.



**Off-site Improvements included in Project Sponsor Scope of Work**

This site plan also illustrates the specific scope area (highlighted in red) of off-site improvements that will be provided by the Project Sponsor as part of the development on Blocks 1 & 2. The Access Study recommendations for more extensive streetscape improvements require additional funding and a longer timeline for delivery that is beyond the scope of the Project Sponsor's development proposal.

- LEGEND:**
- - - LIMIT OF ON-SITE IMPROVEMENTS INCLUDED IN SCOPE OF WORK
  - AREA OF ON-SITE IMPROVEMENTS TO BE MAINTAINED BY PROJECT SPONSOR
  - AREA OF ON-SITE IMPROVEMENTS TO BE OWNED BY BART
  - AREA OF OFF-SITE IMPROVEMENTS INCLUDED IN SCOPE OF WORK

BUILDING D:  
AFFORDABLE HOUSING

BUILDING C:  
OFFICE

BUILDING A:  
MARKET RATE RESIDENTIAL

BUILDING B:  
SENIOR HOUSING

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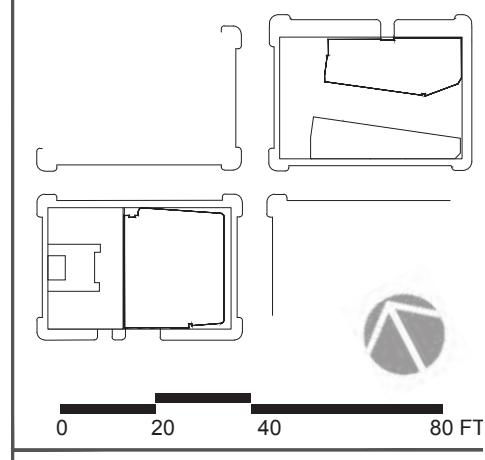
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

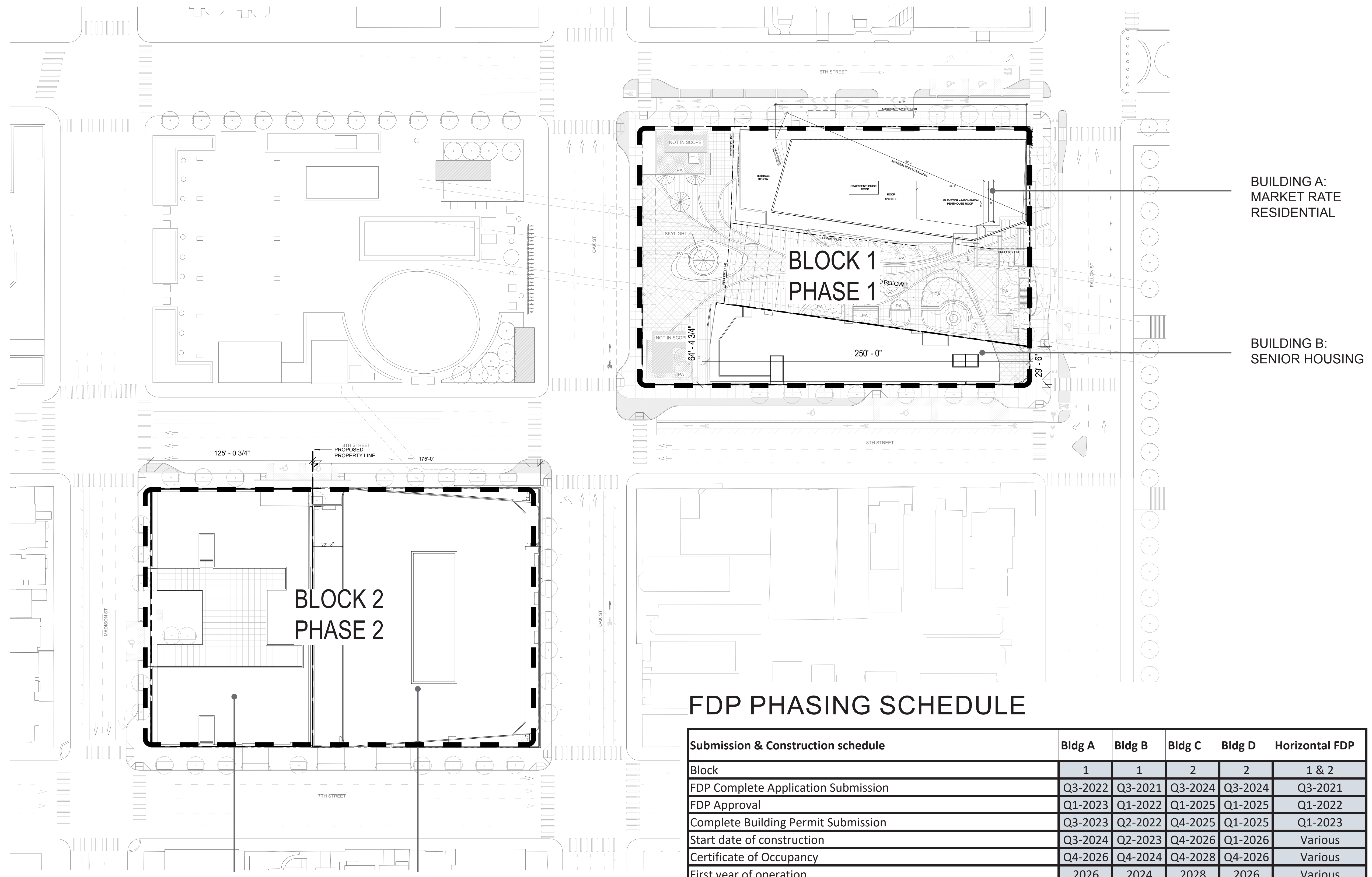
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#2 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/19/2021



**OVERALL L1 FLOOR PLAN BLOCK 1 & 2 OFF-SITE SCOPE OF WORK**

**A1.5**





**FDP PHASING SCHEDULE**

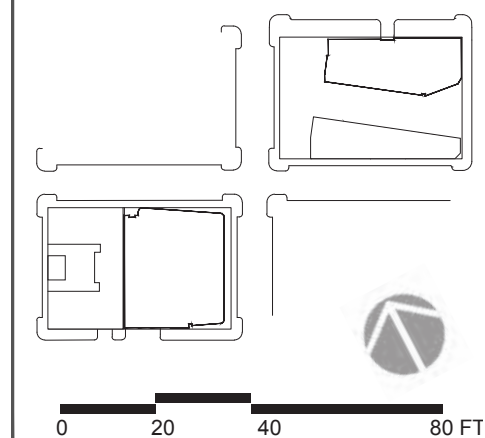
Submission & Construction schedule	Bldg A	Bldg B	Bldg C	Bldg D	Horizontal FDP
Block	1	1	2	2	1 & 2
FDP Complete Application Submission	Q3-2022	Q3-2021	Q3-2024	Q3-2024	Q3-2021
FDP Approval	Q1-2023	Q1-2022	Q1-2025	Q1-2025	Q1-2022
Complete Building Permit Submission	Q3-2023	Q2-2022	Q4-2025	Q1-2025	Q1-2023
Start date of construction	Q3-2024	Q2-2023	Q4-2026	Q1-2026	Various
Certificate of Occupancy	Q4-2026	Q4-2024	Q4-2028	Q4-2026	Various
First year of operation	2026	2024	2028	2026	Various
<ul style="list-style-type: none"> <li>Allow developers two years to submit their Final Development Plan (FDP) after PUD approval (Section 17.140.040)</li> <li>Block 2 construction cannot start until BART Police vacates the premise, and critical BART infrastructure is moved Dec-2025 at the earliest.</li> <li>Assumes PUD/PDP Planning Commission May-2021</li> </ul>					

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

REVISION SCHEDULE

NO.	ISSUE	DATE
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2	RESPONSES TO COMMENTS FROM BART	1/20/2020
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4	REVISED RESPONSE TO POPR COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPRZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPR3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPR4 COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 40' (VIEWED AT 24" X 36")

**PHASING PLAN & FDP PHASING SCHEDULE**

**LAKE MERRITT BART DEVELOPMENT SUMMARY**

updated 04/27/21

Project Site and Shape														
	Block 1				Block 2				TOTAL					
Area	~ 60,000 SF (1.38 Acres)				~ 60,000 SF (1.38 Acres)				2.75 ACRES					
Maximum Length and Width	300 F X 200 F				300 F X 200 F									
Existing Uses to Be Removed														
	Block 1				Block 2				TOTAL					
Office Space	N/A				Office Space: 103,296									
Parking Space	Parking Spaces: 132				Parking Spaces: 82									
Proposed Land Use Program														
	Block 1			Block 2			TOTAL							
	Building A	Building B	Subtotal	Building C	Building D	Subtotal								
Residential (including amenity spaces, circulation and support )	326,055	69,276	395,331		101,703	101,703	497,034							
Residential Parking and associated service areas)	53,500	3,426	56,926		10,850	10,850	67,776							
Office (including circulation and support)	-	-	-	495,333		495,333	495,333							
Day Care	-	-	-		6,200	6,200	6,200							
Commercial Uses:														
Limited-Service Restaurant and Café	4,500	963	5,463				5,463							
Commercial Kitchen		2,029	2,029				2,029							
Retail	-	-	-	11,000		11,000	11,000							
Residential/Commercial Parking & Service	1,100	450	1,550	21,767		21,767	23,317							
<b>Total Building Area</b>	<b>385,155</b>	<b>76,144</b>	<b>461,299</b>	<b>528,100</b>	<b>118,753</b>	<b>646,853</b>	<b>1,108,152</b>							
Proposed Dwelling Units														
	Block 1				Block 2				TOTAL					
	Building A		Building B		Subtotal		Building C		Building D		Subtotal		#	%
	#	%	#	%	#	%	#	%	#	%	#	%	#	%
Studio	108	30%	0	0%	108	24%			18	18%	18	18%	126	23%
1-Bedroom	180	50%	92	95%	272	60%			30	30%	30	30%	302	54%
2-Bedroom	72	20%	5	5%	77	17%			23	23%	23	23%	100	18%
3-Bedroom	0	0%	0	0%	0	0%			29	29%	29	29%	29	5%
<b>Total Dwelling Units</b>	<b>360</b>	<b>100%</b>	<b>97</b>	<b>100%</b>	<b>457</b>	<b>100%</b>			<b>100</b>	<b>100%</b>	<b>100</b>	<b>100%</b>	<b>557</b>	<b>100%</b>
Proposed Residential Density per Block														
	Block 1				Block 2									
*Dwelling per Acre	644				72									
*Note: For density calculation for Block 1, the BART plaza and the Paseo is excluded from the lot area.														
Proposed Parking														
	Block 1			Block 2			TOTAL							
	Building A	Building B	Subtotal	Building C	Building D	Subtotal								
Vehicle Parking Space (Total)	105		105	254	49	303	408							
**Car Share Spaces (Included in total)	2	1	3	(Not Required)	1	1	4							
Bicycle Parking			0			-	0							
Bicycle Parking Long-term	92	14	106	54	27	81	187							
Bicycle Parking Short-term	21	7	28	28	7	35	63							
Total Bicycle parking	113	21	134	82	34	116	250							
**Note: Car share spaces for building B is located in building A garage and for building D is located in building C garage.														
Open Space														
	Block 1			Block 2			TOTAL							
	Building A	Building B	Total Block 1	Building C	Building D	Subtotal								
Publicly Accessible Open Space:														
A. BART Plaza (publicly owned)			11,610											
B. Paseo @ Block 1 (separate parcel)	12,609	3,152	15,761											
C. Publicly Accessible Open Space on Building Parcel	305	***	305	-	-	-								
Total Publicly Accessible Open Space			27,676			-	27,676							
Group Useable Open Space	7,990	1,940	9,930	-	6,800	6,800	16,730							
Private Useable Open Space	12,900		12,900	-	-	-	12,900							
<b>Total Open Space</b>	<b>33,804</b>	<b>5,092</b>	<b>50,506</b>	<b>-</b>	<b>6,800</b>	<b>6,800</b>	<b>57,306</b>							
***Note: Covered open space is excluded from "Publicly Accessible Open Space" calculations.														
Building Characteristics														
	Block 1			Block 2			AVERAGE							
	Building A	Building B	Average	Building C	Building D	Average								
Stories	28	7		19	7									
Height	275'	83'		275'	83'									



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# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPKZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021

DATE:  
SCALE:  
DEVELOPMENT SUMMARY

**LAKE MERRITT BART DEVELOPMENT SUMMARY**

**PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING B (SENIOR BUILDING)**

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 04/29/2021

Property Development Standards						
Code Section for D-LM-2	17.101G.03	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
min lot width		25 ft	29.5 ft	YES	placement of imaginary lot line south of 9th, TBD	
min lot frontage		25 ft	250.0 ft	YES		
min lot area		4,000 sf	11,695 sf	YES		
min front setback		0 ft	0 ft	YES		
max front and street side setback for the first story		5 ft	2' ft	YES	8th Street is considered as the Principal street.	
max front and street side setback for 2/3 story or 35'		5 ft	0 ft	YES		
min interior side setback		0 ft	0 ft	YES		
min corner side setback		0 ft	22 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G	
min rear setback		0 ft	0 ft	YES		
average min setback from Lake Merritt Estuary Channel		60 ft		YES		
Ground floor commercial façade transparency		65 %	65 %	YES	minimum requirement to be detailed at FDP	
min height of ground floor non-residential facilities		15 ft	15 ft	YES		
min width of storefront		15 ft	15 ft	YES		
Use						
Code Section for D-LM-2 & LM-275		REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Permanent Residential Activity	17.101G.03			YES		
Limited-Service Restaurant and Café (Commercial Activities)	17.101G.03			YES		
Commercial Kitchen (Custom Manufacturing)	17.142.100.B.4			YES	Project will employ the PUD Bonus for Additional Permitted Activities, Commercial Kitchen (Custom Manufacturing) (17.142.100.B)	
Height, Density, Bulk & Tower Regulations						
Code Section for LM-275	17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
building base max height		55 (85' W/ CUP) ft	85 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G	
max height		275 ft	85 ft	YES		
max residential density		110 SF/unit	120 SF/unit	YES	11,695 SF Lot Area/97 Unit	
max non residential intensity (FAR)		12	0.29	YES	3,442 SF Retail area / 11,695 SF Lot Area	
Usable Open Space Standards						
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
total number of units	97					
open space requirements (SF)	38 SF/DU	97X38= 3686 SF	5,092 SF	YES	The total open space consists of 20% of paseo plus L7 shared balcony, and the community rooms	
Required Dimensions of Usable Open Space						
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Private		10 ft on G floor	N/A			
Public Ground-Floor Plaza		10 ft	65 ft	YES	the whole width of the paseo	
Rooftop		15 ft	15 ft	YES		
Courtyard		15 ft	N/A ft			
Off-site open space		10 ft	N/A ft			
Community room		250 SF	1,690 SF	YES		
Off Street Parking						
Code Section	17.116.060-080	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Residential (D-LM zones)	No minimum parking requirement	-	-	YES		
Food service	0 for less than 10,000 SF	-	-	YES	Food service area = 3,442 SF	
Code Section	17.116.110	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Car Share Space	One space for 5-100 units	1	1	YES	1 Car share space for Bldg. B will be provided in Bldg. A garage	
Off Street Loading						
Code Section	17.116.120-140	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Residential	Total Floor Area 50,000-149,999 SF	1 space	-	NO	*Total floor area = 76,209 SF Variance required for Bldg-B loading. Residential loading proposed on-street.	
Retail/ Food Service	0 for less than 10,000 SF	0 space			Food service area = 3,442 SF	
Bicycle Parking						
Code Section	17.117	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
long term: units with parking space	0.1	space per DU	10		for 97 DU	
short term: units with parking space	0.05	space per DU	5	YES		
long term: restaurant/café	1	space per 12,000 SF (MIN. 2)	2		Food service area = 3,442 SF	
long term: Commercial Kitchen	1	space per 12,000 SF (MIN. 2)	2			
short term: restaurant/café	1	space per 2000 SF (MIN. 2)	2			
Recycling Space						
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Residential		cubic ft per DU (Min. 10)	194	Min. 194 ft <sup>3</sup>	YES	for 97 DU
Commercial		cubic ft per 1000 sf (Min. 10)	10	Min. 10 ft <sup>3</sup>		Food service area = 3,442 SF

**BUILDING B- SENIOR HOUSING**

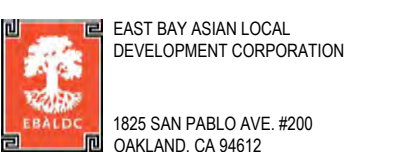
**PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING A (RESIDENTIAL)**

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/30/2021

Property Development Standards						
Code Section for D-LM-2	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
min lot width		25 ft	120.0 ft	YES	placement of imaginary lot line south of 9th, TBD	
min lot frontage		25 ft	74.25 ft	YES	measured on Fallon Street	
min lot area		4,000 sf	19,332 sf	YES	imaginary assigned lot area to Bldgs A + B	
min front setback		0 ft	up to 3 ft	YES	varies at Street Frontages, 3' maximum	
max front and street side setback for the first story		5 ft	3 ft	YES		
max front and street side setback for 2/3 story or 35'		N/A	N/A ft	YES		
min interior side setback		0 ft	0 ft	YES		
min corner side setback		0 ft	0 ft	YES		
min rear setback		0 ft	0 ft	YES		
average min setback from Lake Merritt Estuary Channel		60 ft	N/A	YES		
Ground floor commercial façade transparency		65 %	65 %	YES		
min height of ground floor non-residential facilities		15 ft	16.5 ft	YES	measured from average grade plane	
min width of storefront		15 ft	44.5 ft	YES	measured on 9th Street frontage	
Height, Density, Bulk & Tower Regulations						
Code Section for LM-275	table 17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
building base max height		45 (85 W/ CUP) ft	47 ft	YES	wind screens allowed above 45' per 17.108.030 (f)	
max height		275 ft	275 ft	YES		
max residential density		110 SF/unit	53.7 SF/unit	YES	19,332SF lot area / 360 units	
max non residential intensity (FAR)		12	0.23	YES	based on food retail area only	
min setback of tower from base		50%	65%	YES	395.5ft of 609ft total perimeter set back min 10ft	
max average per story lot coverage above base		75%	65%	YES		
max tower elevation length		195 ft	195 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G	
max diagonal length		234 ft	204.33 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G	
Usable Open Space Standards						
Code Section	table 17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
total number of units	360					
open space requirements (SF)	75 SF/DU	360 x 75 = 27000 SF	33,804 SF	YES	The total open space consists of 80% of paseo plus L5 shared terrace, private balconies, and L5 community room	
Required Dimensions of Usable Open Space						
Code Section	table 17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Private		10 ft on G floor	N/A	N/A		
Public Ground-Floor Plaza		10 ft	65 ft	YES		
Rooftop		15 ft	15 ft	YES		
Courtyard		15 ft	N/A ft	N/A		
Off-site open space		10 ft	N/A ft	N/A		
Community room		250 SF	2,000 SF	YES		
Off Street Parking						
Code Section	17.116.060-080	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Residential (D-LM zones)	No minimum parking requirement	-	105	YES		
Food service	0 for less than 10,000 SF	-	none	YES		
Off Street Loading						
Code Section	17.116.120-140	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Residential	Total Floor Area 50,000 SF or more	1 spaces	2 spaces	YES		
Retail/ Food Service	0 for less than 10,000 SF	0 spaces	0 spaces	YES		
Bicycle Parking						
Code Section	17.117	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
long term: residential unit	1	space per DU	4		360 units / 4 = 90	
short term: residential unit	1	space per DU	20	YES	360 units / 20 = 18	
long term: restaurant/café	1	space per 12,K sf (MIN. 2)	2	YES	4,500 sf, min 2	
short term: restaurant/café	1	space per 2K sf (MIN. 2)	3	YES	4,500 sf / 2,000 = 2.5 rounds to 3	
Recycling Space						
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE	
Residential		cubic ft per DU (Min. 10)	720	Min. 720 ft <sup>3</sup>	YES	stacked vertically in smaller footprint
Commercial		cubic ft per 1000 sf (Min. 10)	9	Min. 10 ft <sup>3</sup>		

**BUILDING A - MARKET RATE RESIDENTIAL**



**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

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6	REVISED RESPONSE TO POPRZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPRZ COMMENTS	03/19/2021

DATE:  
SCALE:  
**ZONING SUMMARY FOR BLOCK 1**

**A1.8**

PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING D (AFFORDABLE HOUSING)

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/18/21

Property Development Standards					
Code Section for D-LM-2	Table 17.101G.03	REQUIRED	PROPOSED	COMPLIANT?	NOTE
min lot width		25 ft	125 ft	YES	
min lot frontage		25 ft	200 SF	YES	
min lot area		4,000 SF	25,000 ft	YES	
min front setback		0 ft	0 ft	YES	
max front and street side setback for the first story		5 ft	5 ft	YES	along Madison St.
max front and street side setback for 2/3 story or 35'		5 ft	0 ft	YES	
min interior side setback		0 ft	0 ft	YES	
min corner side setback		0 ft	0 ft	YES	
min rear setback		0 ft	0 ft	YES	
average min setback from Lake Merritt Estuary Channel		60 ft		YES	
Ground floor commercial façade transparency		65%	65%	YES	minimum requirement to be detailed at FDP
min height of ground floor non-residential facilities		15 ft	15 ft	YES	
min width of storefront		15 ft	20 ft	YES	

Use					
Code Section for LM-275	17.10.180	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Day Care (for more than 15 children)	Community Education Civic Activity	less than 25% of total linear frontage length	100%	YES	Daycare Capacity = Up to 40 children *Day care area = 6,200 SF indoor + 1,500 SF outdoor

Height, Density, Bulk & Tower Regulations					
Code Section for LM-275	Table 17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE
building base max height		55 (85' W/ CUP) ft	85 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max height		275 ft	85 ft	YES	
max residential density		110 SF/unit	250 SF/unit	YES	25,000 SF lot area/ 100 of units
max nonresidential intensity (FAR)		12	0.25	YES	*Daycare area = 6,200 SF

Usable Open Space Standards					
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
total number of units		100			
open space requirements (SF)	60 SF/DU	100X60= 6,000 sf	6,800 sf	YES	*The total open space consists of entrance porch, courtyard/podium and the community rooms

Required Dimensions of Usable Open Space					
Code Section	17.101G.05	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Private		10 ft on G fr	N/A		
Public Ground-Floor Plaza		10 ft	N/A		
Rooftop		15 ft	N/A	YES	
Courtyard		15 ft	16 ft		ranges from 16' to 76'
Off-site open space		10 ft	N/A		
Community room		250 SF	1,200 SF	YES	

Off Street Parking					
Code Section	17.116.060-080	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential (D-LM zones)	No minimum requirement	-	45	YES	
Code Section	17.116.070	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Day Care	No minimum requirement	-	4	YES	
Code Section	17.116.110	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Car Share Space	One space for 5-100 units	1	1	YES	1 Car share space for Bldg. D will be provided in Bldg. C garage

Off Street Loading					
Code Section	17.116.120-140	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential	Total Floor Area 50,000-149,999 SF	1 space	1	YES	*Total floor area = 110,779 SF
Code Section	17.116.130	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Day Care	Less than 50,000 SF	0 space	-	YES	* Total Day Care area = 7,700 SF (6,200 SF indoor + 1,500 SF outdoor space)

Bicycle Parking					
Code Section	17.117	REQUIRED	PROPOSED	COMPLIANT?	NOTE
long term: units with parking space	0.25	space per DU	25	YES	for 100 DU
short term: units with parking space	0.05	space per DU	5		
long term: Day Care	1	space per 12,000 SF (MIN = 2)	2	YES	* Total Day Care area = 7,700 SF (6,200 SF indoor + 1,500 SF outdoor space)
short term: Day Care	1	space per 2,000 SF (MIN = 2)	2	YES	

Recycling Space					
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Residential	2	cubic ft per DU (Min. 10)	200	YES	for 100 DU
Commercial	2	cubic ft per 1000 sf (Min. 10)	12		for 6,200 sf Daycare

BUILDING D - AFFORDABLE HOUSING

PLANNING CODE COMPLIANCE CALCULATIONS FOR BUILDING C (OFFICE)

Oakland Planning Code 1997 (with updates effective March 17, 2016)

updated 03/18/21

Property Development Standards					
Code Section for D-LM-2	table 17.101G.03	REQUIRED	PROPOSED	COMPLIANT?	NOTE
min lot width		50 ft	175 ft	YES	
min lot frontage		50 ft	200 SF	YES	
min lot area		4,000 SF	35,000 ft	YES	
min front setback		0 ft	varies ft	YES	varies between 0-13 ft at 8th Street
max front and street side setback for the first story		5 ft	up to 10 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max front and street side setback for 2/3 story or 35'		5 ft	0 ft	YES	
min interior side setback		0 ft	0 ft	YES	
min corner side setback		0 ft	0 ft	YES	
min rear setback		0 ft	0 ft	YES	
average min setback from Lake Merritt Estuary Channel		60 ft	N/A	YES	
Ground floor commercial façade transparency		65%	65%	YES	minimum requirement to be detailed at FDP
min height of ground floor non-residential facilities		15 ft	16-21 ft	YES	site slope causes variable height
min width of storefront		15 ft	20 ft	YES	

Use					
Code Section for D-LM-2	table 17.101G.01	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Administrative Commercial	frontage linear frontage at ground floor	less than 25% of total linear frontage length	25%	YES	

Height, Density, Bulk & Tower Regulations					
Code Section for LM-275	table 17.101G.04	REQUIRED	PROPOSED	COMPLIANT?	NOTE
building base max height		45 (85' W/ CUP) ft	45 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max height		275 ft	275 ft	YES	
max nonresidential intensity (FAR)		12	13.19	YES	FAR calculated at time of submittal per 2019 office building FAR method per P. Vollmann letter to SCB March 8 2019
setback from tower base		50% perimeter	53.3%	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max average per story lot coverage above base		75% site area	75.0%	YES	Note that Lot area includes 24" wide seismic separation bwn buildings on Block2
max tower elevation length		150 ft	200.00 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G
max diagonal length		180 ft	232.00 ft	YES	Project will employ the PUD Bonus for Development Standards, can use findings from CUP and apply PUD bonus 17.142.100G

Usable Open Space Standards					
Code Section	open space not required for Administrative Commercial Activities				

Required Dimensions of Usable Open Space					
Code Section	open space not required for Administrative Commercial Activities				

Off Street Parking					
Code Section	17.116.080	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Commercial Activity	none required	- spaces	254 spaces	YES	

Off Street Loading					
Code Section	17.116.140	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Retail	less than 25,000 sf	0 spaces	0 spaces	YES	11,000 sf Retail
Administrative Commercial	160,000 sf or more	3 spaces	3 spaces	YES	

Bicycle Parking					
Code Section	17.117.110	REQUIRED	PROPOSED	COMPLIANT?	NOTE
long term: Administrative Commercial	1	per 10K floor area	52	YES	
short term: Administrative Commercial	1	per 20K floor area	26		
long term: Retail	1	space per 12K sf (MIN = 2)	2	YES	11,000 sf Retail
short term: Retail	1	space per 2K sf (MIN = 2)	2	YES	

Recycling Space					
Code Section	17.118	REQUIRED	PROPOSED	COMPLIANT?	NOTE
Commercial	2	cuft per 1000 sf (Min. 10)	1,071	YES	vertical stacking of recycling material in limited footprint

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PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/19/2021

DATE:  
SCALE:  
ZONING SUMMARY FOR BLOCK 2

A1.9

BUILDING C - OFFICE



VIEW FROM I-880



VIEW FROM 9TH ST & FRANKLIN ST



VIEW FROM LAKE MERRITT

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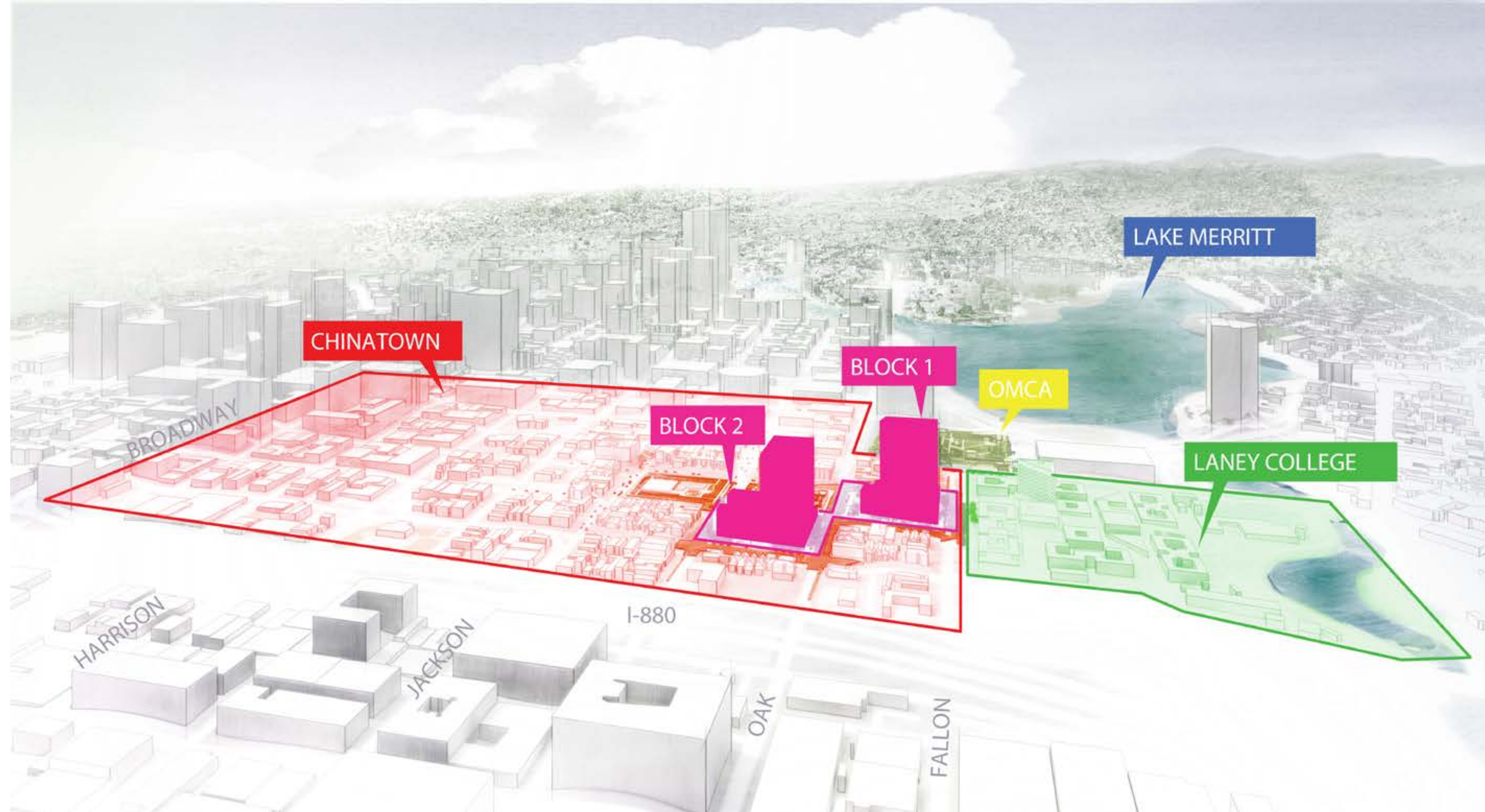
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NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPKZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021

DATE:  
SCALE:

**SKYLINE VIEWS**



**View of Paseo Looking East**



### Connecting Transit to Place

Transit oriented development can enhance the arrival of riders and be a tool to connect people to places, cultures, and ecology. The proposed open space design for the Lake Merritt BART Redevelopment connects this important station with the existing communities and institutions surrounding the station.

Connections to the Oakland Museum of Art, Laney College, the Waterfront, and most importantly Chinatown are the basis of the design organization and will be the inspiration for culture, art, and planting selections. The development of an active pedestrian environment on all sides of the new buildings were designed into the basic fabric of the site. The ground floor and open space for the project is integrated into a complementary whole. Community serving uses and activation on the ground floor will provide eyes on the street as well as new community resources in the new development.

Multi-modal connections and the importance of space planning for the many new and evolving forms of transportation are designed into the base condition of the project with maximum flexibility for the anticipated changes of the future.

Careful consideration of existing BART infrastructure was integrated into the overall project. Advance planning for maintenance, access, and the significant responsibilities of BART Police and other staff were highlighted and designed into the new project to increase flexibility and improve security.

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## LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

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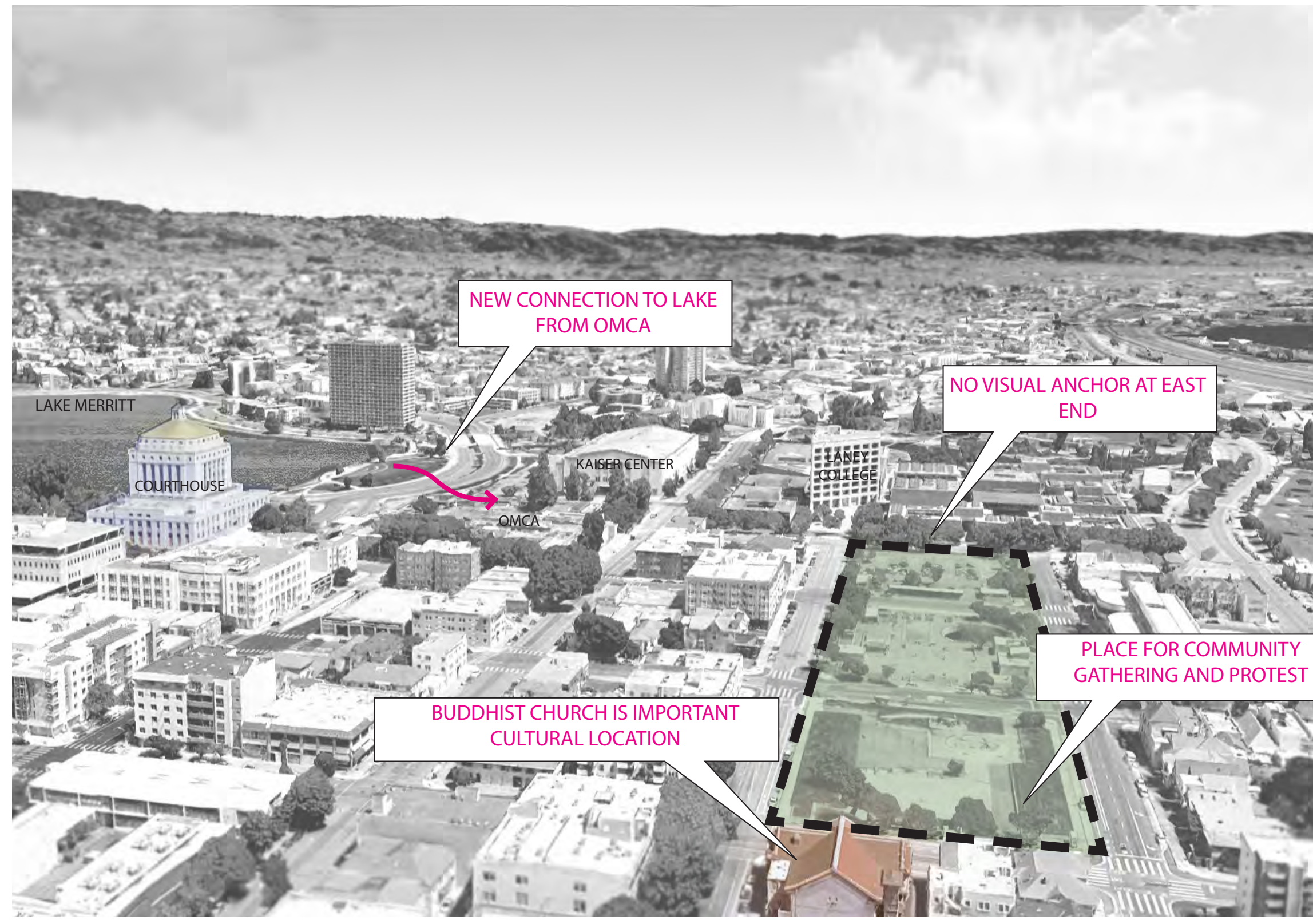
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NO.	ISSUE	DATE
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6	REVISED RESPONSE TO POPPIZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPIZ COMMENTS	03/17/2021

DATE: AUGUST 8, 2019

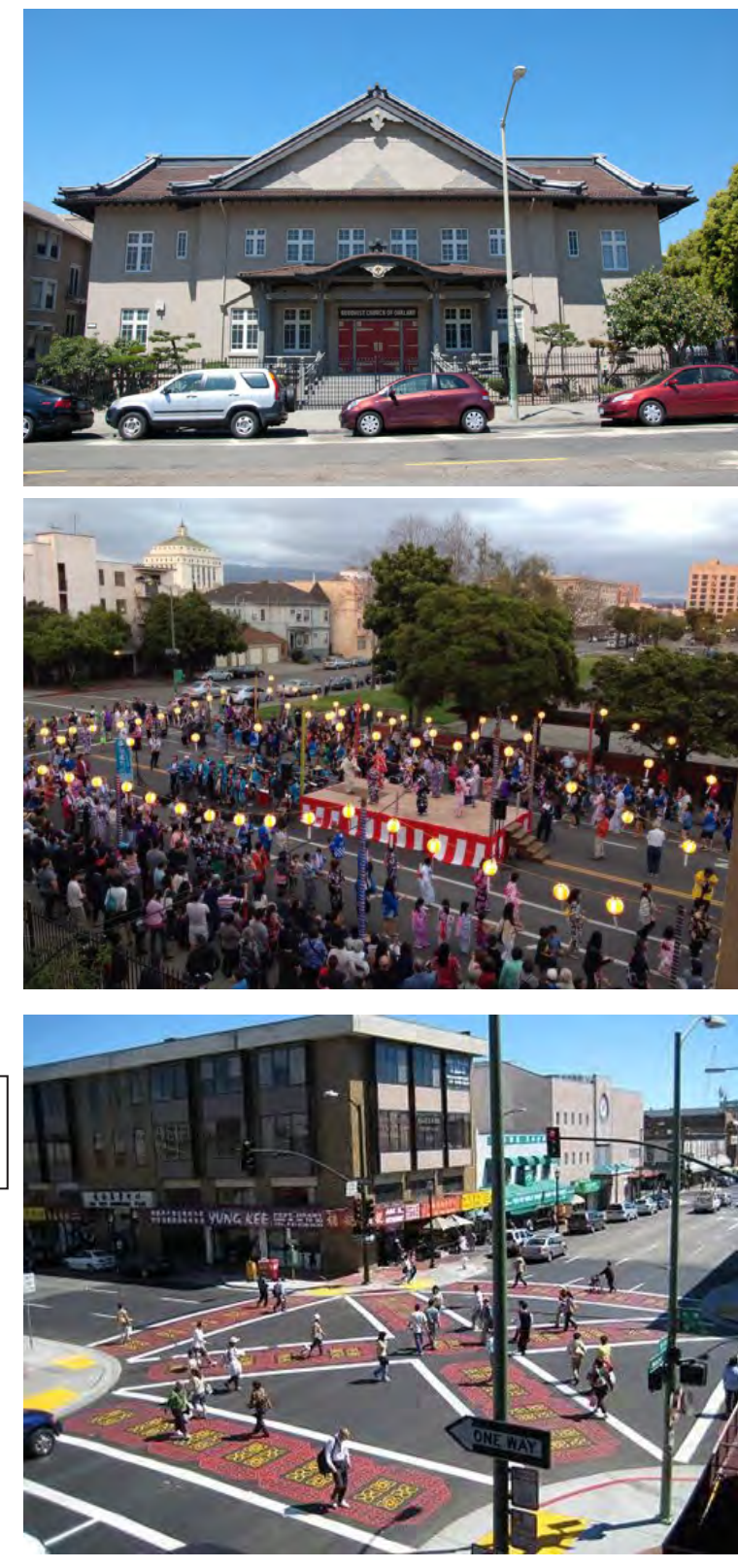
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OPEN SPACE CONCEPT

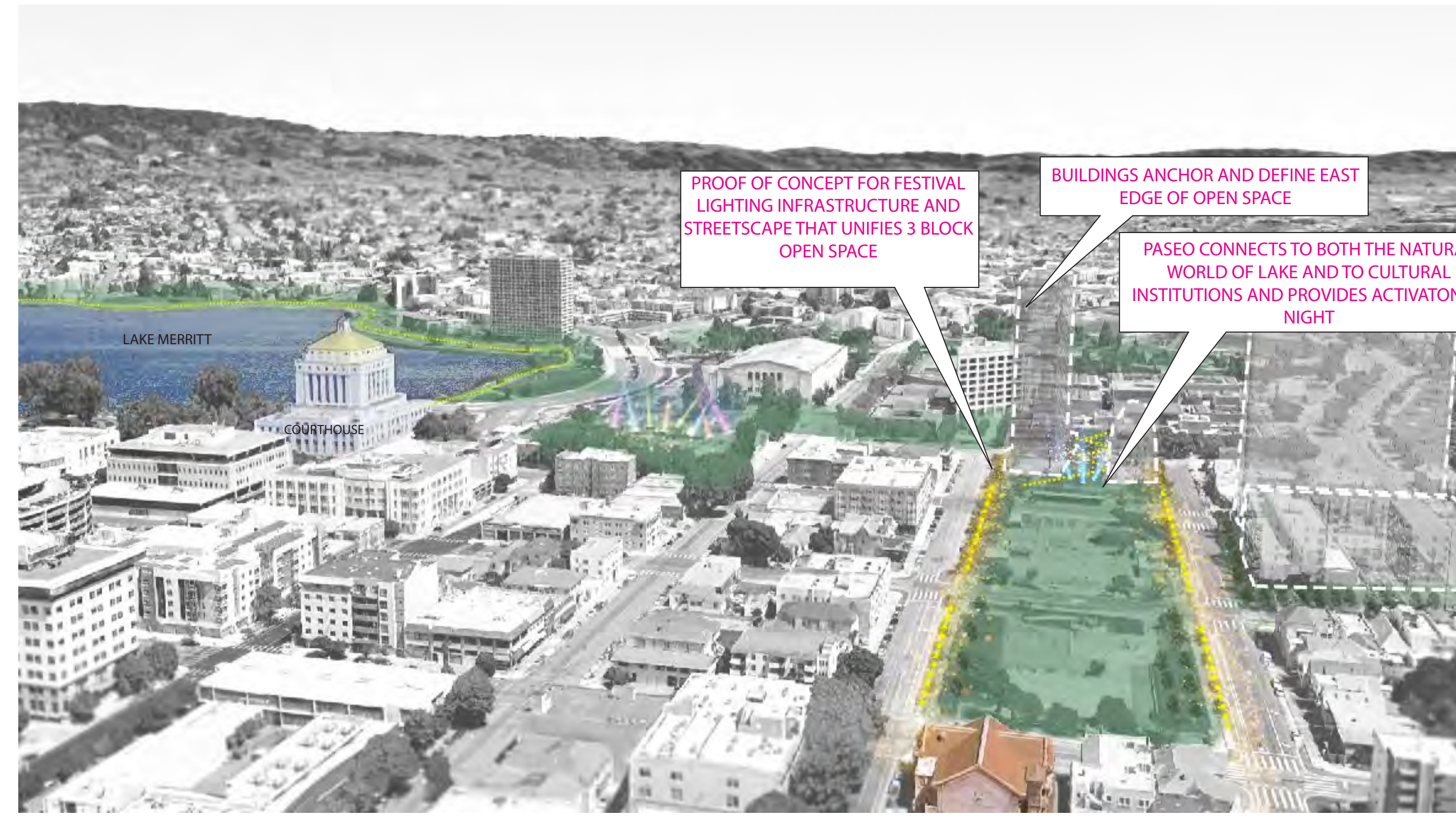
L0.1



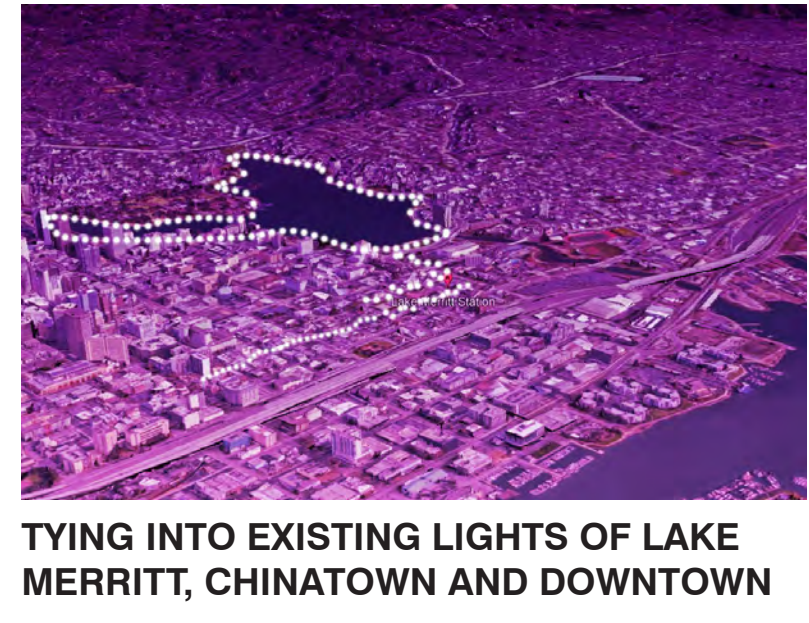
**SITE**  
**SITE TODAY**



**EXISTING CONDITIONS**



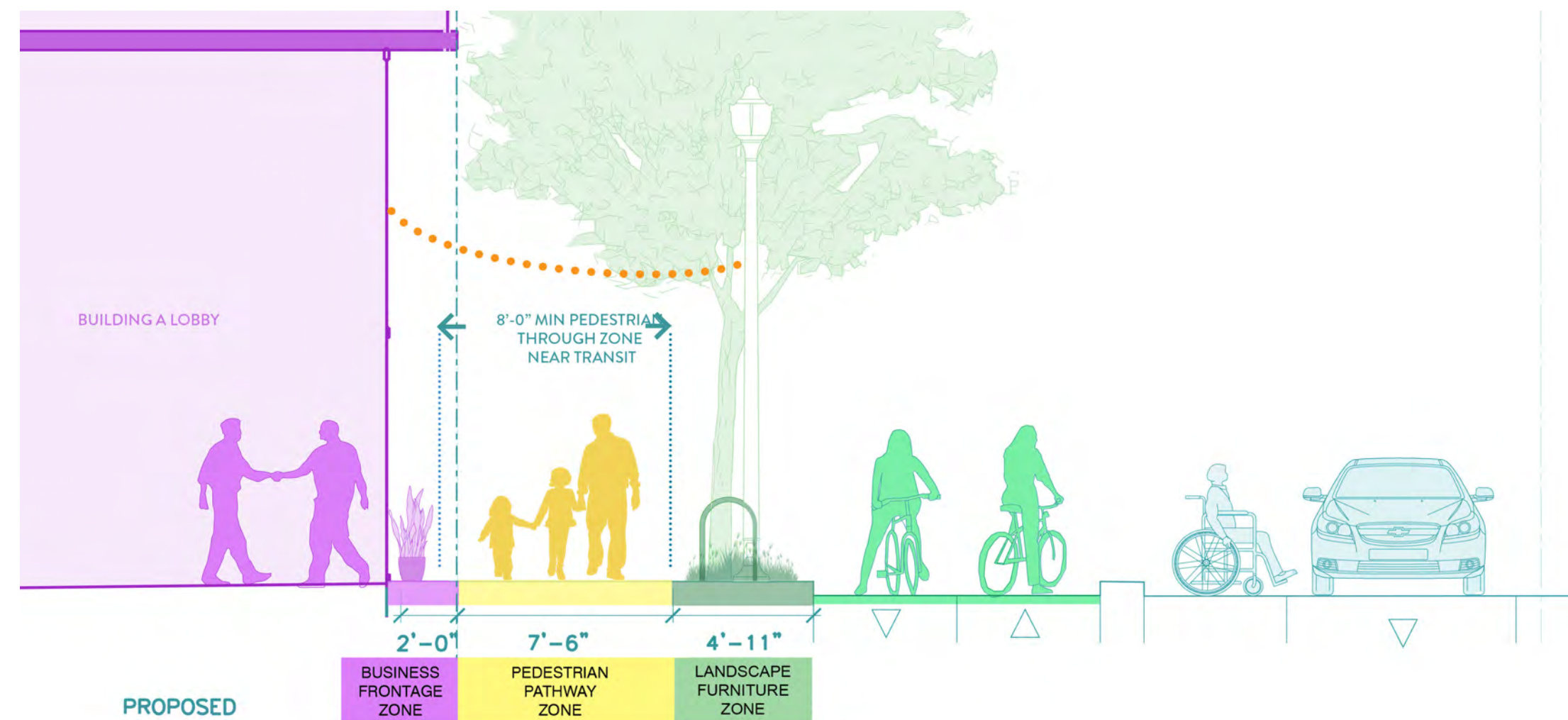
**CONNECT CULTURE AND NATURE**  
**A CATALYST PROJECT THAT PROVIDES THE CORNERSTONE OF OPEN SPACE**



**TYING INTO EXISTING LIGHTS OF LAKE MERRITT, CHINATOWN AND DOWNTOWN**



**NIGHT LIGHTS**



**CONNECTION TO CHINATOWN**  
**OPEN SPACE FUNCTIONS AS A CORRIDOR TO CHINATOWN AND DOWNTOWN OAKLAND**

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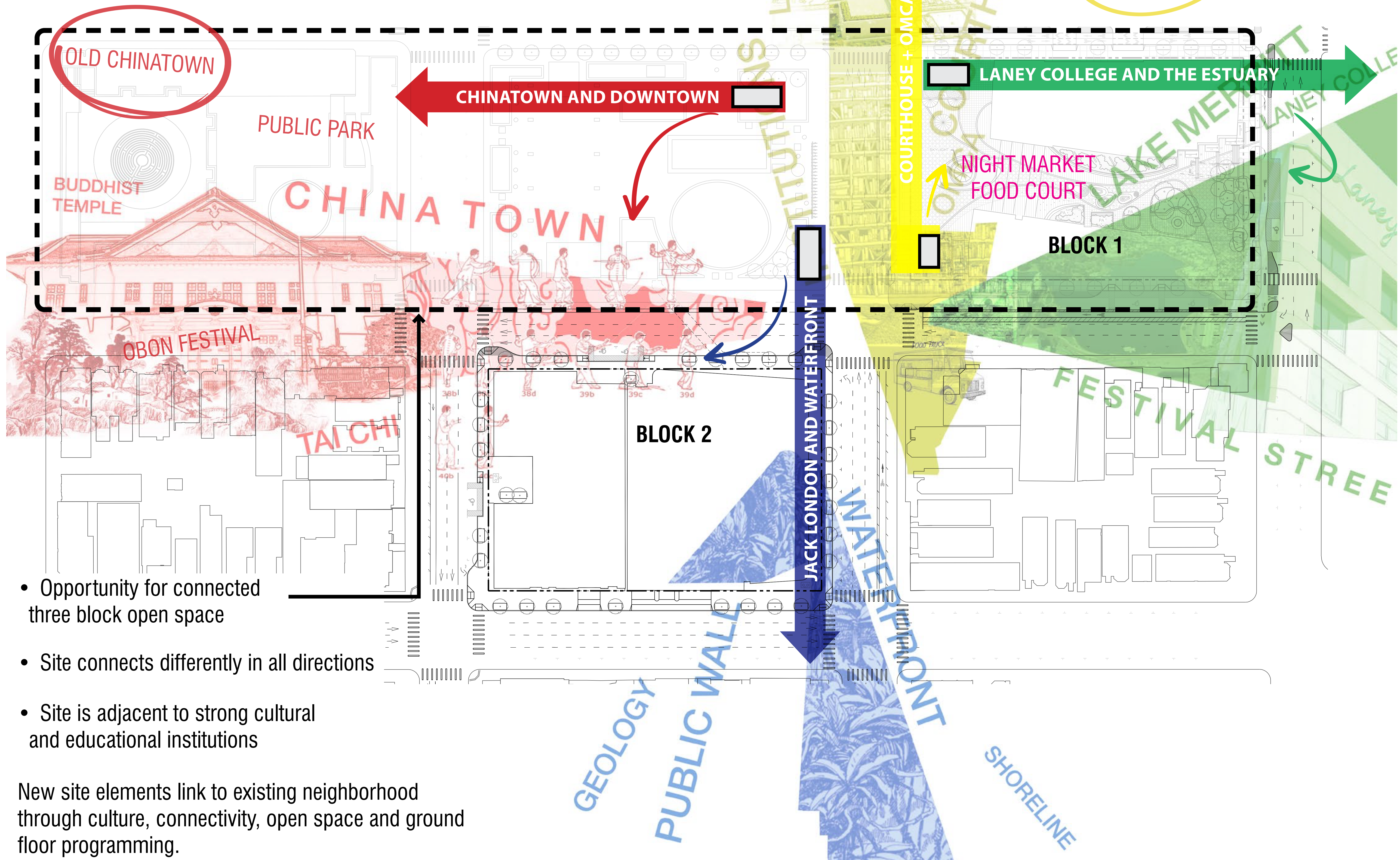
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

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7	REVISED RESPONSE TO POPF COMMENTS	03/17/2021

DATE: AUGUST 8, 2019  
SCALE:  
**OPEN SPACE CONCEPT**



- Opportunity for connected three block open space
- Site connects differently in all directions
- Site is adjacent to strong cultural and educational institutions

New site elements link to existing neighborhood through culture, connectivity, open space and ground floor programming.

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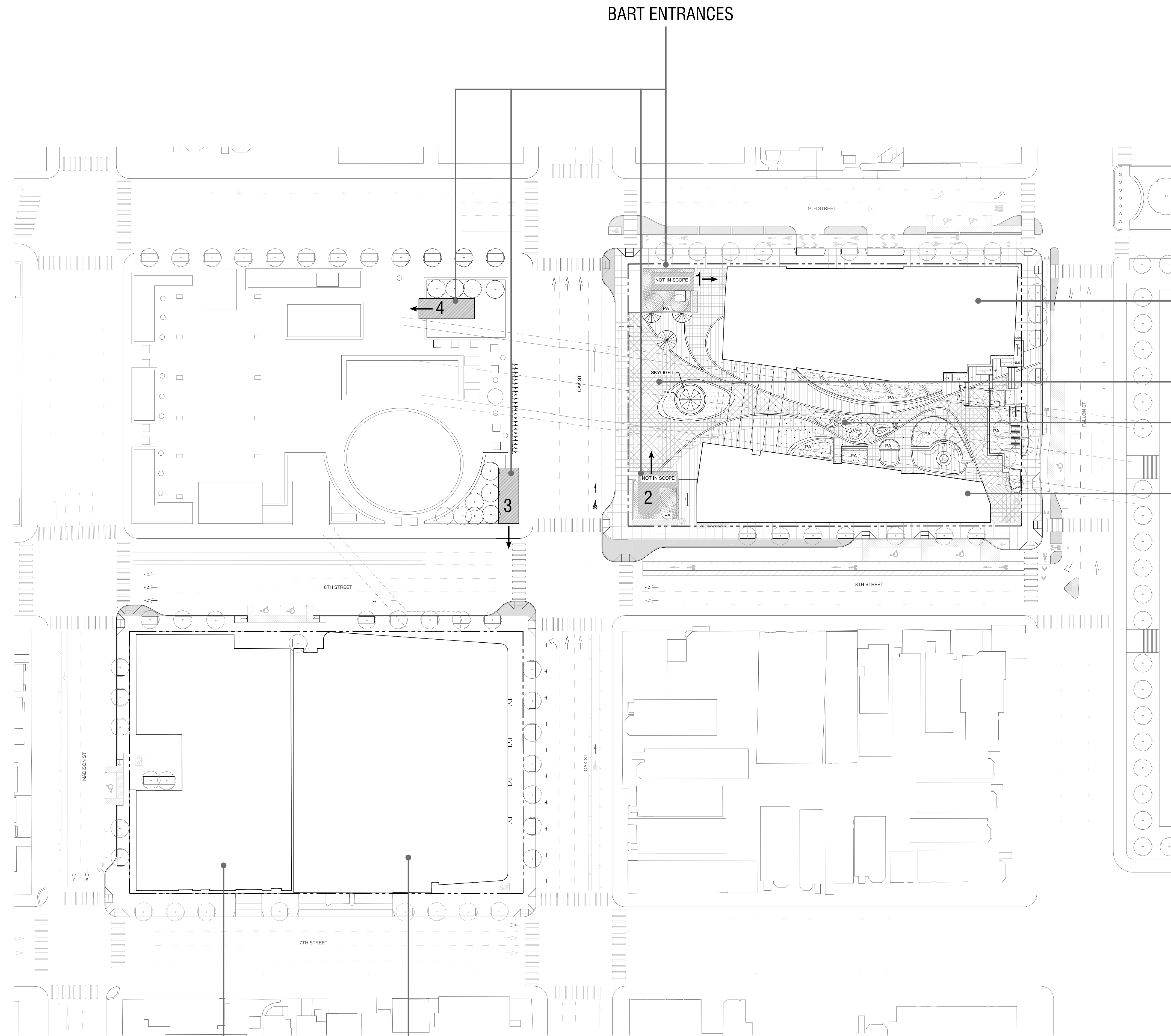
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7	REVISED RESPONSE TO POPPI COMMENTS	03/17/2021

0 20 40 80 FT  
NOT TO SCALE

DATE: AUGUST 8, 2019  
SCALE: 1" = 40' (VIEWED AT 24" X 36")  
URBAN DESIGN CONTEXT DIAGRAM

**L0.2**





BUILDING A:  
MARKET RATE  
RESIDENTIAL

WEST PLAZA

PASEO

BUILDING B:  
SENIOR HOUSING

BUILDING D:  
AFFORDABLE  
HOUSING

BUILDING C:  
OFFICE

**NOTE:**  
"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval." and "BART has completed an initial review of the project PDP and the the project as presened is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

Baseline design and construction in the West Plaza is fully funded by the developer.

Existing bus shelter, headhouses, and skylight to remain.

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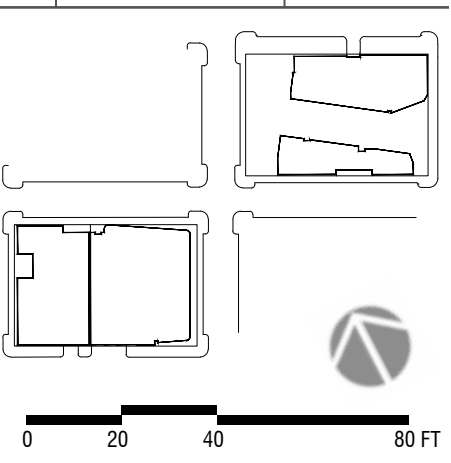
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**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

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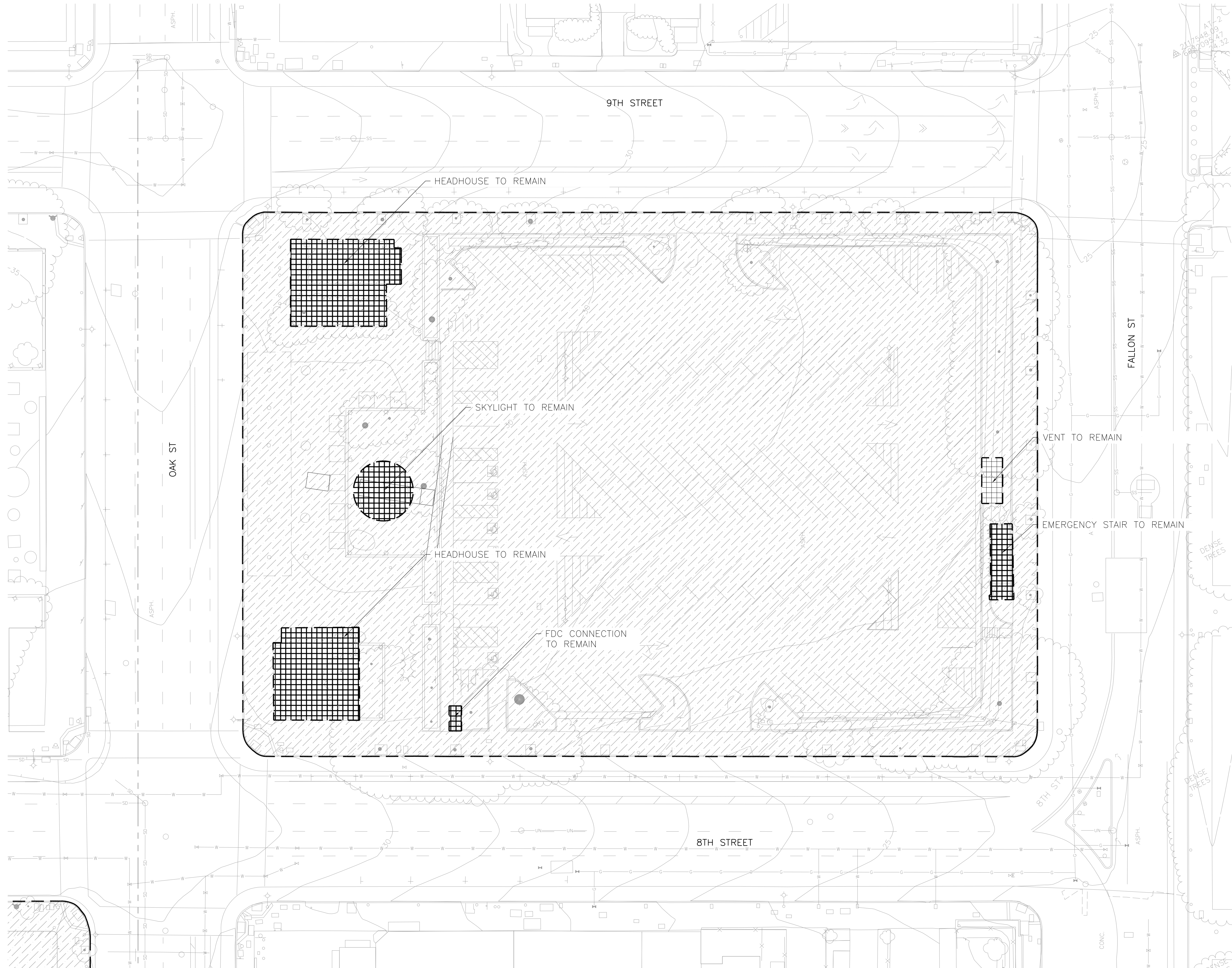
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NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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3	PDP SUBMITTAL #1 TO CITY	2/12/2020
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7	REVISED RESPONSE TO POPPI COMMENTS	03/17/2021



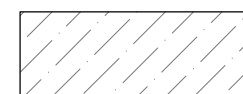
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SCALE: 1" = 40' (VIEWED AT 24" X 36")

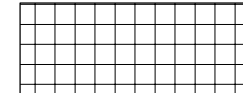
**FOUR-BLOCK AREA  
PLAN**

**L0.3**



**LEGEND**

 TO BE DEMOLISHED

 TO REMAIN

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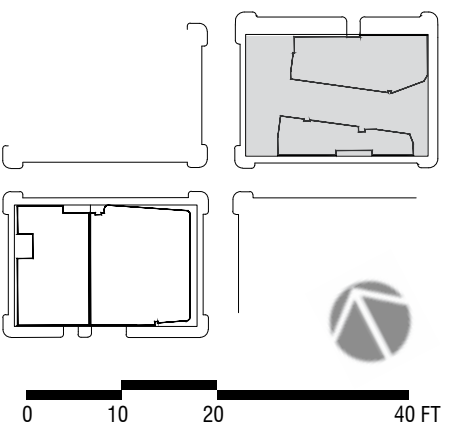
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

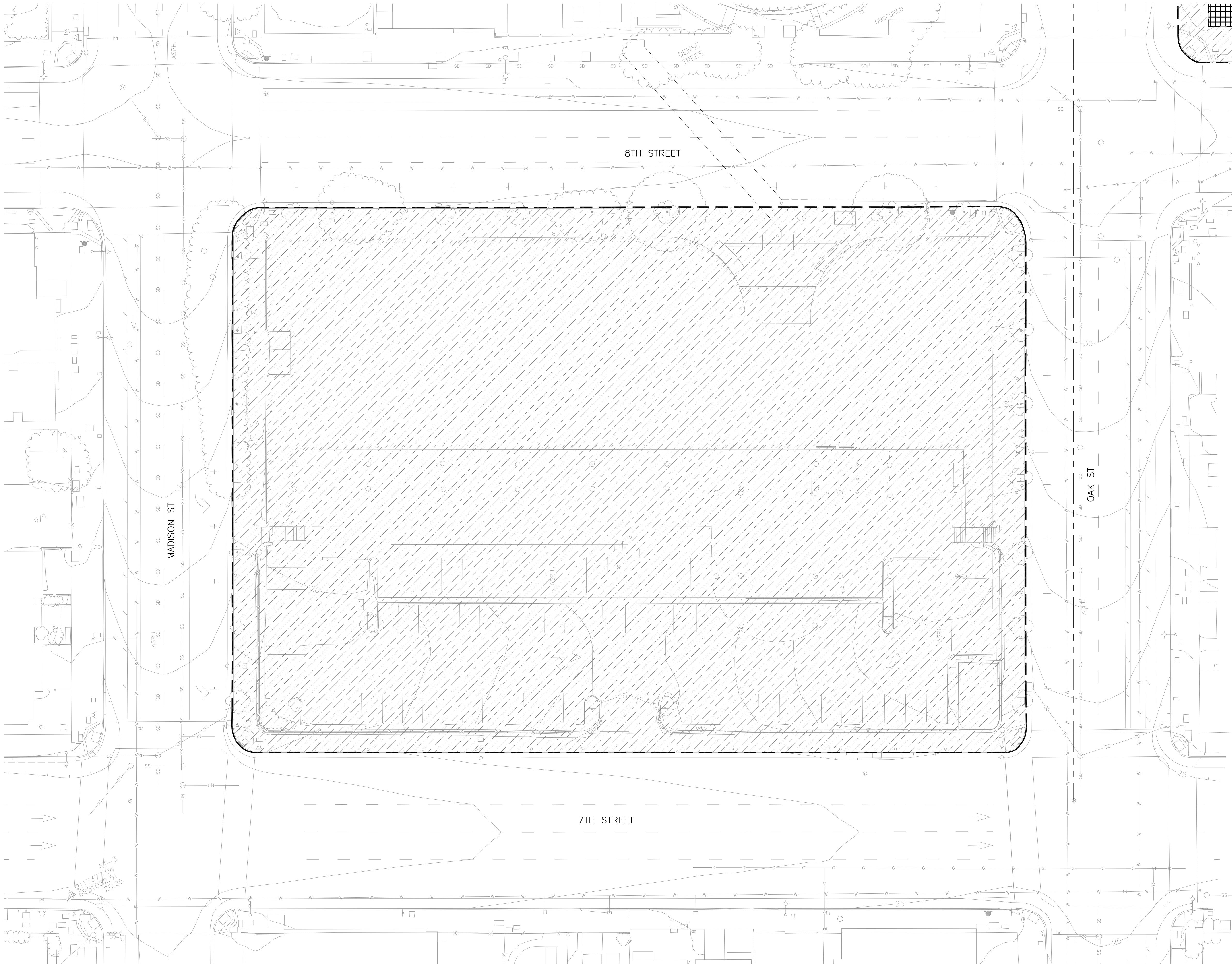
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NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPH COMMENTS	6/8/2020
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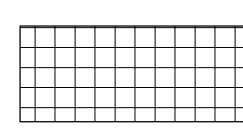


DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 DEMO DIAGRAM**



LEGEND

 TO BE DEMOLISHED

 TO REMAIN

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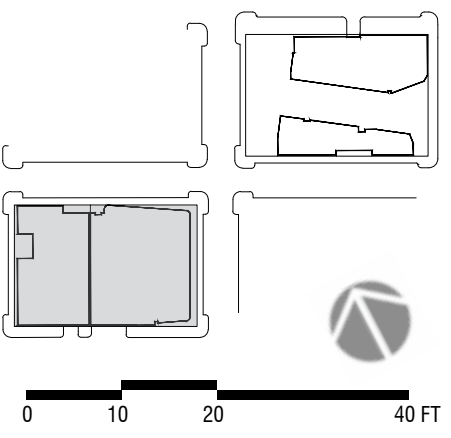
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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REVISION SCHEDULE

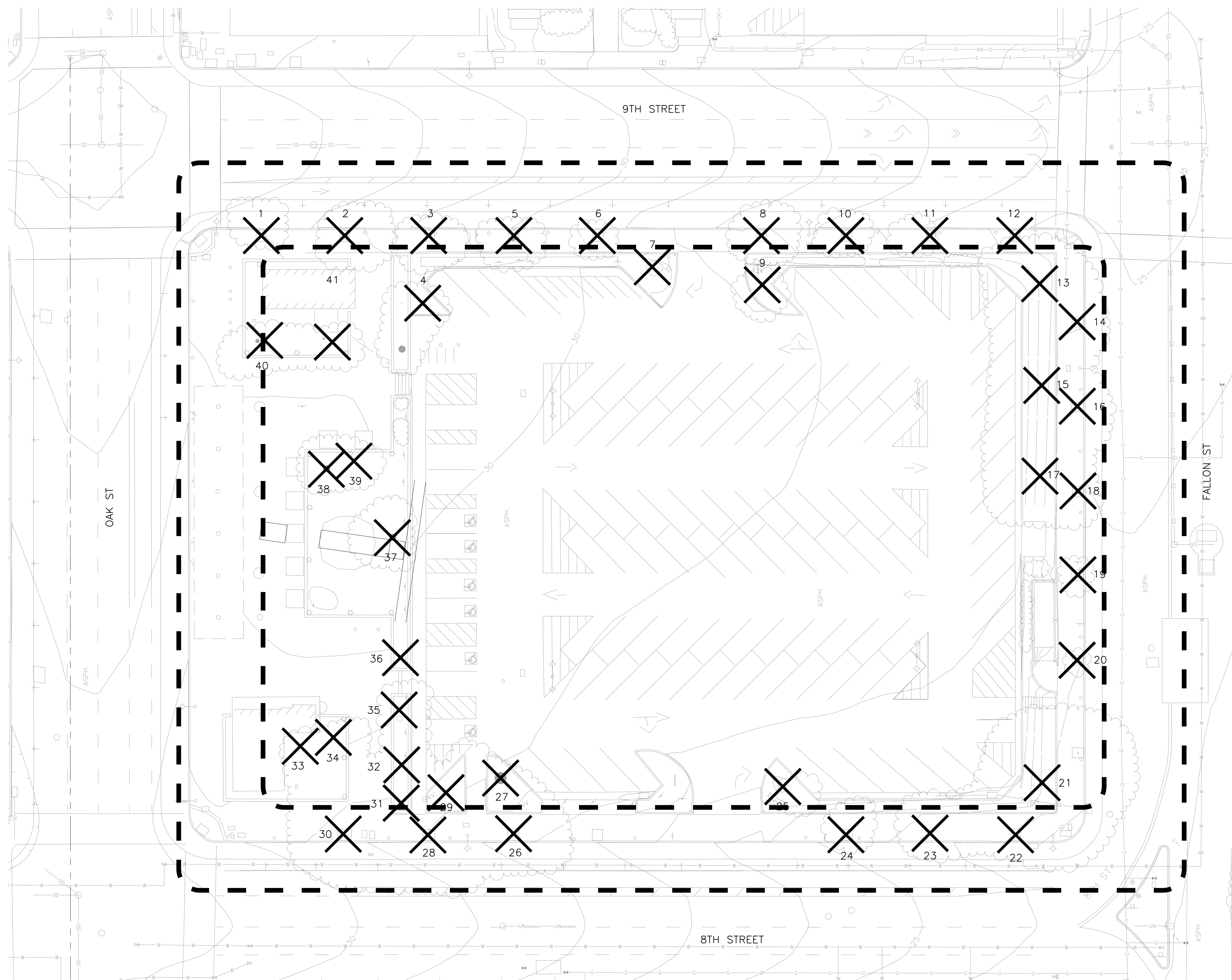
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6	REVISED RESPONSE TO POPH COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPH COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 2 DEMO DIAGRAM**

**L0.5**



Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
1	13"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
2	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
3	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
4	18"	Yes	<i>Olea europaea</i>	Olive Tree
5	24"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
6	6"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
7	10"	Yes	<i>Olea europaea</i>	Olive Tree
8	12"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
9	14"	Yes	<i>Olea europaea</i>	Olive Tree
10	8"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
11	7"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
12	14"	Yes	STUMP	STUMP
13	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
14	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
15	13"	Yes	<i>Platanus racemosa</i>	California Sycamore
16	15"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
17	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
18	5"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
19	14"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
20	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
21	24"	Yes	<i>Platanus racemosa</i>	California Sycamore
22	20"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
23	10"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
24	7"	Yes	<i>Rhus lancea</i>	African Sumac
25	14"	Yes	<i>Olea europaea</i>	Olive Tree
26	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
27	51"	Yes	<i>Olea europaea</i>	Olive Tree
28	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
29	14"	Yes	<i>Olea europaea</i>	Olive Tree
30	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
31	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
32	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
33	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
34	12"	Yes	<i>Olea europaea</i>	Olive Tree
35	15"	Yes	<i>Olea europaea</i>	Olive Tree
36	9"	Yes	<i>Pittosporum undulatum</i>	Victorian box
37	30"	Yes	<i>Olea europaea</i>	Olive Tree
38	30"	Yes	<i>Olea europaea</i>	Olive Tree
39	13"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
40	25"	Yes	<i>Olea europaea</i>	Olive Tree
41	20"	Yes	<i>Pittosporum undulatum</i>	Victorian box

LEGEND  
 30' OFFSET FROM CONSTRUCTION (LOW) FOR TREE ID AND PRESERVATION PURPOSES

**NOTE:**  
 The following are Protected Trees according to City Ordinance Chapter 12.36. **PROTECTED TREES:**  
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**NOTE ON REMOVAL:**  
 1. We are recommending removing 40 protected trees because of overall poor structure and poor health.  
 2. To meet the needs of a multi modal transit center, new sidewalks designs, bike paths, pick up and drop off, ADA multi-modal drop off will all require the relocation and demolition of sidewalks. This will disrupt the roots and will create larger soil volume areas.  
 3. The master plans aim is to make a cohesive neighborhood identity (coordinating with Madison Sq. Park, Bart plaza (TOF), and Block 1 + 2) and to have a cohesive plant palette.  
 4. See page L0.8 and L0.81 for new tree planting. Intent of design to provide street trees within the scope of the project.

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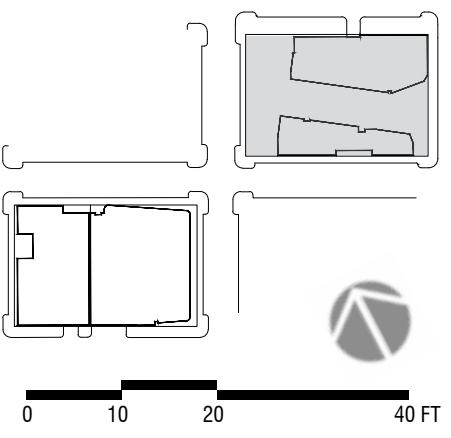
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**LAKE MERRITT BART REDEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

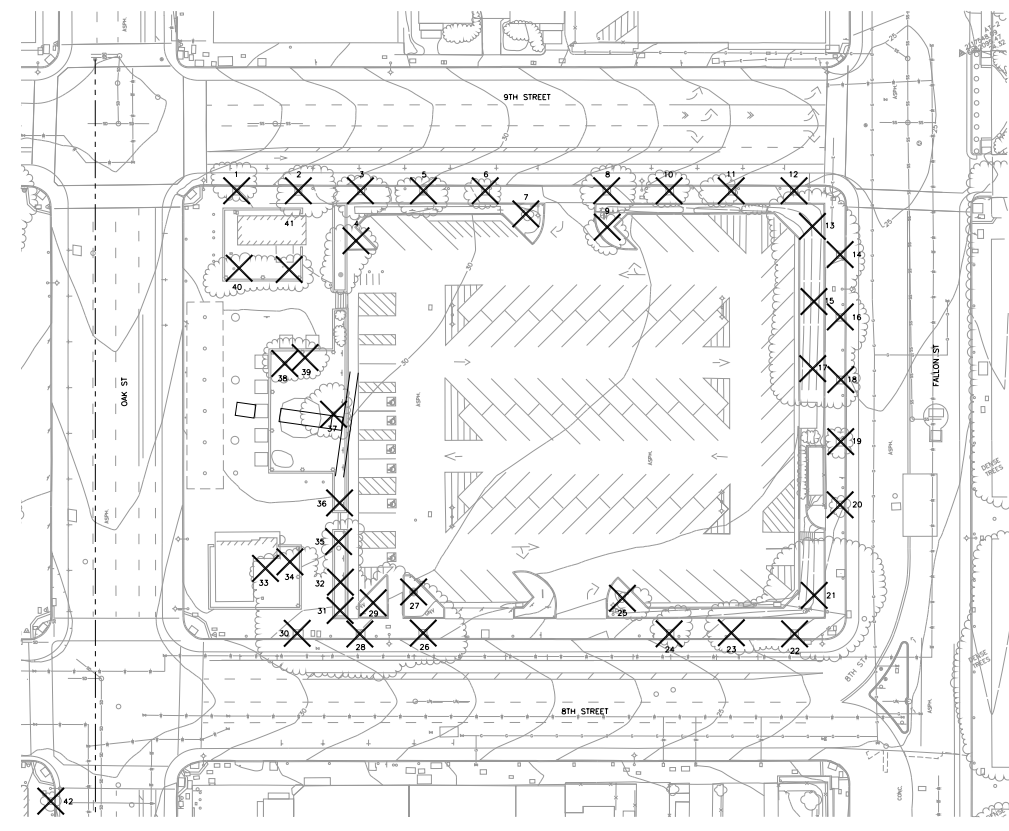
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DATE: AUGUST 8, 2019  
 SCALE: 1" = 20' (VIEWED AT 24" X 36")

**L0.6**



Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
1	13"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
2	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
3	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
4	18"	Yes	<i>Olea europa</i>	Olive Tree
5	24"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
6	6"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
7	10"	Yes	<i>Olea europa</i>	Olive Tree
8	12"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
9	14"	Yes	<i>Olea europa</i>	Olive Tree
10	8"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
11	7"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
12	14"	Yes	STUMP	STUMP
13	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
14	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
15	13"	Yes	<i>Platanus racemosa</i>	California Sycamore
16	15"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
17	15"	Yes	<i>Platanus racemosa</i>	California Sycamore
18	5"	Yes	<i>Afrocarpus falcatus</i>	African Fir Pine
19	14"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
20	12"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
21	24"	Yes	<i>Platanus racemosa</i>	California Sycamore
22	20"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
23	10"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
24	7"	Yes	<i>Rhus lancea</i>	African Sumac
25	14"	Yes	<i>Olea europa</i>	Olive Tree
26	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
27	51"	Yes	<i>Olea europa</i>	Olive Tree
28	16"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
29	14"	Yes	<i>Olea europa</i>	Olive Tree
30	18"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
31	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
32	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
33	12"	Yes	<i>Pittosporum undulatum</i>	Victorian box
34	12"	Yes	<i>Olea europa</i>	Olive Tree
35	15"	Yes	<i>Olea europa</i>	Olive Tree
36	9"	Yes	<i>Pittosporum undulatum</i>	Victorian box
37	30"	Yes	<i>Olea europa</i>	Olive Tree
38	30"	Yes	<i>Olea europa</i>	Olive Tree
39	13"	Yes	<i>Schinus terebinthifolius</i>	Brazilian Pepper Tree
40	25"	Yes	<i>Olea europa</i>	Olive Tree
41	20"	Yes	<i>Pittosporum undulatum</i>	Victorian box

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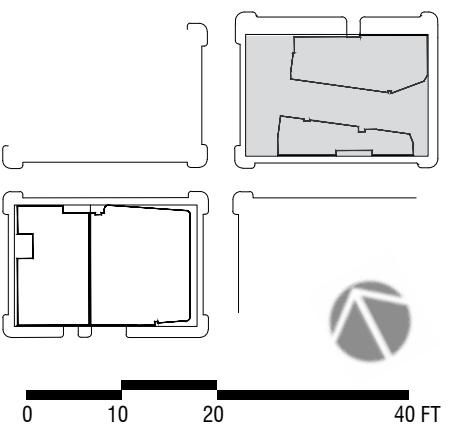
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION -

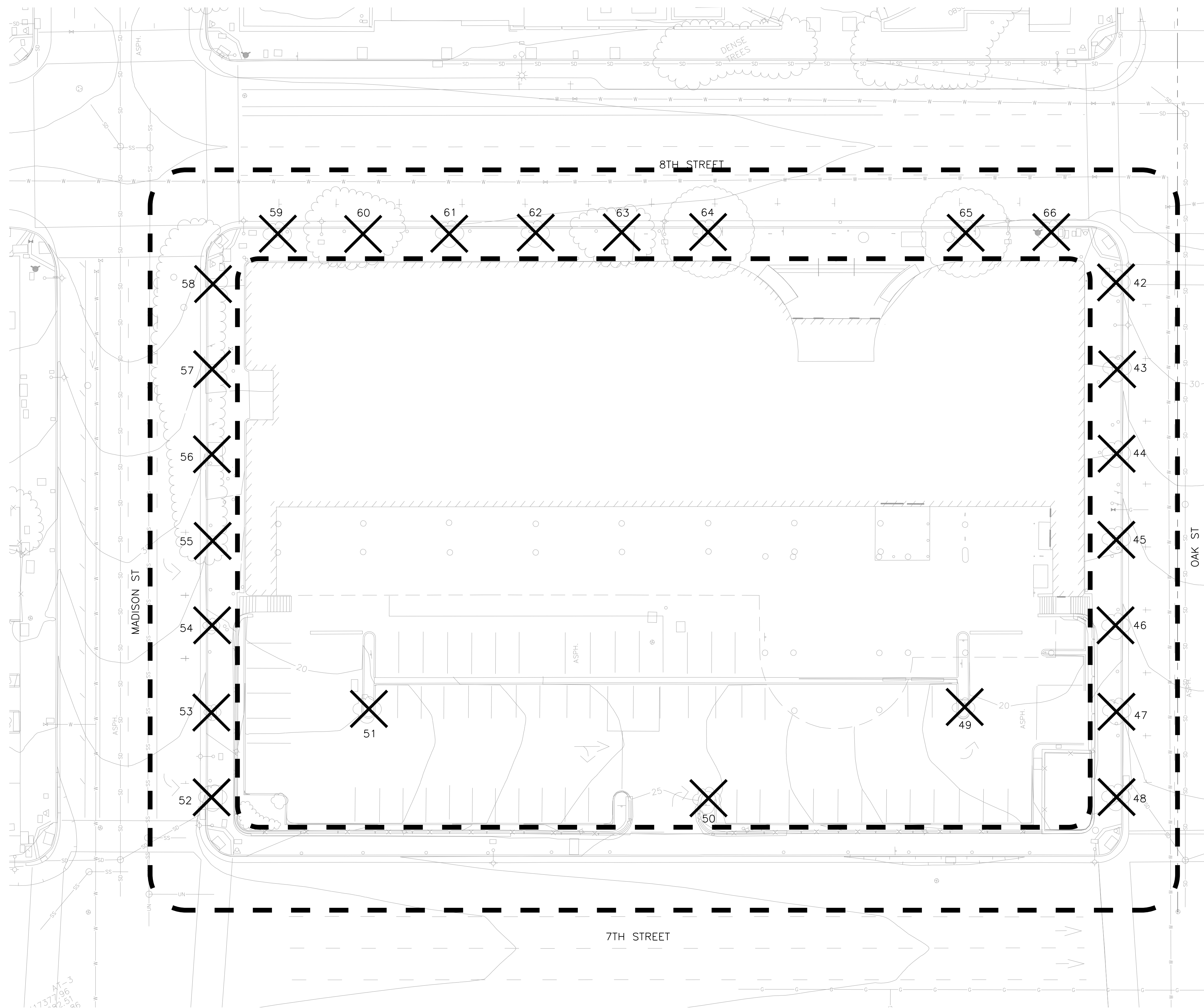
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DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

### BLOCK 1 TREE SURVEY

# L0.61



Number	DBH	Remove	SCIENTIFIC NAME	COMMON NAME
42	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
43	13"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
44	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
45	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
46	3"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
47	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
48	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
49	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
50	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
51	7"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
52	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
53	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
54	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
55	5"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
56	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
57	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
58	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
59	8"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
60	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
61	6"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
62	4"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
63	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
64	12"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
65	10"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash
66	9"	Yes	<i>Fraxinus oxycarpa</i>	Raywood Ash

LEGEND

 30' OFFSET FROM CONSTRUCTION (LOW) FOR TREE ID AND PRESERVATION PURPOSES

NOTE:

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4. See page L0.8 and L0.81 for new tree planting. Intent of design to provide street trees within the scope of the project.

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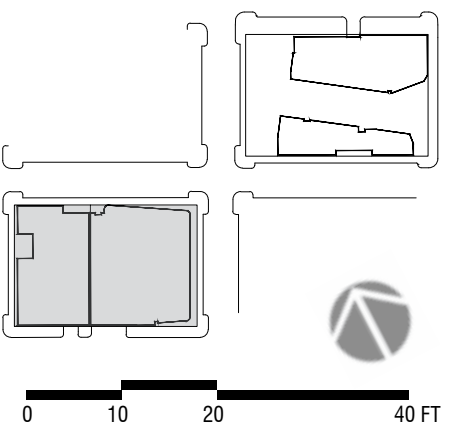
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**LAKE MERRITT BART REDEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

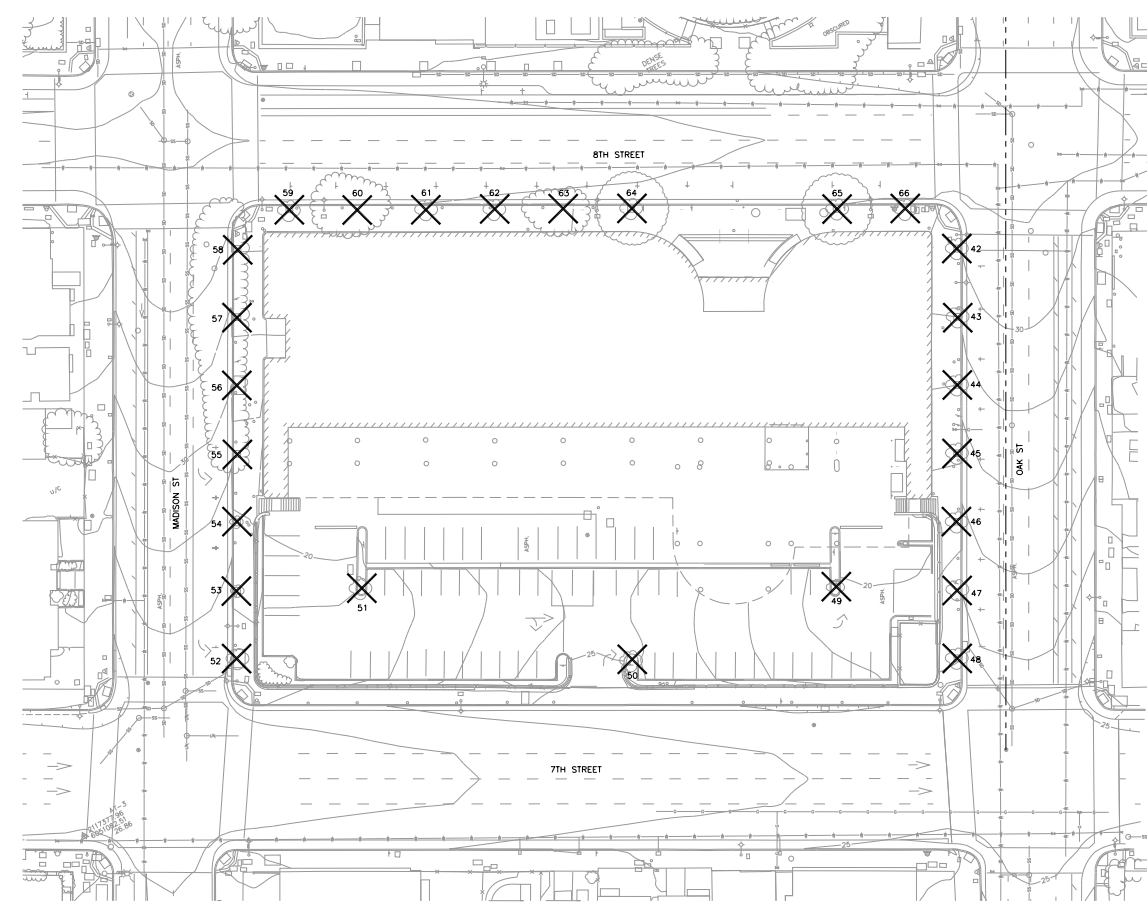
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**BLOCK 2 TREE SURVEY**

**L0.7**



41



42



43



44



45



46



47



48



49



50



51



52



53



54



55



56



57



58



59



60



61



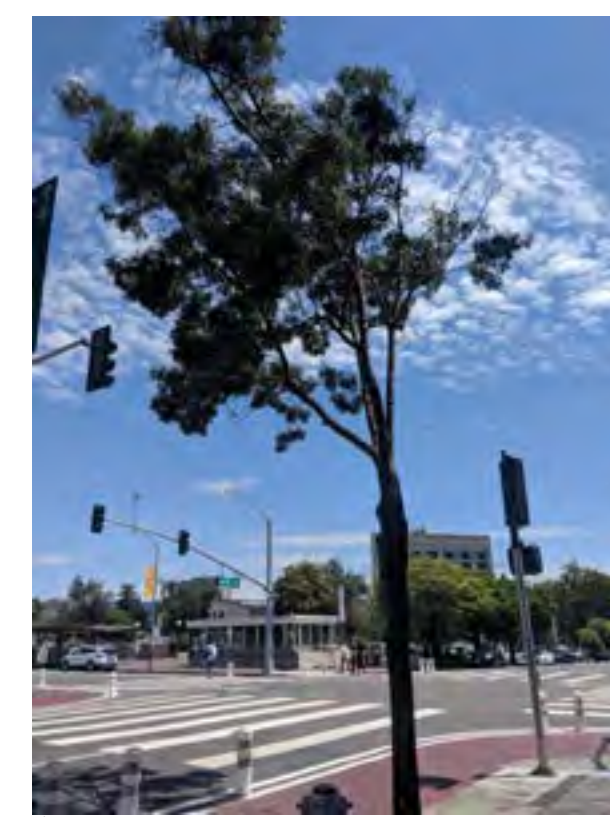
62



63



64



65

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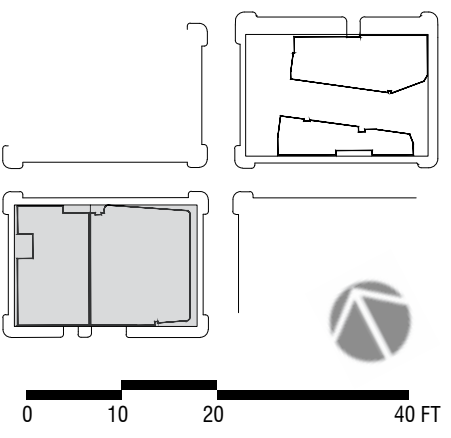
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

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DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

## BLOCK 2 TREE SURVEY

# L0.71



Key Plan

**NOTE:**

1. The project shall demonstrate compliance with the City of Oakland's Landscape Ordinance prior to building permit final. Project will be responsible to carry out any changes that may result from compliance with the Landscape Ordinance, which may include, but not limited to, revised plans, additional technical information, and/or additional planning applications.

2. Species selections may change based on availability, disease, or overall coordination of planting palette.

3. The proposed planting type, character, and water use is in accordance to the City of Oakland's master street tree list. The species selection will develop as we work in conjunction with TOF and Madison Square Park.

4. This is intended to work as a 3 block open space, and the continuity of plant species will be consistent between project blocks.

**LEGEND AND WUCOLS RATING**

- Pollinator
- Water Use Low
- Water Use Regular
- Water Use High
- Oakland Master Street Tree List

**EXISTING TREE SPECIES**

**Oak Street**



Block 2: Eucalyptus microtheca

**PROPOSED TREE SPECIES BY STREET**

**Oak Street - 7 Trees**



Red Horsechestnut, *Aesculus carnea* 'Briotii'  
Upright-oval rounded form, shade tree and ornamental tree. Showy flowers.



Non native, moderate water use, pollinator

**EXISTING STREET TREES WITHIN THE 3 BLOCK OPEN SPACE FRAME**

**9th Street**



Block 1: California Sycamore



Block 1: Olive tree



Block 1: African sumac



Block 1: California Sycamore

**8th Street**



Block 1: California Sycamore



Block 1: African Sumac



Block 1: Olive Tree



Block 2: Eucalyptus microtheca

**Fallon Street**



Block 1: California Sycamore



Block 1: African sumac



Block 1: African Fir Pine

**PROPOSED TREE SPECIES**

**3 Block Open Space Frame - 75 Trees**



Block 1 & 2: Cork Oak, *Quercus suber*  
Medium-sized, evergreen tree, with open spreading form. Interesting bark texture.



Non native, Low water use



Enlargement of Cork Oak

The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads.

Plant areas shown over the tunnel structure must comply with BES standards.

**Madison Street**



Block 2: Eucalyptus microtheca

**7th Street**



Block 2: Eucalyptus microtheca

**9th + 8th**



Block 2: Eucalyptus microtheca

**Madison Street - 5 Trees**



Brisbane Box, *lophostemon confertus*  
Large, upright, evergreen tree. Effective screen or wind break.



Water regular, non-native, pollinator

**7th Street - 8 Trees**



Brisbane Box, *lophostemon confertus*  
Large, upright, evergreen tree. Effective screen or wind break.



Water regular, non-native, pollinator

**9th & 8th Opposite Open Space Frame**



Paperbark Tree, *Melaleuca quinquenervia*  
Multi trunk. Erect, spreading form with low canopy. Evergreen. Showy flowers in Summer or Fall. Paper like bark.



Drought tolerant, non-native, pollinator

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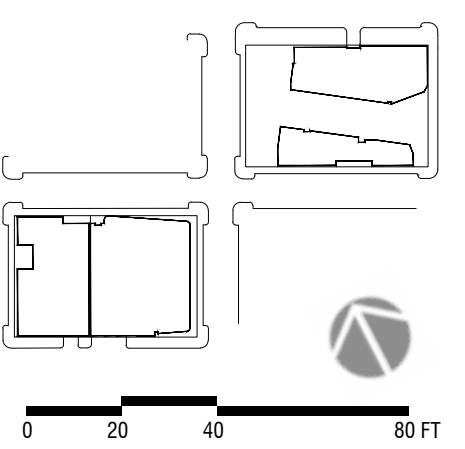
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

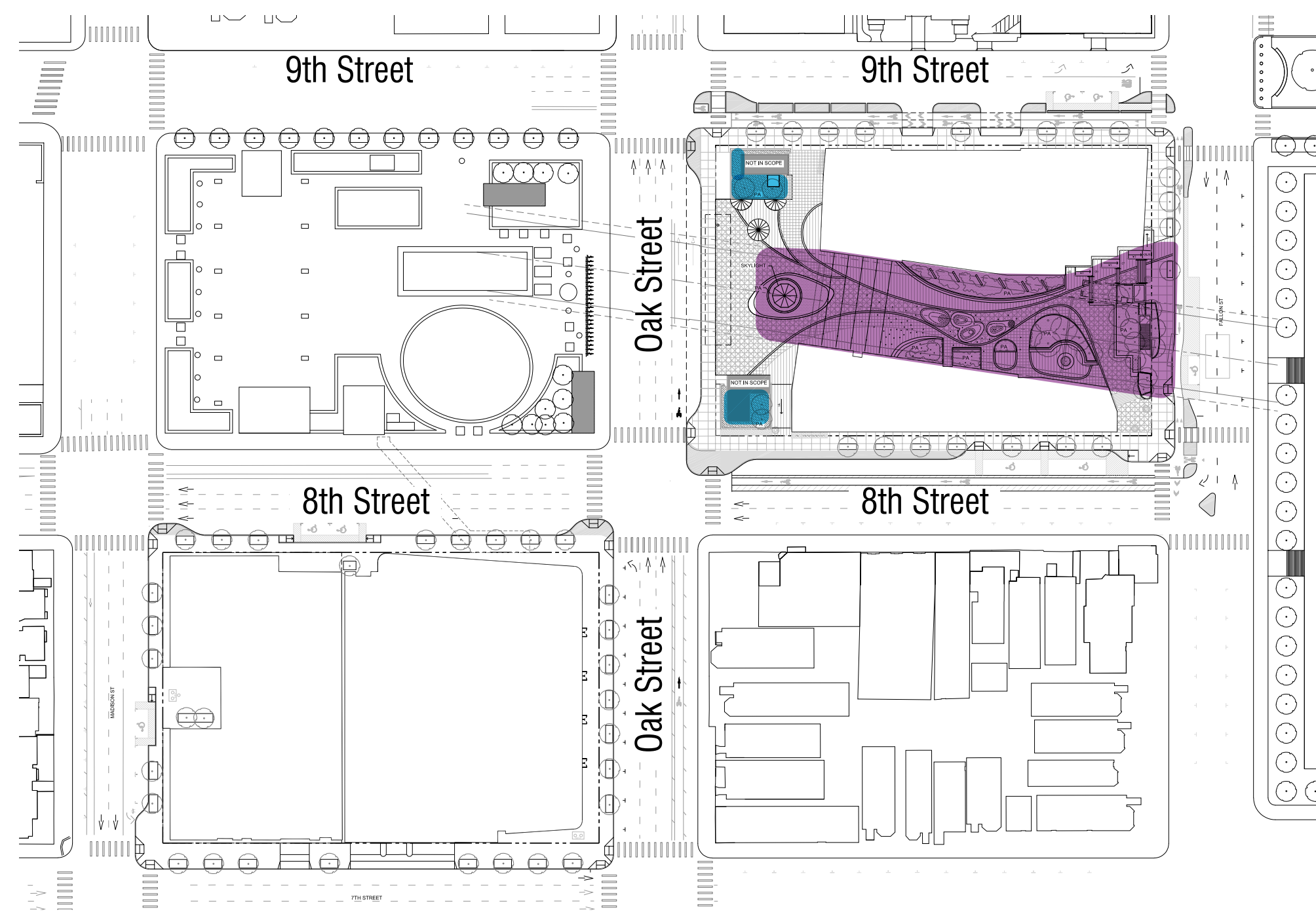
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DATE: AUGUST 8, 2019  
SCALE: 1" = 40' (VIEWED AT 24" X 36")

**L0.8**





Key Plan

Proposed Concrete Planters



Headhouse planters and Paseo planters will be constructed with concrete at seat height.

- NOTE:**
1. The project shall demonstrate compliance with the City of Oakland's Landscape Ordinance prior to building permit final. Project will be responsible to carry out any changes that may result from compliance with the Landscape Ordinance, which may include, but not limited to, revised plans, additional technical information, and/or additional planning applications.
  2. Species selections may change based on availability, disease, or overall coordination of planting palette.
  3. We're proposing planting type, character, and water use in accordance to the City of Oakland's approved street tree list. The species selection will develop as we work in conjunction with TOF and Madison Square Park.
  4. Species selected are sourced from Oakland's approved street tree species list.

LEGEND AND WUCOLS RATING

- Pollinator
- Water Use Low
- Water Use Regular
- Water Use High
- Oakland Master Street Tree List

Existing Skylight Planter Planting



Skylight Planter: Defensive planting for skylight  
 Ornamental grass mix: Carex  
 Skylight Planter: Peruvian Pepper tree

No ground cover or understory planting noted at the skylight planter.

Proposed Skylight and Paseo Planting



Paseo Planter: *Sesleria nitida* mix with *euphorbia*  
Regular water use, non natives  
 Vine on parking garage: *Passiflora caerulea*, Low water use, non native  
 Enlargement: *Accacia cognata* 'Cousin It' and Aeoniums mix.  
Low water use, non native  
 Paseo Planter: Western Sword Fern, Regular water, native  
 Paseo Planter: Sweet Box, *Sarcococca confusa*  
Regular water, non native, pollinator  
 Tree Choice: Fern Pine, *Afrocarpus Gracilior*  
Non native, regular to low water  
 Tree Choice: Flowering Dogwood, *Cornus florida*  
Non native, regular water

Existing Headhouse Planter Planting



N. Headhouse: Olive tree  
 N. Headhouse: Victorian Box  
 S. Headhouse: Olive tree  
 S. Headhouse: Victorian Box

No ground cover planting noted  
 Agapanthus as understory planting found in the northern headhouse planter.

Proposed Headhouse Planting



Monkey Grass, Mondo grass  
Non native, regular to low water use  
 Green Leaf Japanese Maple, *Acer Platanum*, with Mondo grass  
Non native, regular water  
 Tree Choice: Fern Pine, *Afrocarpus Gracilior*  
Non native, regular to low water  
 Tree Choice: Flowering Cherry Tree, *Prunus 'Kwanzan'*  
Non native, regular water use

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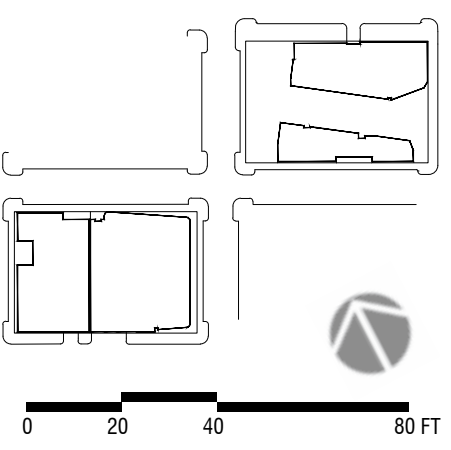
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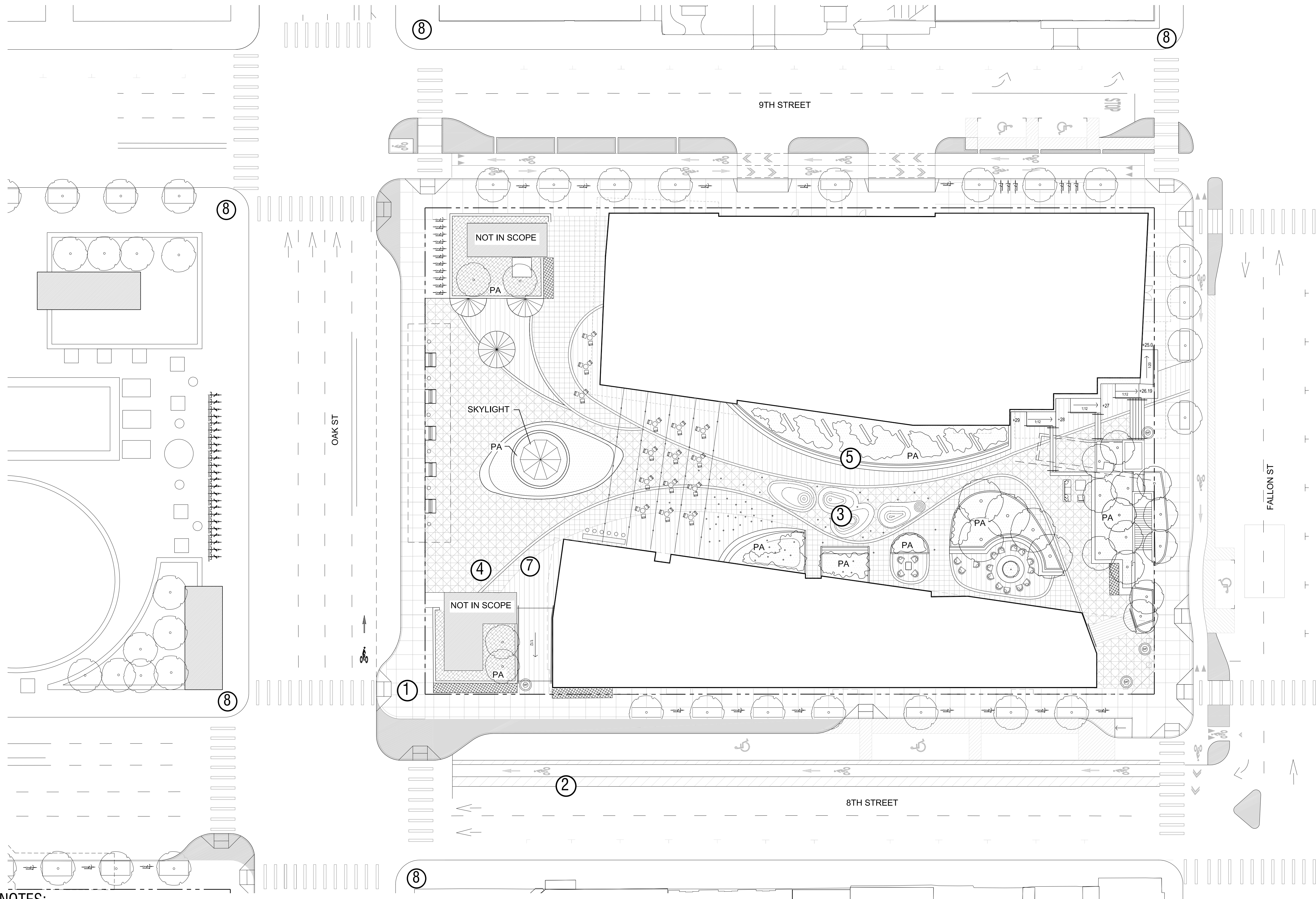
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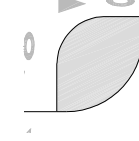


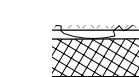
**NOTES:**

- ① See Civil Drawings for limit of project sidewalk improvements.
- ② Street access plan striping shown for planning purposes only.
- ③ Paseo design under study. Furniture is movable. Typical conditions shown.
- ④ All open space elements to be maintained by BART will be designed to BFS standards.
- ⑤ The plants shown above the tunnel may need to be removed depending on the structural evaluation of soil loads. Plant areas shown over the tunnel structure must comply with BFS standards.
- ⑥ BART security cameras to be included as agreed to with BART.
- ⑦ Property lines between the headhouse and building frontage of Building A and Building B will be called out in the paving with physical markings. See Civil Drawings for Property Lines
- ⑧ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping and crosswalks as necessary.

**NOTE:**

"BART must comply with requirements enforced by CPUC as BART operates under a CPUC permit for rail operations. In addition, BART must respond and comply with Homeland security requirements impacting mass transit. These changes often result from terrorist activities throughout the world. Therefore it is imperative BART review any conditions or proposal that may impact station or system operations prior to project approval" and "BART has completed an initial review of the project PDP and the project as presented is reasonable to be constructed assuming design criteria outlined in the BART BFS are complied with and staging of the construction is done so as to not impact station and rail operations."

**LEGEND**

-  BULBOUT
-  PAINT STRIPING
-  ADA PARKING OR LOADING
-  SCOOTER CORRAL

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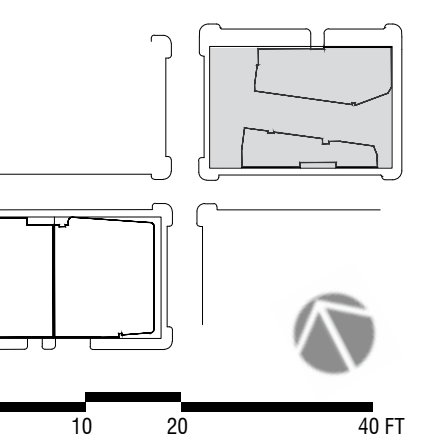
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**BLOCK 1 OVERALL PLAN**

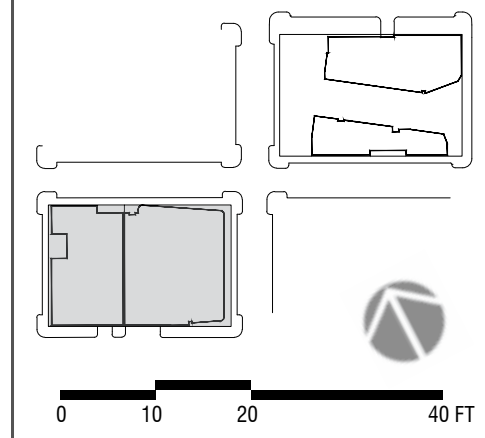
**L2.1**

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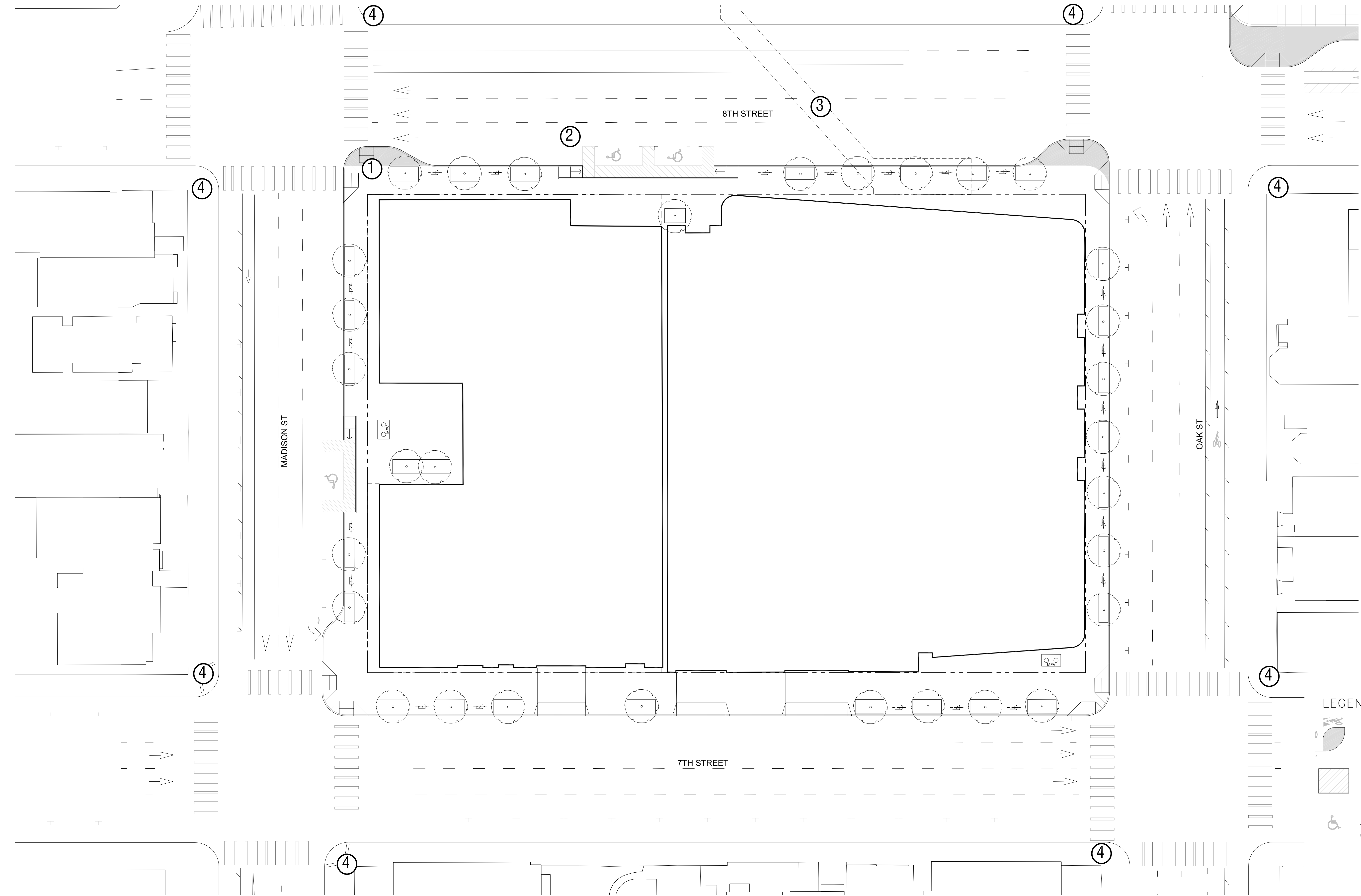
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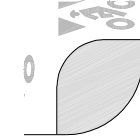




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**BLOCK 2 OVERALL PLAN**

**L2.2**



**LEGEND**

-  BULBOUT
-  PAINT STRIPING
-  ADA PARKING OR LOADING

- NOTES:**
- ① See Civil Drawings for limit of project sidewalk improvements.
  - ② Street access plan striping shown for planning purposes only.
  - ③ Sub surface tunnel (approximate location based On As-Built Drawings from Bart Project #17hc-110).
  - ④ Adjacent corners will be updated to dual directional curbs if existing conditions lack the dual directional curb and are not covered by another project. Striping as necessary.

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## TRANSIT



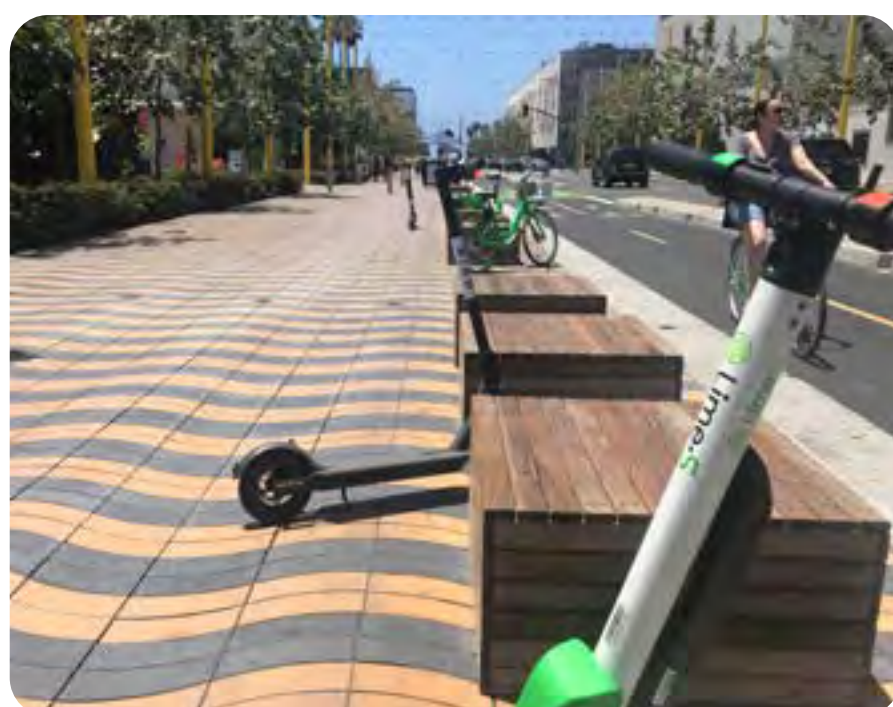
BIKE SHARE



BART STATION



BIKE PARKING



MODE TRANSFER AND WAITING AREAS

## DINING



OUTDOOR DINING



FOOD COURT

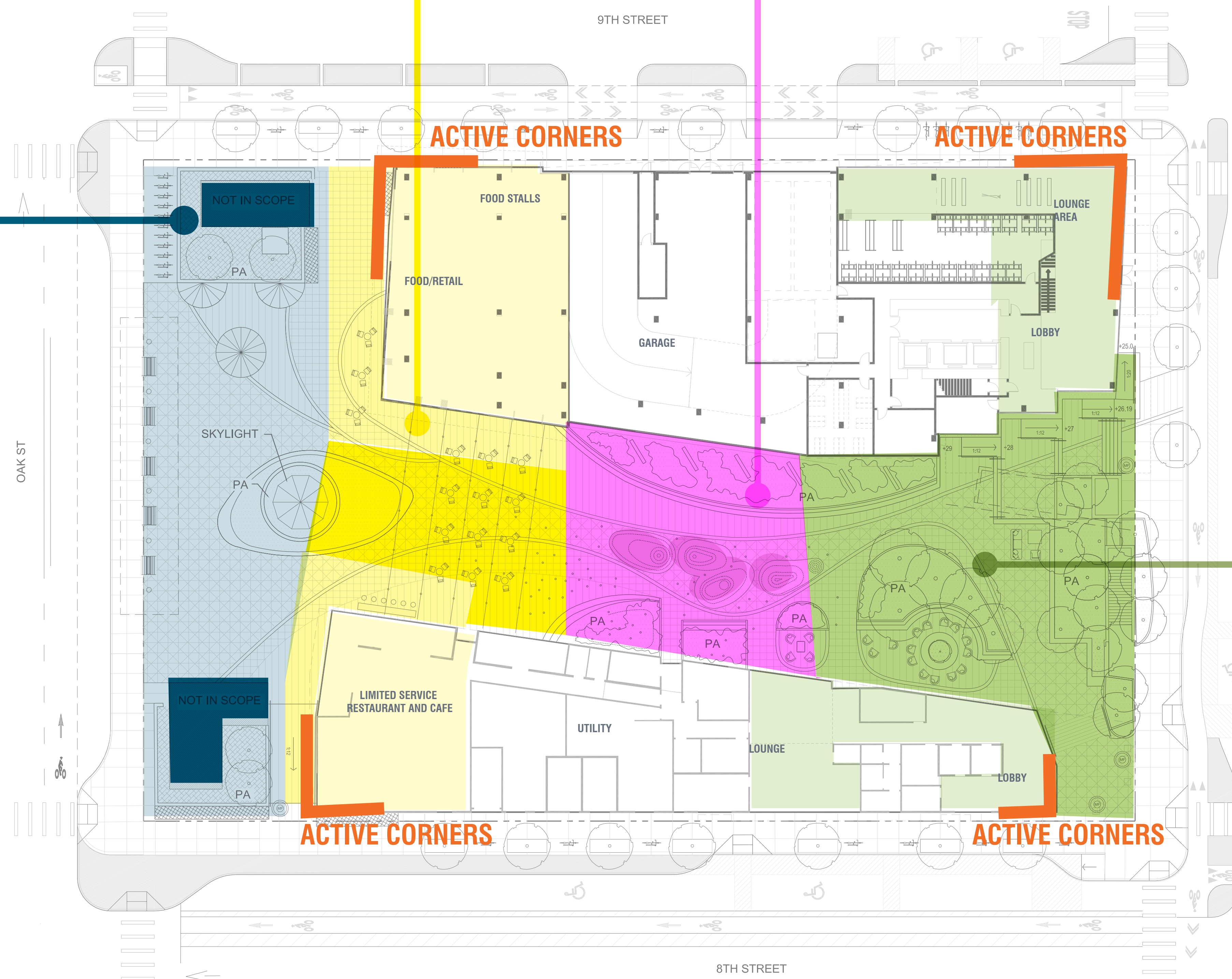
## ART + PLAY



PUBLIC ART/ PUBLIC CULTURE



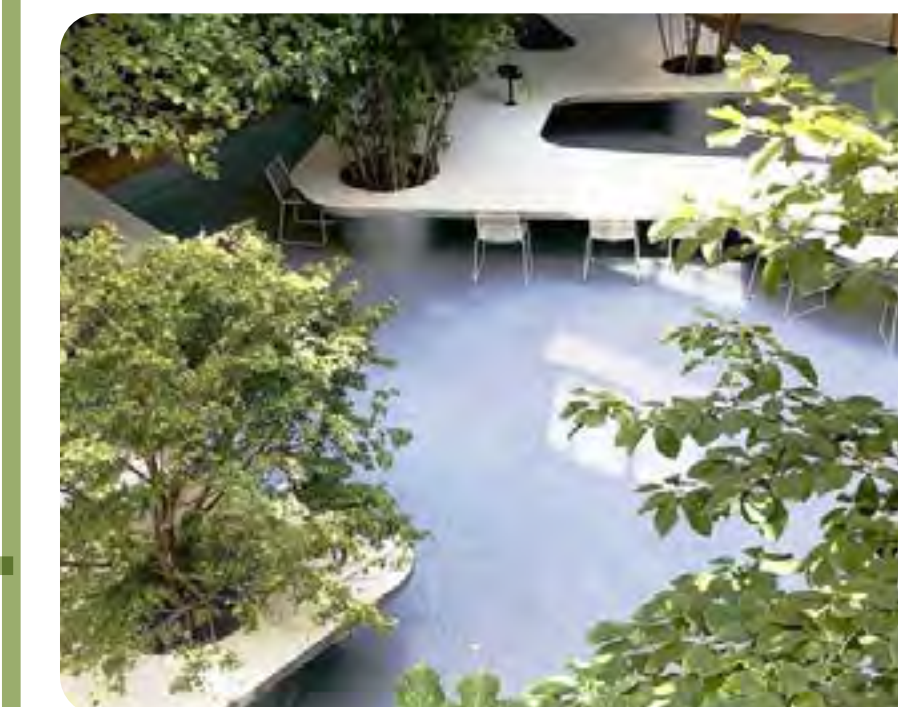
INTERACTIVE SCULPTURE



## RECREATION



COMMUNAL SEATING



GARDEN FURNISHINGS



MAKER SPACE

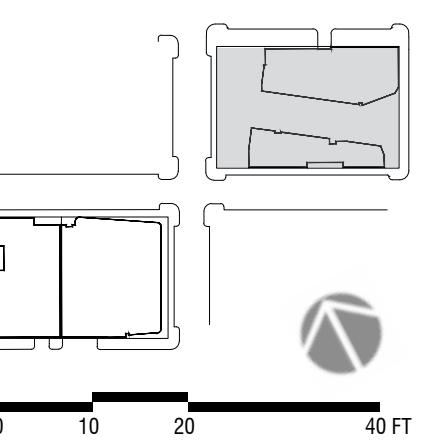
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### BLOCK 1 PROGRAM

# L2.3

## DAYCARE



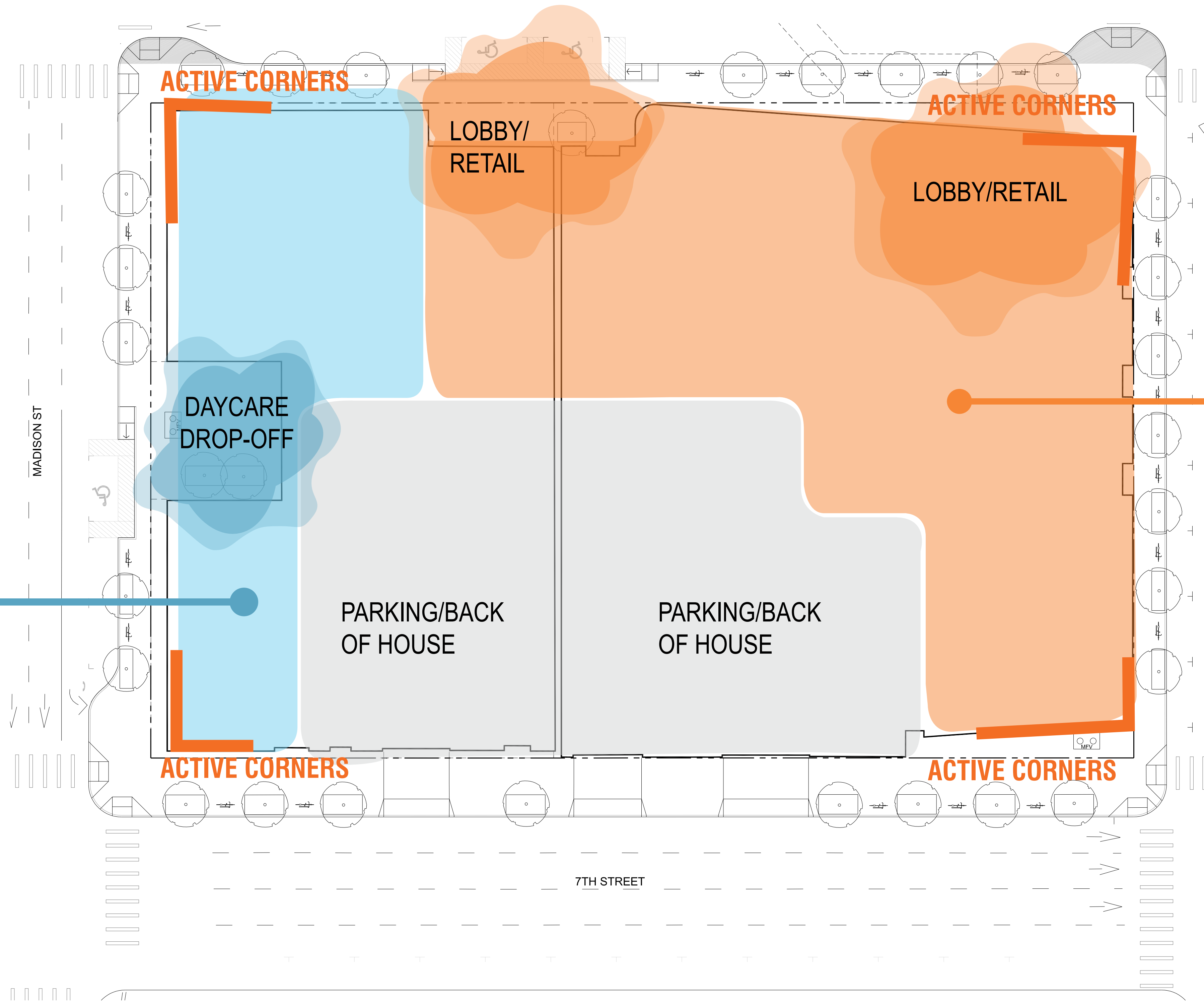
ADJACENCY TO PUBLIC PARKS



DAYCARE DROP-OFF



DAYCARE



## LOBBY/RETAIL



LOBBY



SEMI PUBLIC LOBBY



INFORMAL WORKSPACES

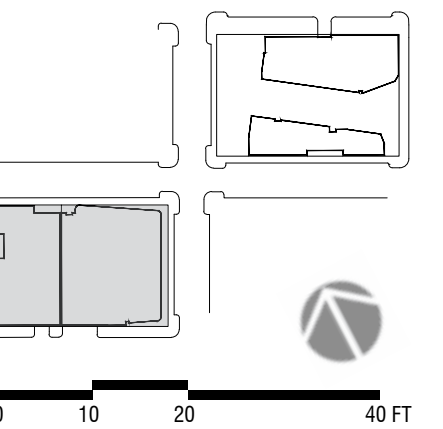
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### BLOCK 2 PROGRAM

# L2.4



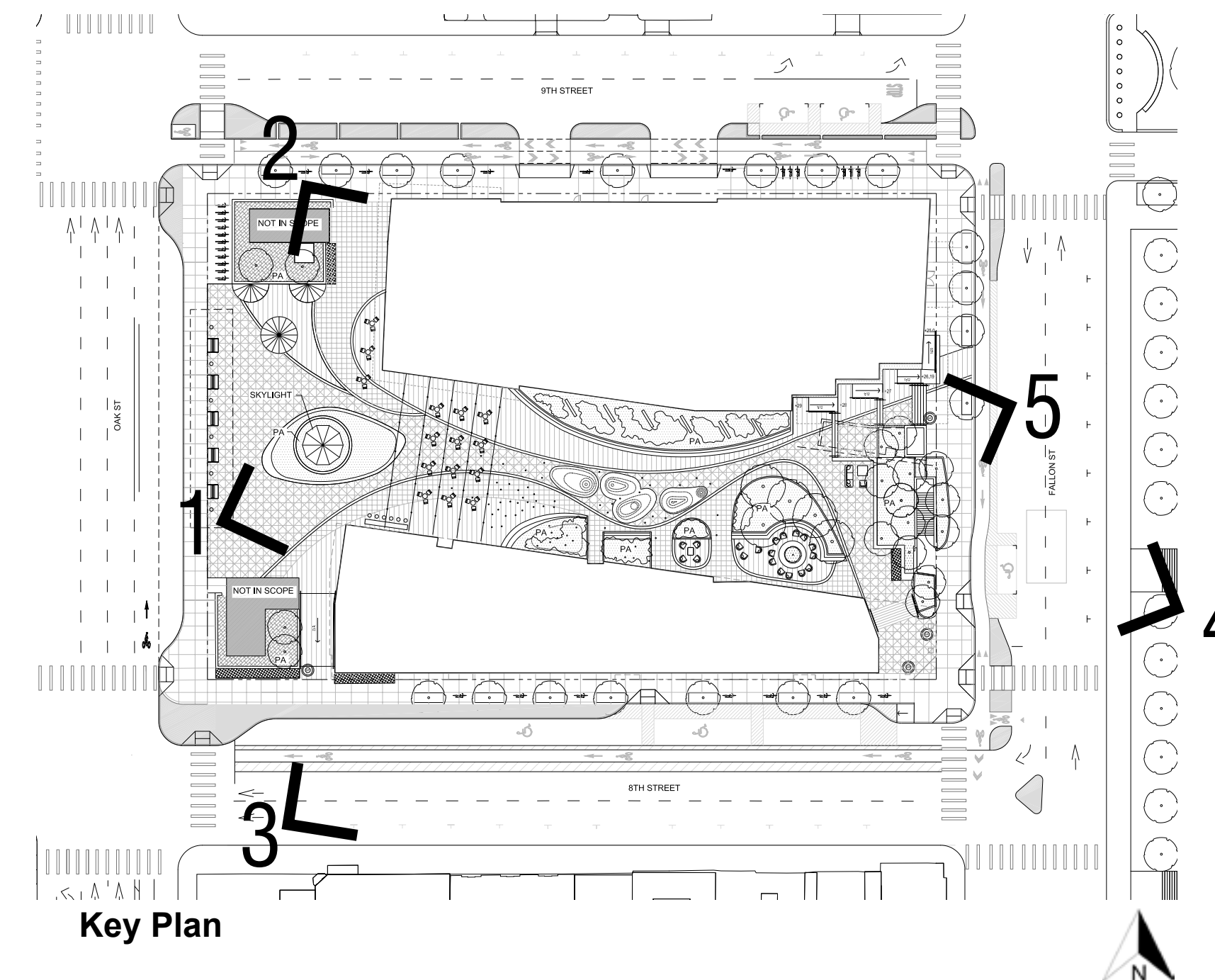
1 Paseo from Oak St.



2 9th St. looking in towards Building A and Paseo



4 Paseo from Fallon St.



3 8th St. looking towards Building B



5 Fallon St. looking at Building A lobby and Paseo

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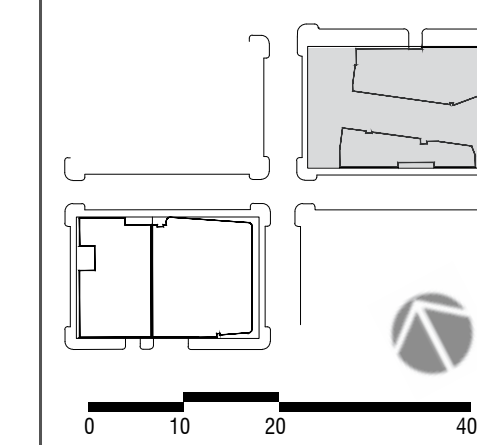
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**BLOCK 1 VIEWS**

**L2.5**

Lake Merritt BART TOD Site #1 - Proposed Street Design Options by Access Plan Team

	8th Street		Fallon Street			9th Street			Oak Street		
	Option 1	Option 2	Option 1	Option 2	Option 3	Option 1	Option 2	Option 3	Option 1	Option 2	Option 3
<b>Travel Lanes</b>	2 lanes WB		1 lane NB, 1 lane SB			1 lane WB, 1 lane EB	3 lanes EB	2 lanes EB	2 lanes NB		3 lanes NB
<b>Transit?</b>	Yes - outside lane should be 11'. Awaiting Confirmation from AC Transit if Bus Only Lane is Needed		Yes - lanes should be 11'			No - 10' Lanes OK, consider 11' outside lane if significant shuttle/paratransit/truck activity anticipated			Yes - outside lane should be 11'; install bus bulb on east side. Move layover elsewhere on the route due to spatial constraints.		Yes - outside lane should be 11' - could stripe outside lane as bus only lane
<b>Traffic Operations</b>	One-Way WB		Existing two-way operations; consider extending two-way operations to 7th Street			Two-Way Conversion (assuming TOD/outside funding is identified to build at least to Harrison Street)			One-way operations		
<b>Curb Use/Parking Lane</b>	Create wide shuttle zone/ accessible passenger loading zone the whole length of block. Design as a pull out with a boarding island. Retain existing Shuttle Zone for most of north side; add accessible passenger loading needed for senior housing entrance; assume no metered parking on north side.		Passenger loading needed along Laney frontage, west side pick up and drop off near paseo entrance			Passenger loading zone likely most of block (for BART and tower) plus commercial loading (? for tower). 5' buffer preferred for passenger/goods loading.			Passenger loading zone on west side - add passenger side buffer if space allows; bus stop on east side. Layover becomes a pullout or moves to another location if not enough space with double bus stop. No metered parking.		
<b>Bikeway</b>	One-way WB In-Roadway Protected Bike Lane	Buffered bike lane WB (existing condition)	In-Roadway, Directional Protected Bike Lanes	In-Roadway, Two-Way Protected Bike Lane	Shared Street (?) with Two-Way Protected Bike Lane	Raised (or in roadway) Two-Way Protected Bike Lane	Raised One-Way Protected Bike Lane	In-Roadway One-Way Protected Bike Lane	Raised Two-Way Protected Bike Lane	In-Roadway Two-Way Protected Bike Lane	In-Roadway Two-Way Protected Bike Lane
<b>Bike Implications/Notes</b>	Upgrade existing bike lane as safety/comfort enhancement though lower bike priority	Not upgraded due to bike priority on 9th Street	Cross-section should continue between 7th and 10th			Addresses BART and TOD tower access needs; Consider directional PBLs west of Oak Street		Does not resolve need for two-way bike traffic to BART and TOD site	Would need channelization of pedestrian coming from passenger loading; buffer needs to be at least 4' for loading		Would need channelization of pedestrian coming from passenger loading plus mid-block ramps
<b>Sidewalks (including furnishings zone)</b>	Widen north sidewalk widened by 1.5'		West Sidewalk Widened by 6'	West Sidewalk Widened by X'	West Sidewalk Widened by X'	South sidewalk widened by 6.5'	West Sidewalk Widened by X'	West Sidewalk Widened by X'	No sidewalk widening (beyond bus bulb)		No sidewalk widening
<b>Intersection Notes</b>	Protected intersection at 8th/Oak and 8th/Fallon plus pedestrian safety upgrades		Upgrade intersections at 9th/Fallon and 8th/Fallon with Protected Intersections and Pedestrian Safety Enhancements			Special signalization (bike contra-flow signal), pedestrian safety upgrades needed at most signals; 9th/Fallon and 9th/Oak protected intersections plus pedestrian safety enhancements. Need to assess need for left-turn pockets.		9th/Fallon and 9th/Oak protected intersections plus pedestrian safety enhancements	Special signalization (bike contra-flow signal); pedestrian safety upgrades needed at most signals; 9th/Oak and 8th/Oak protected intersections plus pedestrian safety enhancements		
<b>Pedestrian Implications/Notes</b>	Consideration of matching DOSP sidewalk widening (at least on north side of street) and cross-section		Consideration of mid-block pedestrian crossings (or channelization) given location of double staircase + ramp		May address need for mid-block	Consideration of Pedestrian access across the PBL from the heavily used passenger loading zone					
<b>Access Team Recommends Moving the Cross-Section Forward?</b>	Yes - recommend assuming north curbline/sidewalk consistent with DOSP	No - adding the protection to the bike lane will enhance safety and comfort	Yes - Recommend directional protected bike lanes between 7th and 9th Street with two-way conversion between 7th and 8th. Consider EITHER (1) directional or (2) two-way protected bike lanes between 9th and 10th, depending on how realistic East Bay Greenway project on 10th Street is.	No - not a strong reason to do a two-way protected bike lane south of 9th Street.	No - may detract from TOD paseo. Would require traffic diversion, Laney College stakeholder engagement, and slow design speed.	Yes - between Oak and Fallon Streets. Raising it will allow for a comfortable streetscape in front of the tower. Note that DOT would rather see that investment made to extend the protected bike lane farther west. PBLs can continue as directional or two-way west of Oak Street.	No - does not resolve need for two-way bike traffic along TOD site	No - does not resolve need for two-way bike traffic along TOD site	No - would not be consistent with DOSP though might function well against passenger loading zone today.	Yes - could be converted consistent with DOSP cross-section future. Consistent curbline needed with bus bulb.	No - inconsistent with recently installed pedestrian safety project + ongoing need to address High Injury Intersections
<b>Consistency with Other Plans</b>	-Consistent with Let's Bike Oakland -Allows for long-term two-way conversion and consistency with Downtown Oakland Specific Plan assuming streetscape is entirely rebuilt OR streetscape can be designed to make sense with a sidewalk extension of 8.5'	-Inconsistent with Let's Bike Oakland, which calls for protected bike lane -Allows for long-term two-way conversion and consistency with Downtown Oakland Specific Plan assuming streetscape is entirely rebuilt OR streetscape can be designed to make sense with a sidewalk extension of 8.5'	-Consistent with Let's Bike Oakland -Consistent with Downtown Oakland Specific Plan -Inconsistent with East Bay Greenway	-Consistent with Let's Bike Oakland -Consistent with Downtown Oakland Specific Plan -Consistent with East Bay Greenway	-Inconsistent with Let's Bike Oakland -Consistent with Downtown Oakland Specific Plan -Consistent with East Bay Greenway	-Allows for near-term OR long-term two-way conversion and consistency with Downtown Oakland Specific Plan (DOSP) -DOSP shows directional protected bike lanes, but suggest need for two-way between Oak and Fallon Streets -Consistent with Let's Bike Oakland	-Consistent with Alameda CTC Access Project, but widens buffer -Narrows the double-wide passenger loading zone on west, but provides a buffer between the passenger loading zone and the travel lane -Two travel lanes consistent with OakDOT pedestrian safety project -Allows for long-term two-way conversion and consistency with Downtown Oakland Specific Plan -Consistent with Let's Bike Oakland				

For the block one street designs a series of options were developed by the design team for evaluation by the Access Plan Team (Fehr and Peers and Eisen|Letunic). Comments for each option are shown above and a preferred direction was selected for the project plans. Any time signals are added/alterd we will recommend the traffic engineer to provide an accessible pedestrian signal (aps) upgrade as referenced in Oakland Walks! Pedestrian Plan (2017, specifically recommended at 9th and Fallon), the draft Downtown Specific Plan, in PR AG and MUTCD). Because the final street design and full spectrum of transportation issues will continue to evolve options were selected for maximum future flexibility as well as their success meeting the current stated programs of various transportation entities. A future proofing test for the Downtown Oakland Specific Plan is provided for each option to demonstrate flexibility. As additional criteria become apparent, additional testing of the flexibility of the scheme can be added

**LAKE MERRITT BART REDEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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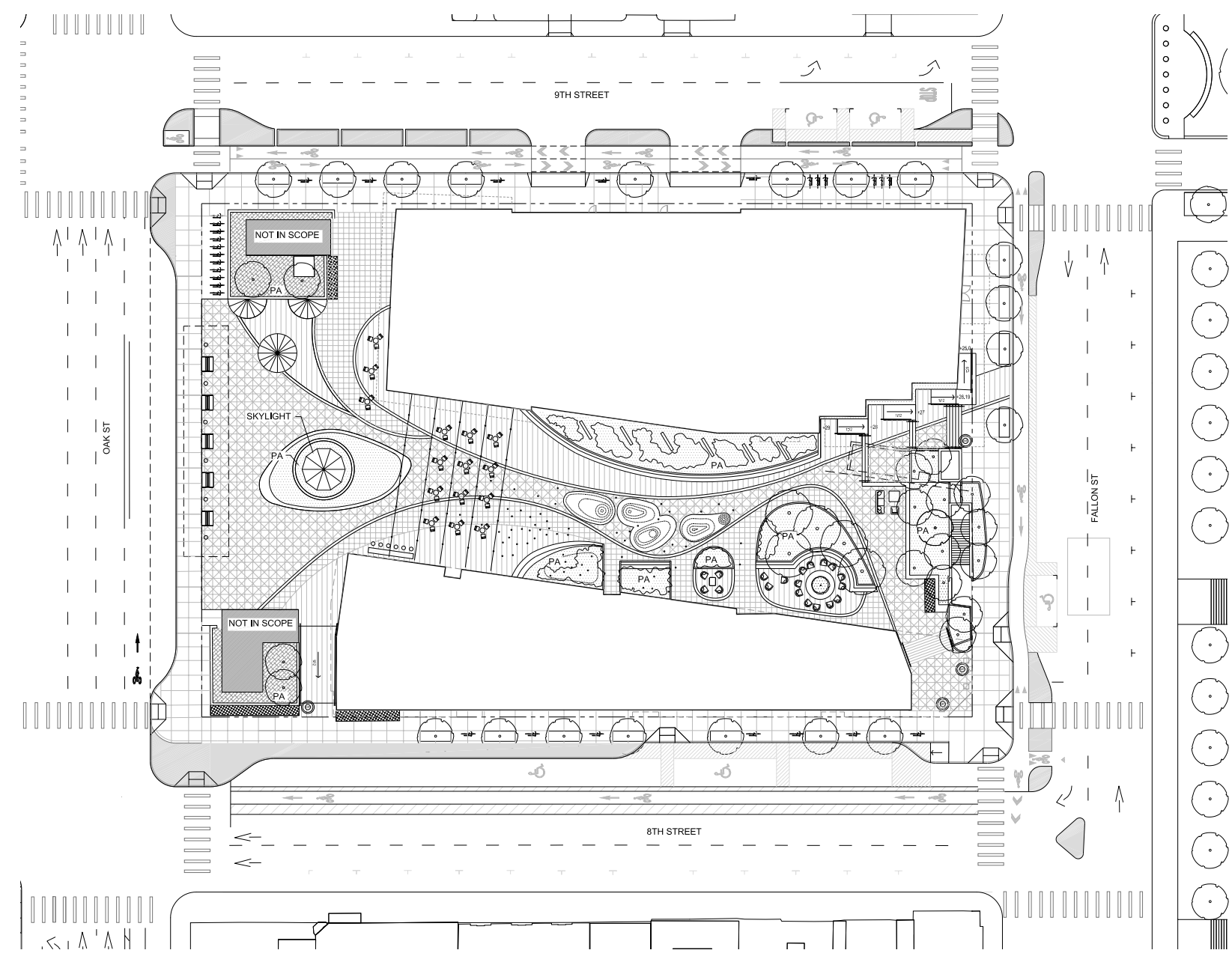
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6	REVISED RESPONSE TO POPPI COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPPI COMMENTS	03/17/2021

DATE: AUGUST 8, 2019

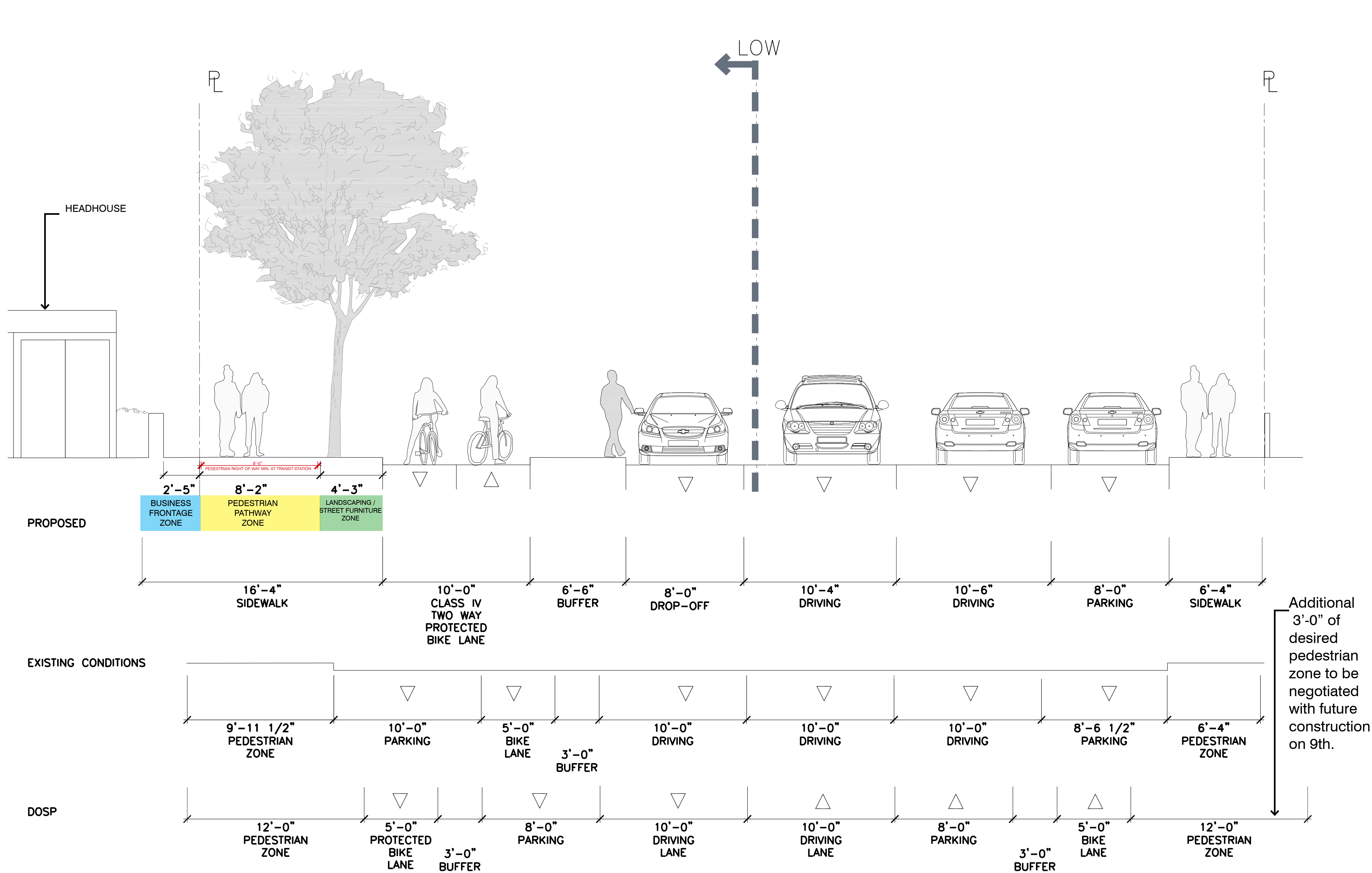
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BLOCK 1 ACCESS FRAMEWORK

L3.1

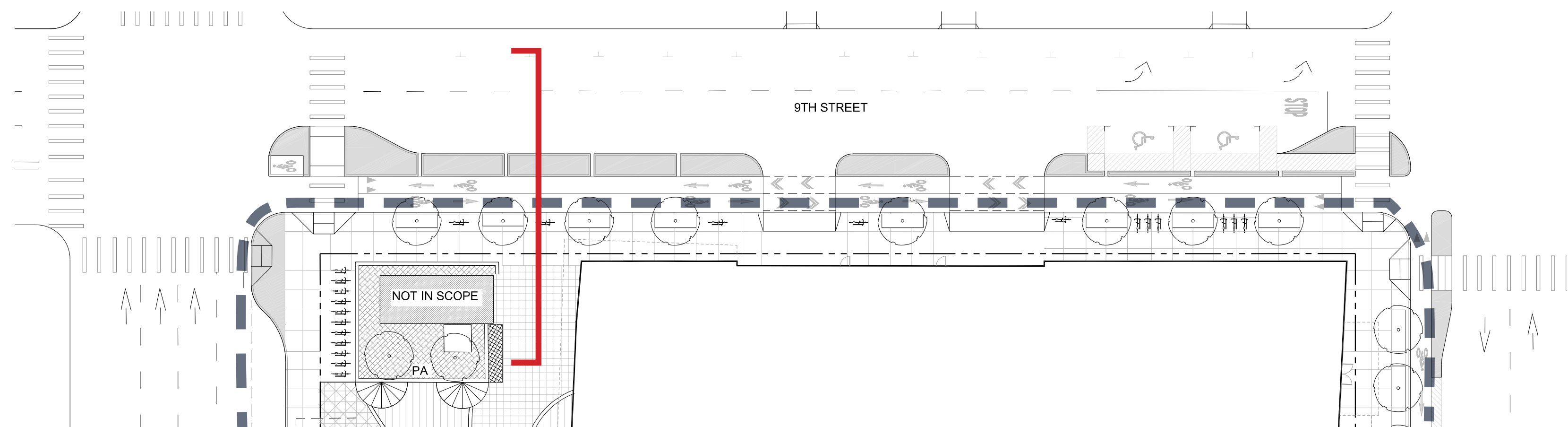


Key Plan



Typical Cross Section

Note:  
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Plan

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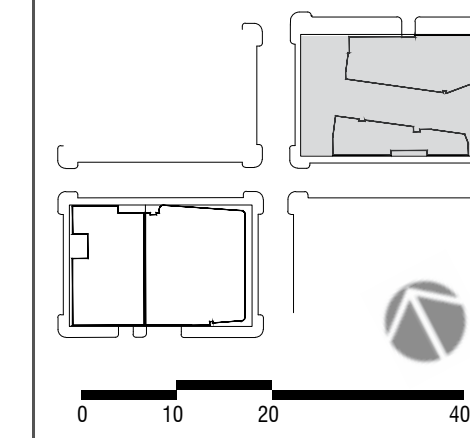
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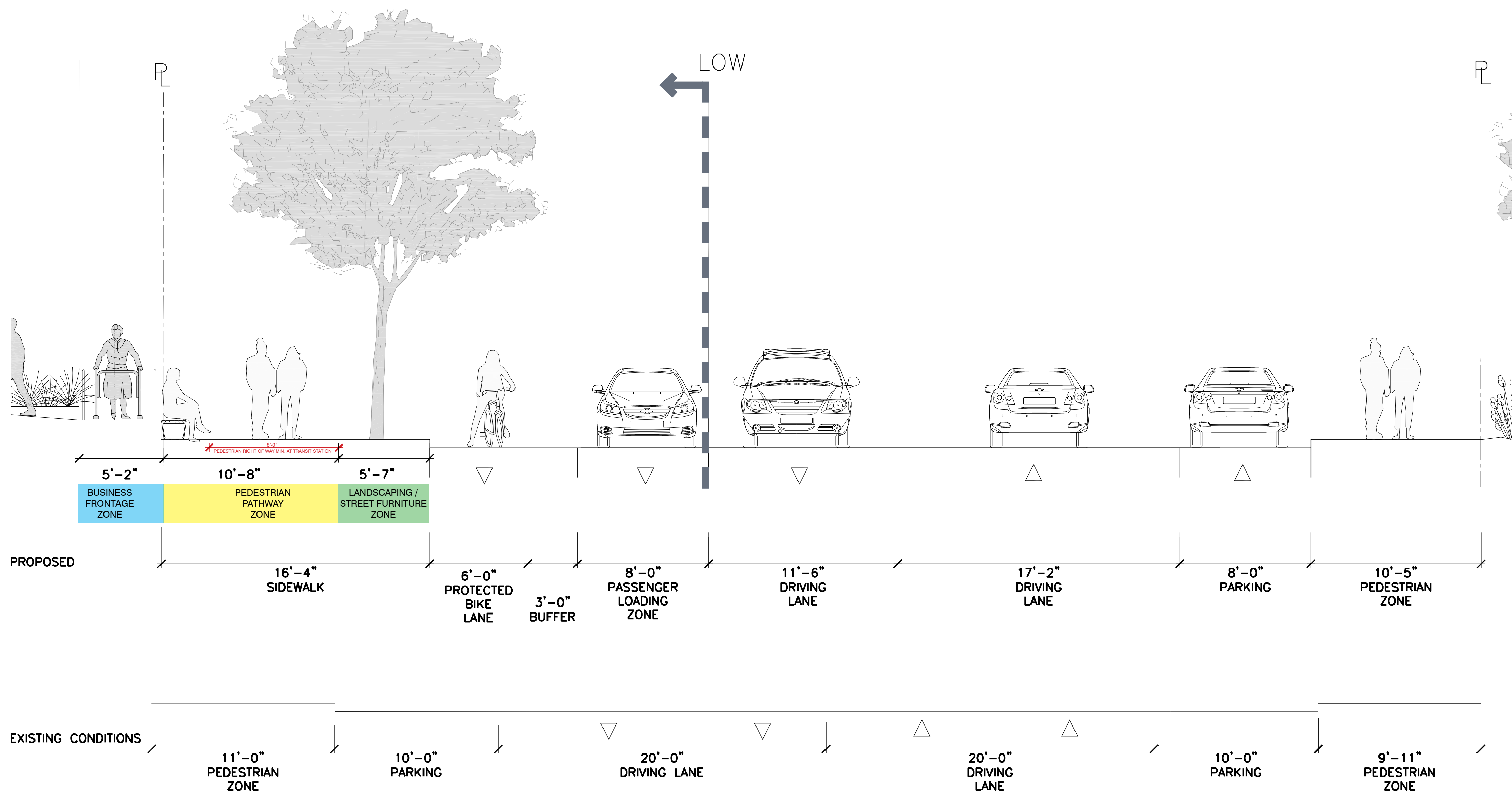
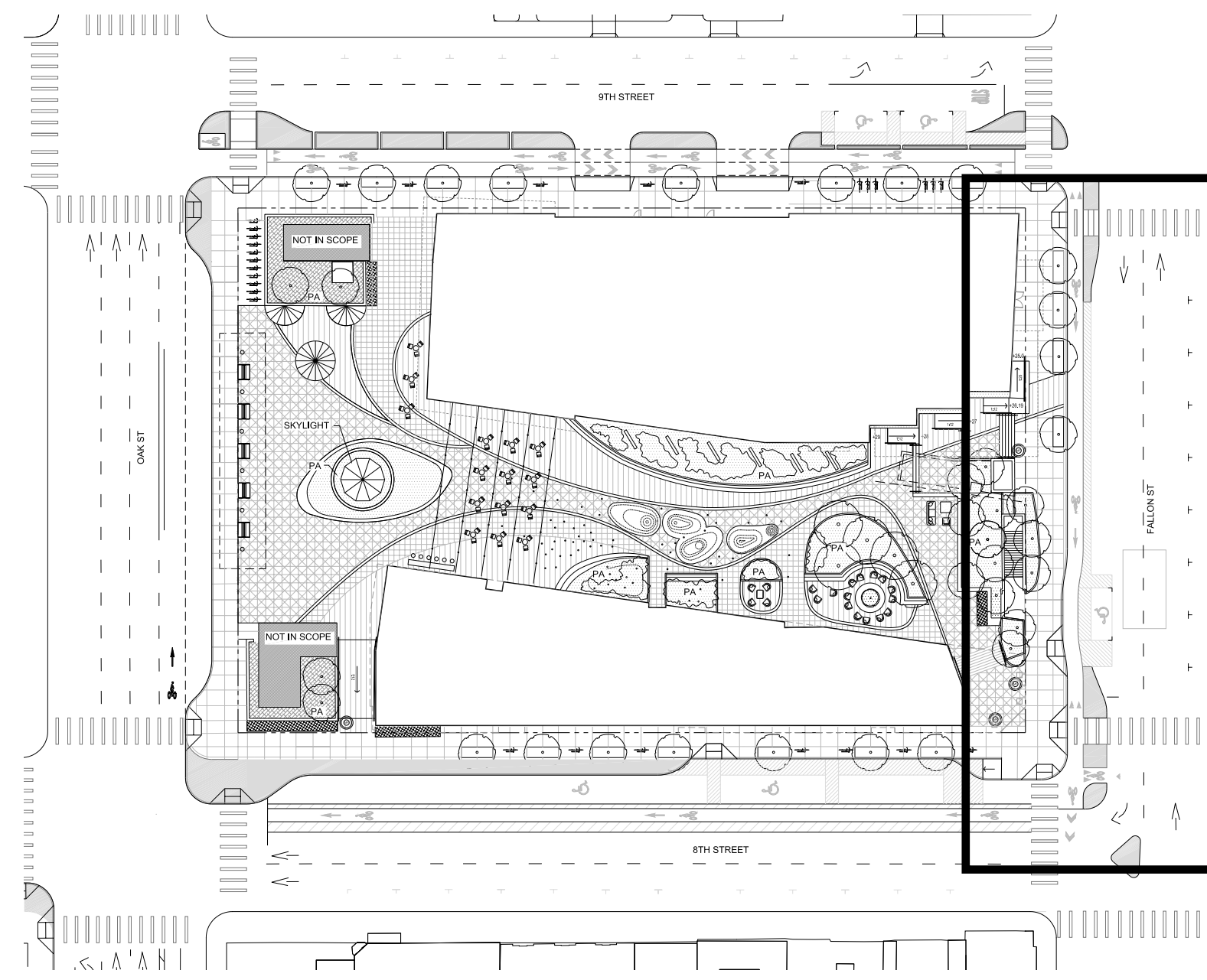
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**BLOCK 1 9TH STREET SECTIONS**

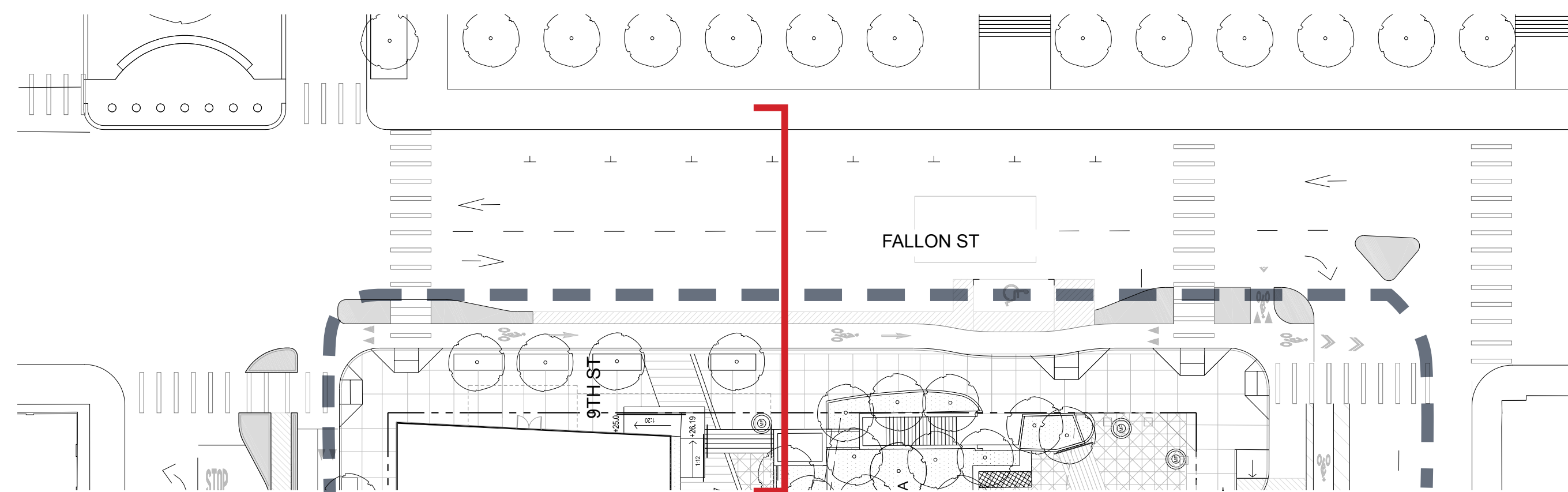
**L3.2**





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**Typical Cross Section**



**Plan**



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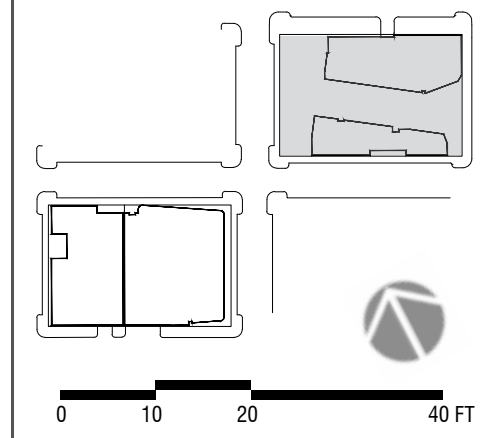
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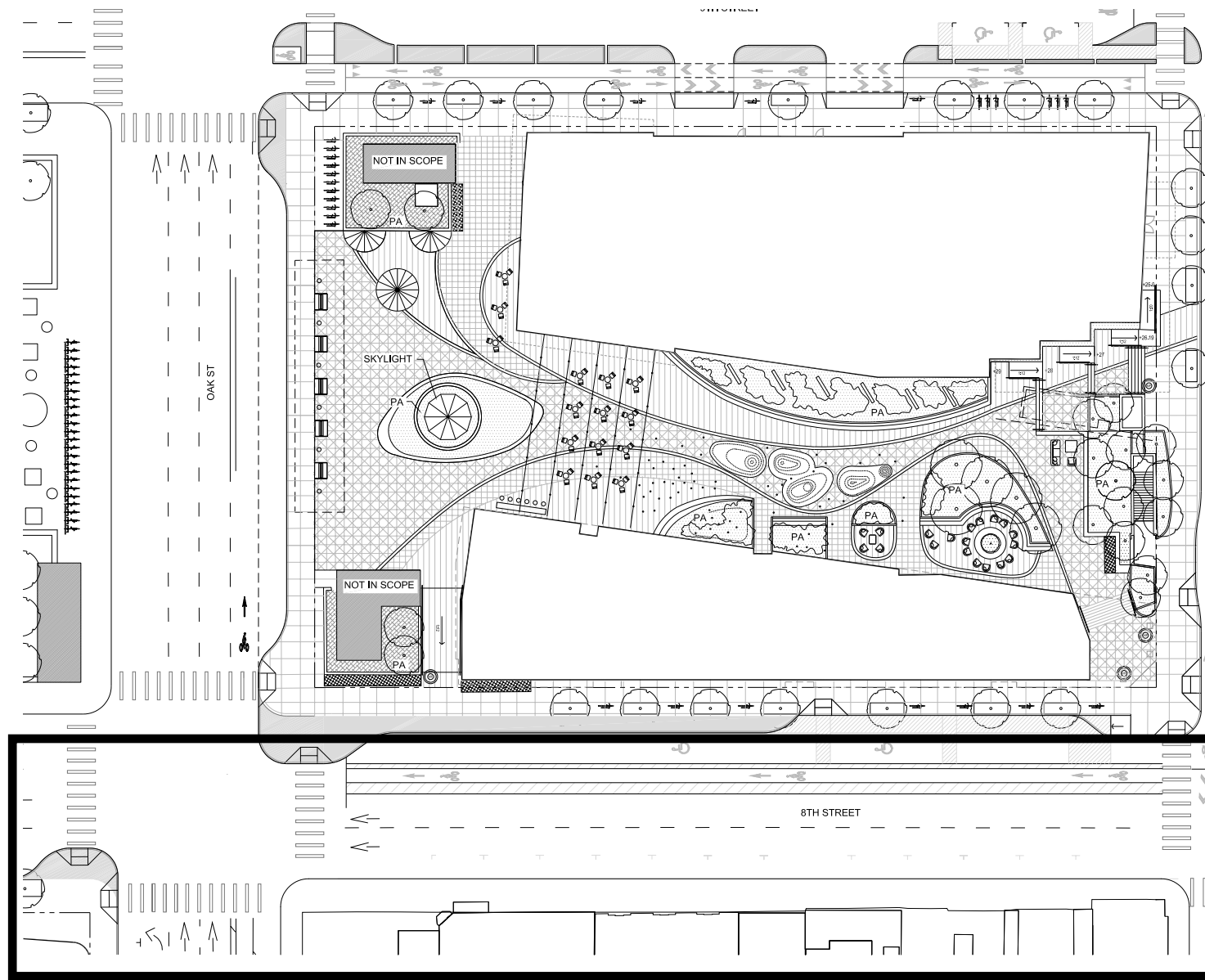
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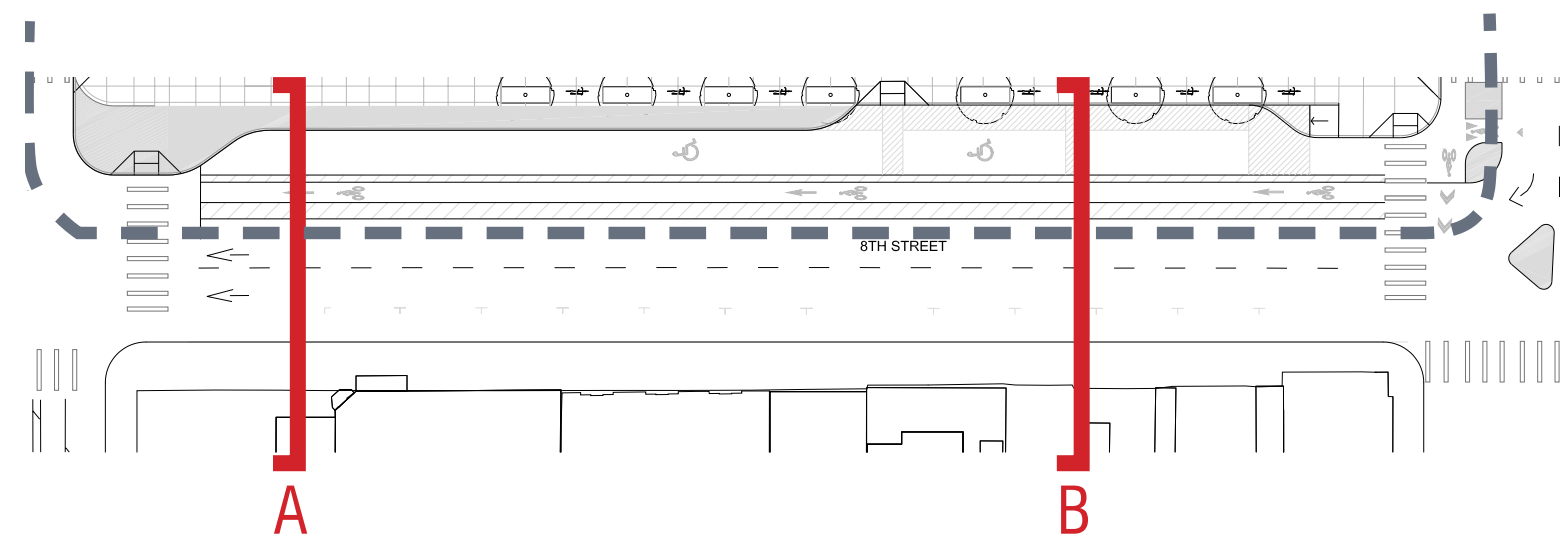


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**BLOCK 1 FALLON STREET SECTIONS**

**L3.3**



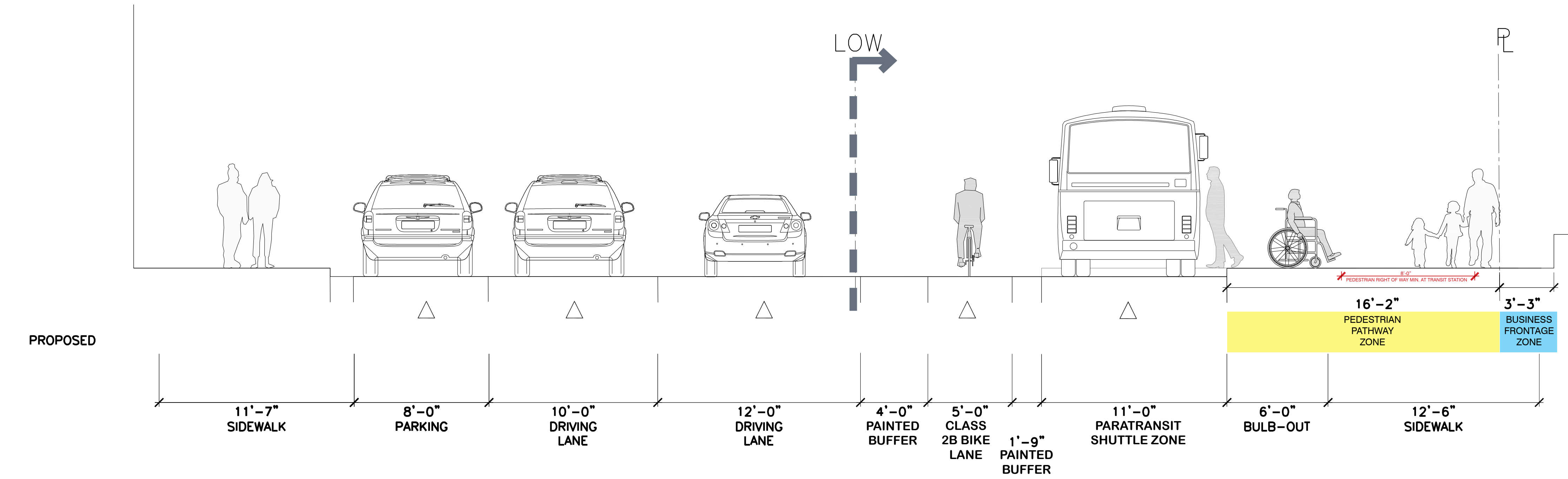
Key Plan



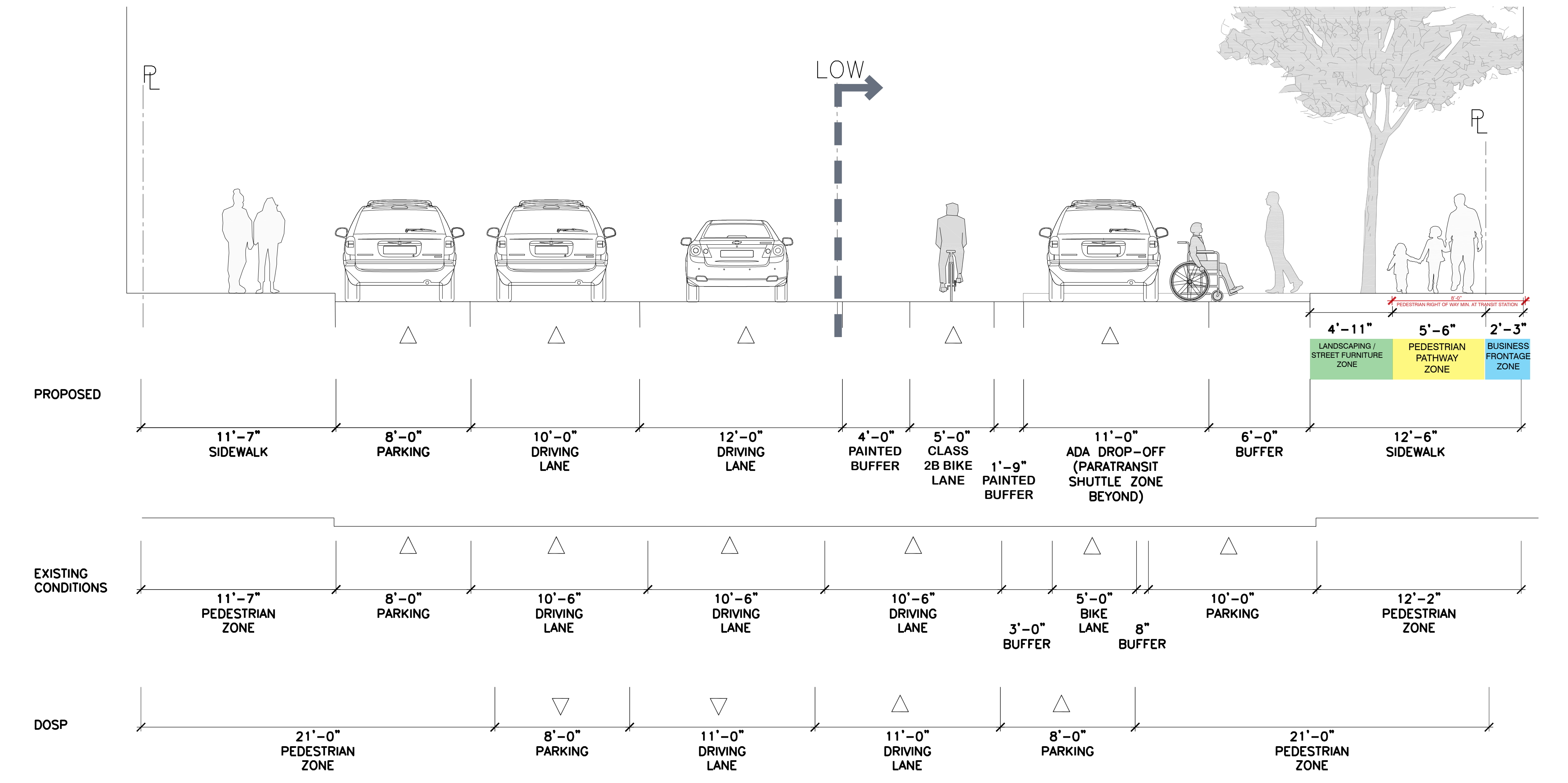
Plan



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Typical Cross Section: A  
Shuttle Zone



Typical Cross Section: B  
Not official BART Loading

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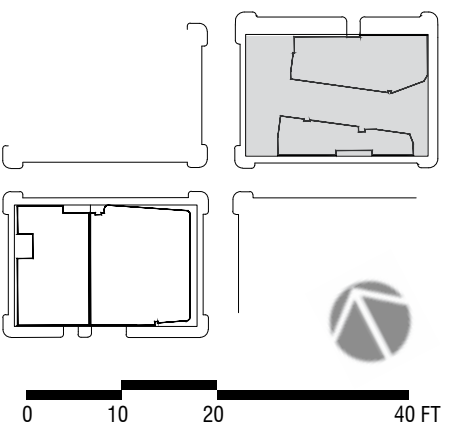
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

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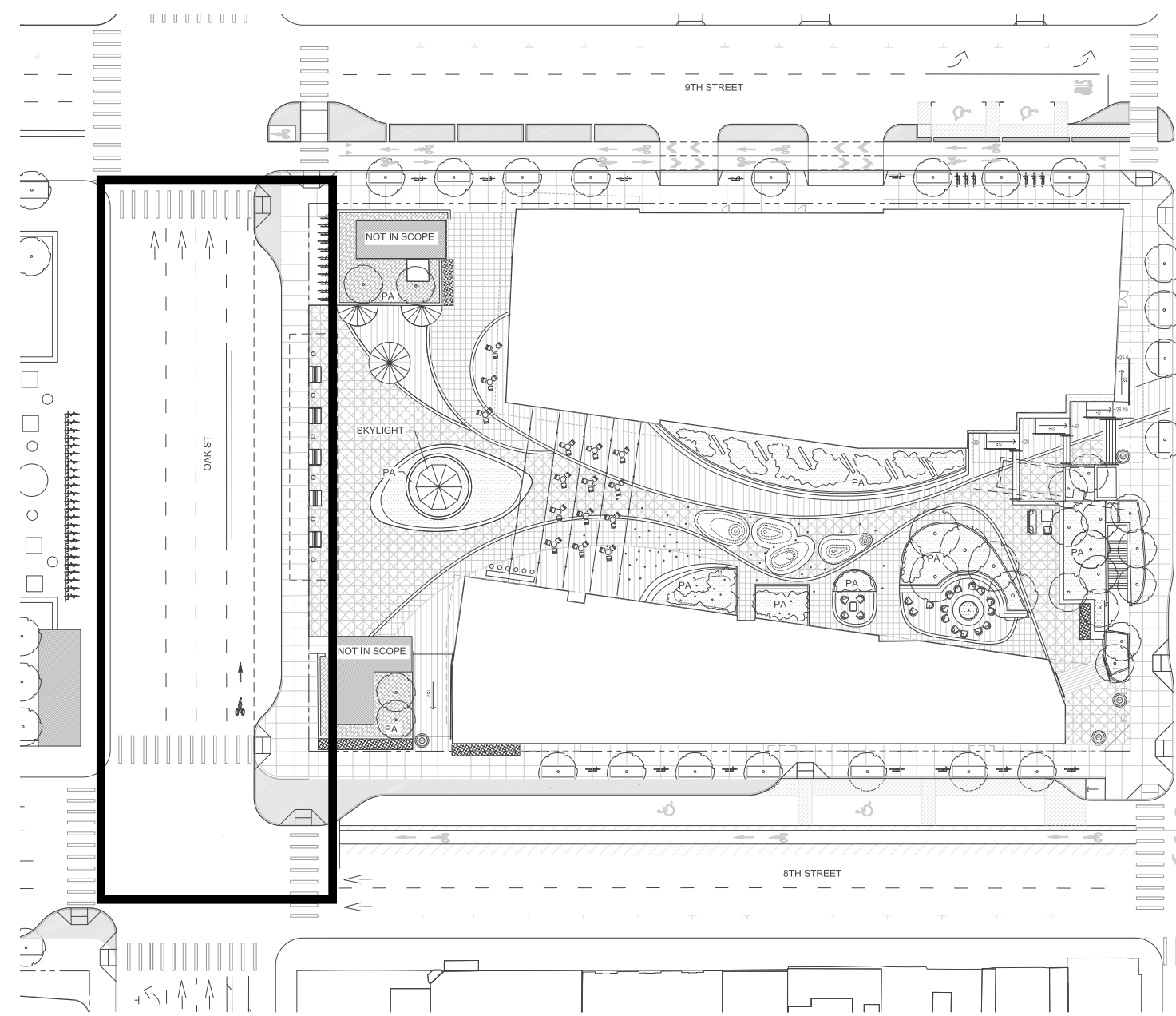
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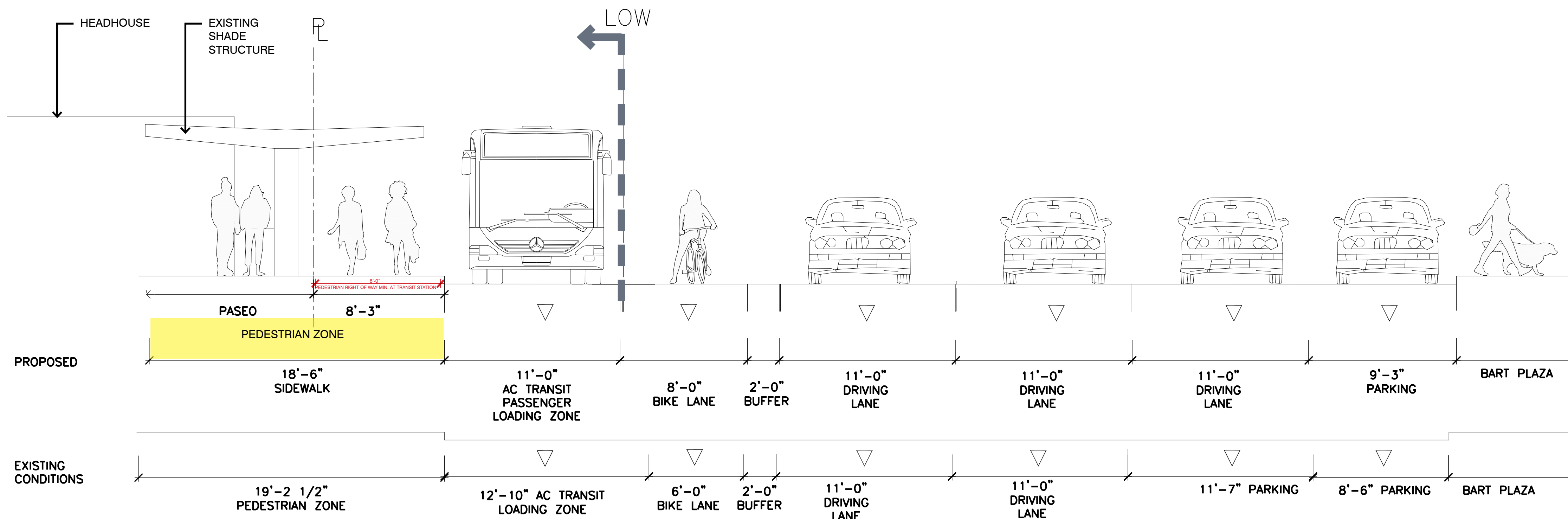
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### BLOCK 1 8TH STREET SECTIONS

# L3.4

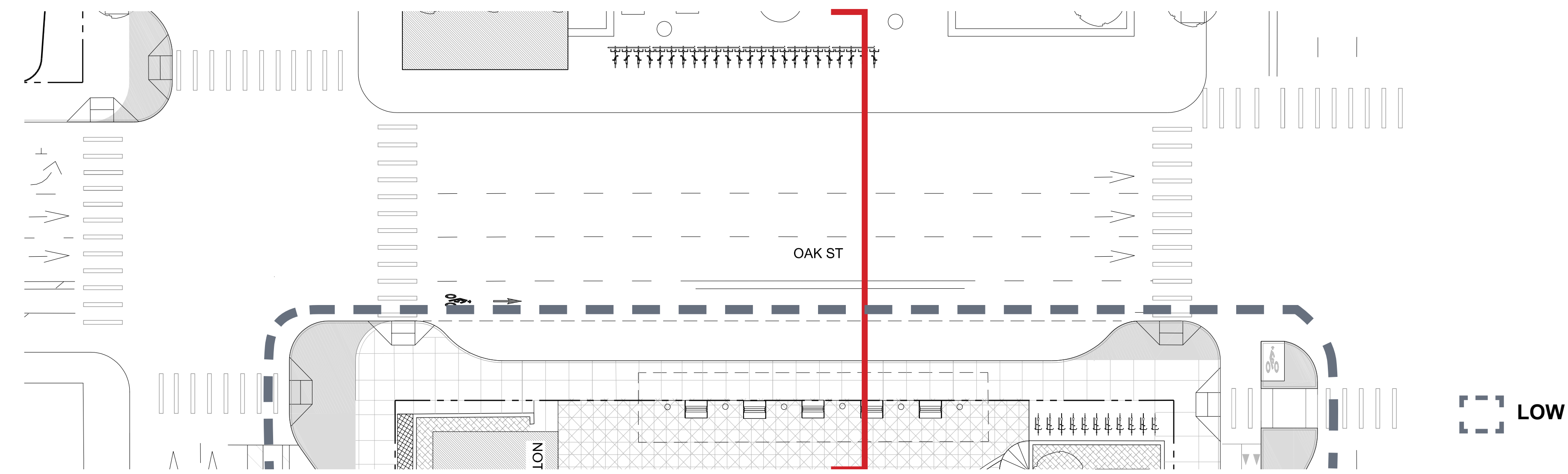


Key Plan



Typical Cross Section

Note:  
Alameda CTC is proposing to construct a two-way bike lane on the west side of Oak, between 3rd and 9th Streets as part of the Oakland-Alameda Access Project. This facility has been planned in conjunction with City of Oakland Planning and OakDot.



Plan

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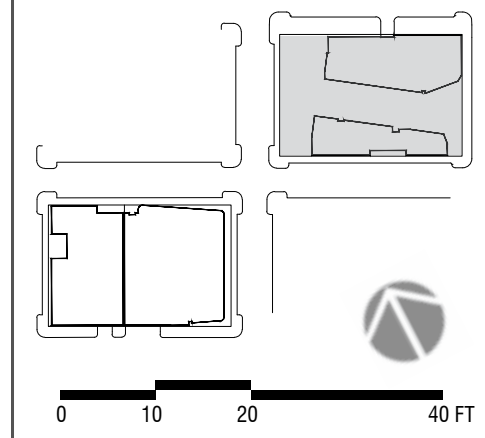
# LAKE MERRITT BART REDEVELOPMENT

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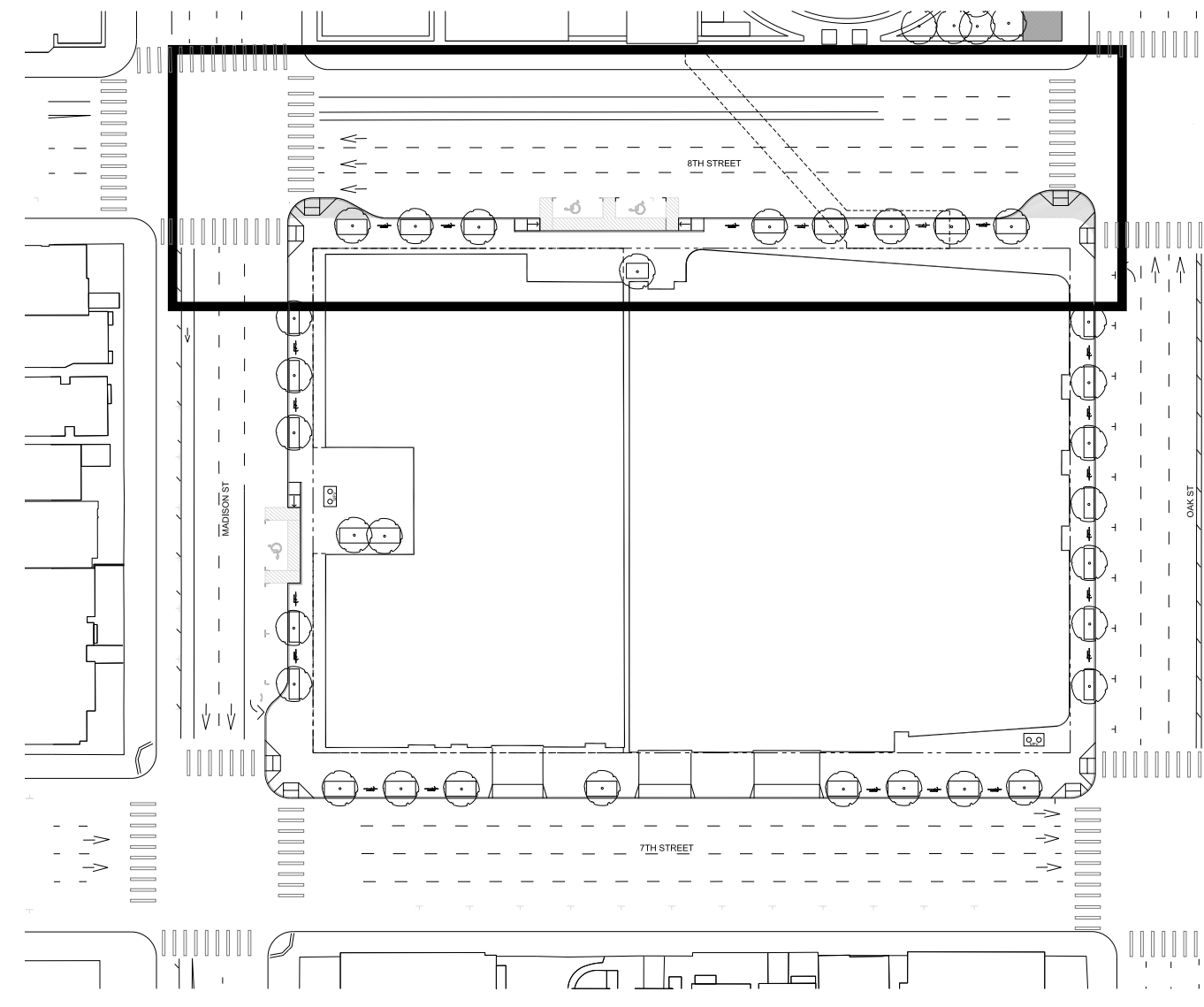
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**BLOCK 1 OAK STREET SECTIONS**

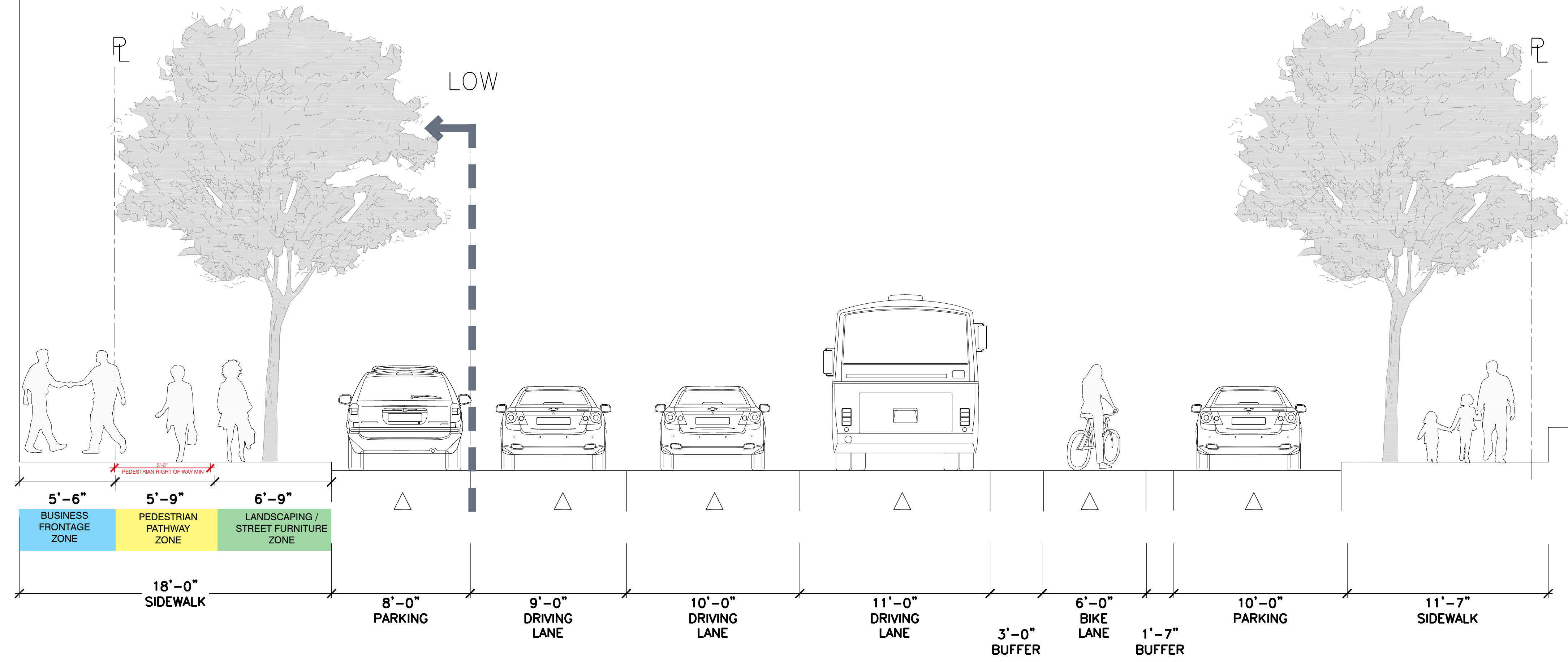
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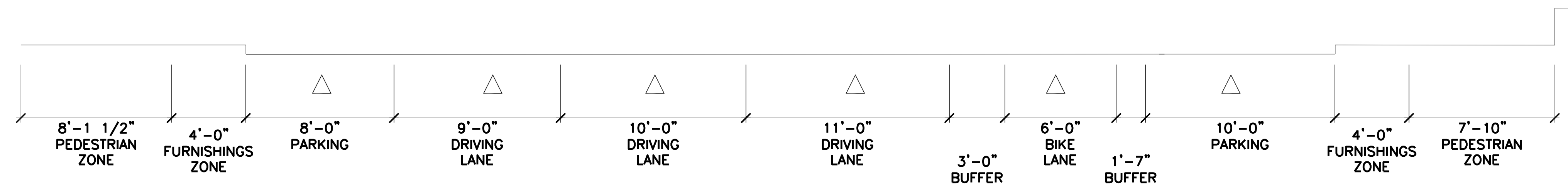
Key Plan



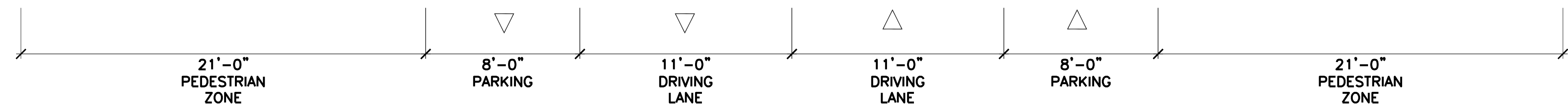
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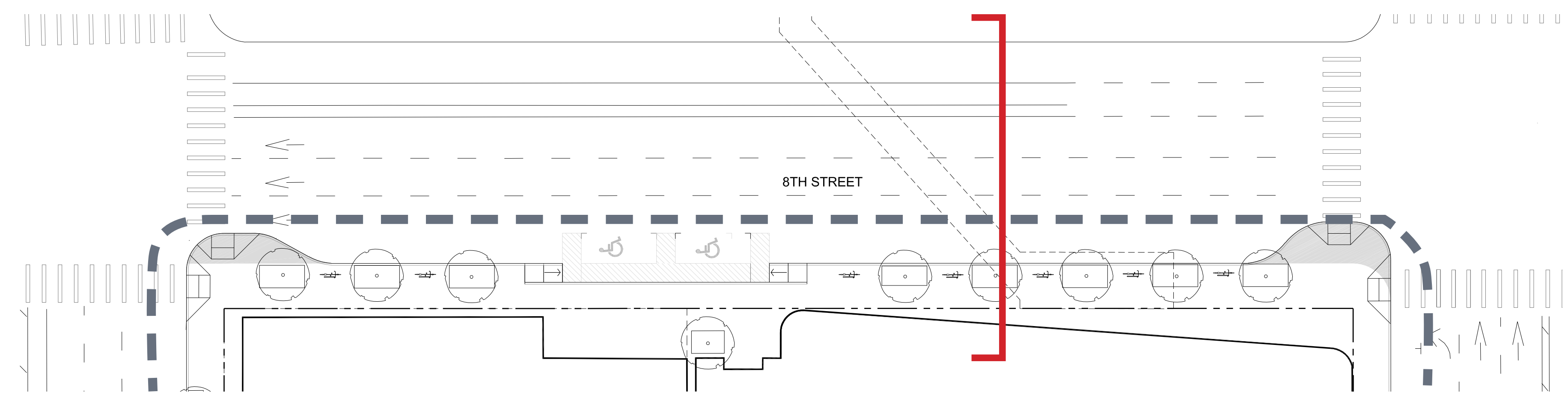
EXISTING CONDITIONS



DOSP



Typical Cross Section



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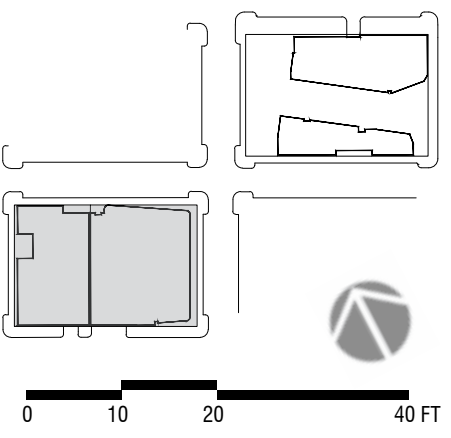
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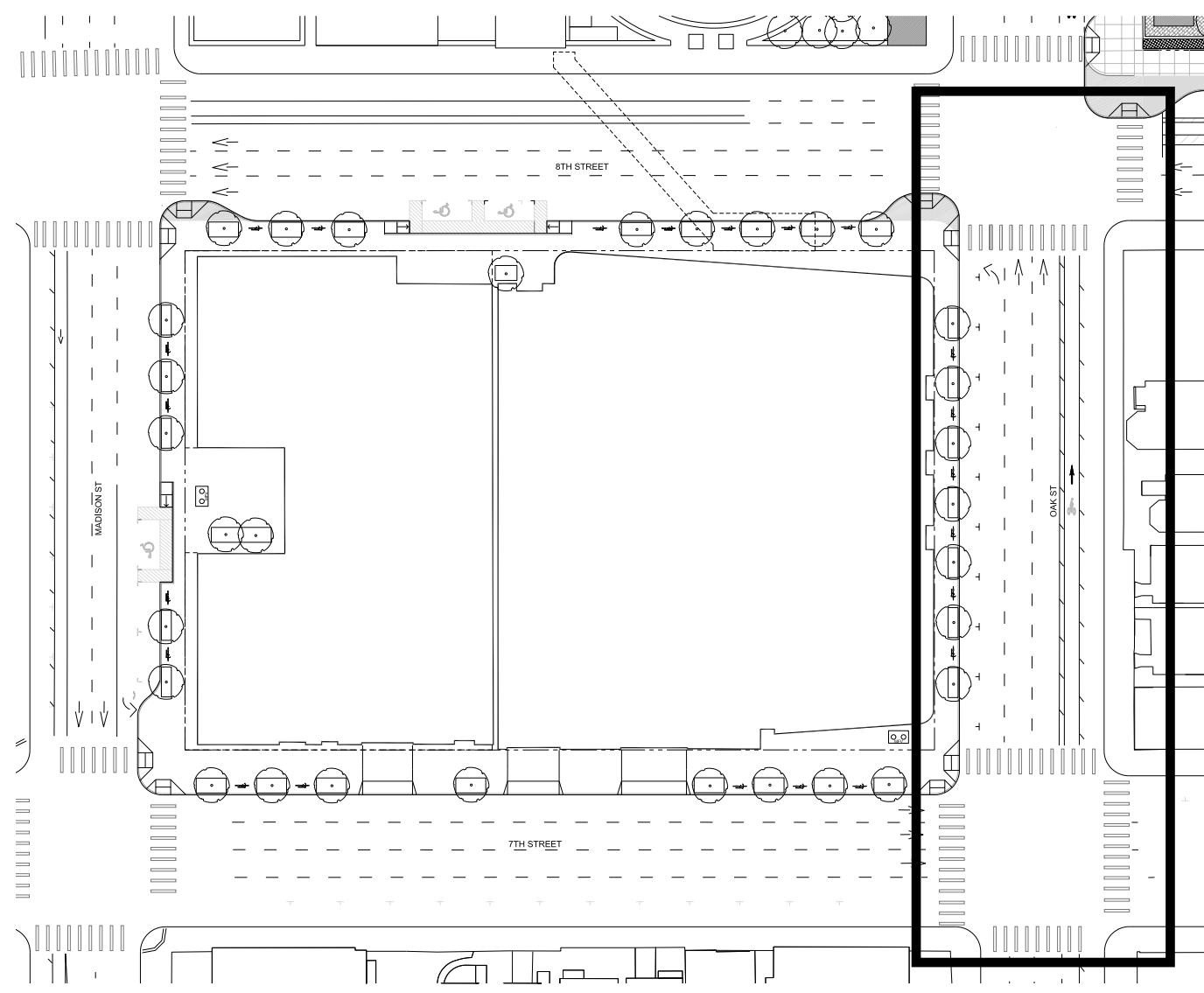
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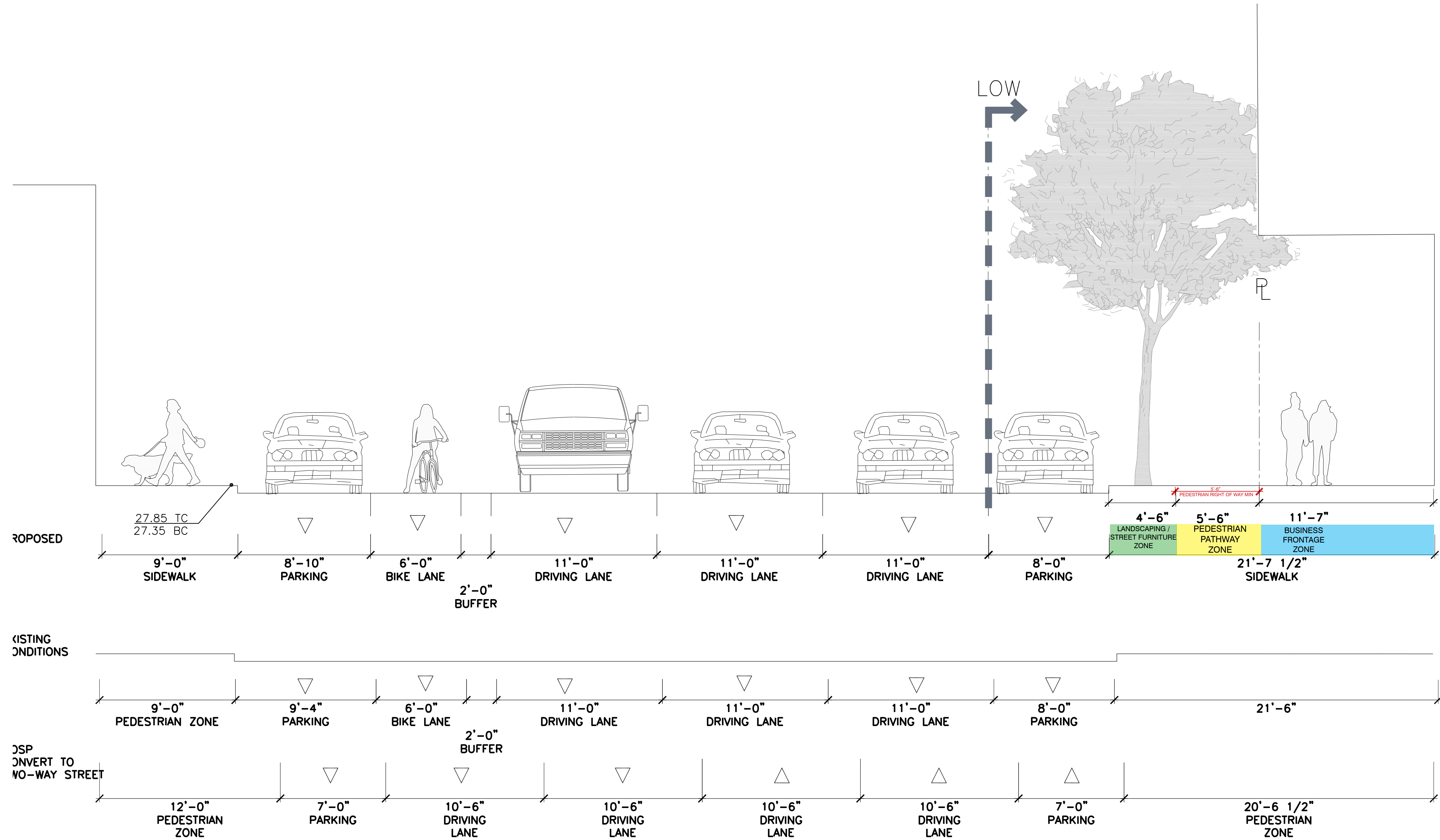
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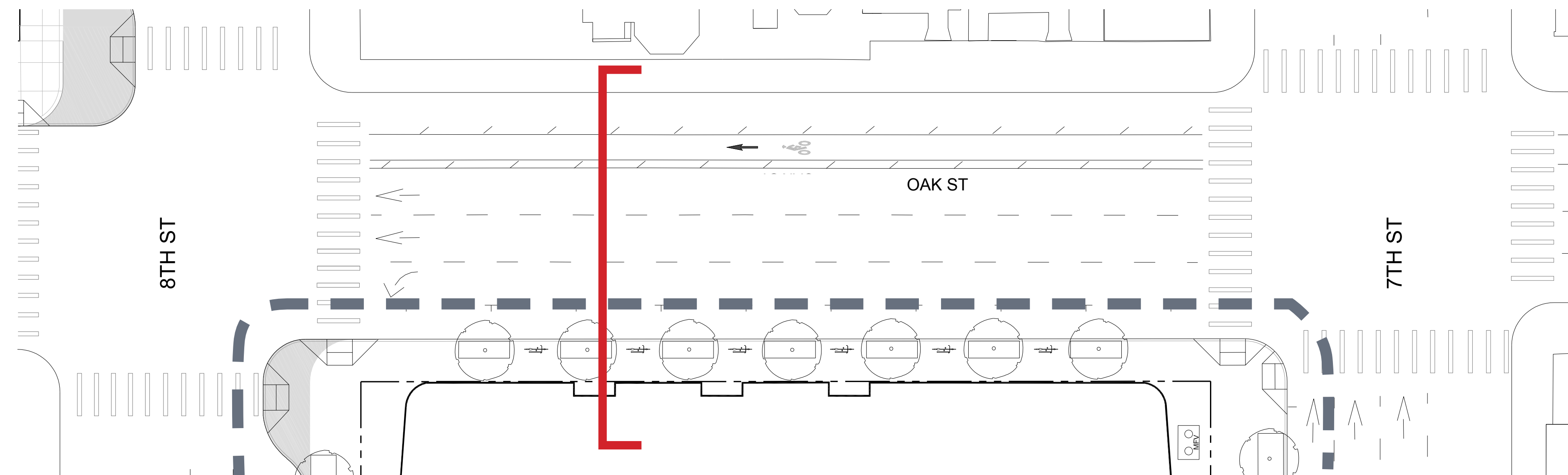
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**BLOCK 2  
8TH STREET  
SECTIONS**



Key Plan



Typical Cross Section



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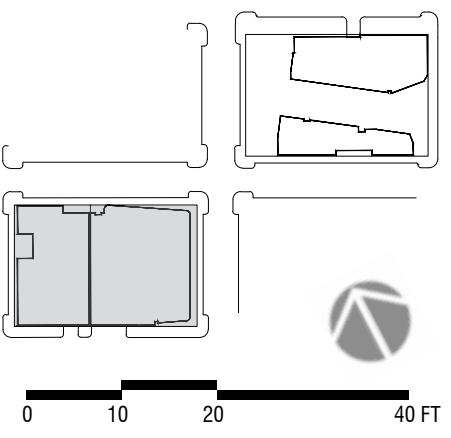
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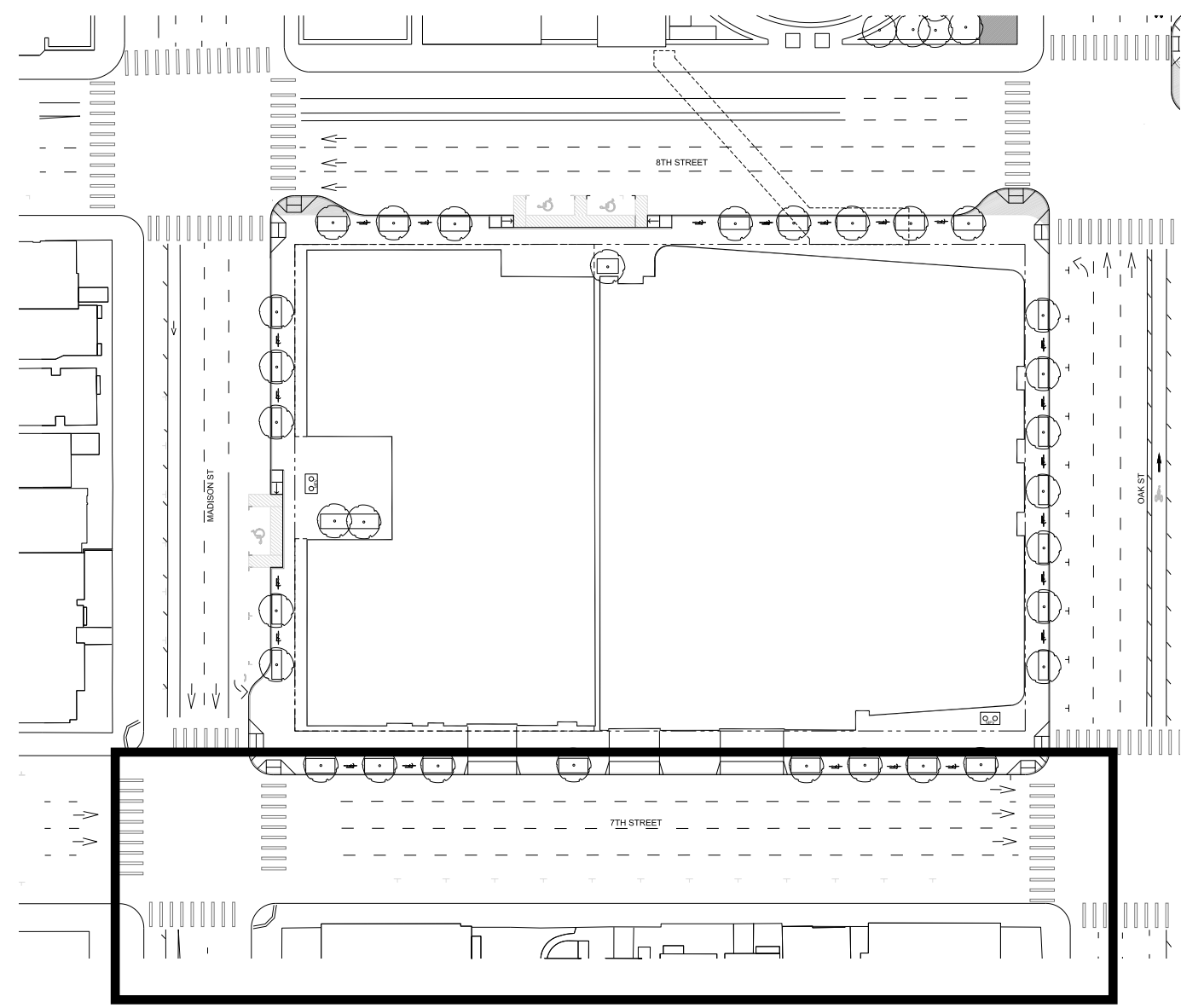
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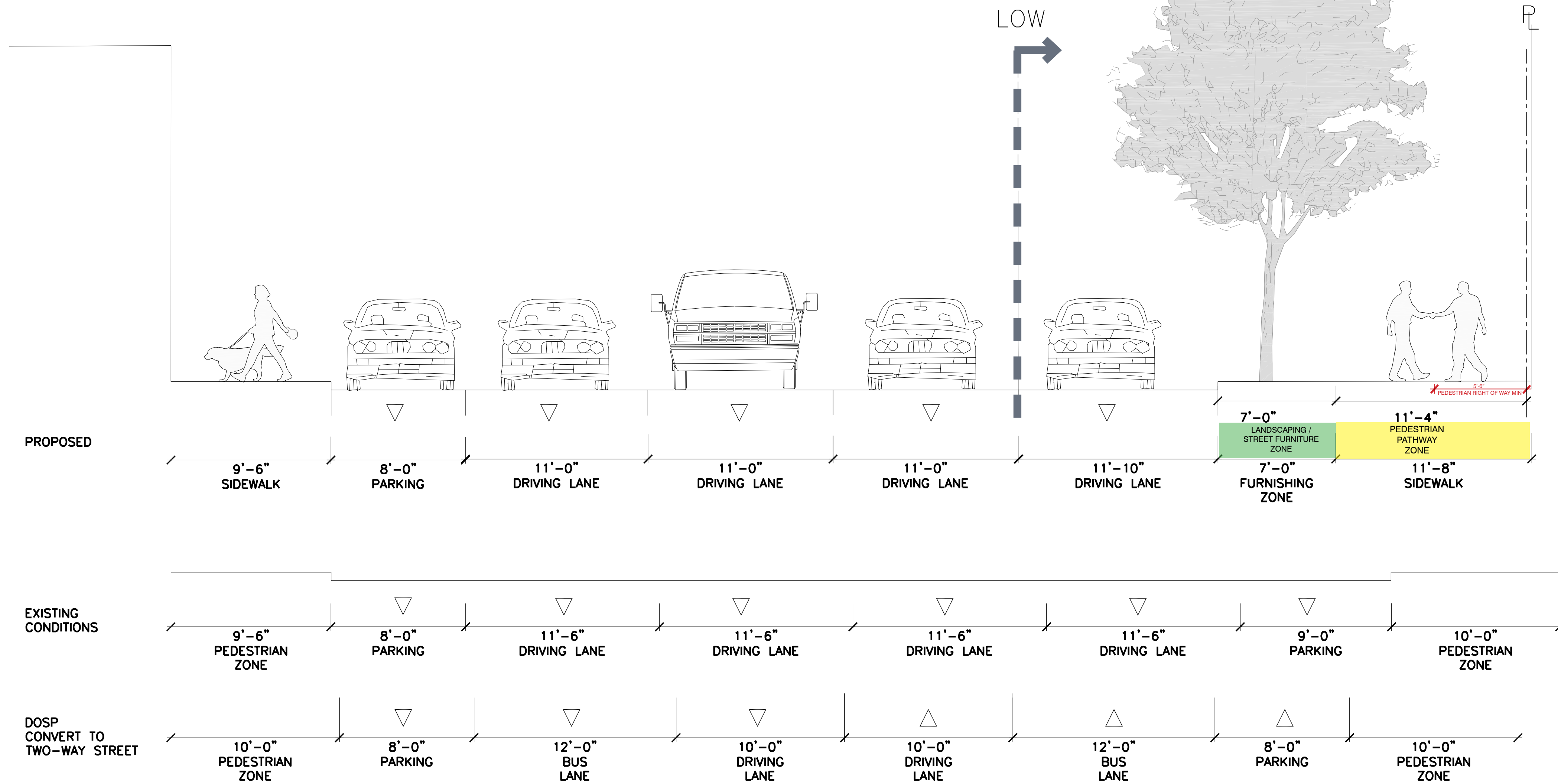
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**BLOCK 2 OAK STREET SECTIONS**

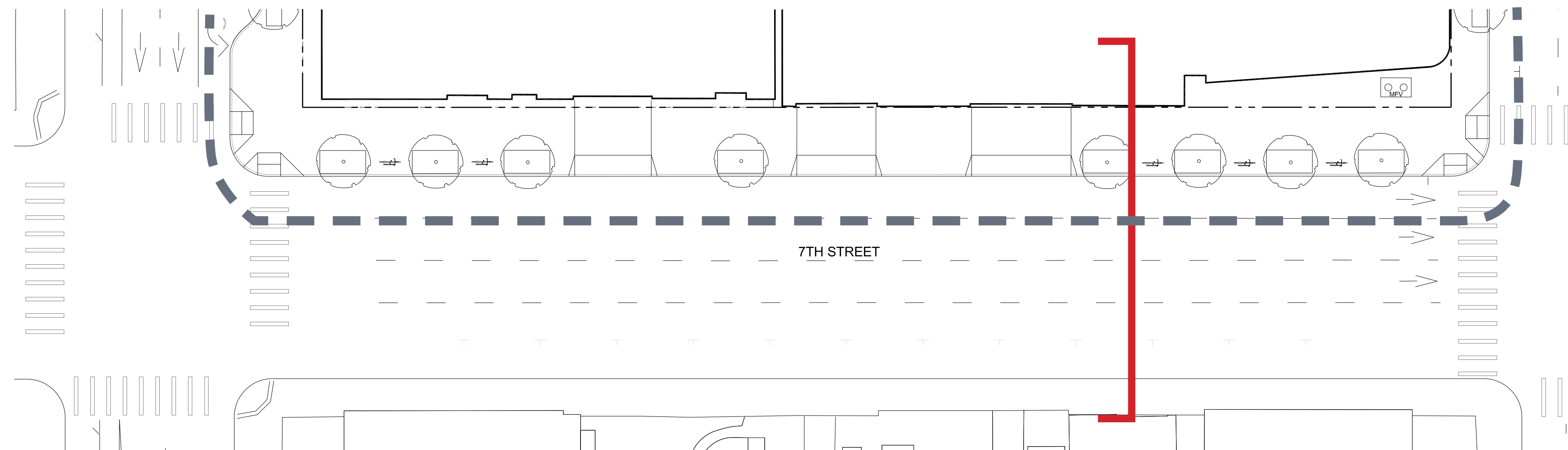
**L3.7**



Key Plan



Typical Cross Section



Plan



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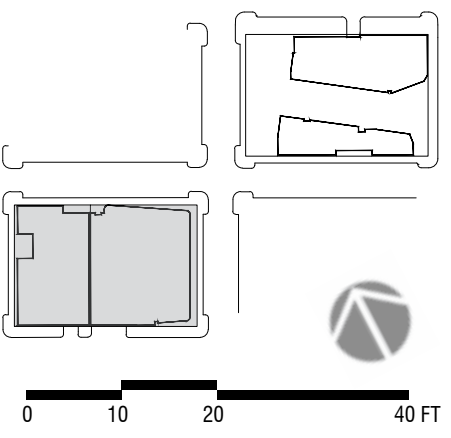
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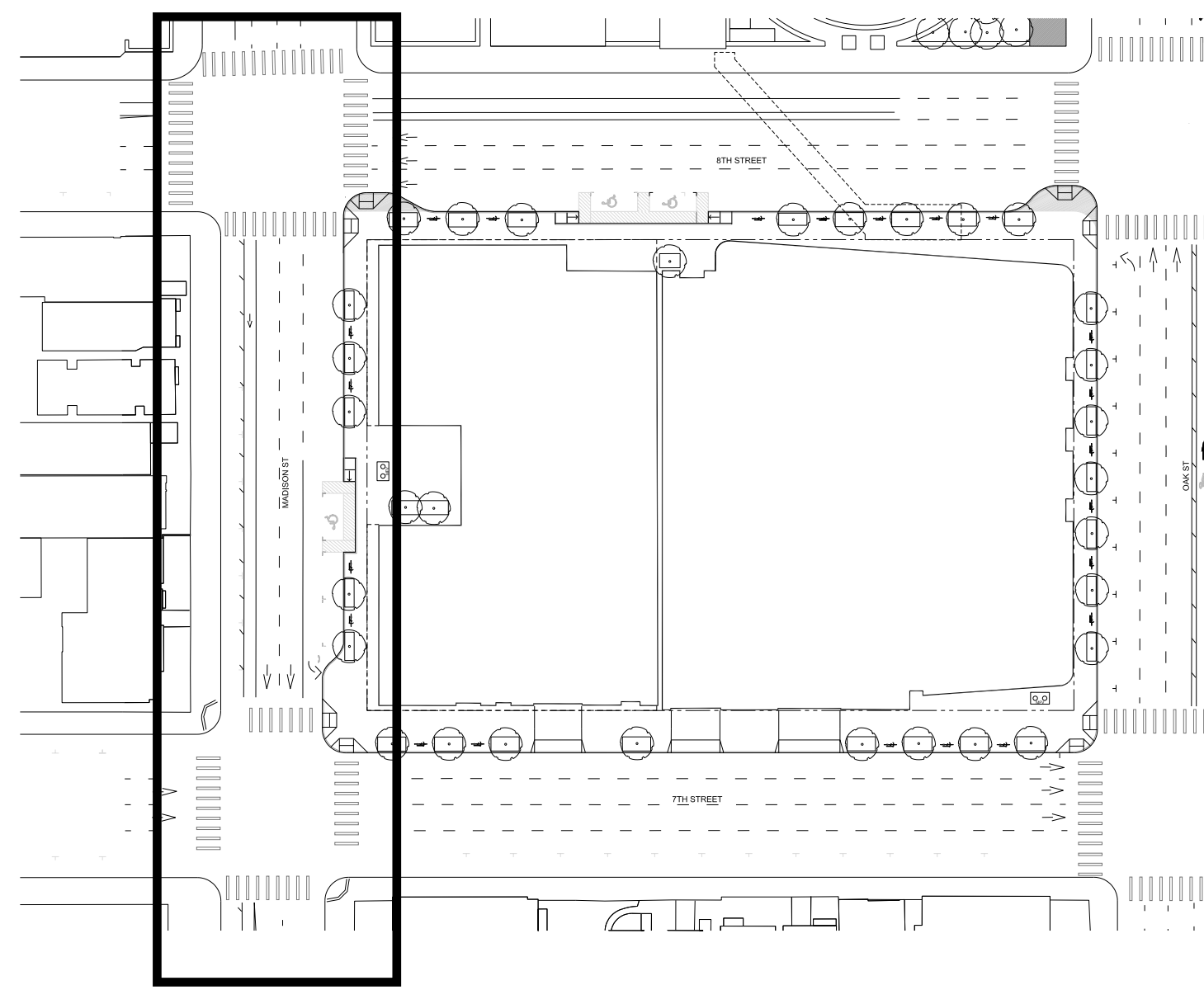
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7	REVISED RESPONSE TO POP#4 COMMENTS	03/17/2021



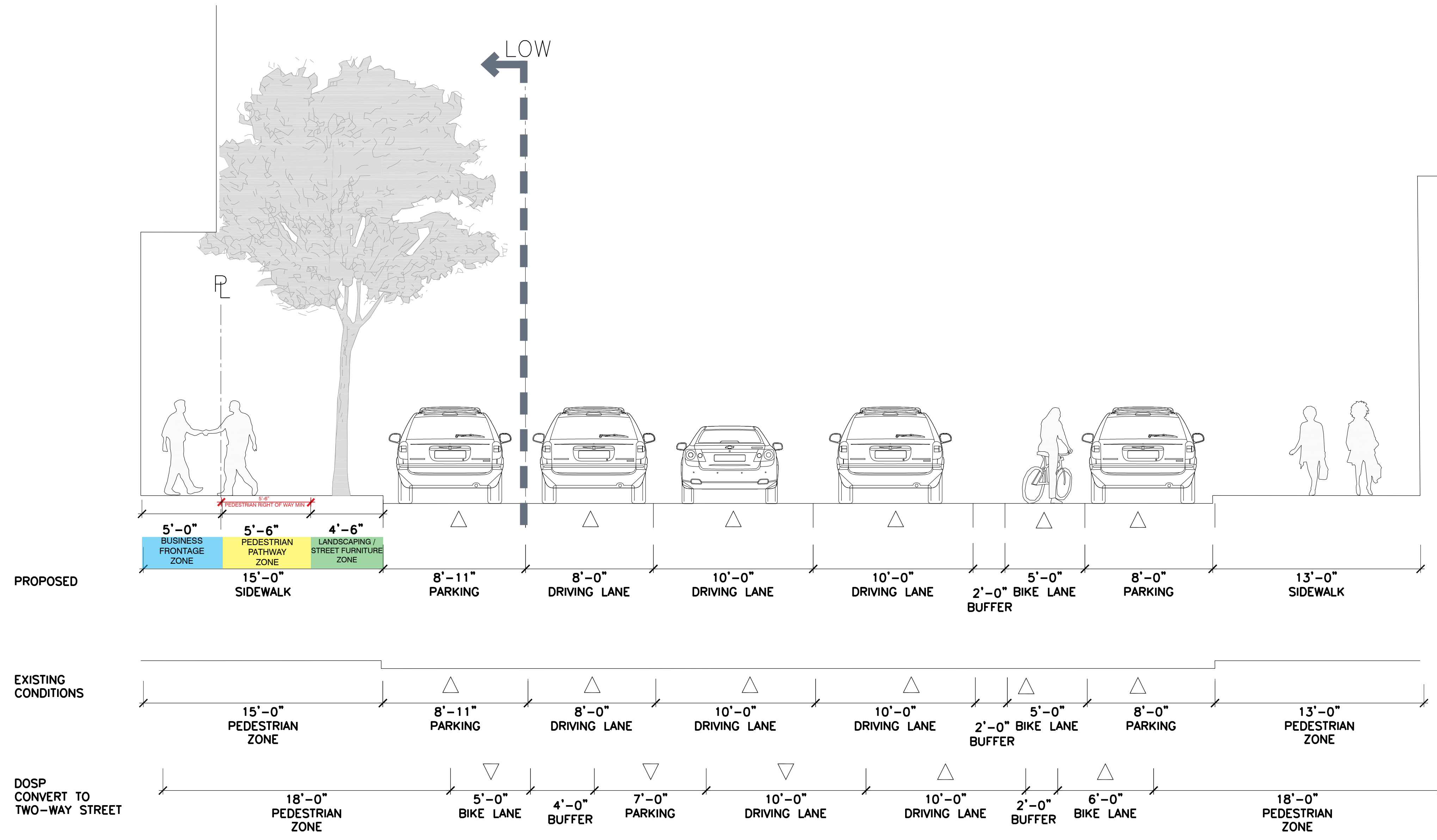
DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

### BLOCK 2 7TH STREET SECTIONS

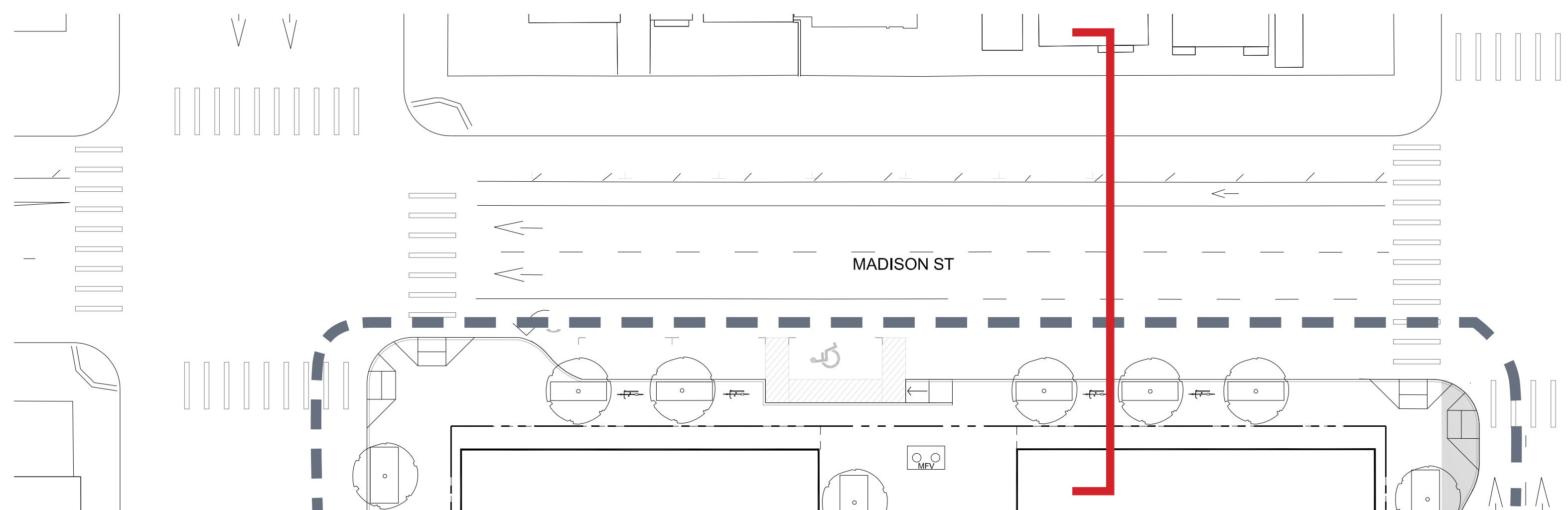
# L3.8



Key Plan



Typical Cross Section



Plan

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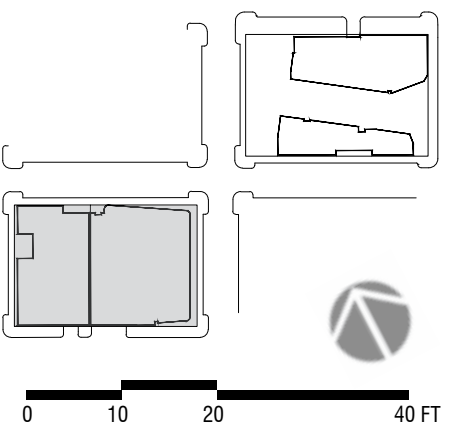
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION

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DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 MADISON STREET SECTIONS**

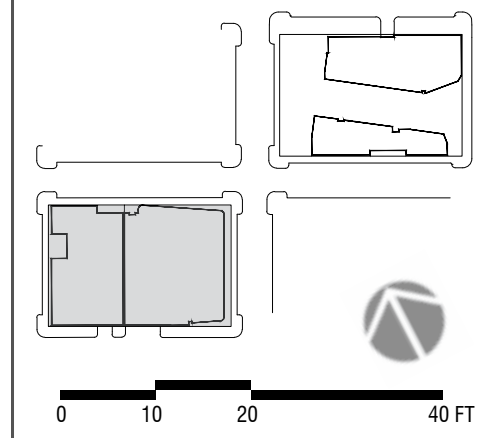
**L3.9**

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DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**4 BLOCK PUBLIC LOADING AND PARKING**

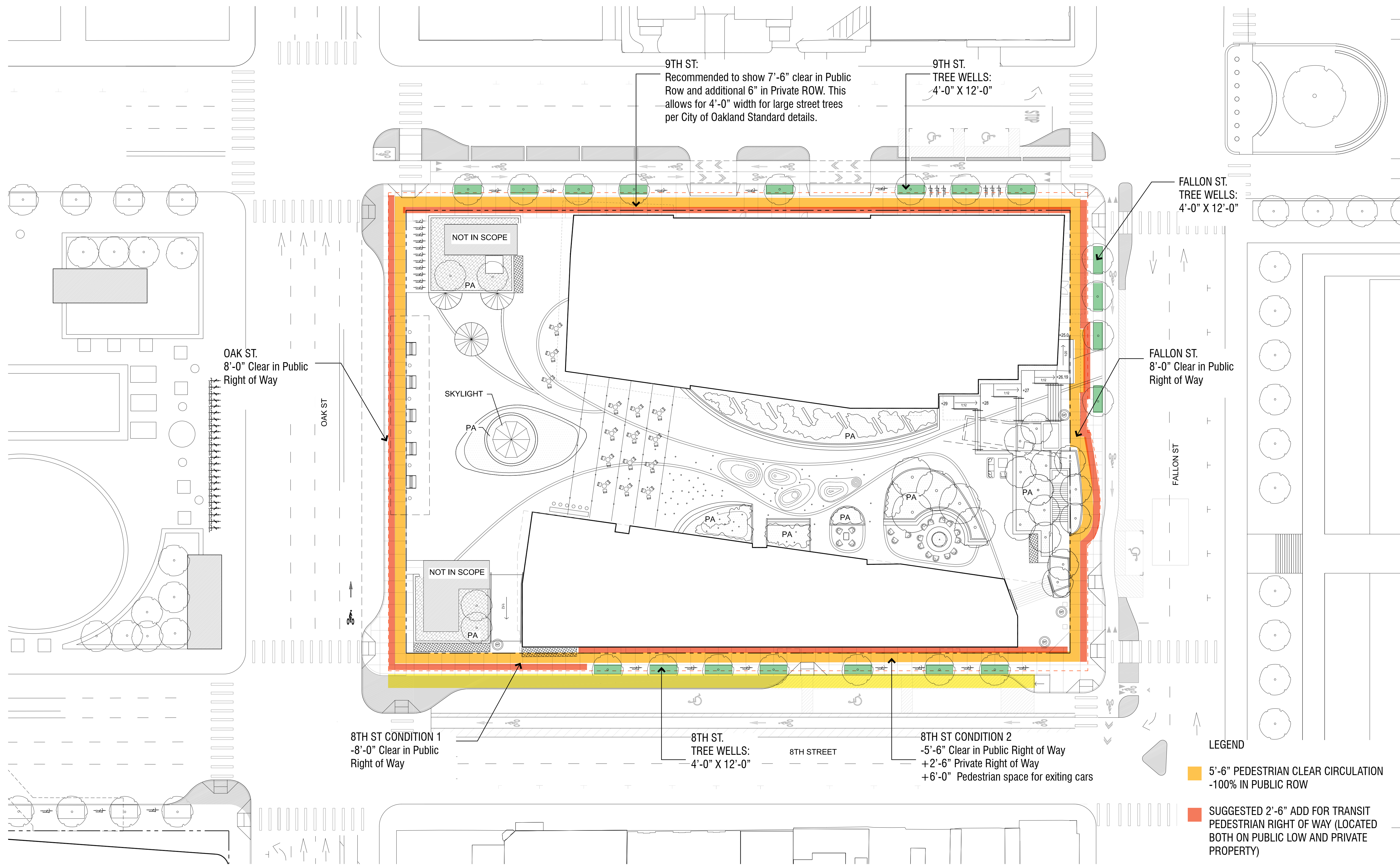
**L4.1**



- LEGEND**
- PASSENGER LOADING ZONE
  - ADA PARKING
  - ADA LOADING ZONE
  - BUS/SHUTTLE ZONE
  - ADA BUS/SHUTTLE ZONE
  - AC TRANSIT
  - AUTO PARKING
  - RESERVED FOR BART STATION AGENT PARKING
  - RESERVED FOR POLICE PARKING
  - P PARKING GARAGE
  - LANE TRANSITIONS ON MODIFIED INTERSECTIONS

- NOTES:**
1. Locations shown are diagrammatic based on information at this time.
  2. Transportation planning decisions and phasing will affect ultimate build-out.
  3. BART station agent parking is subject to BART and City coordination.
  4. The final design will be ADA compliant.





9TH ST.  
Recommended to show 7'-6" clear in Public Row and additional 6" in Private ROW. This allows for 4'-0" width for large street trees per City of Oakland Standard details.

9TH ST.  
TREE WELLS:  
4'-0" X 12'-0"

FALLON ST.  
TREE WELLS:  
4'-0" X 12'-0"

FALLON ST.  
8'-0" Clear in Public Right of Way

OAK ST.  
8'-0" Clear in Public Right of Way

8TH ST CONDITION 1  
-8'-0" Clear in Public Right of Way

8TH ST.  
TREE WELLS:  
4'-0" X 12'-0"

8TH ST CONDITION 2  
-5'-6" Clear in Public Right of Way  
+ 2'-6" Private Right of Way  
+ 6'-0" Pedestrian space for exiting cars

LEGEND

- 5'-6" PEDESTRIAN CLEAR CIRCULATION -100% IN PUBLIC ROW
- SUGGESTED 2'-6" ADD FOR TRANSIT PEDESTRIAN RIGHT OF WAY (LOCATED BOTH ON PUBLIC LOW AND PRIVATE PROPERTY)
- ADDITIONAL CLEAR PED MOVEMENT AREAS
- 8'-0" OFFSET FROM PROPERTY LINE
- PROPERTY LINE
- 12' x 4' TREE WELLS MEETS MINIMUM STANDARD FOR A LARGE TREE.

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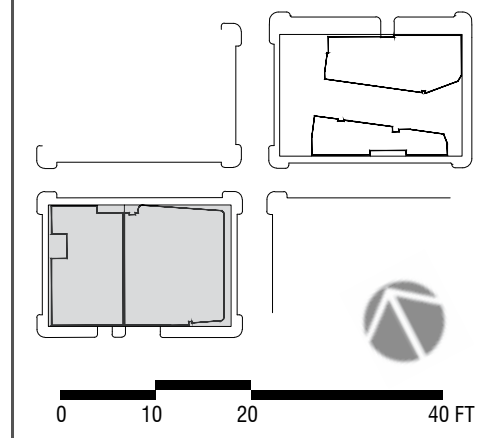
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

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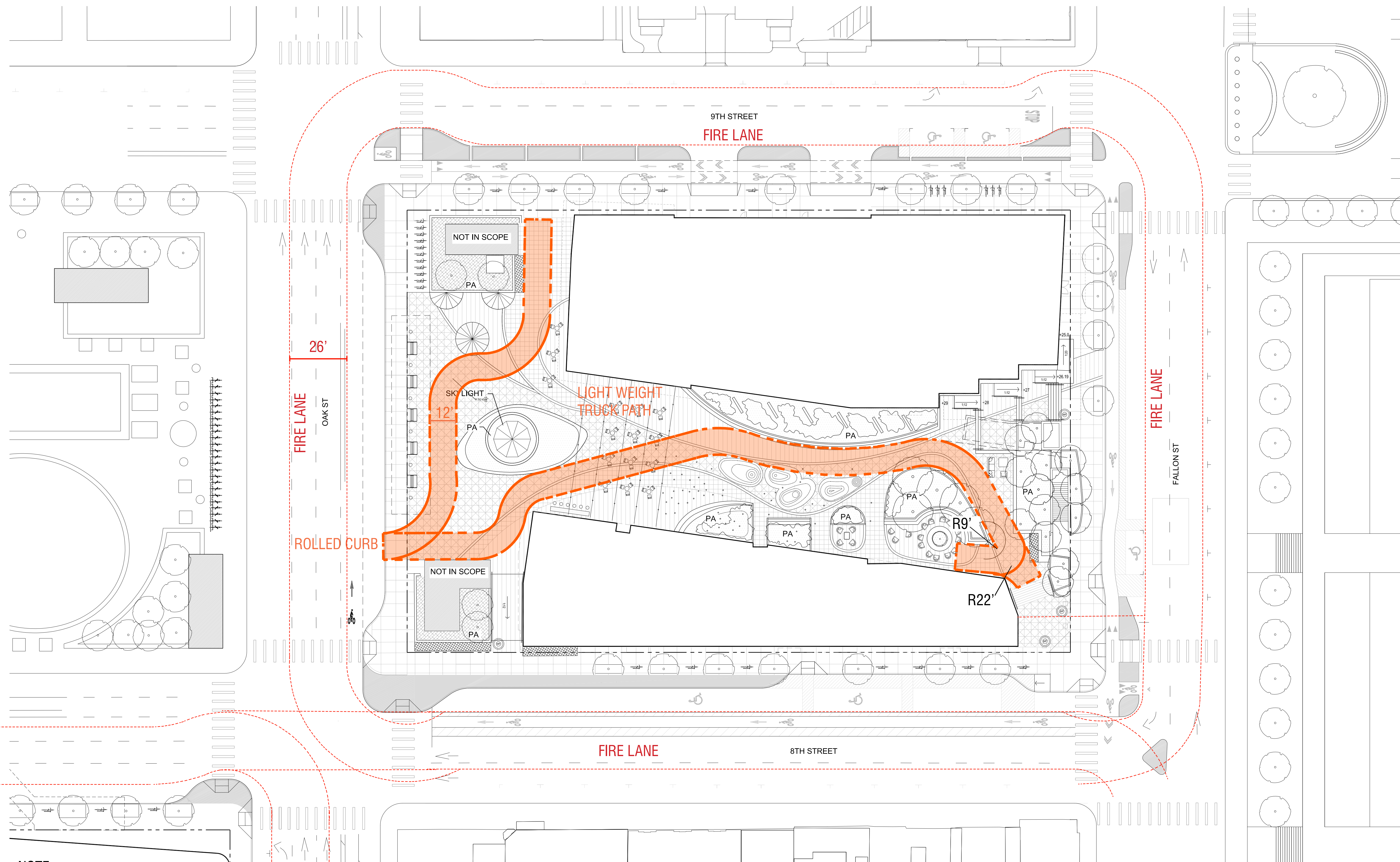
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DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

# BLOCK 1 PEDESTRIAN RIGHT OF WAY

## L4.2



**NOTE:**

1. Security cameras will be installed in the West Plaza and Paseo.
2. Final design of paseo under study.
3. Truck path inner radius minimum is 18'-0", location diagrammatic
4. Separation between maintenance path and play structure to be considered.
5. BART station agent parking will be located to the satisfaction of the city and BART
6. Location of Public Art, Moveable Furniture, and Paseo hardscape improvements to be approved by BART at the FDP to meet their requirements for a light-weight service maintenance vehicle

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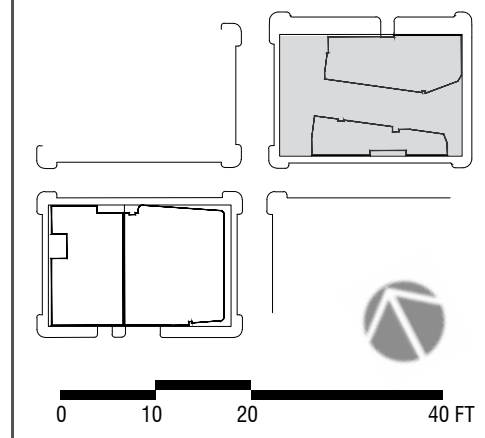
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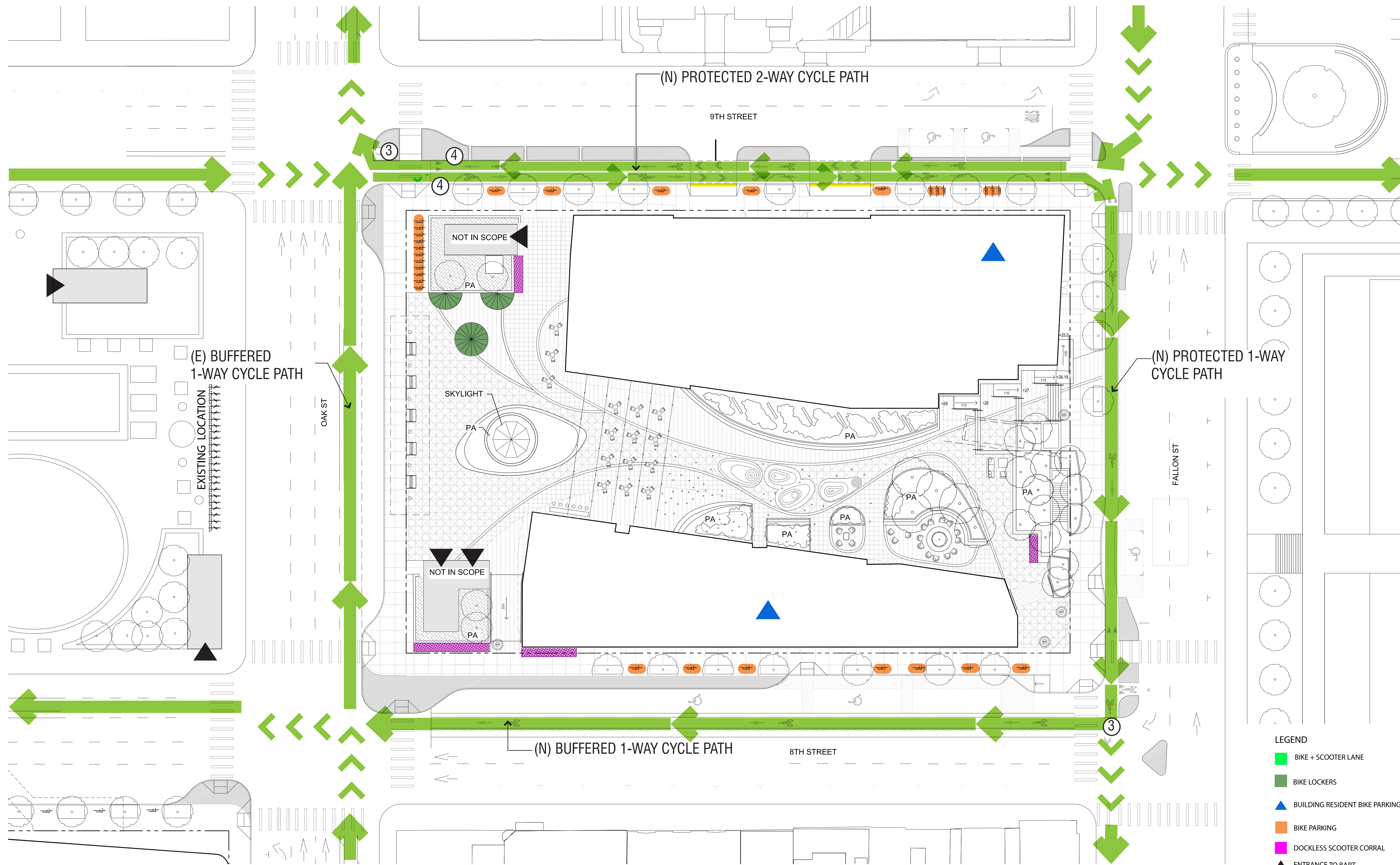
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**BLOCK 1 BART MAINTENANCE AND SECURITY**



- NOTES:**
- ① Locations shown are diagrammatic based on information at this time.
  - ② Transportation planning decisions and phasing will affect ultimate build-out.
  - ③ Directional bike transitions are coordinating with city wide planning and subject to change. See Access plans for planning purposes.
  - ④ 1'-0" detectable edge at two-way protected bike lane.

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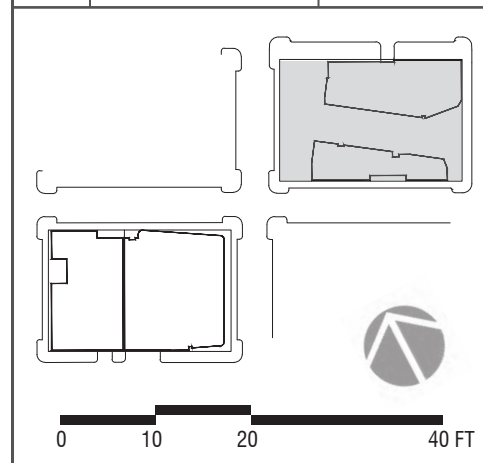
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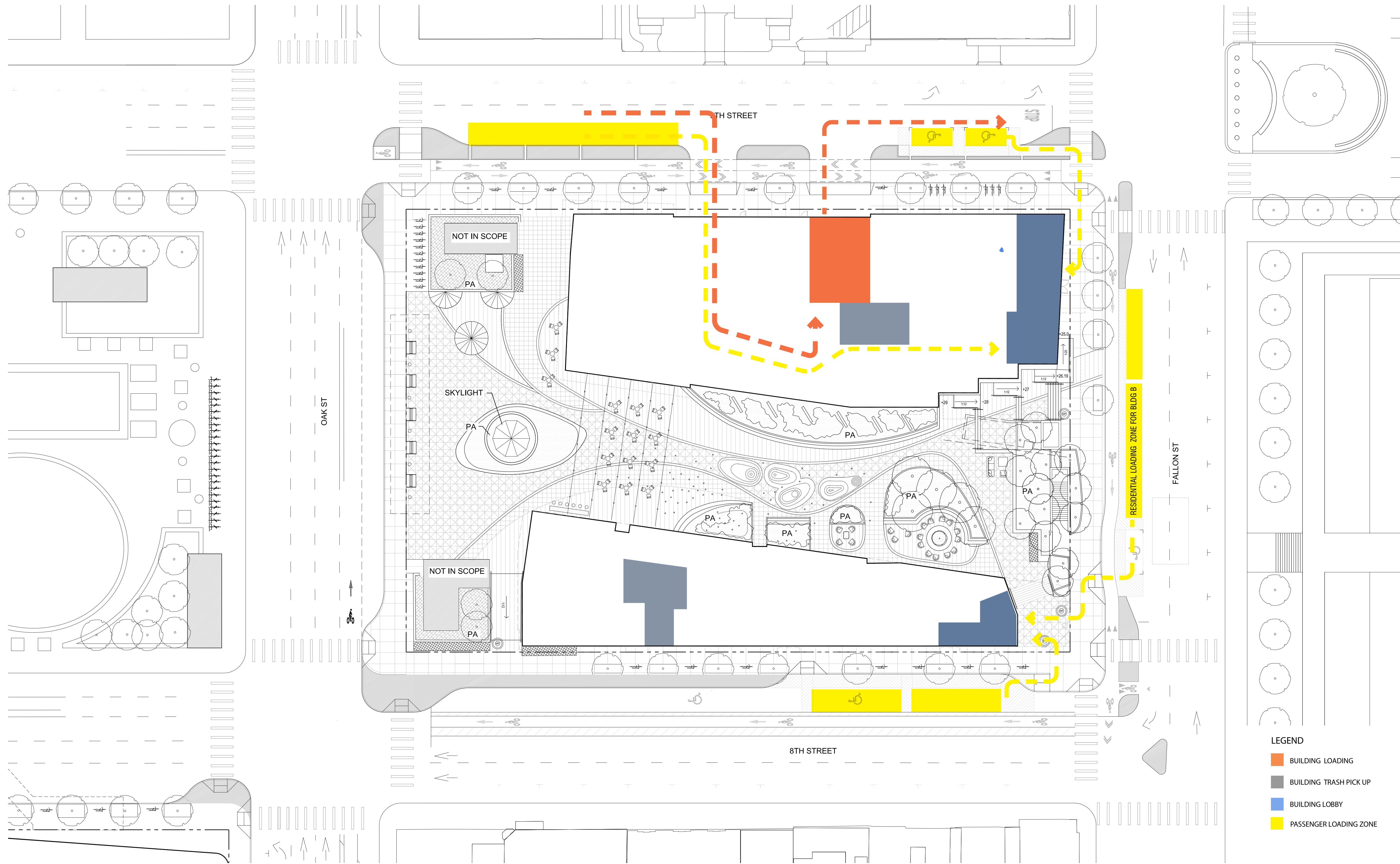
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**BLOCK 1 BICYCLE AND SCOOTER ACCESS**



**NOTES:**

1. Locations shown are diagrammatic based on information at this time.
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**LEGEND**

- BUILDING LOADING
- BUILDING TRASH PICK UP
- BUILDING LOBBY
- PASSENGER LOADING ZONE

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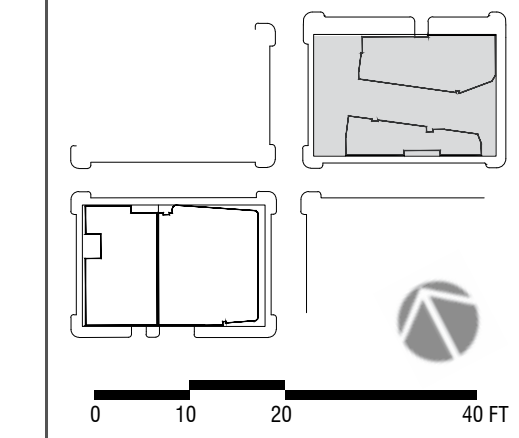
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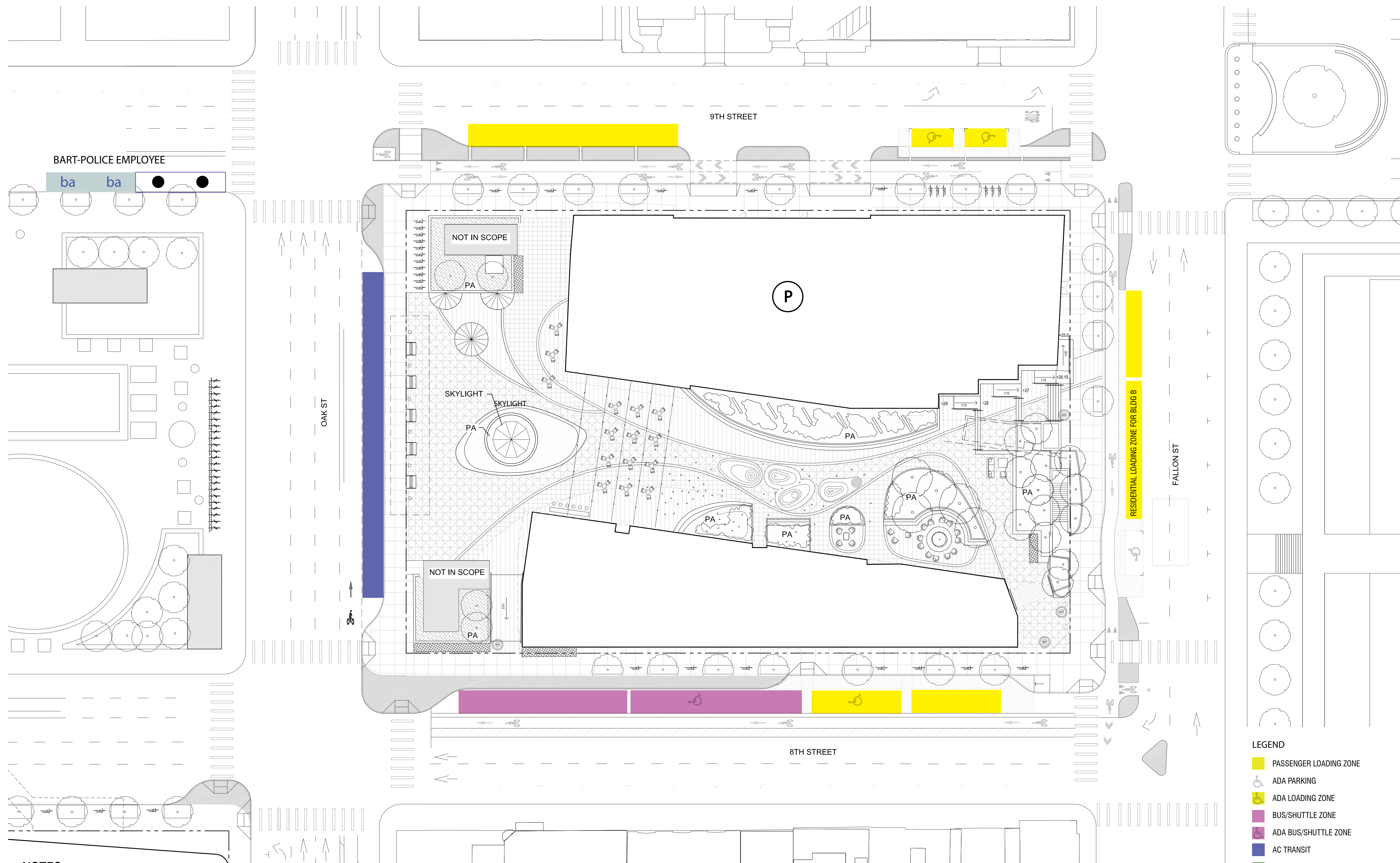
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**BLOCK 1 BUILDING ACCESS**



- NOTES:**
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  2. Transportation planning decisions and phasing will affect ultimate build-out.
  3. The final design will be ADA compliant.
  4. BART station agent parking will be located to the satisfaction of the city and BART

- LEGEND**
- PASSENGER LOADING ZONE
  - ♿ ADA PARKING
  - ♿ ADA LOADING ZONE
  - BUS/SHUTTLE ZONE
  - ♿ ADA BUS/SHUTTLE ZONE
  - AC TRANSIT
  - AUTO PARKING
  - ba RESERVED FOR BART STATION AGENT PARKING
  - RESERVED FOR POLICE PARKING
  - P PARKING GARAGE

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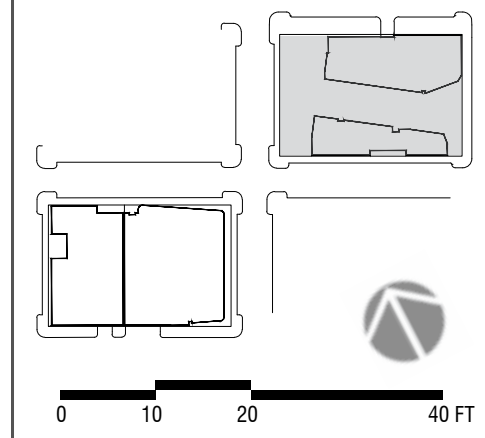
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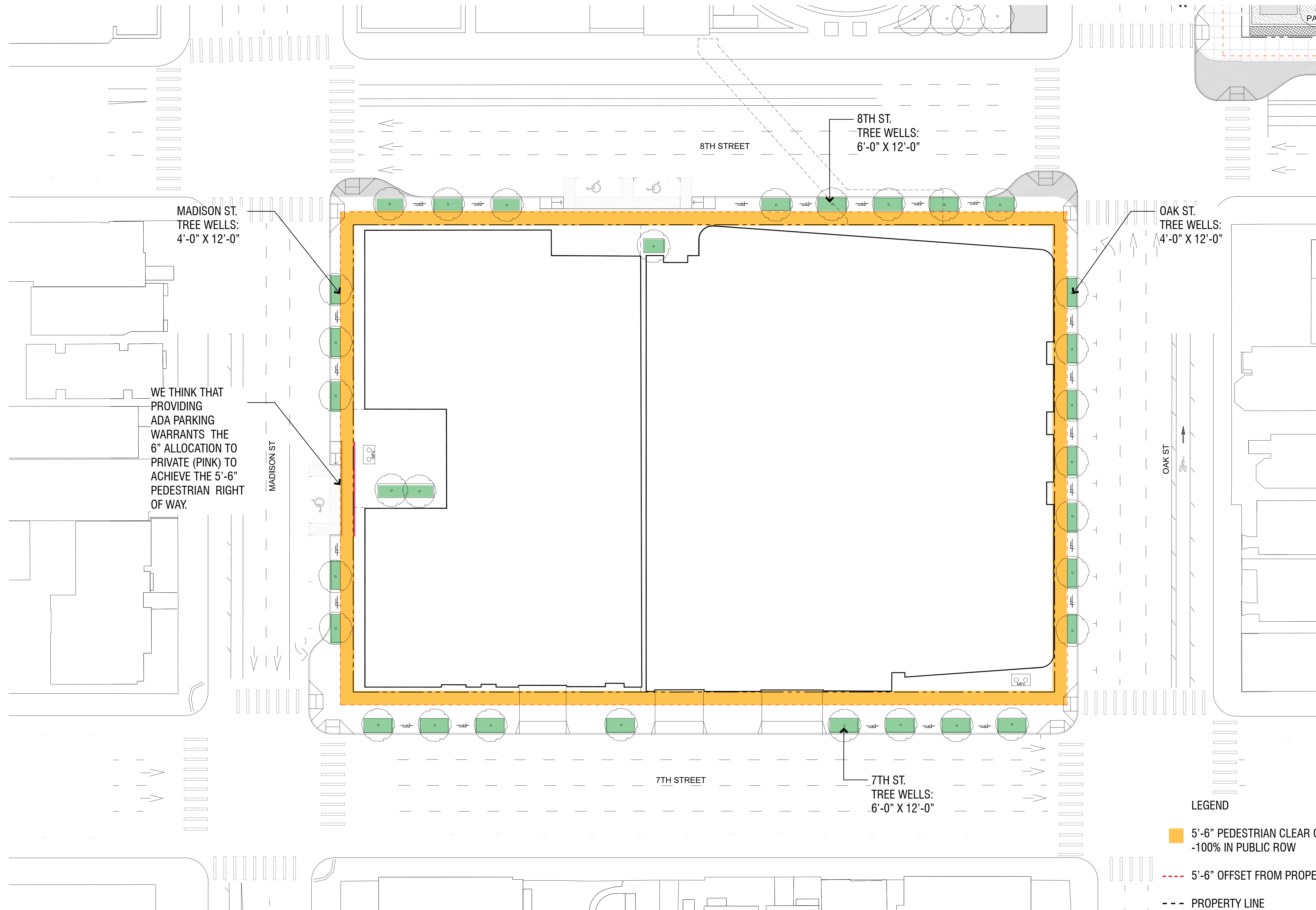
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**BLOCK 1 PUBLIC LOADING AND PARKING**



MADISON ST.  
TREE WELLS:  
4'-0" X 12'-0"

8TH ST.  
TREE WELLS:  
6'-0" X 12'-0"

OAK ST.  
TREE WELLS:  
4'-0" X 12'-0"

7TH ST.  
TREE WELLS:  
6'-0" X 12'-0"

WE THINK THAT  
PROVIDING  
ADA PARKING  
WARRANTS THE  
6" ALLOCATION TO  
PRIVATE (PINK)  
TO ACHIEVE THE  
5'-6" PEDESTRIAN  
RIGHT OF WAY.

**LEGEND**

- 5'-6" PEDESTRIAN CLEAR CIRCULATION  
-100% IN PUBLIC ROW
- 5'-6" OFFSET FROM PROPERTY LINE
- PROPERTY LINE
- 12' x 4' OAK AND MADISON  
12' x 6' 7th and 8th  
TREE WELLS MEETS MINIMUM  
STANDARD FOR A LARGE TREE.

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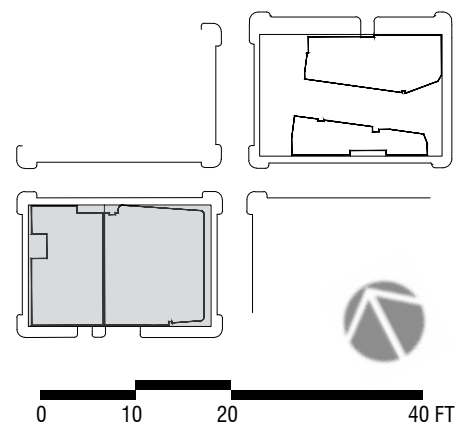
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**BLOCK 2 PEDESTRIAN RIGHT OF WAY**



- NOTES:**
- ① Locations shown are diagrammatic based on information at this time.
  - ② Transportation planning decisions and phasing will affect ultimate build-out.
  - ③ Directional bike transitions are coordinating with city wide planning and subject to change. See Access plans for planning purposes.

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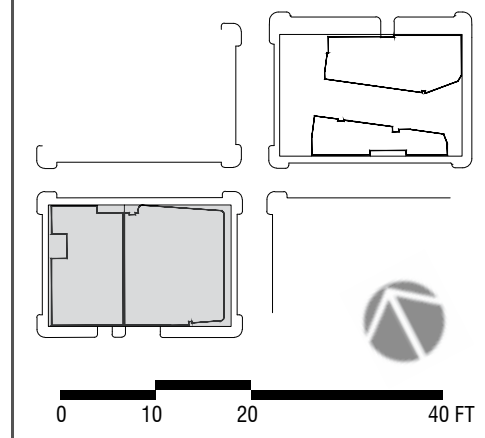
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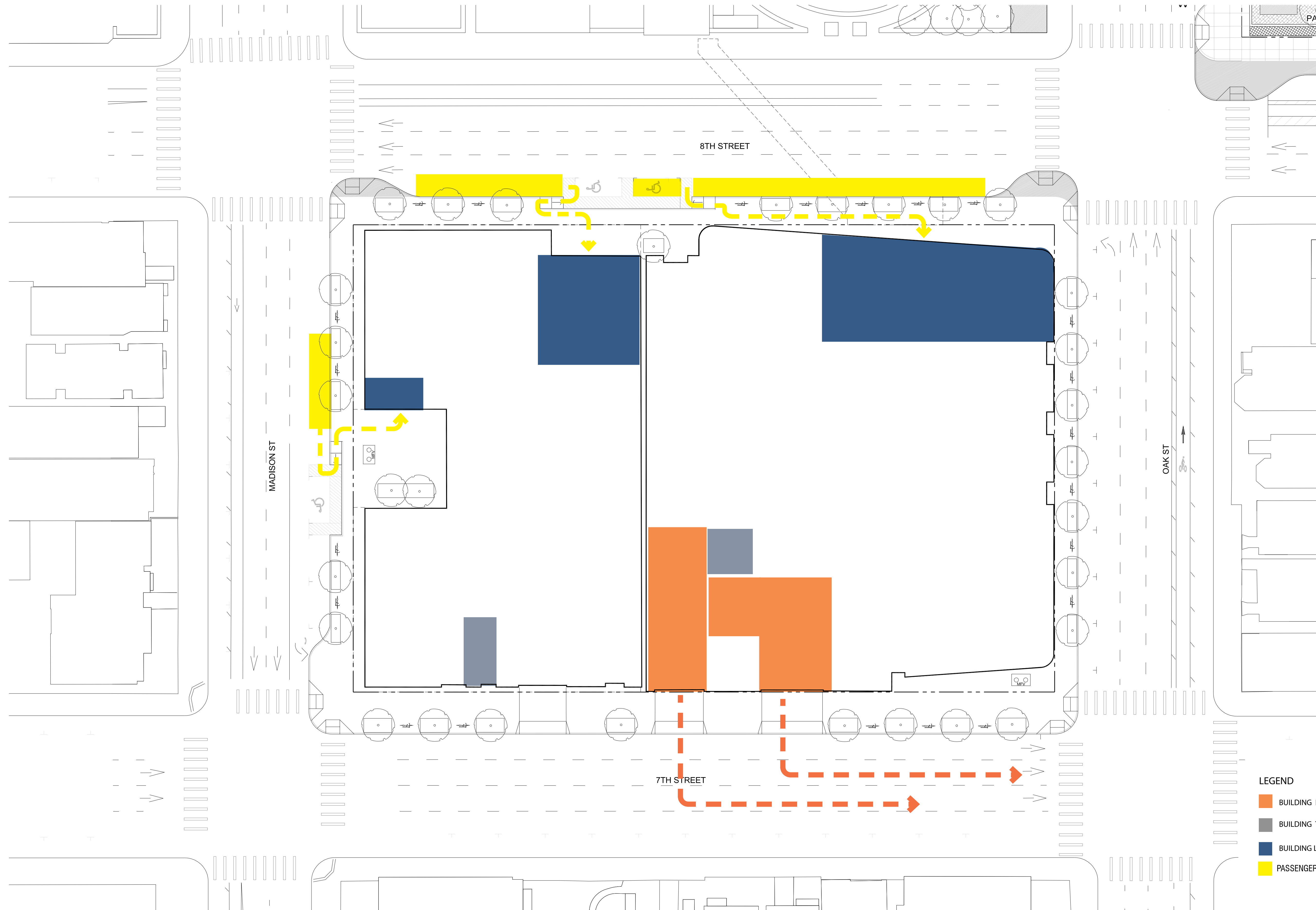
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**BLOCK 2 BICYCLE AND SCOOTER ACCESS**



- LEGEND**
- BUILDING LOADING
  - BUILDING TRASH PICK UP
  - BUILDING LOBBY
  - PASSENGER LOADING ZONE

- NOTES:**
1. Locations shown are diagrammatic based on information at this time.
  2. Transportation planning decisions and phasing will affect ultimate build-out.

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101 MISSION ST. #200  
SAN FRANCISCO, CA 94105

**PYATOK**  
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**SCB** SOLOMON CORDWELL BUENZ ARCHITECTS  
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**INWILLERUEHL**  
318 HARRISON ST. STE 301  
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(510)891-1686  
www.inwilleruehl.com

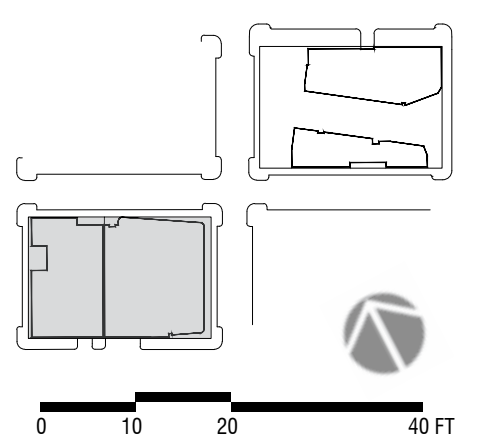
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225 BUCKLEUP DR. SUITE 200  
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(650) 483-4300  
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

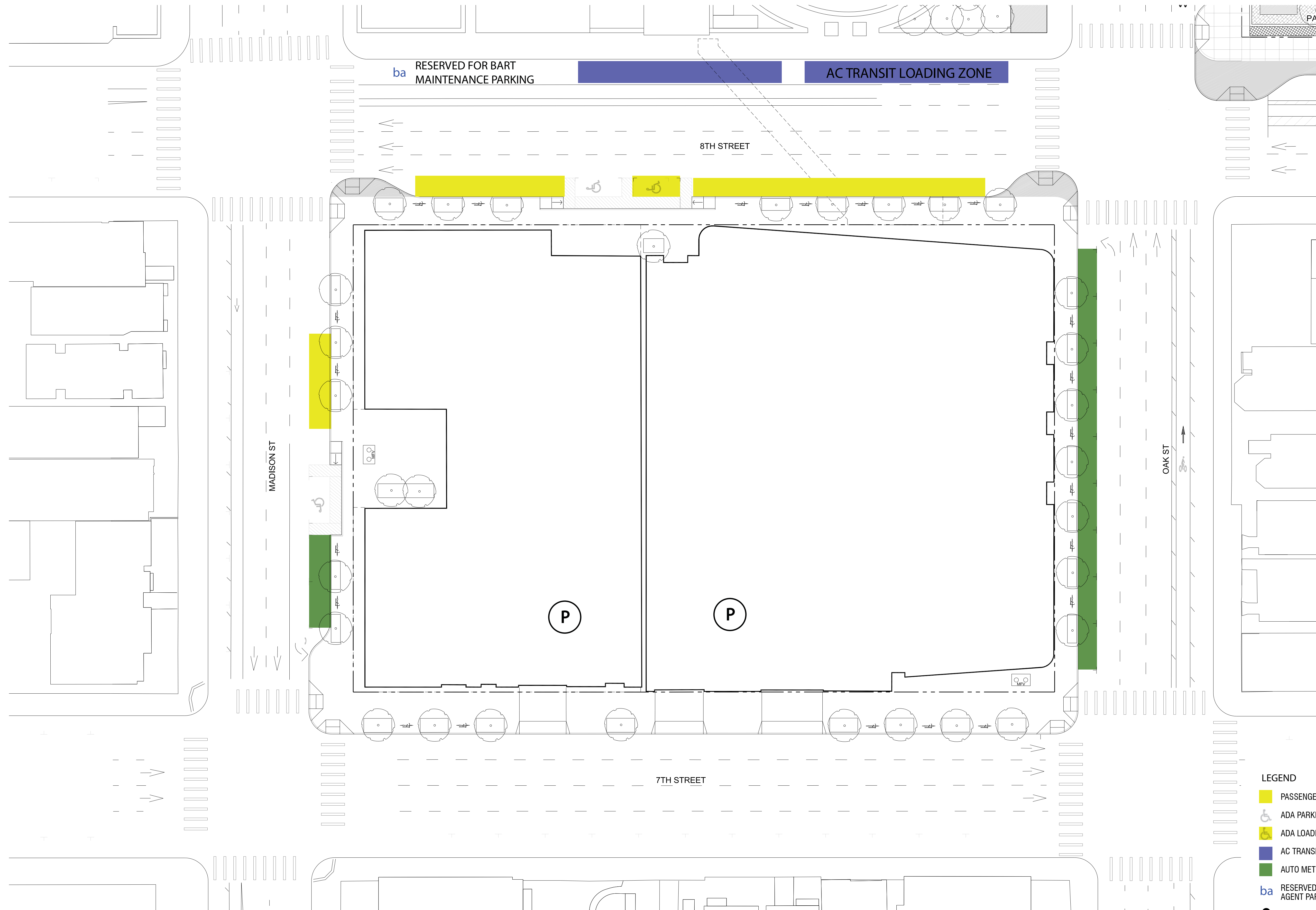
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NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPIZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPIZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPIZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPIZ COMMENTS	03/17/2021



**BLOCK 2 BUILDING ACCESS**





**NOTES:**

1. Locations shown are diagrammatic based on information at this time.
2. Transportation planning decisions and phasing will affect ultimate build-out.

**LEGEND**

- PASSENGER LOADING ZONE
- ♿ ADA PARKING
- ♿ ADA LOADING ZONE
- AC TRANSIT
- AUTO METERED PARKING
- ba RESERVED FOR BART STATION AGENT PARKING
- RESERVED FOR POLICE PARKING
- P PARKING GARAGE

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**ENWILLERUEHL**  
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(510)891-1686  
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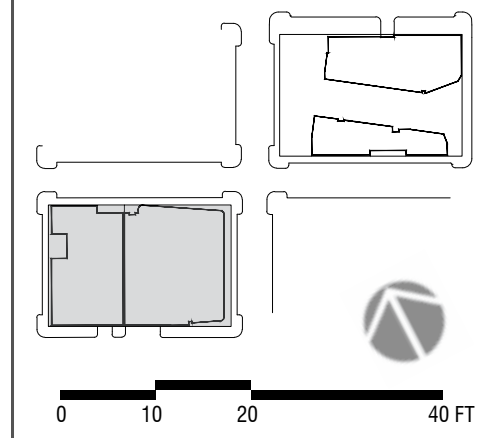
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Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

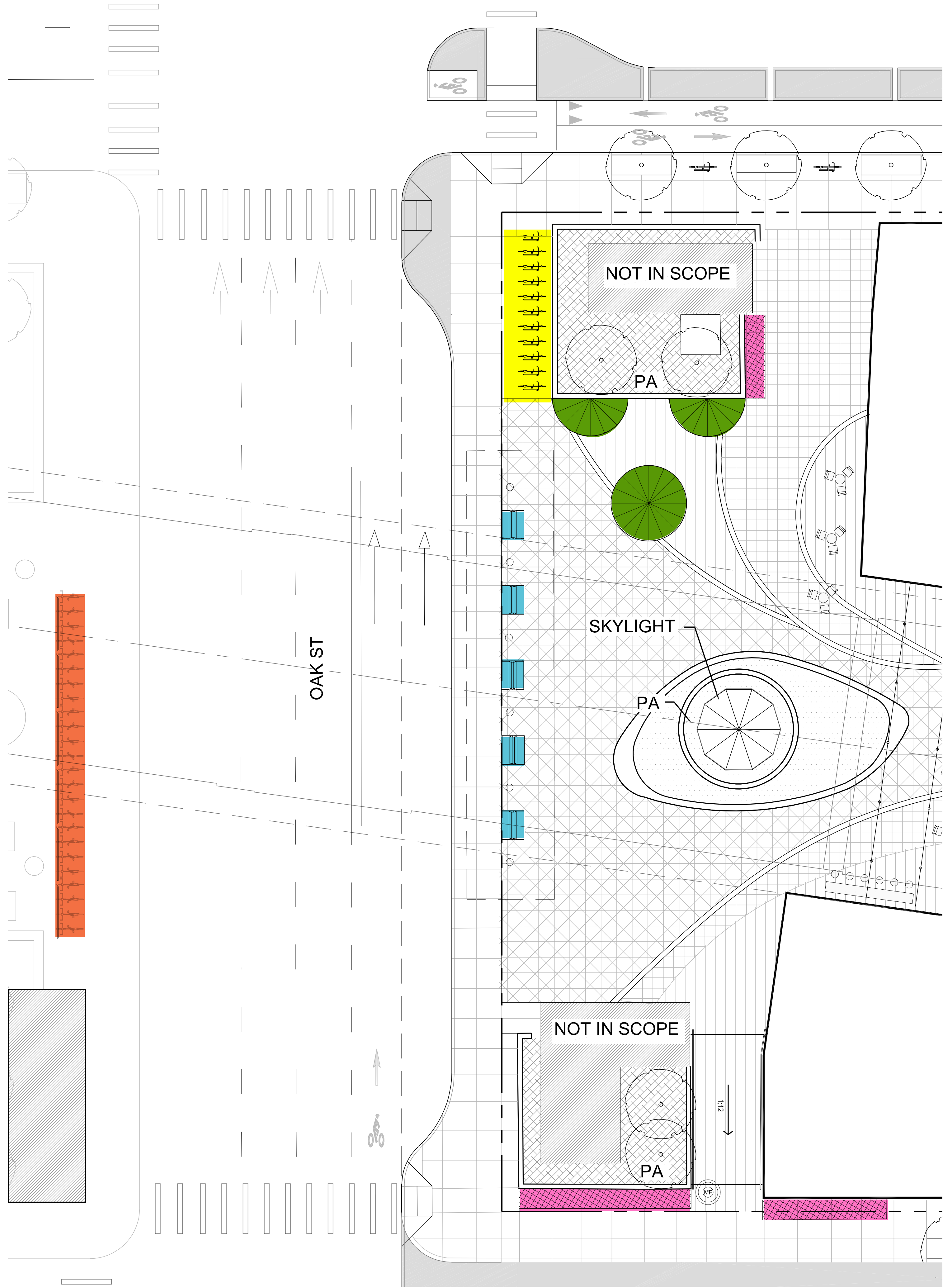
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REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP#4 COMMENTS	03/17/2021






DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 2 PUBLIC LOADING AND PARKING**



BART TRANSIT FACILITIES  
BLOCK 1

	KEY	EXISTING	PROPOSED
BIKE RACKS		2 RACKS ACCOMMODATES 25 BIKES	1 RACK 12- INDIVIDUAL U-RACKS ACCOMMODATES 46 BIKES
BIKE LOCKERS		ACCOMMODATES 24 BIKES	ACCOMMODATES 32 BIKES
DOCKLESS SCOOTER CORRAL		0	4 CORRALS TOTTALLING 372 SQFT
EXISTING SHARED BIKE DOCK (BAY WHEELS)		1 DOCK TOF BLOCK 70'-0" x 5'-0"	1 DOCK TOF BLOCK 70'-0" x 5'-0"
SEATING FOR AC TRANSIT * FINAL MANAGEMENT & OPERATION STRATEGY TO BE DEVELOPED		0	5 DUAL FACING BENCHES WILL COMPLY WITH BFS 

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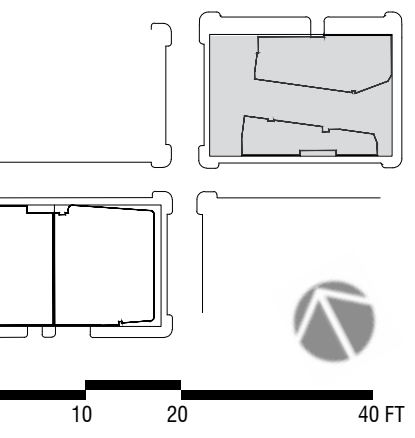
**LAKE  
MERRITT  
BART**  
REDEVELOPMENT  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

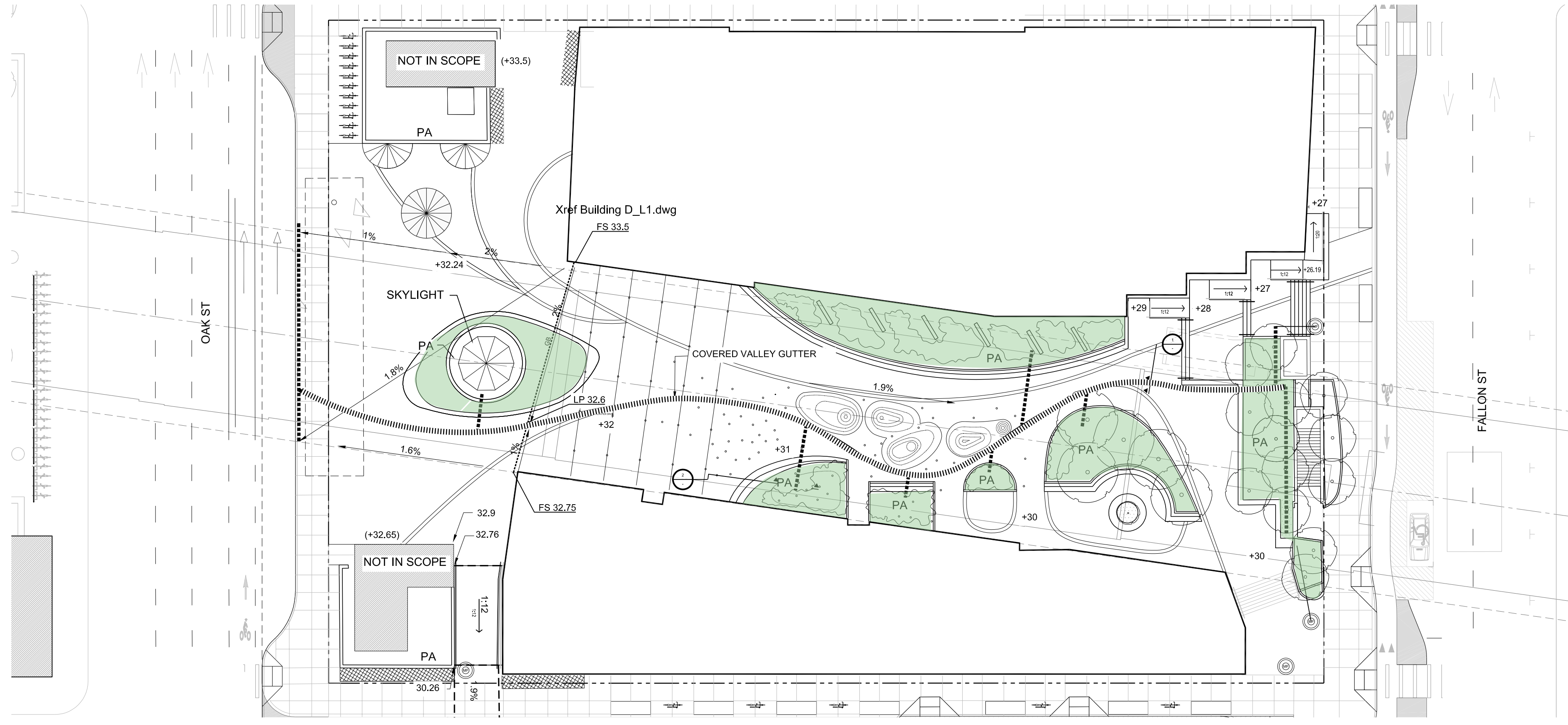
- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPA COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPA COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPA COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1  
DETAILED DESIGN  
BART ARRIVAL  
EXPERIENCE**



**LEGEND**  
 ■ PLANTED AREA

**3** DRAINAGE PLAN OF THE PASEO  
 NTS

- NOTES:**
1. All planting and irrigation above tunnel will be designed to meet BFS standards.
  2. Waterproofing design to be determined. Final design will comply with BFS standards.
  3. Existing drainage (see Civil C2.1 and C2.2) to be replaced with new drainage trench drain system.

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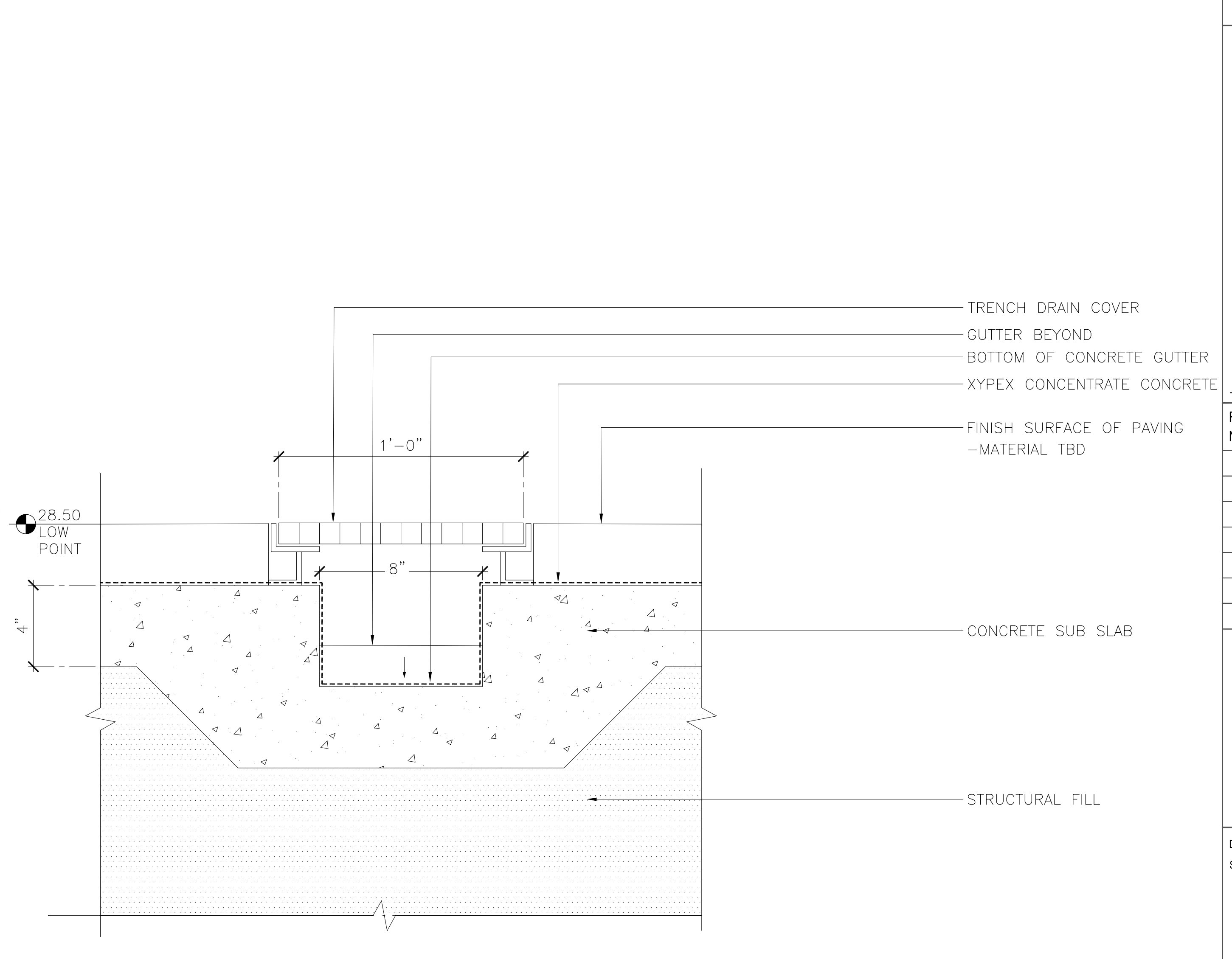
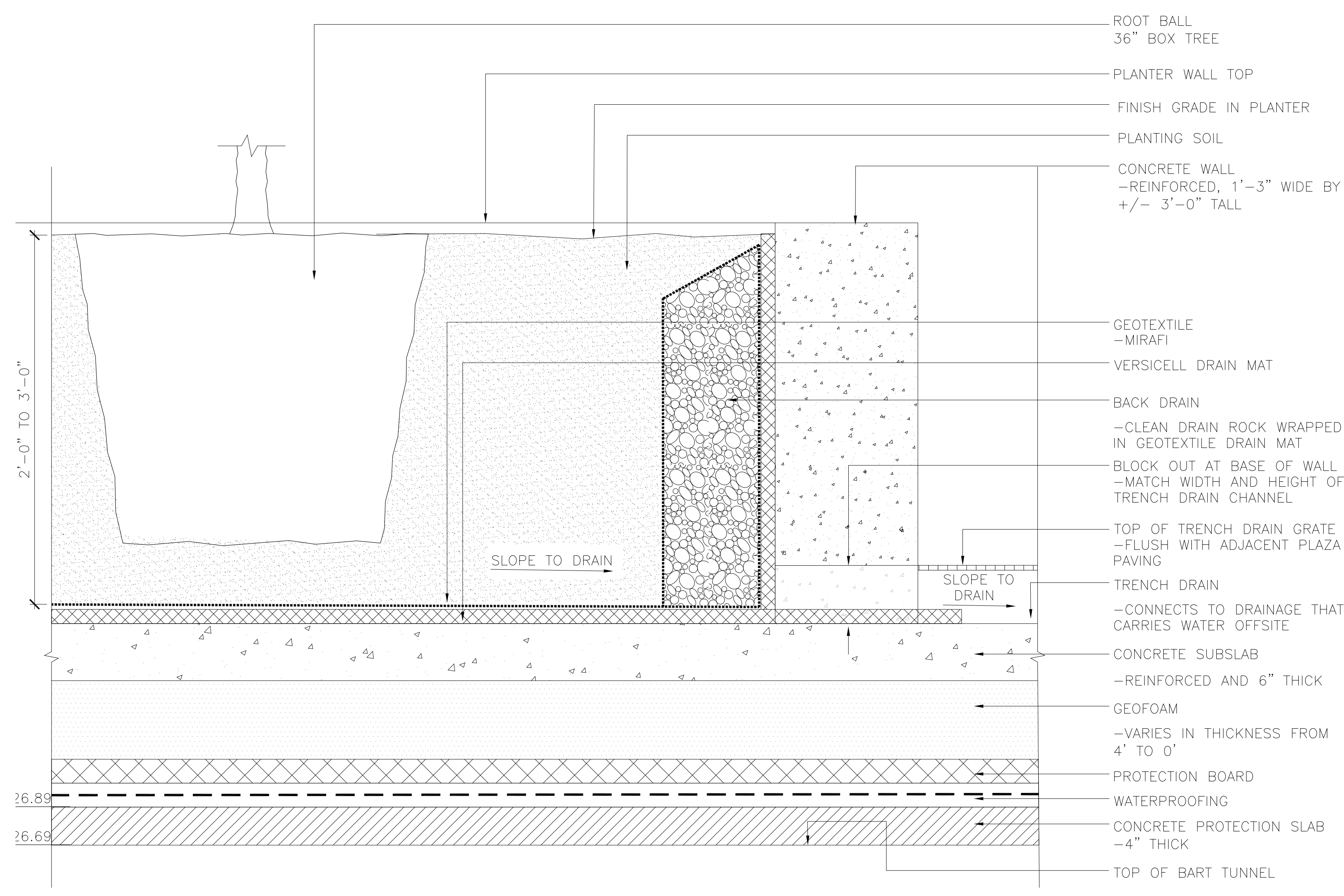
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 (650) 483-6300  
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**2** PLANTER WITH CONNECTION TO TRENCH DRAIN  
 SCALE: 1-1/2"=1'-0"  
 0 6" 1' 2' TYPICAL SECTION

**1** TRENCH DRAIN  
 SCALE: 3"=1'-0"  
 0 6" 1' TYPICAL SECTION

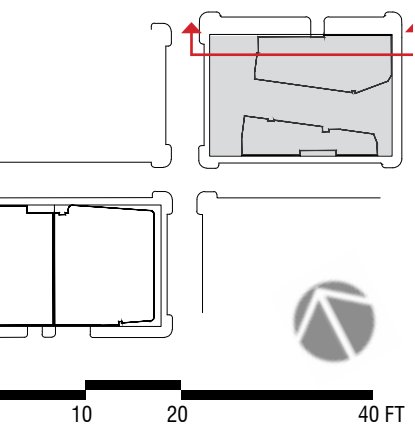
**LAKE MERRITT BART REDEVELOPMENT**  
 Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

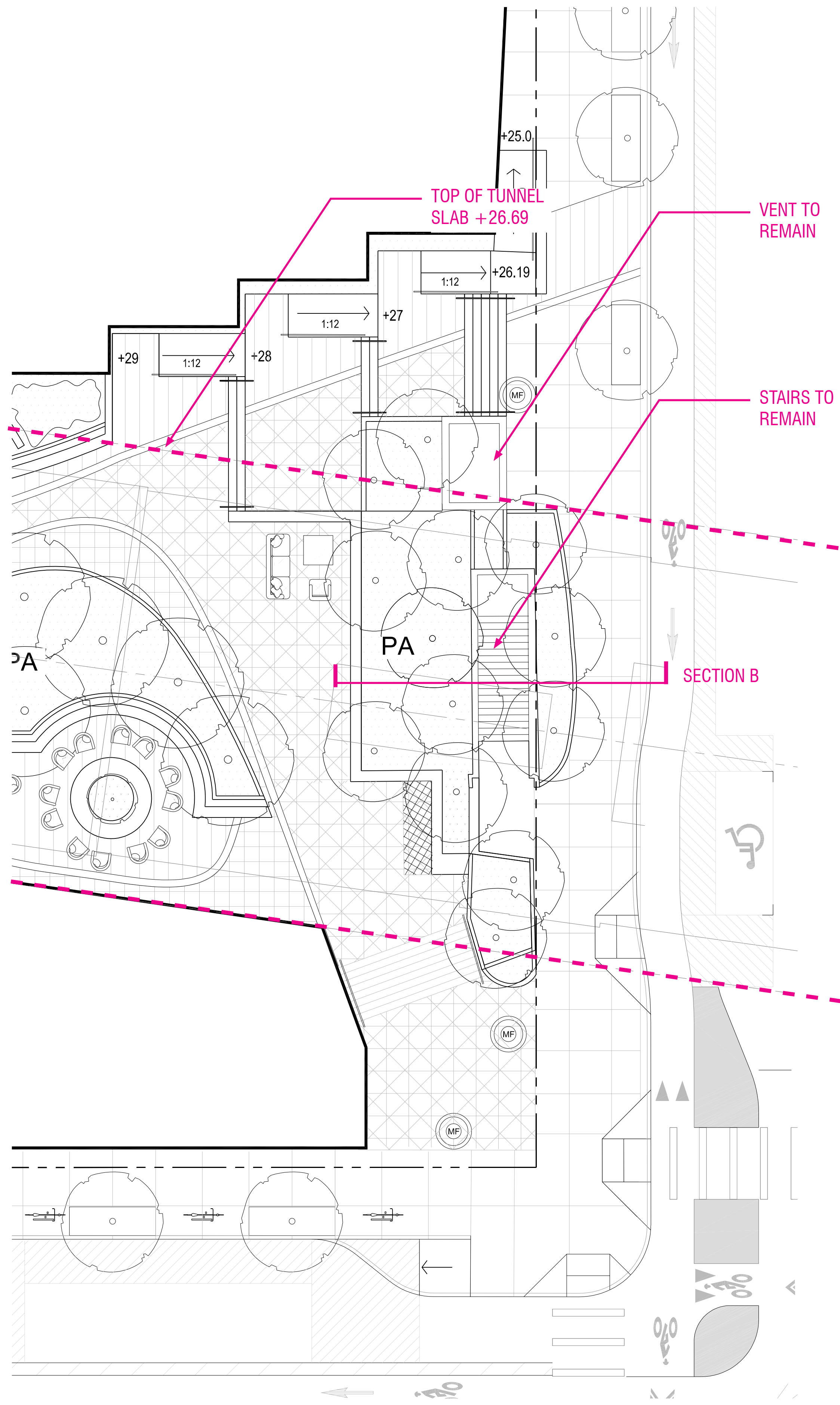
**REVISION SCHEDULE**

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPH COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPH COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPH COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPH COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
 SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 DETAILED DESIGN PASEO DRAINAGE**



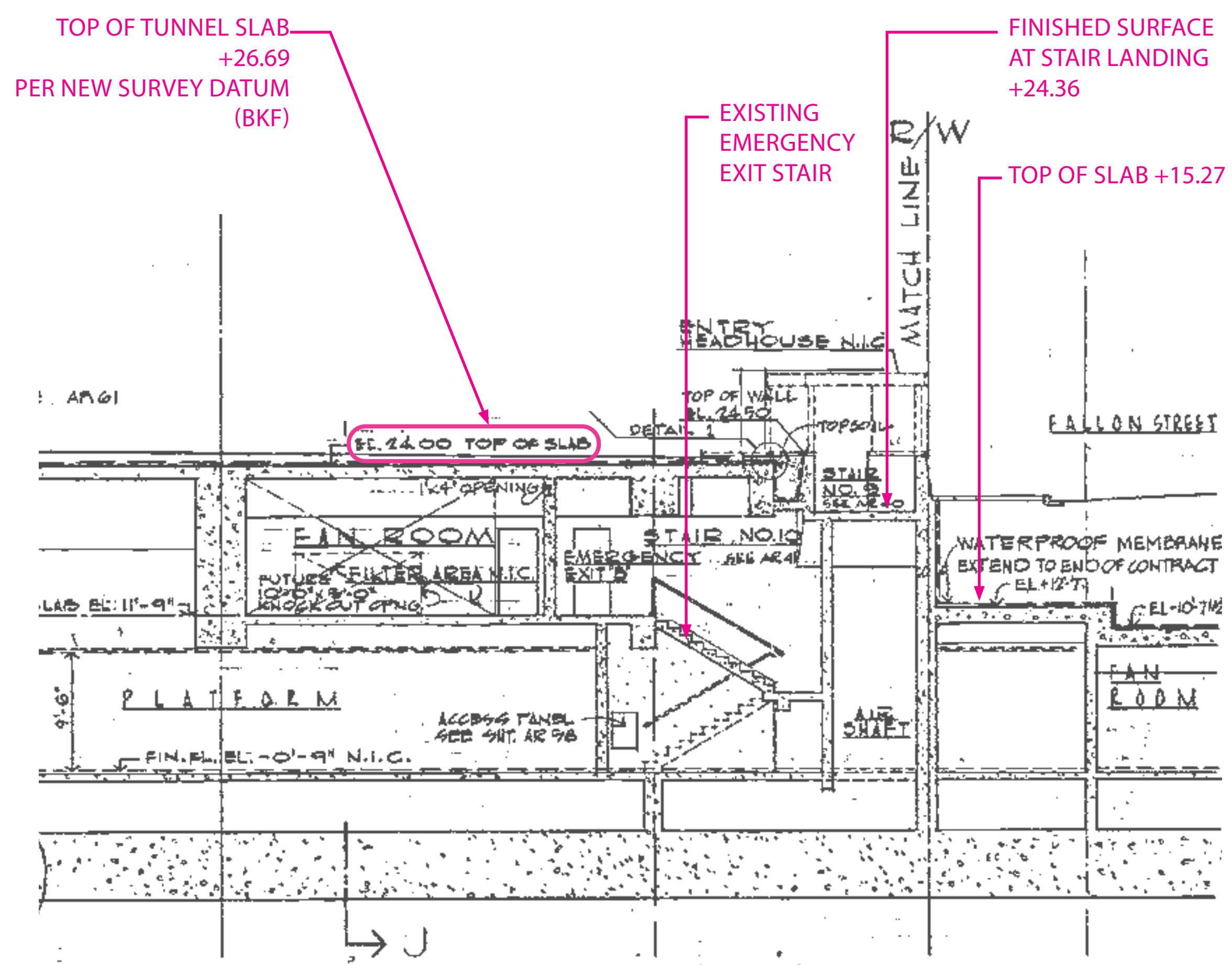
RENDERING OF THE PASEO ENTRANCE ON FALLON



VENT: TO REMAIN  
FINAL DESIGN TREATMENT TO BE DEVELOPED WITH BART. OPPORTUNITY FOR ART.



STAIR: TO REMAIN  
FINAL DESIGN TREATMENT TO BE DEVELOPED WITH BART. OPPORTUNITY FOR ART.



SECTION B  
STAIR: TO REMAIN AS-BUILT OVERLAY AT EMERGENCY EXIT STAIR

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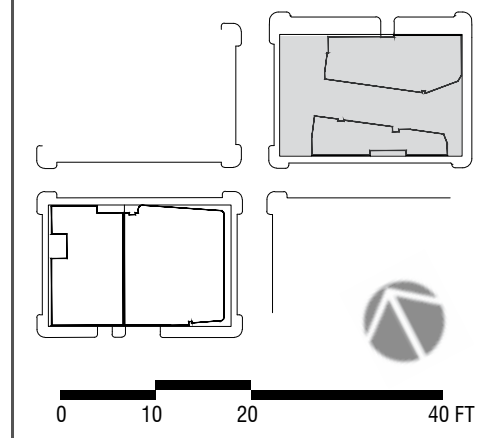
# LAKE MERRITT BART REDEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP#1 COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP#2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP#3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP#4 COMMENTS	03/17/2021



DATE: AUGUST 8, 2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 DETAILED DESIGN EAST END**

# LAKE MERRITT BART DEVELOPMENT

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

CITY OF OAKLAND

ALAMEDA COUNTY

STATE OF CALIFORNIA

### PROJECT DESCRIPTION

**BLOCK 1**  
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 1, BOUND BY 8TH STREET, FALLON STREET, 9TH STREET, AND OAK STREET. A NEW MARKET RATE RESIDENTIAL BUILDING (BUILDING A), A NEW SENIOR HOUSING BUILDING (BUILDING B), AND A NEW PEDESTRIAN PASEO WILL BE CONSTRUCTED ON THE PROPERTY SITE AND WILL MAINTAIN AND IMPROVE ACCESS TO THE EXISTING LAKE MERRITT BART STATION.

**BLOCK 2**  
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 2, BOUND BY 7TH STREET, OAK STREET, 8TH STREET, AND MADISON STREET. A NEW OFFICE BUILDING (BUILDING C) AND A NEW AFFORDABLE HOUSING BUILDING (BUILDING D) WILL BE CONSTRUCTED ON THE PROPERTY SITE.

### GENERAL NOTES

- SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY UNDER THE SUPERVISION OF DAVIS THRESH, PLS #6868, PERFORMED ON MAY 13TH, MAY 15TH, AND MAY 22ND, 2019. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
- FEMA DESIGNATED FLOOD ZONE:** PURSUANT TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY, NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, COMMUNITY NO. 06001C0067H, EFFECTIVE DATE DECEMBER 21, 2018, THE SUBJECT PROPERTY LIES WITHIN FLOOD ZONE "X" - AREAS DETERMINED OF MINIMAL FLOOD HAZARD.
- UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES. AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
- BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HEREON IS BASED UPON RESOLUTIONS OF RECORD STREET AND LOT DIMENSIONS AND COLLECTED STREET MONUMENT LOCATIONS WITHIN THE SURROUNDING STREETS. MONUMENT COLLECTION WAS CONDUCTED ON APRIL 19, 2019. NO CURRENT MAP OR RECORD OF SURVEY CURRENTLY EXISTS FOR THE MAPPED BLOCKS; DEEDS MAKE REFERENCE TO KELLERSBERGER'S MAP OF OAKLAND FILED IN BOOK 7 OF MISCELLANEOUS MAPS AT PAGE 3, ALAMEDA COUNTY RECORDS.
- BENCHMARK:** FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD88)
- HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.

### PROJECT DATA

**OWNERS:**  
 STRADA INVESTMENT GROUP  
 101 MISSION STREET, SUITE 420  
 SAN FRANCISCO, CA 94105  
 PHONE: (415) 263-9151  
 CONTACT: WILLIAM GOODMAN  
 EAST BAY ASIAN LOCAL DEVELOPMENT CORP  
 1825 SAN PABLO AVENUE, SUITE 200  
 OAKLAND, CA 94612  
 PHONE: (510) 287-5353  
 CONTACT: ANDREW MATSAS

**ARCHITECTS:**  
 PYATOK ARCHITECTS  
 1611 TELEGRAPH AVENUE, SUITE 200  
 OAKLAND, CA 94612  
 PHONE: (510) 465-7010  
 CONTACT: PETER WALLER  
 SOLOMON CORDWELL BUENZ ARCHITECTS  
 255 CALIFORNIA STREET, 3RD FLOOR  
 SAN FRANCISCO, CA 94111  
 PHONE: (415) 216-2450  
 CONTACT: CYRIL CHONG

**CIVIL ENGINEER:**  
 BKF ENGINEERS  
 255 SHORELINE DRIVE, SUITE 200  
 REDWOOD CITY, CA 94065  
 PHONE: (650) 482-6377  
 CONTACT: SIMON NORTH

**LANDSCAPE ARCHITECT:**  
 EINWILLERKUEHL LANDSCAPE ARCHITECTURE  
 318 HARRISON STREET, SUITE 301  
 OAKLAND, CA 94607  
 PHONE: (510) 891-1696  
 CONTACT: SARAH KUEHL

**ASSESSOR PARCEL NO.:**  
 001-0169-001 (BLOCK 1)  
 001-0171-002 (BLOCK 2)

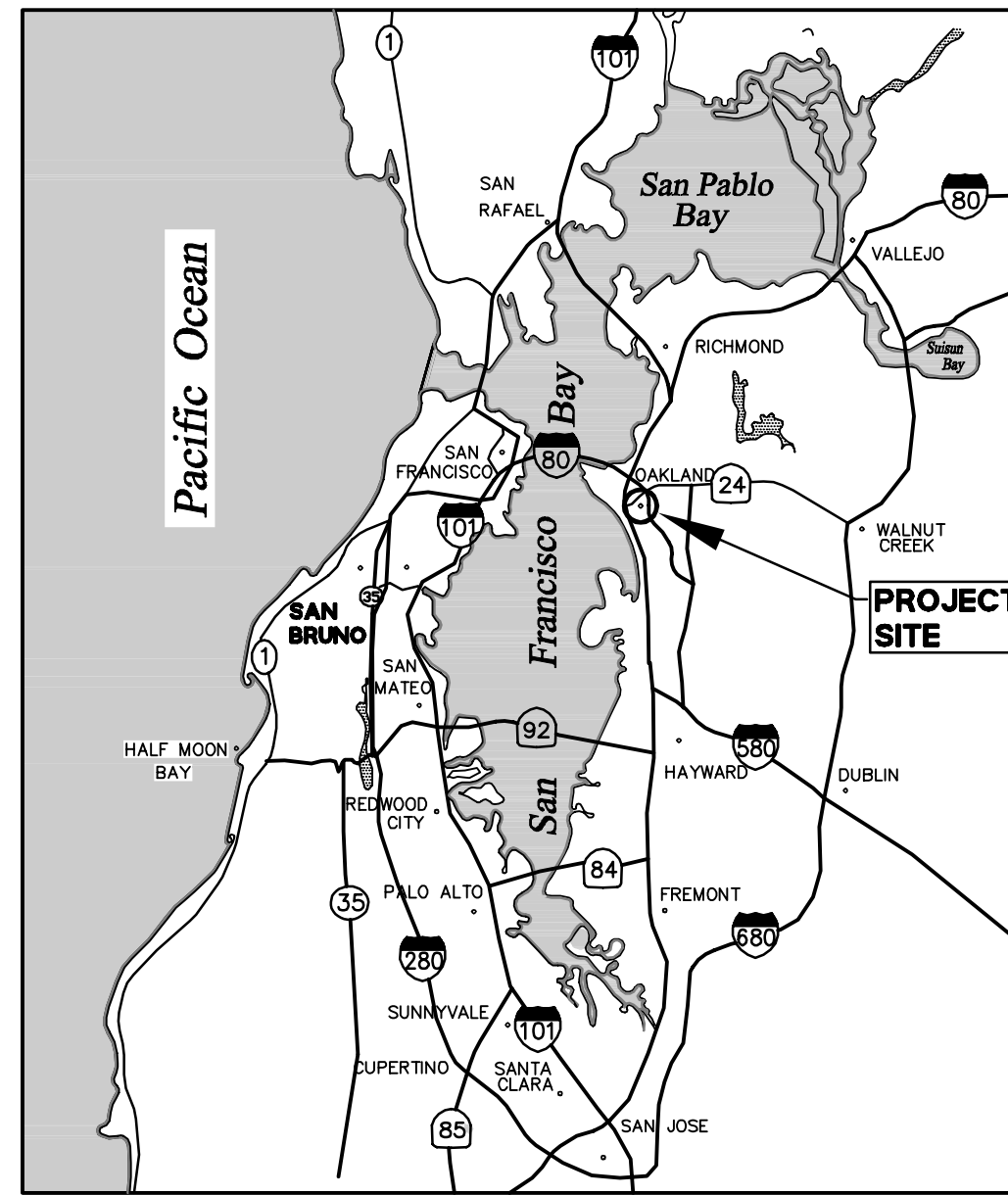
**EXISTING LAND USE:** COMMERCIAL

**PROPOSED LAND USE:** MIXED USE

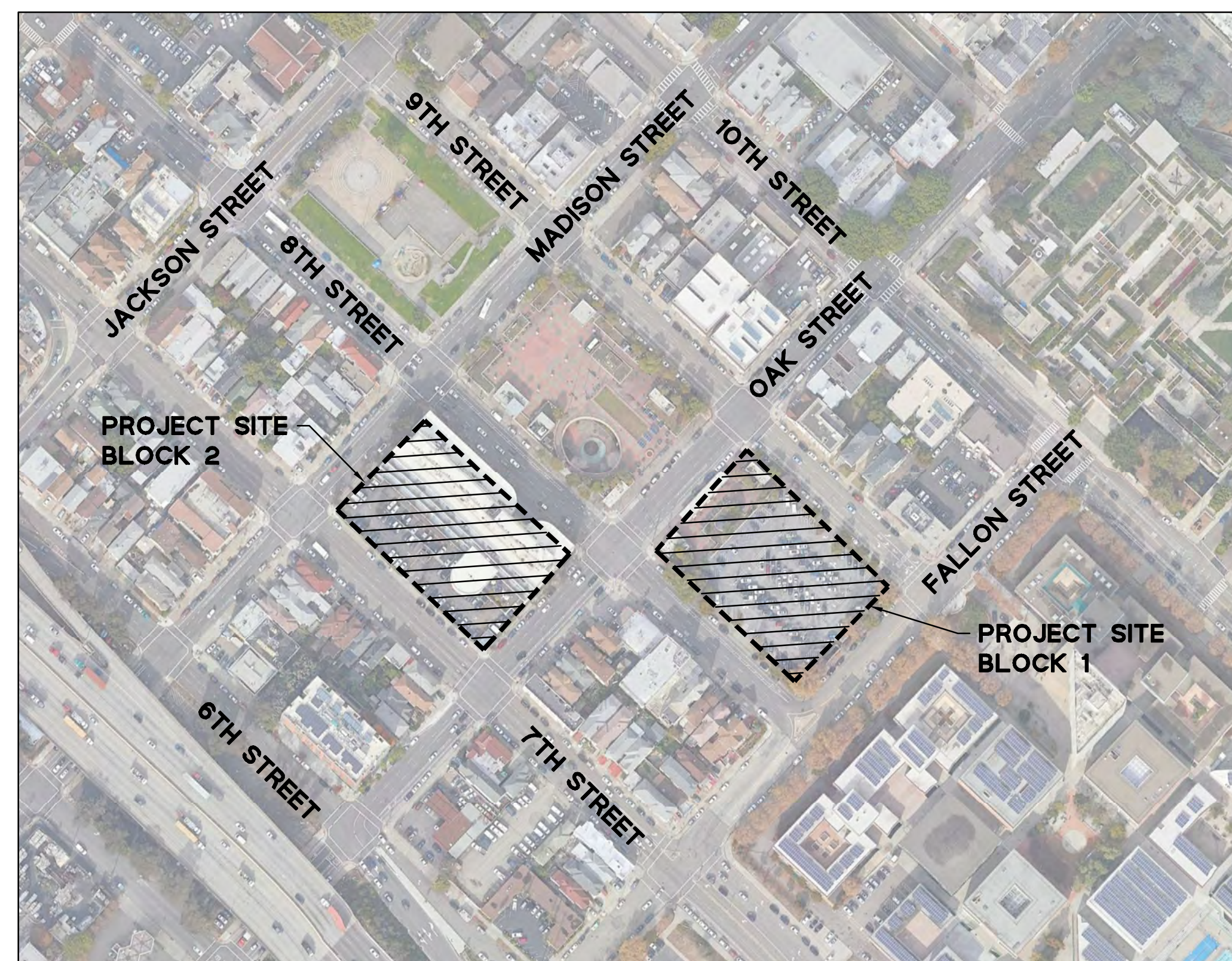
**LAND AREA:**  
 1.383 ACRES (BLOCK 1)  
 1.377 ACRES (BLOCK 2)

### UTILITY INFORMATION:

WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD)  
 FIRE PROTECTION: CITY OF OAKLAND / EBMUD  
 SEWAGE DISPOSAL: CITY OF OAKLAND  
 STORM DRAIN: CITY OF OAKLAND  
 GAS: PACIFIC GAS & ELECTRIC (PG&E)  
 ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E)  
 TELEPHONE: AT&T  
 CABLE TELEVISION: COMCAST



LOCATION MAP  
NTS



VICINITY MAP  
NTS

### ABBREVIATIONS

AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BACK OF WALK
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EV	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
HCR	=	HANDICAP RAMP
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
P	=	POST
PKM	=	PARKING METER
SDCO	=	STORM DRAIN CLEANOUT
SDDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UB	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	RIGHT OF WAY
---	---	CURB AND GUTTER
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWOUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	CATCH BASIN
---	---	SSMH
---	---	SDMH
---	---	SDDI
---	---	WATER VALVE
---	---	GAS VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DCDA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE

### SHEET INDEX

SHEET NO	DESCRIPTION
C1.0	TITLE SHEET
C2.0	OVERALL SHEET INDEX
C2.1	EXISTING CONDITIONS (BLOCK 1)
C2.2	EXISTING CONDITIONS (BLOCK 2)
C3.1	PRELIMINARY DEMOLITION PLAN (BLOCK 1)
C3.2	PRELIMINARY DEMOLITION PLAN (BLOCK 2)
C4.1	PRELIMINARY SITE PLAN (BLOCK 1)
C4.2	PRELIMINARY SITE PLAN (BLOCK 2)
C5.1	PRELIMINARY GRADING PLAN (BLOCK 1)
C5.2	PRELIMINARY GRADING PLAN (BLOCK 2)
C5.3	PRELIMINARY SECTIONS
C6.1	PRELIMINARY UTILITY PLAN (BLOCK 1)
C6.2	PRELIMINARY UTILITY PLAN (BLOCK 2)
C7.1	PRELIMINARY STORMWATER PLAN (BLOCK 1)
C7.2	PRELIMINARY STORMWATER PLAN (BLOCK 2)
C8.1	PRELIMINARY DETAILS

### ENGINEER'S STATEMENT

THESE CONSTRUCTION DOCUMENTS HAVE BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

*Simon North*  
 SIMON R. NORTH, P.E.  
 VICE PRESIDENT  
 BKF ENGINEERS



04/26/2021  
DATE

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
 1825 SAN PABLO AVE. #200  
 OAKLAND, CA 94612

STRADA  
 101 MISSION ST. #420  
 SAN FRANCISCO, CA 94105

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## LAKE MERRITT BART DEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION

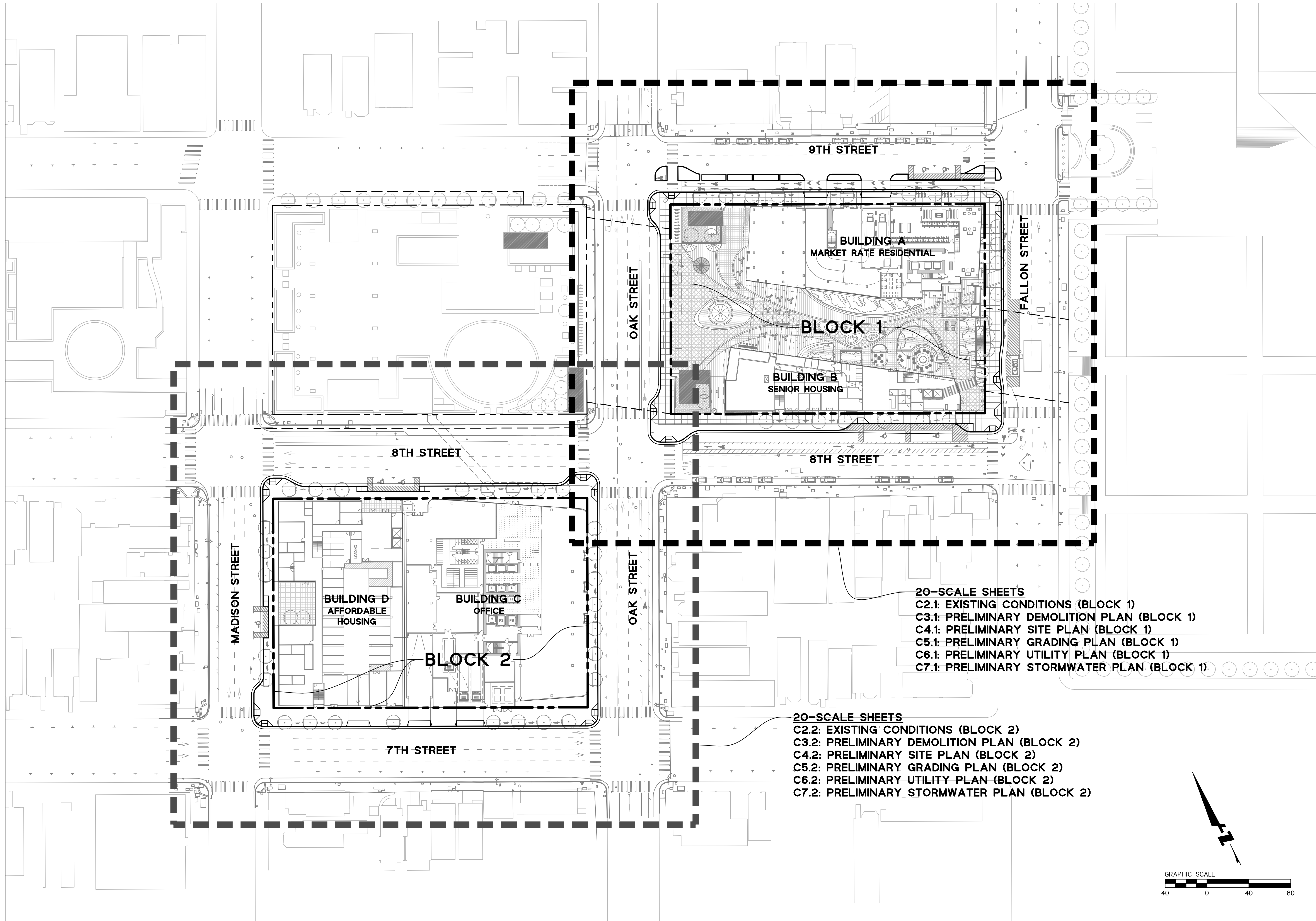
### REVISION SCHEDULE

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
 DATE: 04/26/2021  
 SCALE: AS NOTED

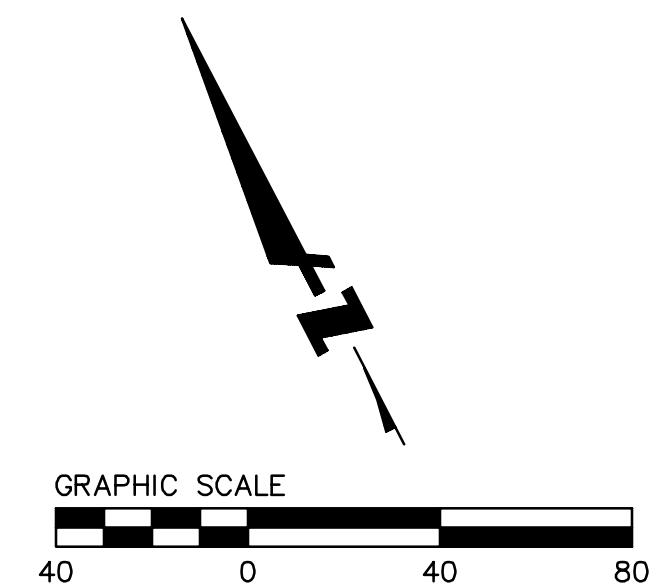
### TITLE SHEET

# C1.0



- 20-SCALE SHEETS**  
 C2.1: EXISTING CONDITIONS (BLOCK 1)  
 C3.1: PRELIMINARY DEMOLITION PLAN (BLOCK 1)  
 C4.1: PRELIMINARY SITE PLAN (BLOCK 1)  
 C5.1: PRELIMINARY GRADING PLAN (BLOCK 1)  
 C6.1: PRELIMINARY UTILITY PLAN (BLOCK 1)  
 C7.1: PRELIMINARY STORMWATER PLAN (BLOCK 1)

- 20-SCALE SHEETS**  
 C2.2: EXISTING CONDITIONS (BLOCK 2)  
 C3.2: PRELIMINARY DEMOLITION PLAN (BLOCK 2)  
 C4.2: PRELIMINARY SITE PLAN (BLOCK 2)  
 C5.2: PRELIMINARY GRADING PLAN (BLOCK 2)  
 C6.2: PRELIMINARY UTILITY PLAN (BLOCK 2)  
 C7.2: PRELIMINARY STORMWATER PLAN (BLOCK 2)



**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
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**LAKE MERRITT BART DEVELOPMENT**  
 Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

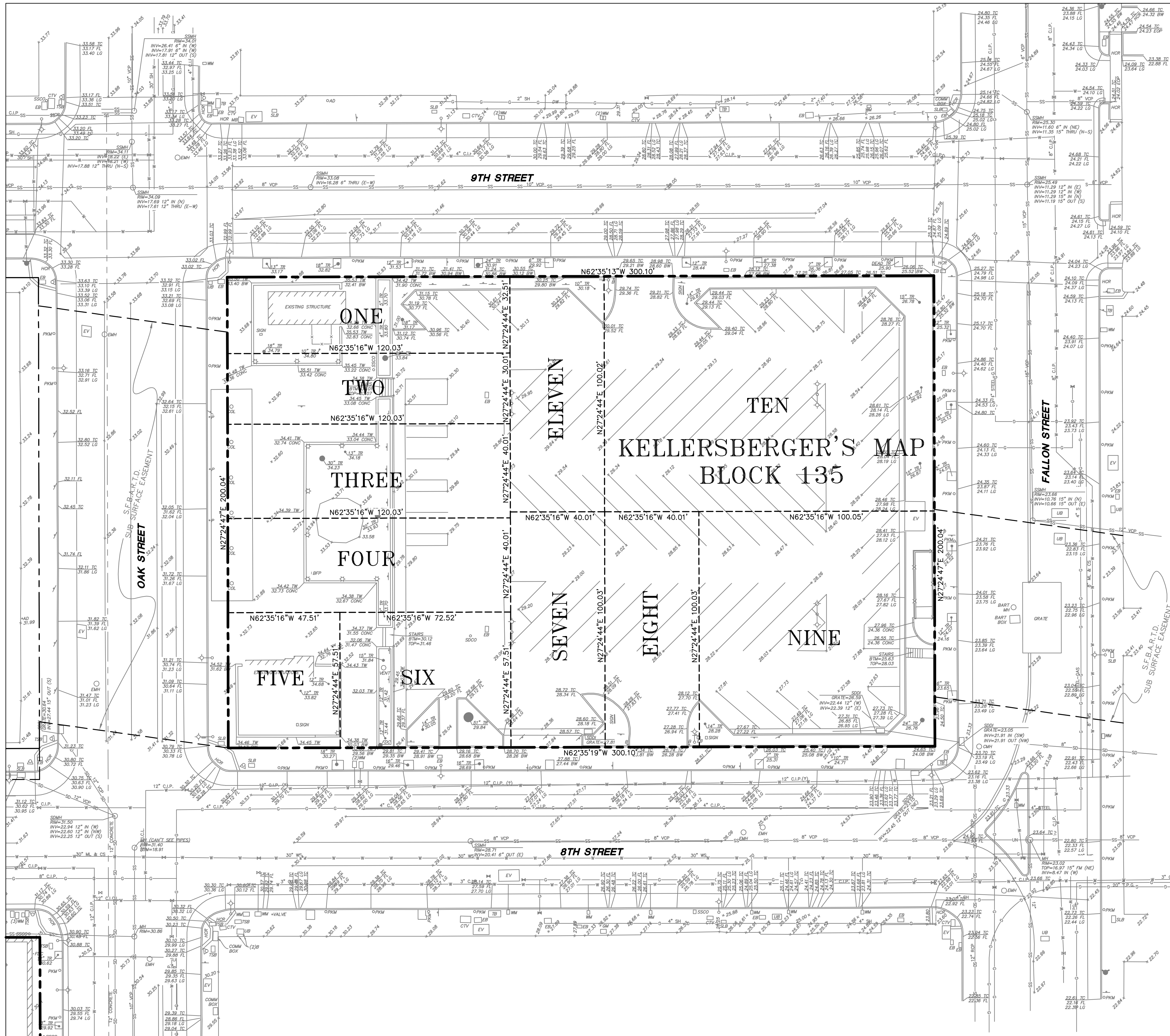
- PRELIMINARY - NOT FOR CONSTRUCTION

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
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5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
 DATE: 04/26/2021  
 SCALE: 1" = 40'

**OVERALL SHEET INDEX**

**C2.0**



**ANNOTATION & LEGEND**

- AD = AREA DRAIN
- B = BOLLARD
- BFP = BACKFLOW PREVENTOR
- BR = BIKE RACK
- BW = BACK OF WALK
- CLDR = CENTERLINE OF DOOR
- CMH = COMMUNICATION MANHOLE
- COL = COLUMN
- COMM = COMMUNICATION
- CONC = CONCRETE
- CTV = CABLE TELEVISION
- DW = DRIVEWAY
- EB = ELECTRICAL BOX
- EMH = ELECTRICAL MANHOLE
- EP = EDGE OF PAVEMENT
- EV = ELECTRICAL VAULT
- FDC = FIRE DEPARTMENT CONNECTION
- FL = FLOWLINE
- HCR = HANDICAP RAMP
- LG = LIP OF GUTTER
- MB = MAIL BOX
- MH = MANHOLE
- P = POST
- PKM = PARKING METER
- SDCO = STORM DRAIN CLEANOUT
- SDDI = STORM DRAIN DROP INLET
- SDMH = STORM DRAIN MANHOLE
- SLB = STREET LIGHTING BOX
- SSCO = SANITARY SEWER CLEANOUT
- SSMH = SANITARY SEWER MANHOLE
- TB = TELEPHONE BOX
- TC = TOP OF CURB
- TR = TREE
- TSB = TRAFFIC SIGNAL BOX
- TW = TOP OF WALL
- UB = UTILITY BOX
- UV = UTILITY VAULT
- WM = WATER METER
- WP = WATER PIPE
- ☆ = AREA/YARD LIGHT
- ⊙ = FIRE HYDRANT
- ⊙ = SIGN
- ⊙ = STREET LIGHT
- ⊙ = GAS VALVE
- ⊙ = WATER VALVE
- ⊙ = SIGNAL LIGHT
- E—E— = ELECTRICAL LINE
- G—G— = GAS LINE
- SD—SD— = STORM DRAIN LINE
- SS—SS— = SANITARY SEWER LINE
- UN—UN— = UNKNOWN UTILITY LINE
- W—W— = DOMESTIC WATER LINE

**NOTES**

FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019

ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

**HORIZONTAL CONTROL**

HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS83), EPOCH 2017.00

**BENCHMARK**

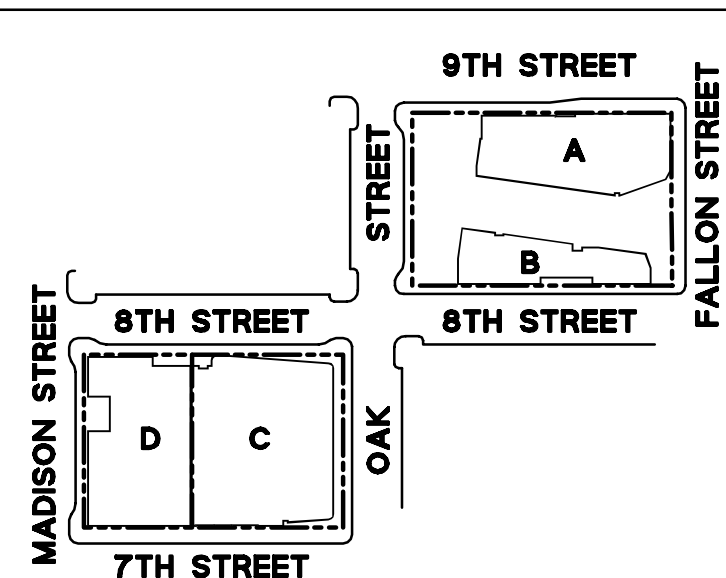
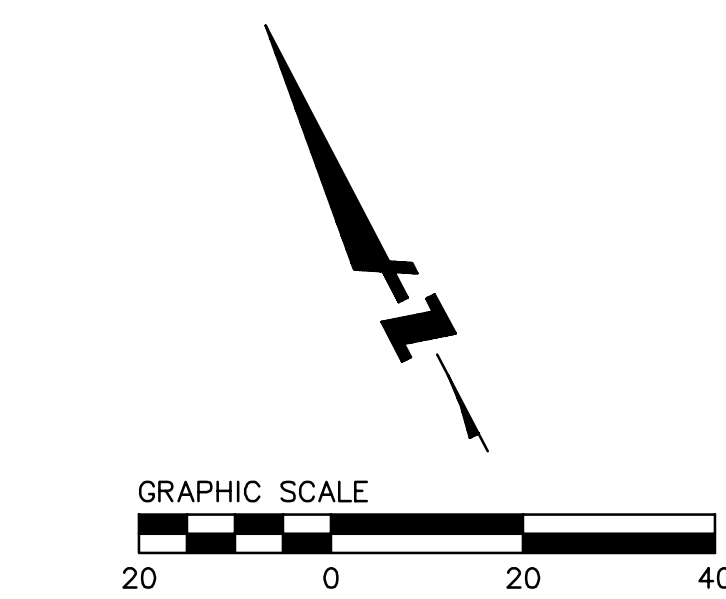
FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET.  
ELEVATION = 23.062 (NAVD88)

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION

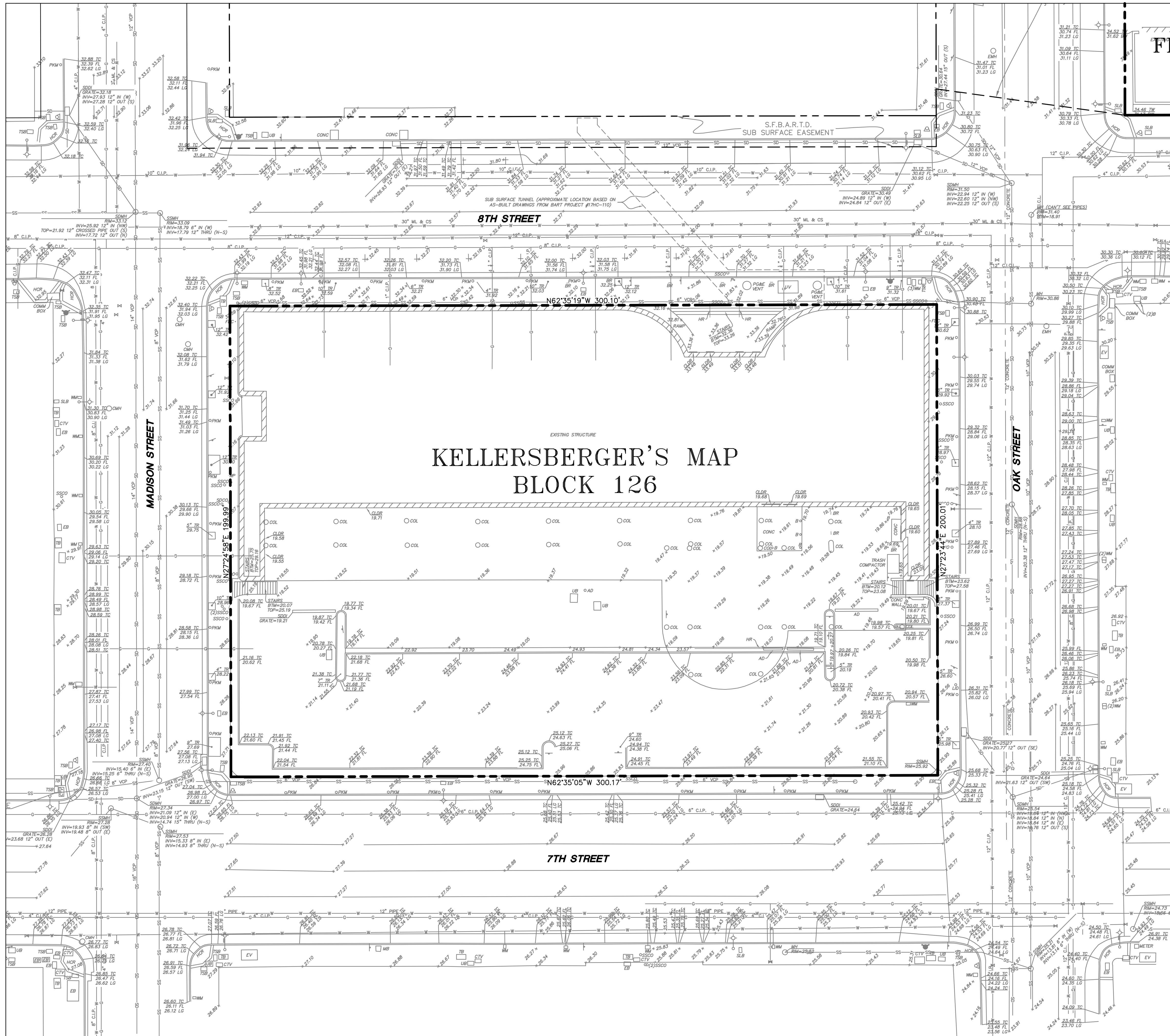
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JOB NUMBER: 20190110  
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SCALE: 1" = 20'

**EXISTING CONDITIONS (BLOCK 1)**

**C2.1**



# KELLERSBERGER'S MAP BLOCK 126

### ANNOTATION & LEGEND

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### NOTES

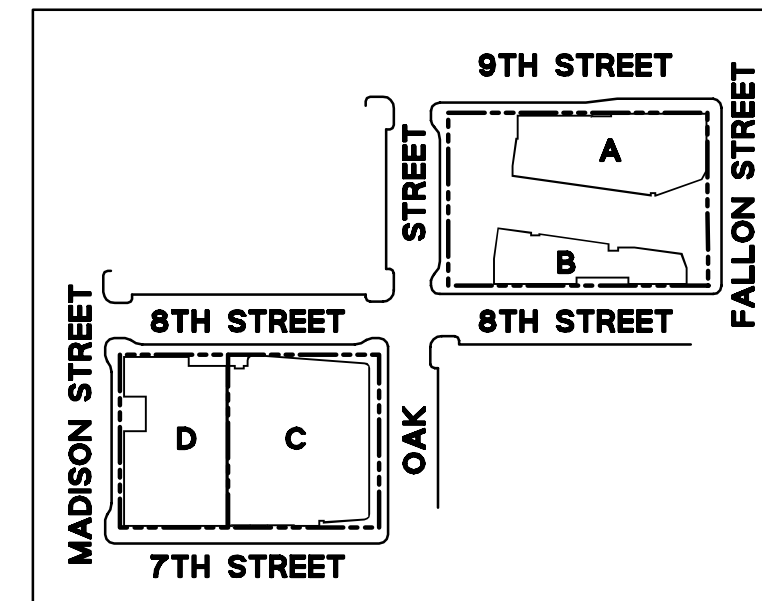
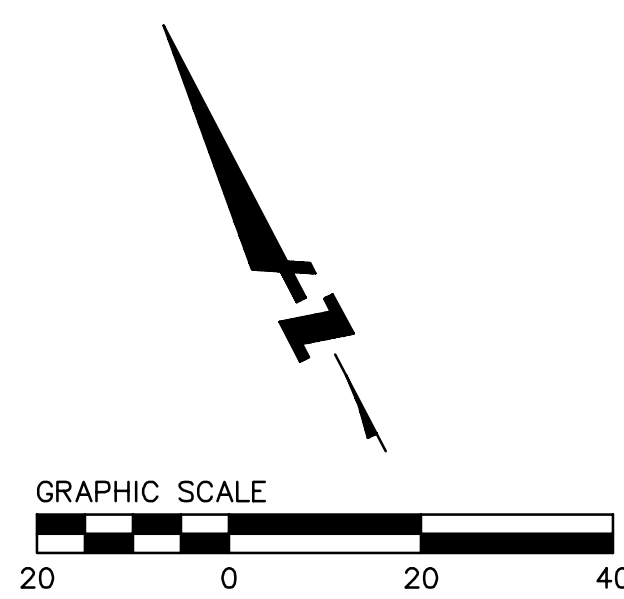
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### BENCHMARK

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## LAKE MERRITT BART DEVELOPMENT

Oakland, CA 94607

### PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION

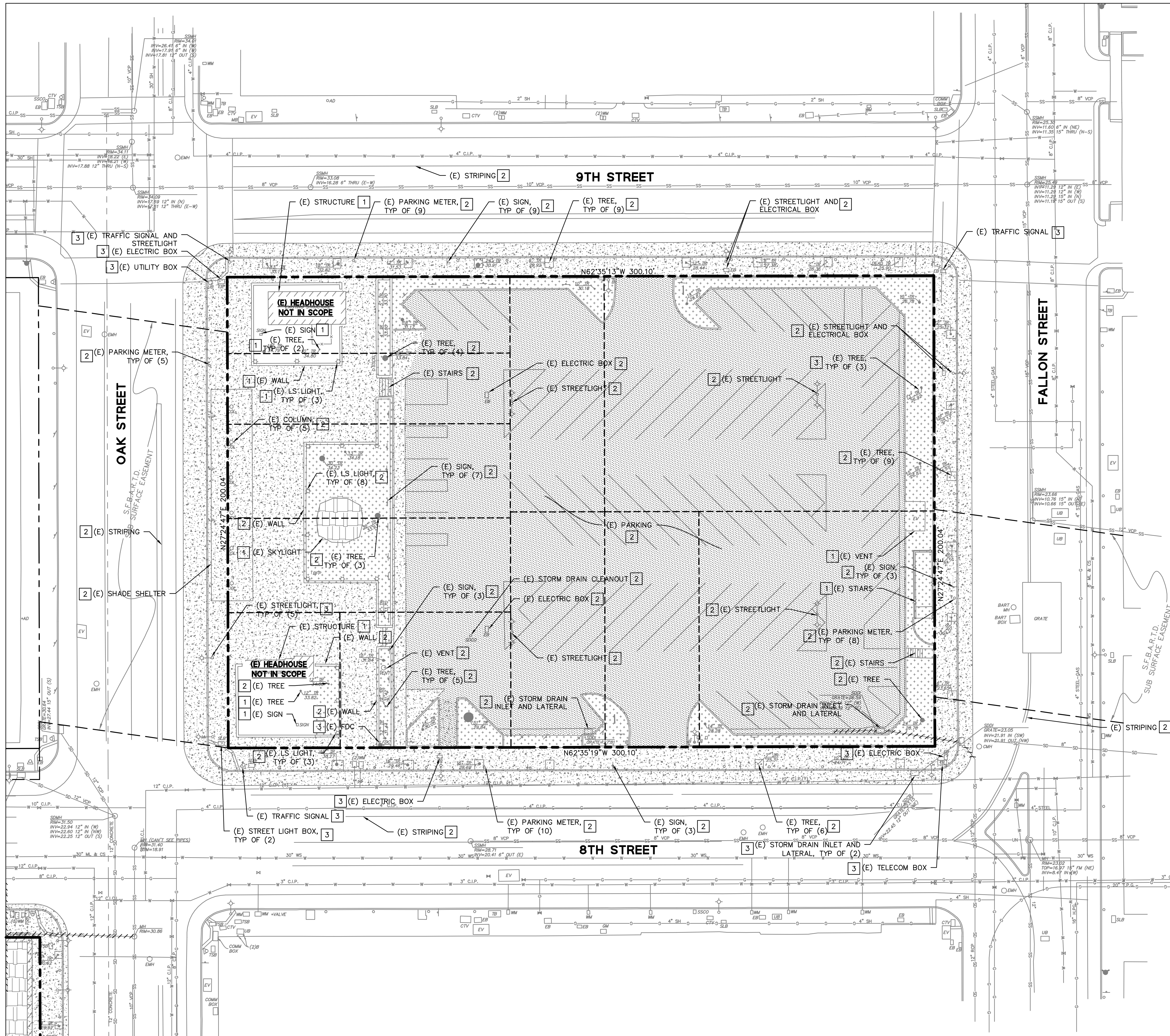
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JOB NUMBER: 20190110  
 DATE: 04/26/2021  
 SCALE: 1" = 20'

EXISTING CONDITIONS (BLOCK 2)

# C2.2





**DEMOLITION LEGEND:**

- SAWCUT DEMO & REMOVE EXISTING ASPHALT PARKING LOT & FULL DEPTH AC
- SAWCUT DEMO & REMOVE EXISTING CONCRETE HARDSCAPE, INCLUDING SIDEWALK, CURB, & GUTTER
- EXISTING BUILDING TO BE DEMOLISHED
- EXISTING LANDSCAPE TO BE REMOVED
- SAWCUT LINE
- EXISTING UTILITY LINE TO BE ABANDONED/REMOVED AS NEEDED

**DEMOLITION KEYNOTES:**

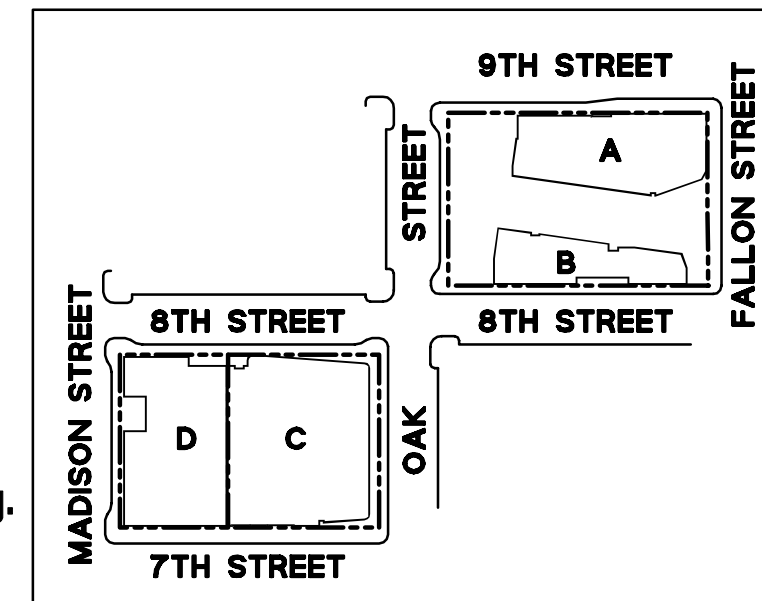
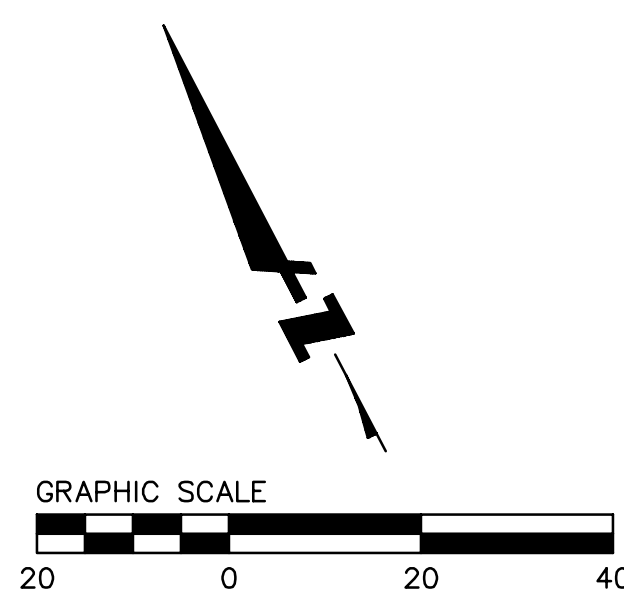
- 1 TO REMAIN, PROTECT IN PLACE
- 2 TO BE REMOVED
- 3 TO BE RELOCATED

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

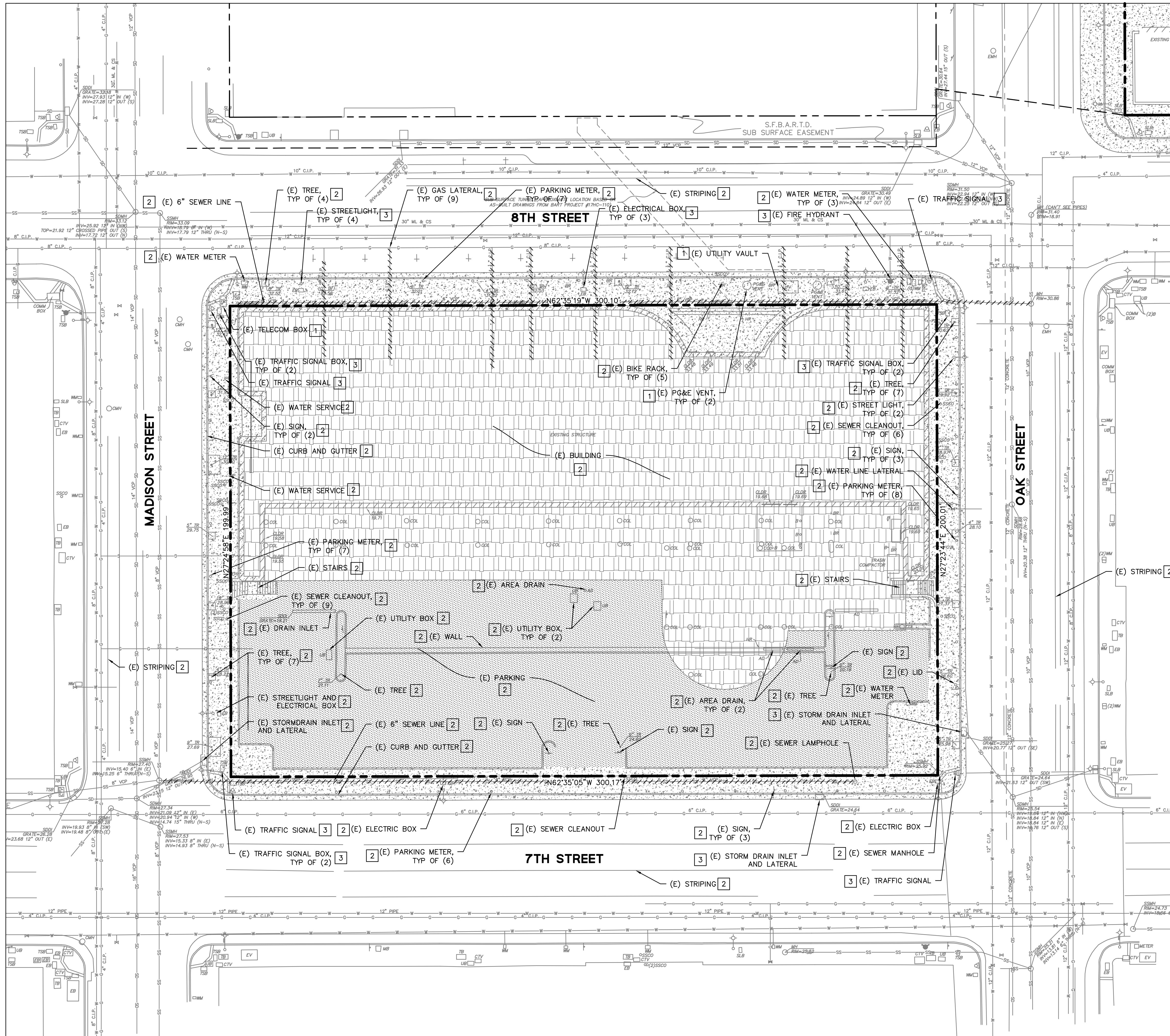
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JOB NUMBER: 20190110  
DATE: 04/26/2021  
SCALE: 1" = 20'  
**PRELIMINARY DEMOLITION PLAN (BLOCK 1)**

**C3.1**



**DEMOLITION LEGEND:**

- SAWCUT DEMO & REMOVE EXISTING ASPHALT PARKING LOT & FULL DEPTH AC
- SAWCUT DEMO & REMOVE EXISTING CONCRETE HARDSCAPE, INCLUDING SIDEWALK, CURB, & GUTTER
- EXISTING BUILDING TO BE DEMOLISHED
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**DEMOLITION KEYNOTES:**

- 1** TO REMAIN, PROTECT IN PLACE
- 2** TO BE REMOVED
- 3** TO BE RELOCATED

**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
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318 HARRISON ST. STE 301  
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(510) 811-1686  
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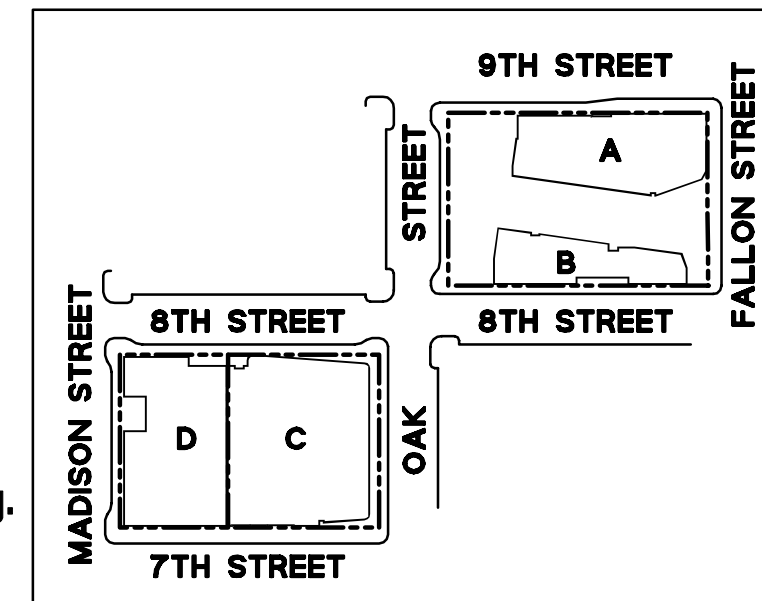
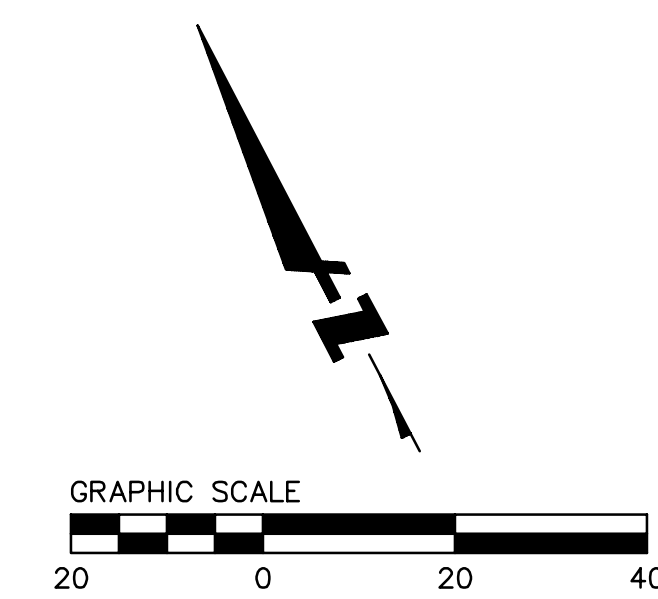
**BKF100**  
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**LAKE MERRITT BART DEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION

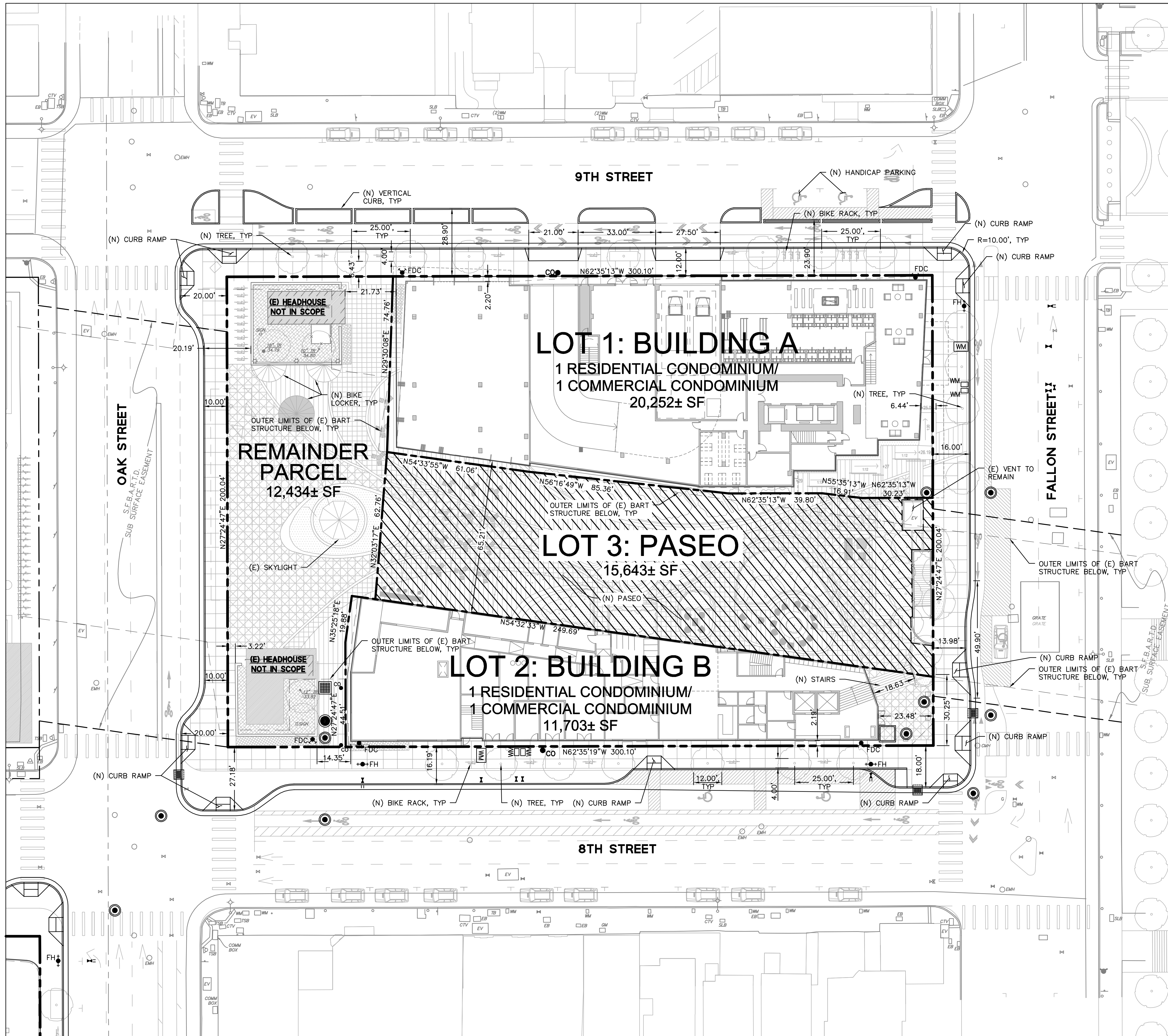
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JOB NUMBER: 20190110  
DATE: 04/26/2021  
SCALE: 1" = 20'

**PRELIMINARY DEMOLITION PLAN (BLOCK 2)**

**C3.2**



**SITE PLAN NOTES:**

1. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
2. SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
3. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
4. ALL PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS.
5. COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
6. SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
7. FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- EASEMENT LINE
- PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

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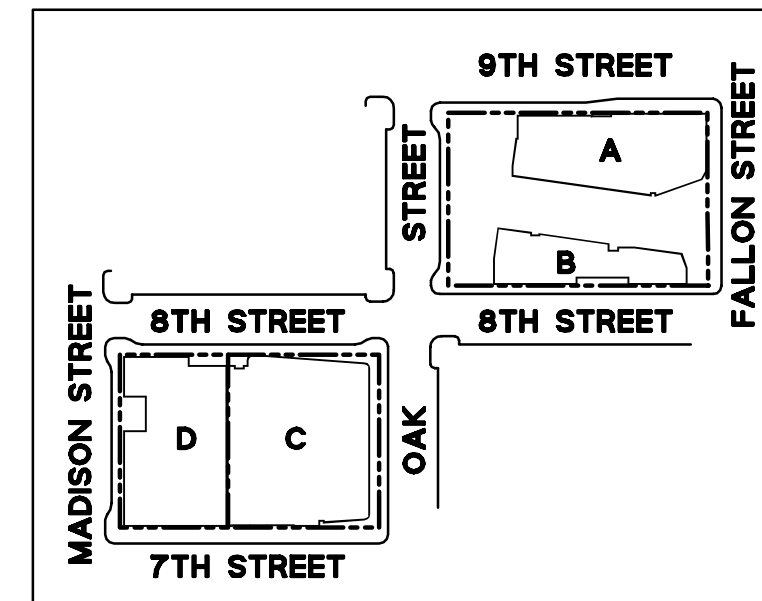
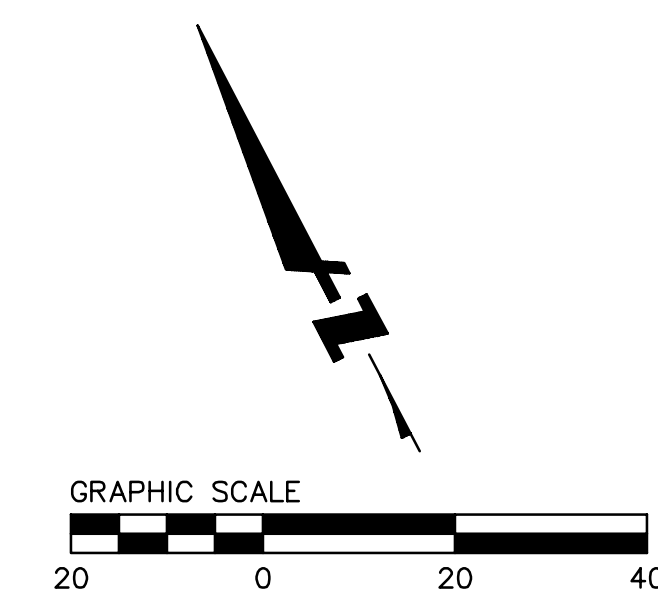
**LAKE MERRITT BART DEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION

**REVISION SCHEDULE**

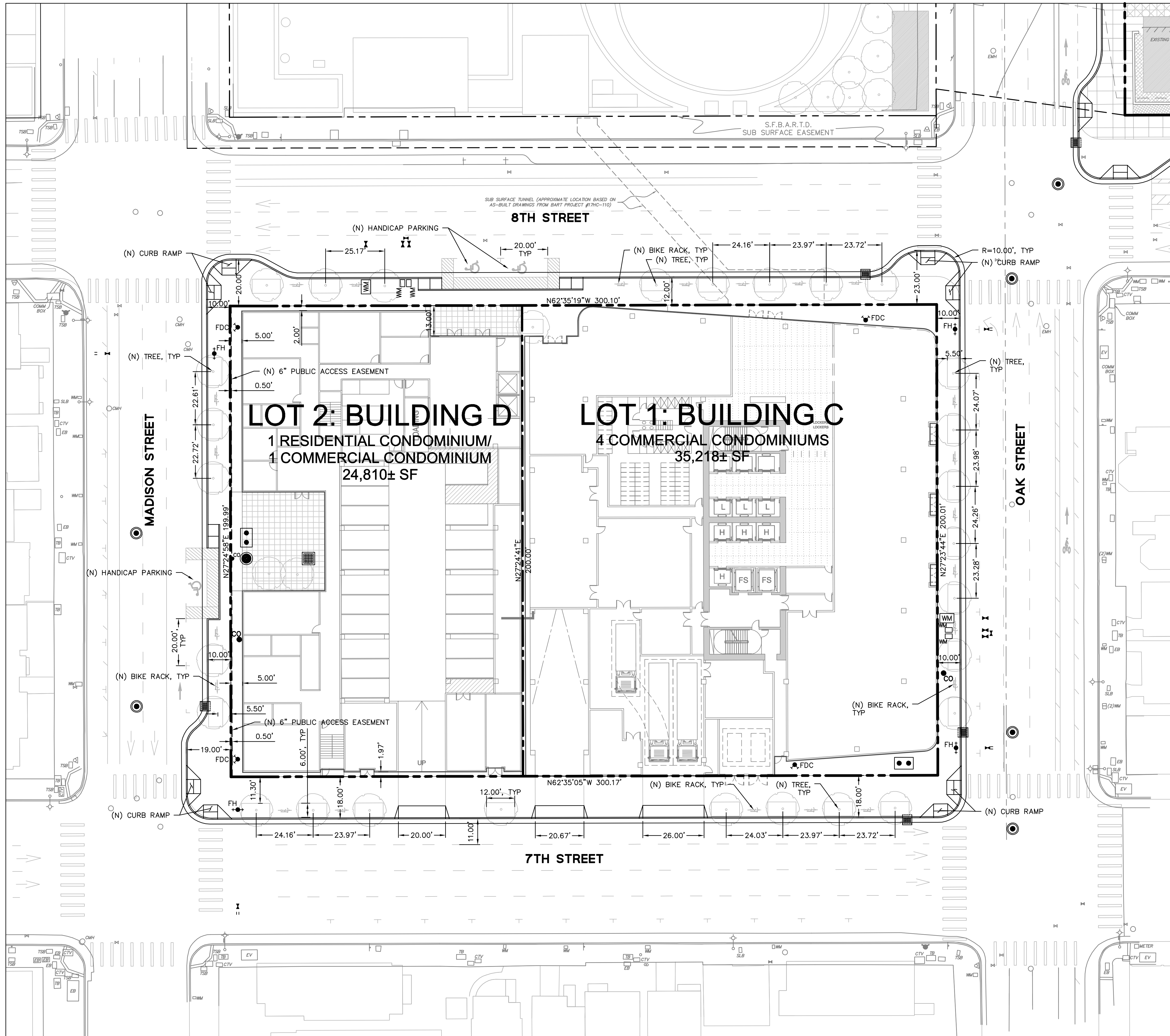
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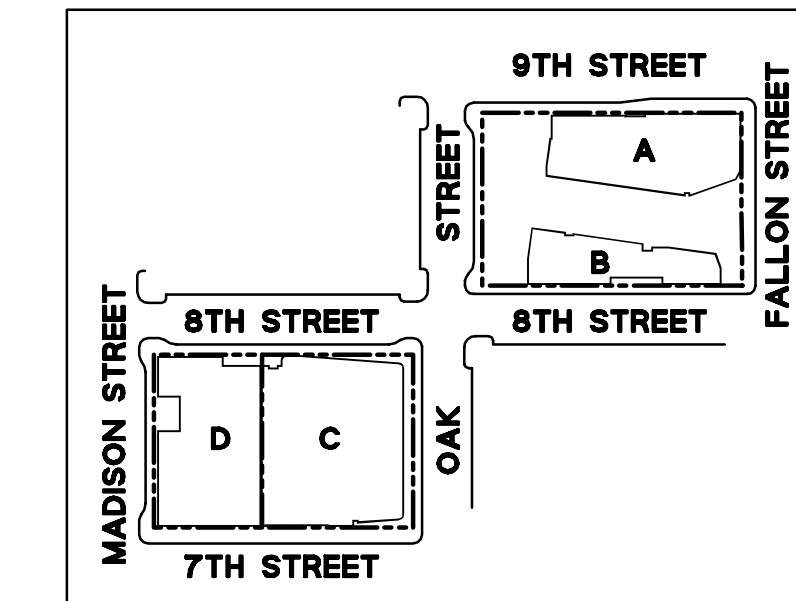
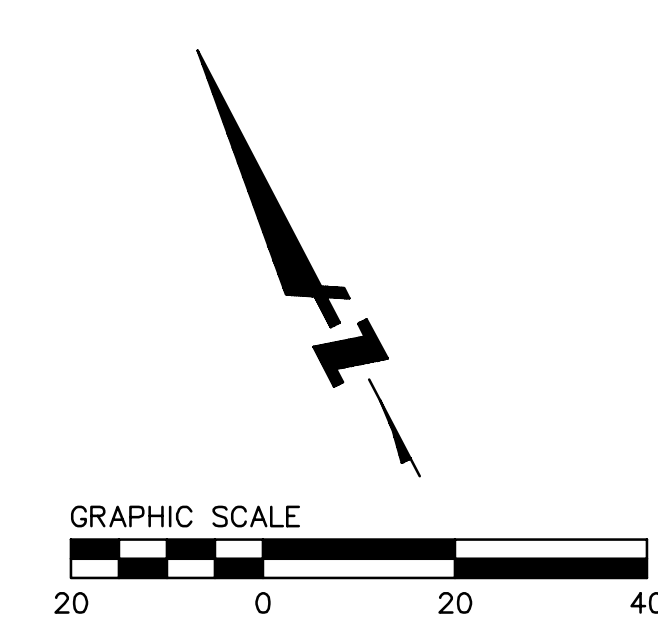
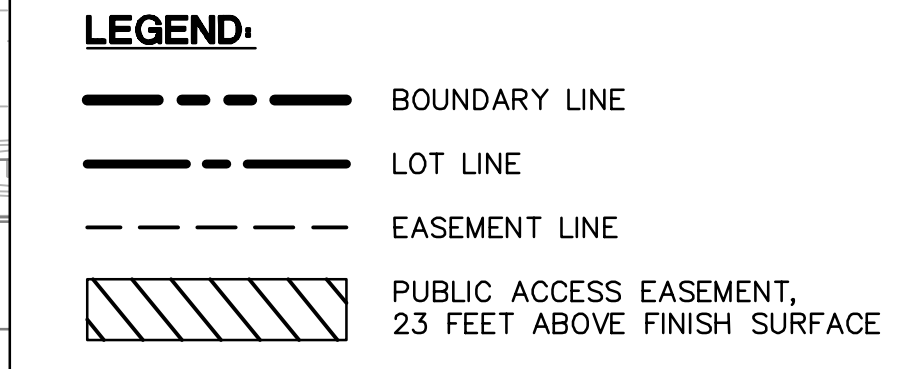
JOB NUMBER: 20190110  
DATE: 04/26/2021  
SCALE: 1" = 20'

**PRELIMINARY SITE PLAN (BLOCK 1)**

**C4.1**



- SITE PLAN NOTES:**
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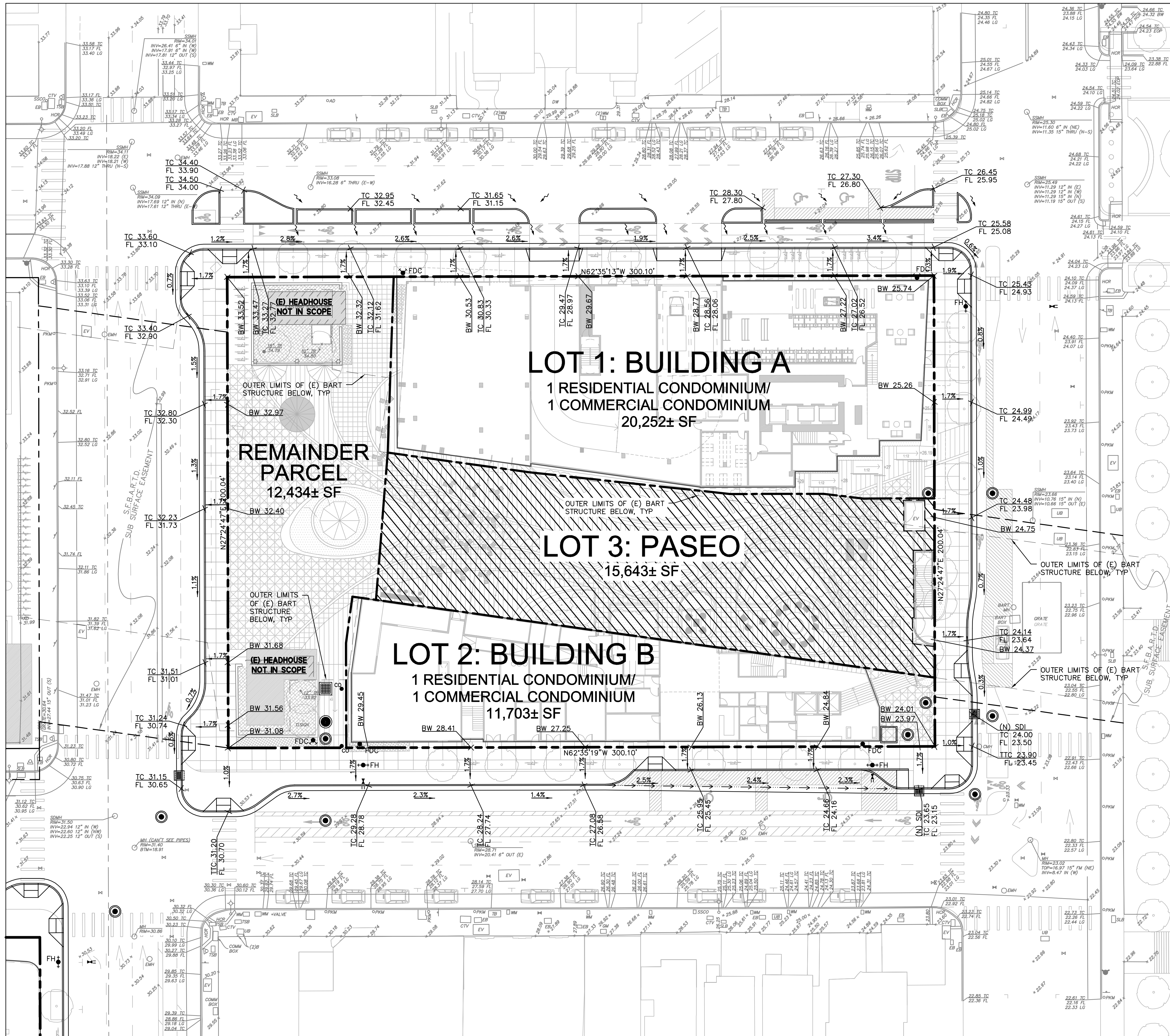
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**PRELIMINARY SITE PLAN (BLOCK 2)**

**C4.2**

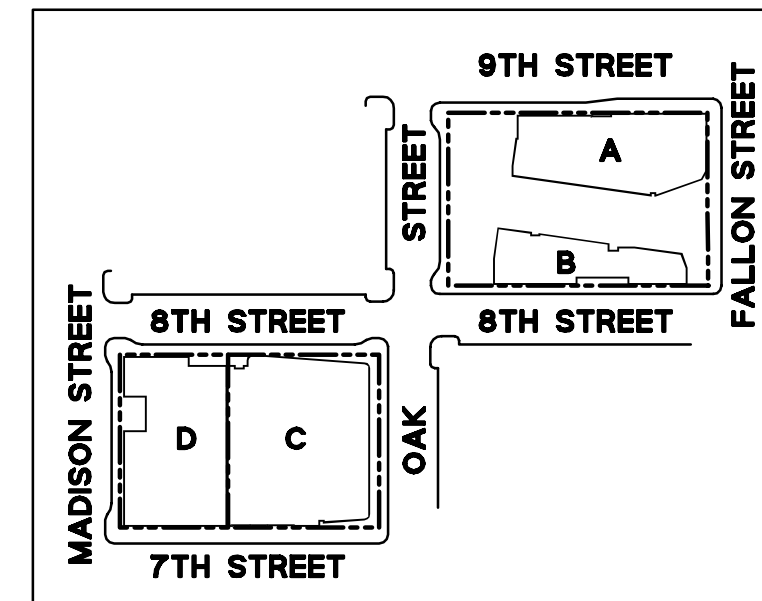
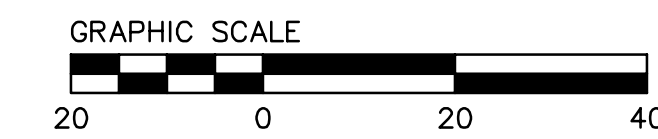


**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05', HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

**LEGEND:**

- BOUNDARY LINE
- - - LOT LINE
- - - EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE



**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

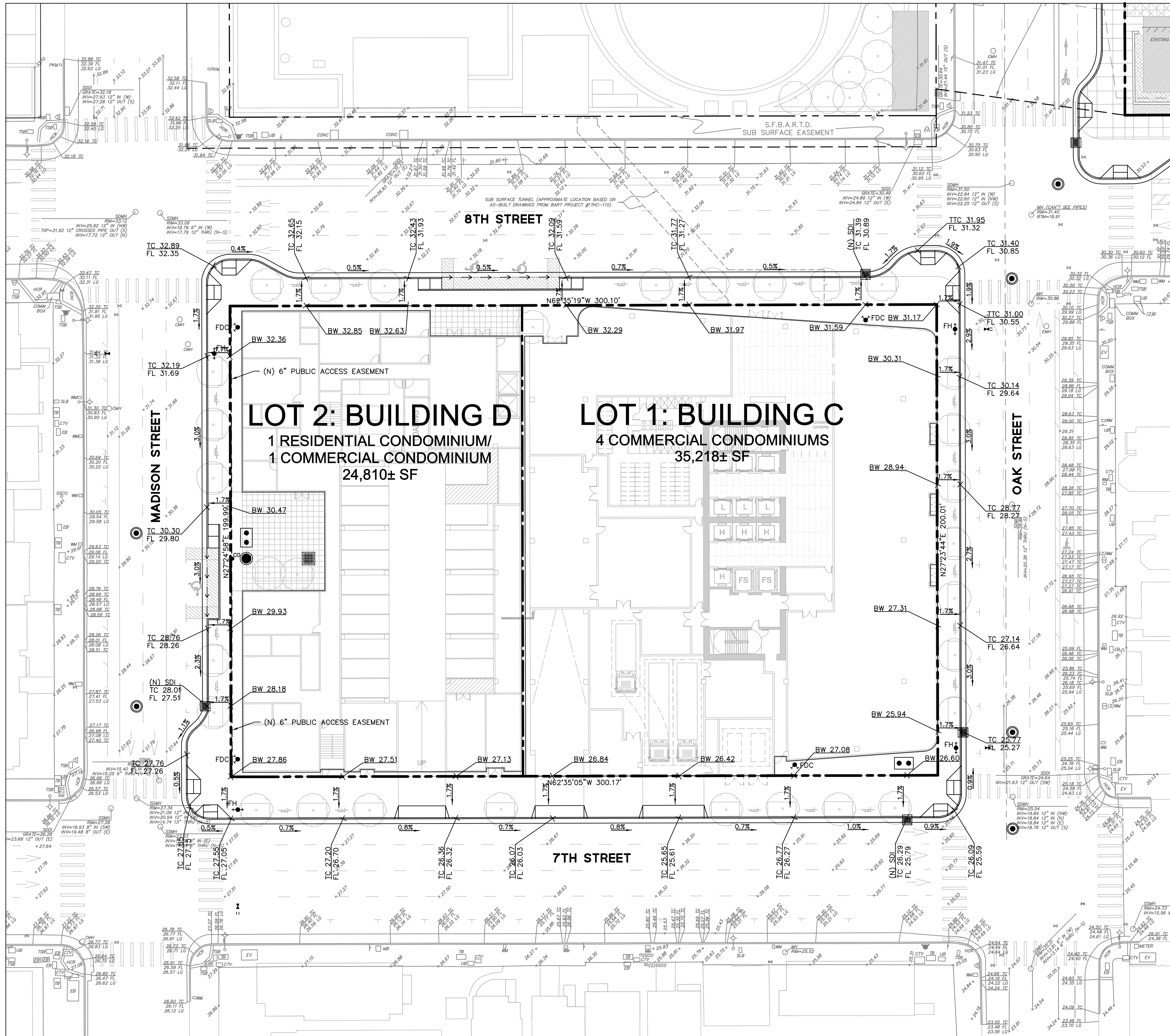
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REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

JOB NUMBER: 20190110  
DATE: 04/26/2021  
SCALE: 1" = 20'

**PRELIMINARY GRADING PLAN (BLOCK 1)**

**C5.1**

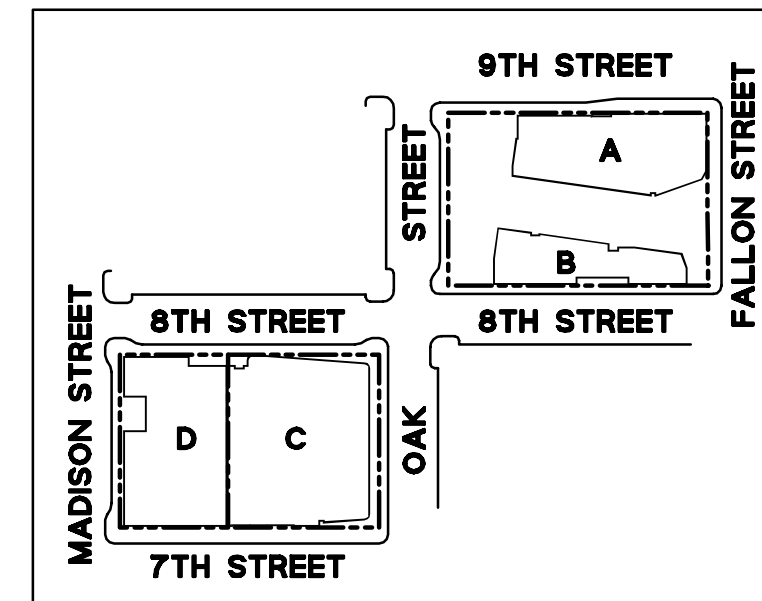
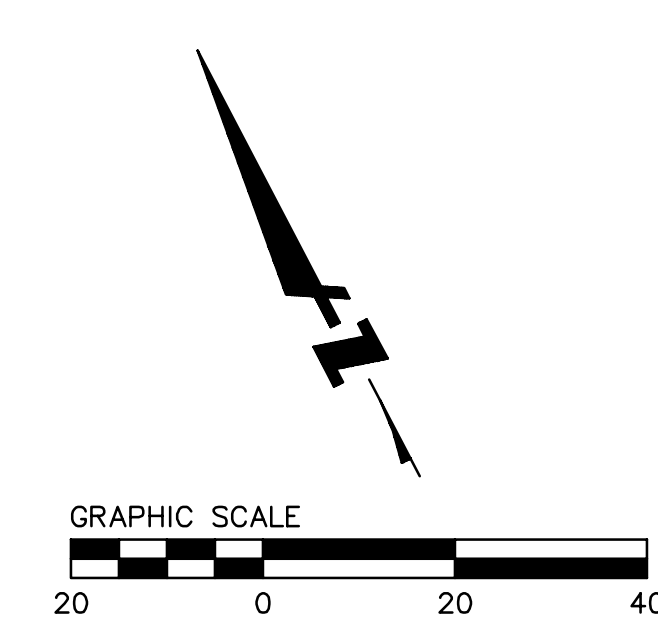


**GRADING NOTES:**

1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05', HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
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**LEGEND:**

- BOUNDARY LINE
- LOT LINE
- - - EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE



**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

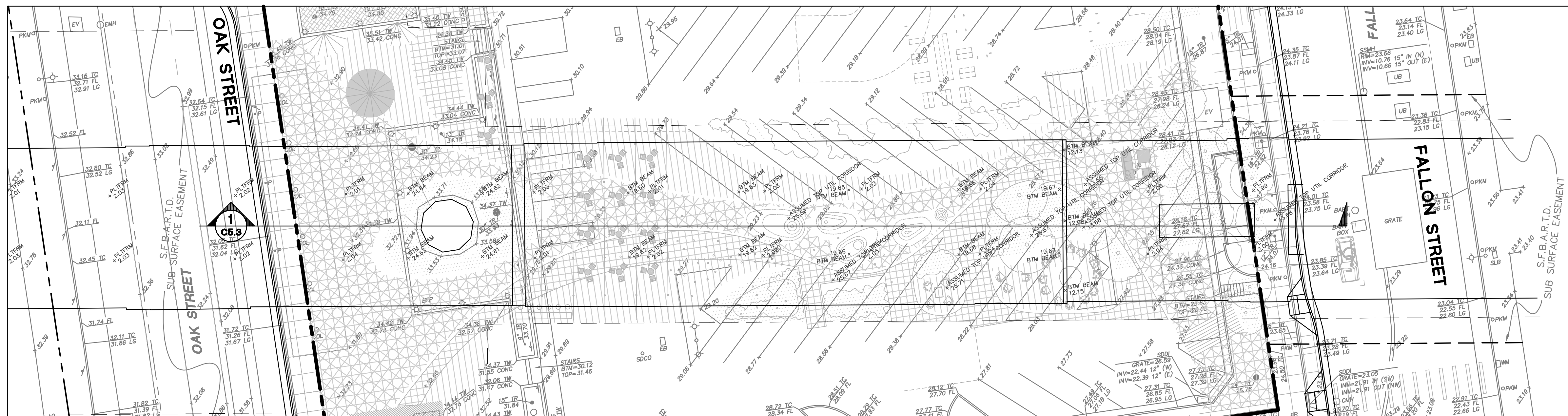
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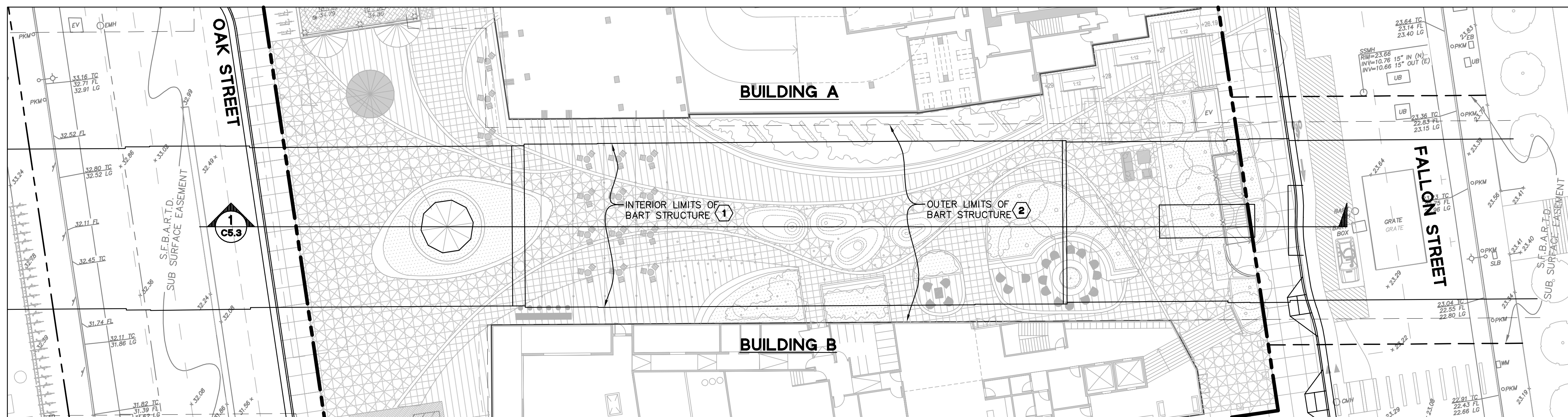
**PRELIMINARY GRADING PLAN (BLOCK 2)**

**C5.2**



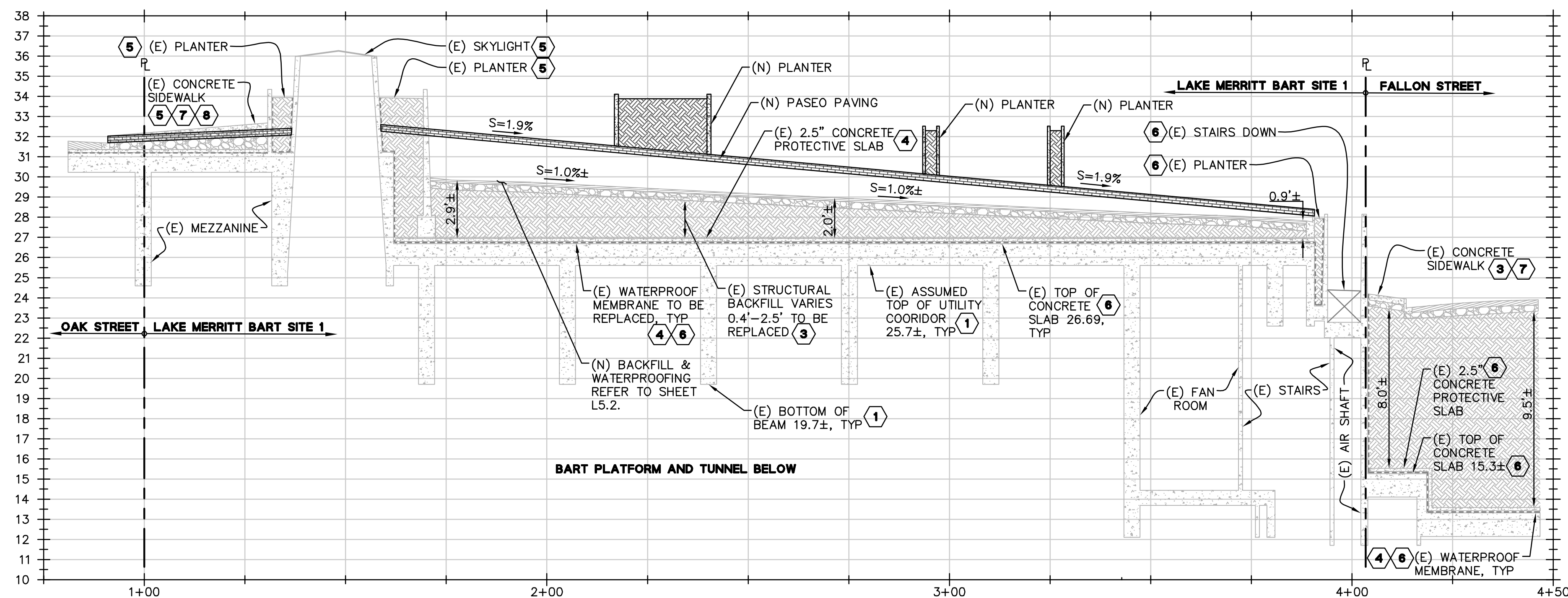
**EXISTING PLAN VIEW**

SCALE: 1" = 20'



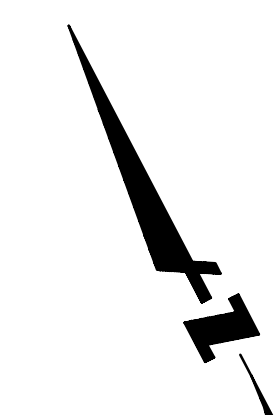
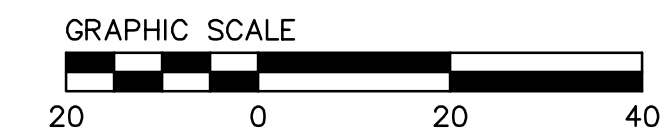
**PROPOSED PLAN VIEW**

SCALE: 1" = 20'



**LAKE MERRITT BART TUNNEL PROFILE**

HORZ SCALE: 1" = 20' - VERT SCALE: 1" = 4'

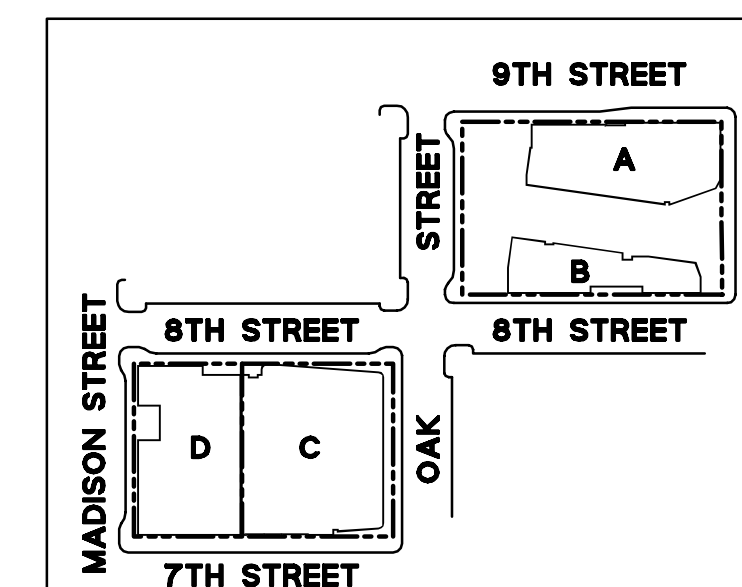


**NOTES:**

1. REFER TO THE KEYNOTES BELOW THAT REFERENCE VARIOUS BART AS-BUILT RECORD DRAWINGS USED TO CREATE THESE PROFILES AND SECTIONS. NOTE THAT ANY ELEVATIONS TAKEN FROM THESE AS-BUILT PLANS HAVE BEEN INCREASED BY 2.69' TO CORRELATE WITH THE NAVD88 SURVEY COORDINATE SYSTEM.

**KEYNOTES:**

- (1) INTERIOR TUNNEL LIMITS OBTAINED FROM SCAN INFORMATION COLLECTED ON 9/15/19, PER BKF "UNDERGROUND BART STATION EXHIBIT", DATED 9/23/19
- (2) EXTERIOR TUNNEL LIMITS WERE DETERMINED BY OFFSETTING THE CALCULATED, BEST-FIT CENTERLINE OF THE PLATFORM BY 31.33 FT, PER BKF "UNDERGROUND BART STATION EXHIBIT" DATED 9/23/19
- (3) REFER TO SHEET CT5-2 FROM BART CONTRACT #K0071-K007
- (4) REFER TO SHEET AR61-1 FROM BART CONTRACT #K0071-K007
- (5) REFER TO SHEET AR29-1 FROM BART CONTRACT #K0071-K007
- (6) REFER TO SHEET AR30-1 FROM BART CONTRACT #K0071-K007
- (7) REFER TO SHEET CT3-2 FROM BART CONTRACT #K0071-K007
- (8) REFER TO SHEET CT9-1 FROM BART CONTRACT #K0071-K007



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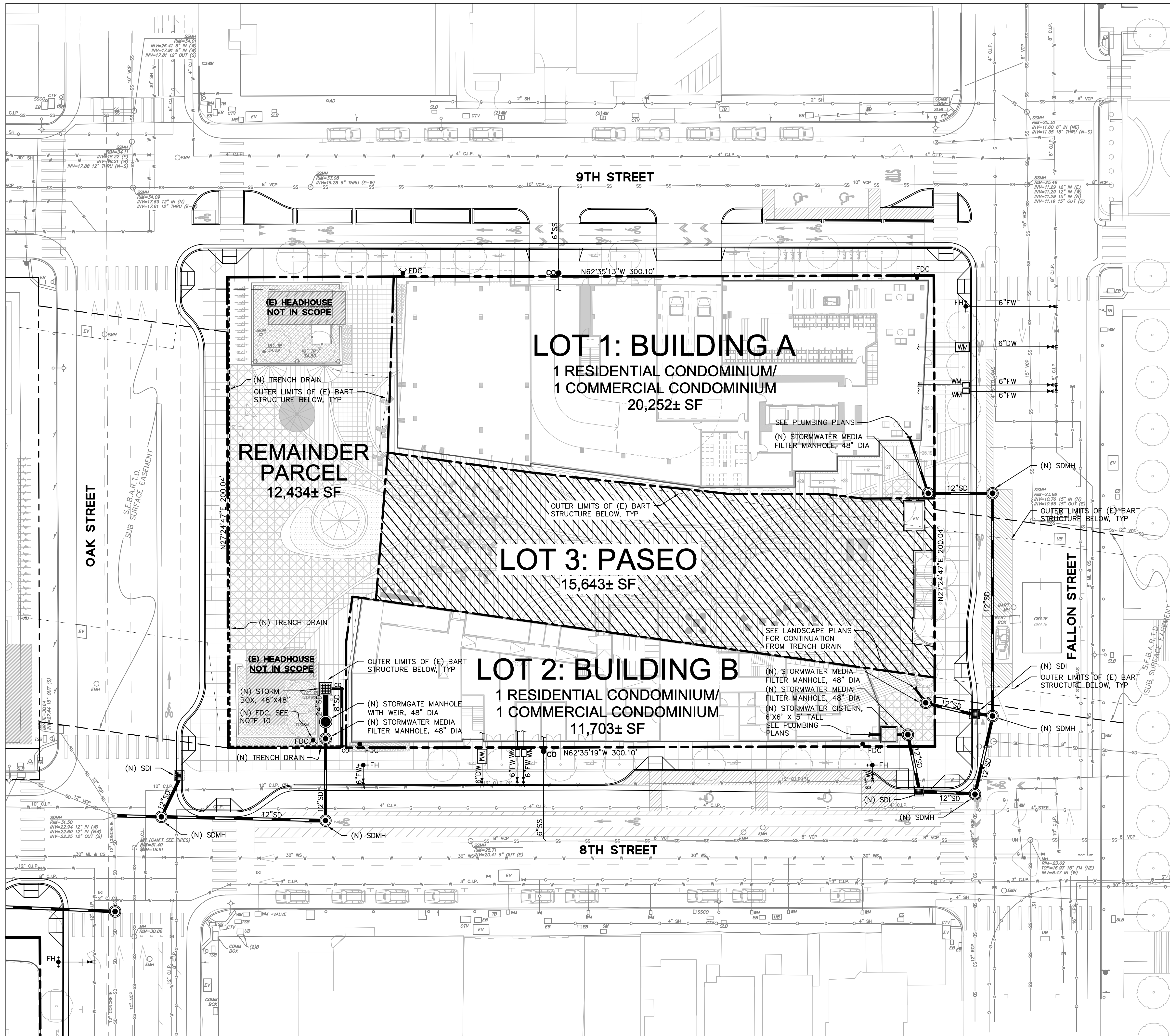
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**PRELIMINARY SECTIONS**

**C5.3**

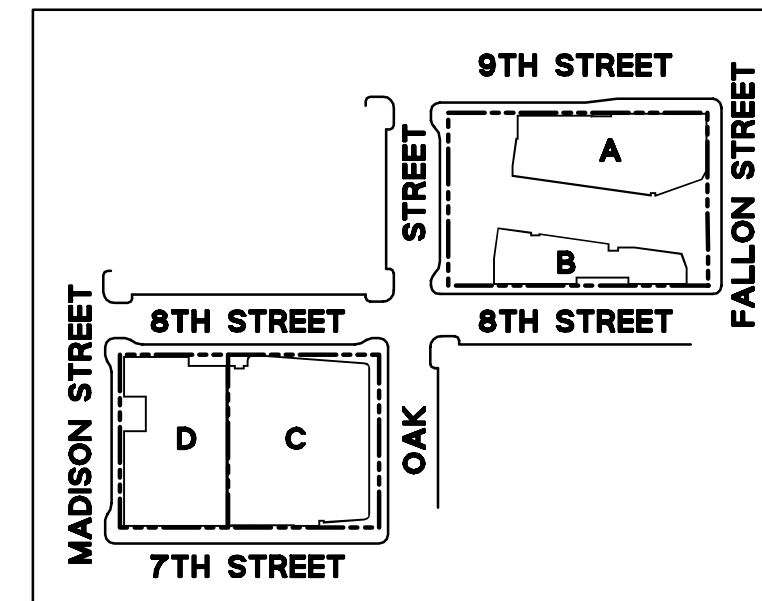
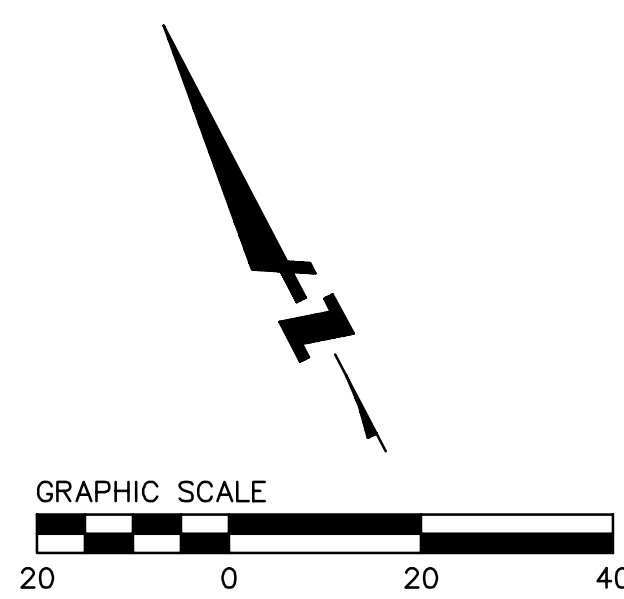


**UTILITY NOTES:**

1. INFORMATION REGARDING EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM RECORD DATA AND IS NOT MEANT TO BE A FULL CATALOG OF EXISTING CONDITIONS. CONTRACTOR SHALL CONDUCT FIELD INVESTIGATIONS, SUCH AS POTHOLES, AS REQUIRED TO VERIFY THE LOCATIONS, ELEVATIONS, AND CONNECTION POINTS OF ALL EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES, WHETHER SHOWN ON THESE PLANS OR NOT, PRIOR TO THE COMMENCEMENT OF WORK AND UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
2. DOMESTIC WATER, FIRE WATER, AND SANITARY SEWER LATERAL SIZES TO BE CONFIRMED DURING THE DETAILED DESIGN PHASE.
3. CONTRACTOR SHALL COORDINATE GRAVITY UTILITY WORK WITH ALL JOINT TRENCH/RULE 20 UNDERGROUNDING WORK. IF JOINT TRENCH SCOPE OF WORK IS TO BE PERFORMED FIRST, CONTRACTOR SHALL STAKE LOCATIONS AND ELEVATIONS OF ALL PROPOSED GRAVITY UTILITY CROSSINGS. JOINT TRENCH TO BE INSTALLED WITH MINIMUM 12" VERTICAL CLEARANCE TO PROPOSED GRAVITY UTILITY AT ALL CROSSINGS.
4. ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
5. ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
6. ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
7. FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POTHOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR CROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
8. DRAINS SHOWN ON CIVIL PLANS ARE NOT INTENDED TO BE THE FINAL NUMBER AND LOCATION OF ALL DRAINS, PLACEMENT AND NUMBER OF LANDSCAPING DRAINS ARE HIGHLY DEPENDENT ON GROUND COVER TYPE AND PLANT MATERIAL. CONTRACTOR SHALL ADD ADDITIONAL AREA DRAINS AS NEEDED AND AS DIRECTED BY THE LANDSCAPE ARCHITECT OR CIVIL ENGINEER.
9. THE LOCATION OF THE 12" WATER LINE IN 8TH STREET HAS BEEN SHOWN BASED ON RECORD INFORMATION PROVIDED BY EAST BAY MUD. EXACT LOCATION TO BE VERIFIED AND CONFIRMED PRIOR TO INSTALLATION OF THE PROPOSED DOMESTIC AND FIRE WATER SERVICES TO BUILDING B.
10. THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.

**LEGEND:**

- BOUNDARY LINE
- LOT LINE
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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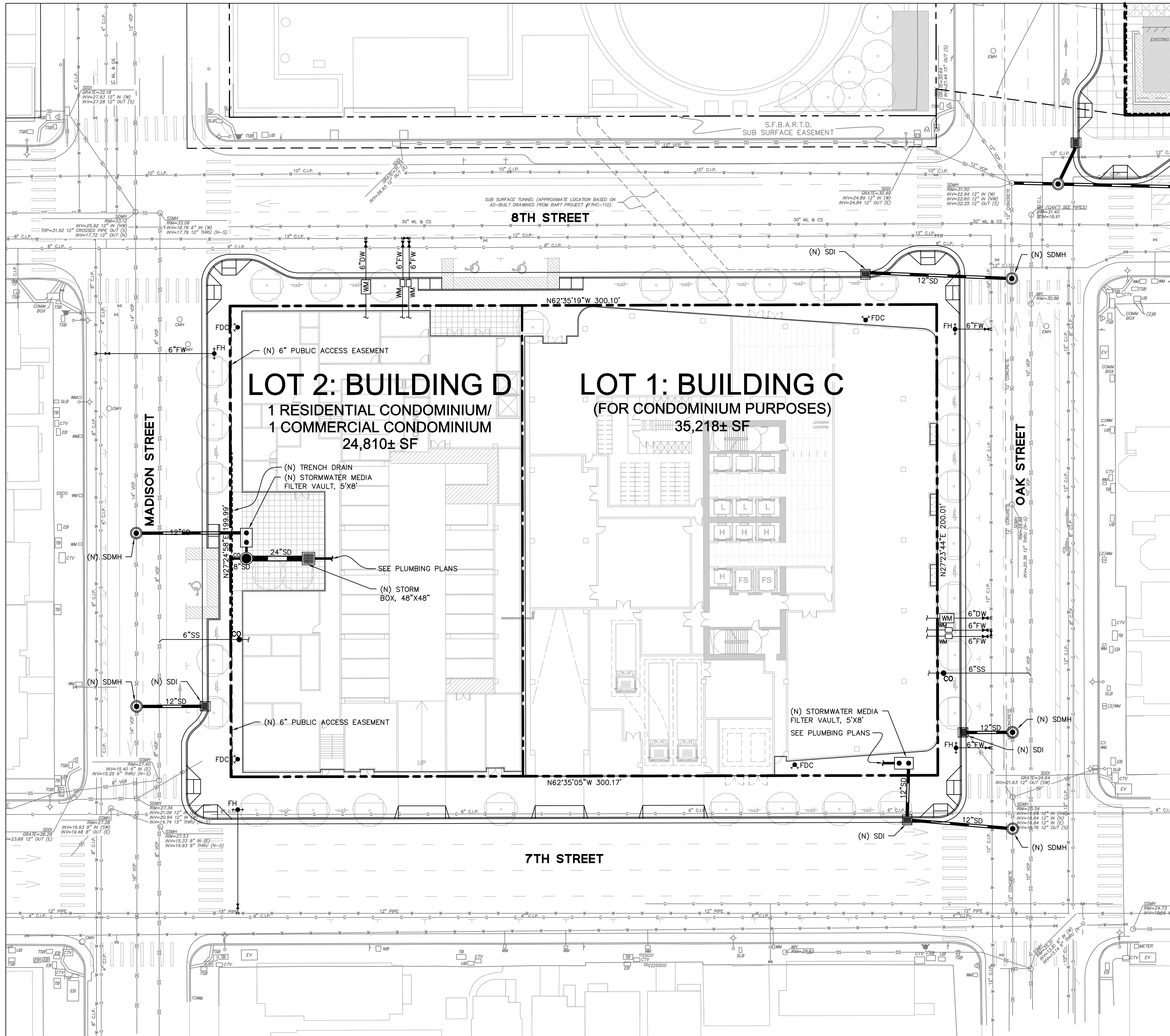
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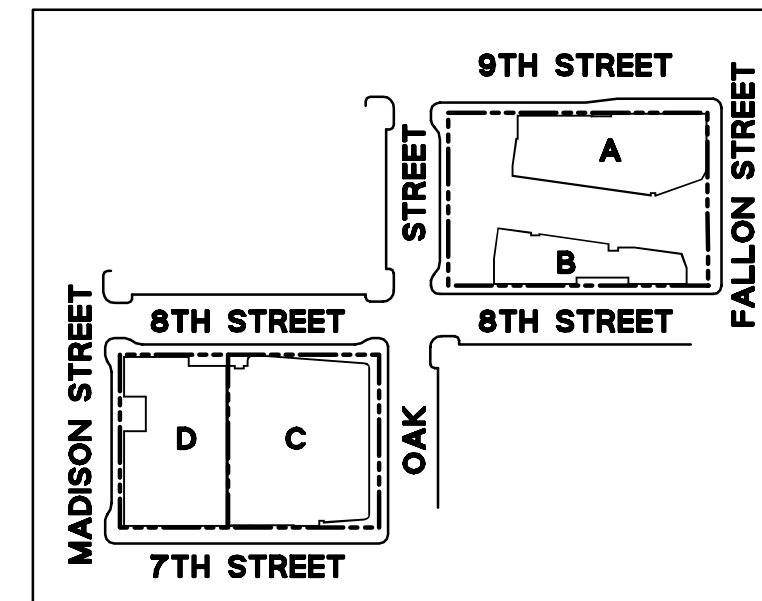
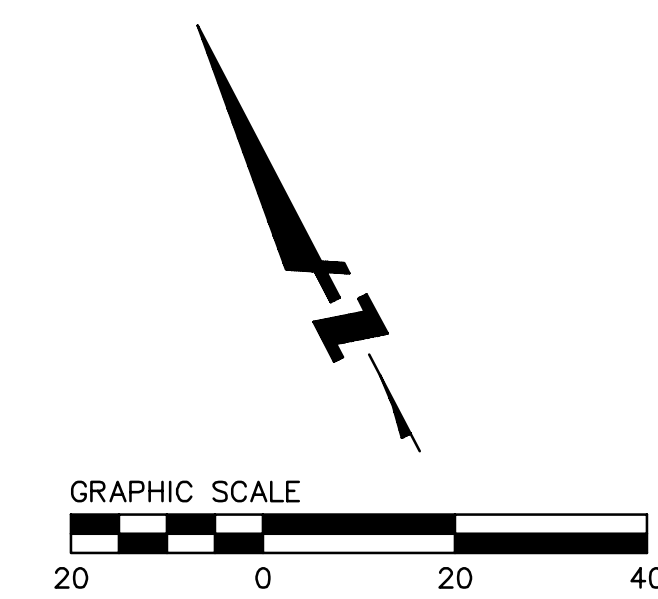
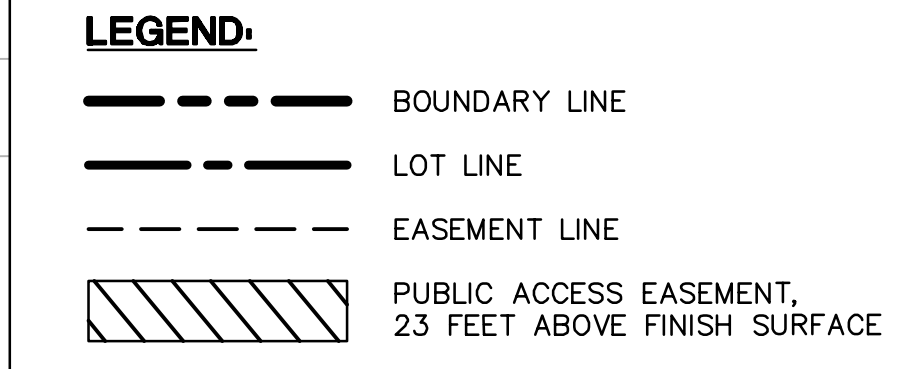
**PRELIMINARY UTILITY PLAN (BLOCK 1)**

**C6.1**





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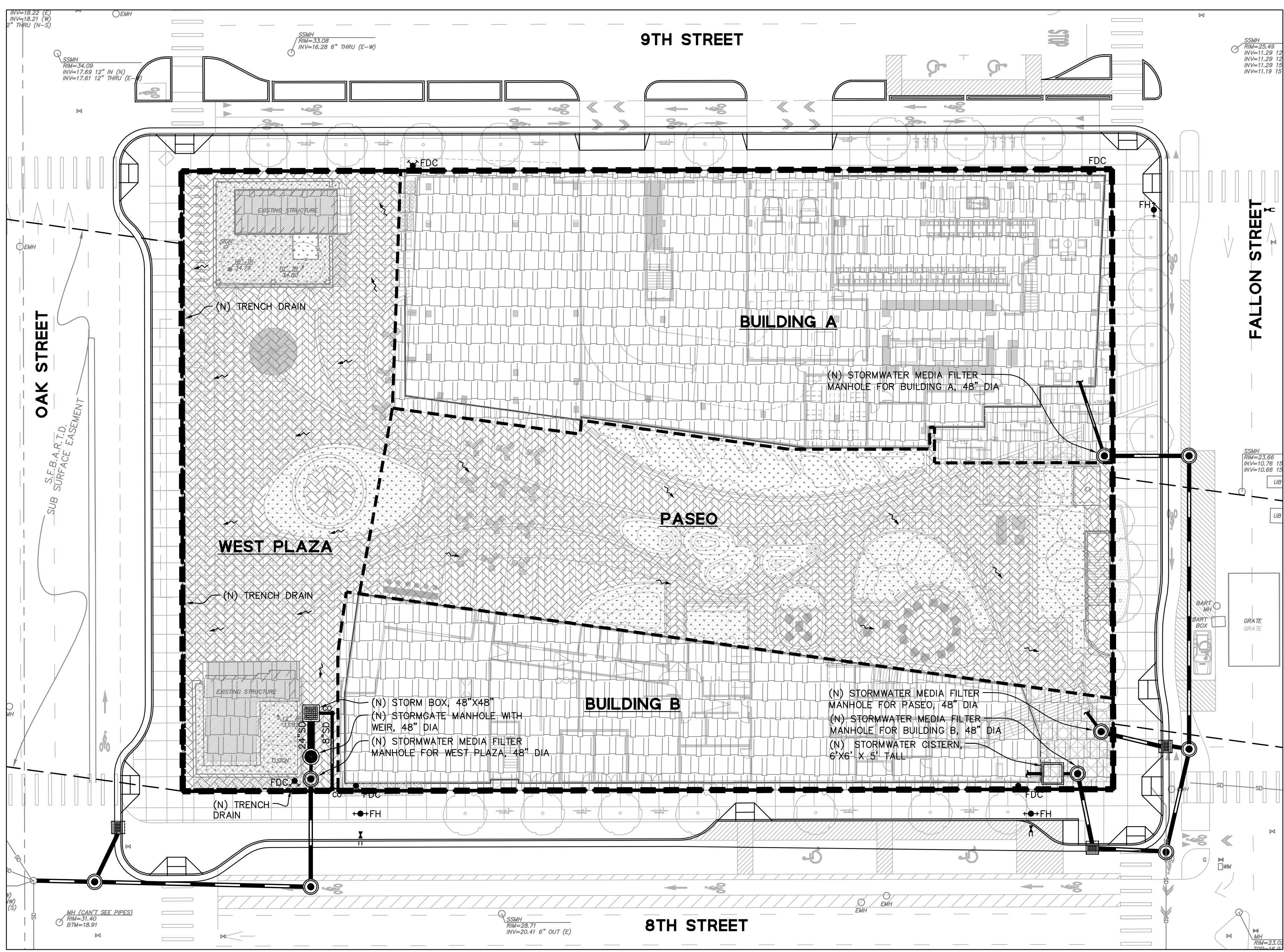
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**PRELIMINARY UTILITY PLAN (BLOCK 2)**

**C6.2**



**STORMWATER COMPLIANCE DATA (BLOCK 1)**

PER THE MUNICIPAL REGIONAL STORMWATER PERMIT ORDER NO. R2-0074, CERTAIN DEVELOPMENT PROJECTS THAT QUALIFY AS "SPECIAL PROJECTS" ARE ELIGIBLE FOR LOW IMPACT DESIGN TREATMENT REDUCTION CREDITS. THE LID TREATMENT REDUCTION CREDIT IS THE MAXIMUM PERCENTAGE OF THE AMOUNT OF RUNOFF THAT MAY BE TREATED WITH EITHER TREE-BOX-TYPE HIGH FLOWRATE BIOFILTERS OR VAULT-BASED HIGH FLOWRATE MEDIA FILTERS. THIS PROJECT IS CLASSIFIED AS A CATEGORY C SPECIAL PROJECT (TRANSIT ORIENTED) AND QUALIFIES FOR A TOTAL LID TREATMENT REDUCTION CREDIT OF 100% AS DESCRIBED BELOW.

CRITERIA FOR CATEGORY C (TRANSIT ORIENTED DEVELOPMENT) SPECIAL PROJECTS TO BE CONSIDERED A CATEGORY C SPECIAL PROJECT, A PROVISION C.3 REGULATED PROJECT MUST MEET ALL OF THE FOLLOWING CRITERIA:

1. BE CHARACTERIZED AS A NON AUTO-RELATED LAND USE PROJECT. THAT IS, CATEGORY C SPECIFICALLY EXCLUDES ANY REGULATED PROJECT THAT IS A STAND-ALONE SURFACE PARKING LOT; CAR DEALERSHIP; AUTO AND TRUCK RENTAL FACILITY WITH ONSITE SURFACE STORAGE; FAST-FOOD RESTAURANT, BANK OR PHARMACY WITH DRIVE-THROUGH LANES; GAS STATION, CAR WASH, AUTO REPAIR AND SERVICE FACILITY; OR OTHER AUTO RELATED PROJECT UNRELATED TO THE CONCEPT OF TRANSIT-ORIENTED DEVELOPMENT.
2. IF A COMMERCIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1.
3. IF A RESIDENTIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST A DENSITY OF 25 DU/AC.
4. IF A MIXED-USE DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1 OR A DENSITY OF 25 DU/AC.

**100% LID TREATMENT REDUCTION CREDIT**

1. 50% REDUCTION CREDIT - PROJECT IS LOCATED WITHIN A 1/4 MILE RADIUS OF A TRANSIT HUB
2. 30% REDUCTION CREDIT - MIXED USE PROJECT WITH DENSITY GREATER THAN 100 DU/ACRE
3. 20% REDUCTION CREDIT - 0% OF TOTAL POST-PROJECT IMPERVIOUS SURFACE IS DEDICATED TO AT-GRADE, SURFACE PARKING

**STORMWATER LID TREATMENT SIZING**

TOTAL LID TREATMENT REDUCTION CREDIT = 100%

TOTAL BLOCK 1 IMPERVIOUS AREA OF 56,508 SF ALLOWED TO BE TREATED W/ NON-LID TREATMENT MEASURES (MEDIA FILTER UNITS)

**BUILDING A**  
Qc3 = CIA = (0.9)\*(0.2 IN/HR)\*(20,334 SF)  
Qc3 = 0.0840 CFS

A 48" MEDIA FILTER MANHOLE WITH (2) 12"X12" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.11 CFS.

**BUILDING B**  
Qc3 = CIA = (0.9)\*(0.2 IN/HR)\*(11,486 SF)  
Qc3 = 0.0475 CFS

A 48" MEDIA FILTER MANHOLE WITH (1) 12"X12" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.05 CFS.

**PASEO**  
Qc3 = CIA = (0.9)\*(0.2 IN/HR)\*(13,117 SF)  
Qc3 = 0.0542 CFS

A 48" MEDIA FILTER MANHOLE WITH (1) 12"X18" STACKED CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.07 CFS.

**WEST PLAZA**  
Qc3 = CIA = (0.9)\*(0.2 IN/HR)\*(11,534 SF)  
Qc3 = 0.0477 CFS

A 48" MEDIA FILTER MANHOLE WITH (2) 12" CARTRIDGES TO BE PROVIDED. TREATMENT FLOW CAPACITY = 0.06 CFS.

**STORMWATER AREA SUMMARY**

THE CITY OF OAKLAND STORM DRAINAGE DESIGN GUIDELINES ESTABLISHES A 25% GOAL FOR PEAK FLOW REDUCTION COMPARED TO EXISTING CONDITIONS, TO THE EXTENT POSSIBLE. DUE TO THE FACT THAT ALMOST 90% OF THE SITE IS COVERED BY PERMANENT STRUCTURE, INCLUDING THE EXISTING BART TUNNEL, THERE IS LIMITED SPACE FOR DETENTION MEASURES ON-SITE. AS A RESULT, A 25% PEAK FLOW REDUCTION WAS ACCOMPLISHED IN ALL AREAS EXCEPT FOR THE PASEO.

**BUILDING A**  
EXISTING IMPERVIOUS SURFACE 17,982 SF  
EXISTING PERVIOUS SURFACE 2,352 SF  
20,334 SF

PROPOSED IMPERVIOUS SURFACE 20,334 SF  
PROPOSED PERVIOUS SURFACE 0 SF  
20,334 SF

**BUILDING B**  
EXISTING IMPERVIOUS SURFACE 9,854 SF  
EXISTING PERVIOUS SURFACE 1,632 SF  
11,486 SF

PROPOSED IMPERVIOUS SURFACE 11,486 SF  
PROPOSED PERVIOUS SURFACE 0 SF  
11,486 SF

**PASEO**  
EXISTING IMPERVIOUS SURFACE 14,331 SF  
EXISTING PERVIOUS SURFACE 998 SF  
15,329 SF

PROPOSED IMPERVIOUS SURFACE 13,117 SF  
PROPOSED PERVIOUS SURFACE 2,212 SF  
15,329 SF

**WEST PLAZA**  
EXISTING IMPERVIOUS SURFACE 9,392 SF  
EXISTING PERVIOUS SURFACE 3,476 SF  
12,868 SF

PROPOSED IMPERVIOUS SURFACE 11,534 SF  
PROPOSED PERVIOUS SURFACE 1,334 SF  
12,868 SF

**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION

**REVISION SCHEDULE**

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

**STORMWATER PEAK FLOW REDUCTION**

PEAK FLOW CALCULATED PER CITY OF OAKLAND STORM DRAINAGE DESIGN STANDARDS.

DESIGN STORM = 10-YEAR  
MEAN ANNUAL PRECIPITATION (MAP) = 21 INCHES  
TIME OF CONCENTRATION = 5 MINUTES

DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:

**BUILDING A**  
Qex-10yr = CIA = (0.90)(3.48 IN/HR)(17,982 SF)  
Qex-10yr = 1.293 CFS  
Qpr-10yr = CIA = (0.90)(3.48 IN/HR)(20,334 SF)  
Qpr-10yr = 1.462 CFS

PERCENT REDUCTION IN PEAK FLOW  
% REDUCTION = 100 - ((1.462/1.293)\*100) = **-13.1%**

**BUILDING B**  
Qex-10yr = CIA = (0.90)(3.48 IN/HR)(9,854 SF)  
Qex-10yr = 0.709 CFS  
Qpr-10yr = CIA = (0.90)(3.48 IN/HR)(11,486 SF)  
Qpr-10yr = 0.826 CFS

PERCENT REDUCTION IN PEAK FLOW  
% REDUCTION = 100 - ((0.826/0.709)\*100) = **-16.5%**

**PASEO**  
Qex-10yr = CIA = (0.90)(3.48 IN/HR)(14,331 SF)  
Qex-10yr = 1.030 CFS  
Qpr-10yr = CIA = (0.90)(3.48 IN/HR)(13,117 SF)  
Qpr-10yr = 0.943 CFS

PERCENT REDUCTION IN PEAK FLOW  
% REDUCTION = 100 - ((0.943/1.030)\*100) = **±8.4%**

THE PASEO IMPROVEMENTS PROVIDE AN 8.4% PEAK FLOW REDUCTION.

**WEST PLAZA**  
Qex-10yr = CIA = (0.90)(3.48 IN/HR)(9,392 SF)  
Qex-10yr = 0.675 CFS  
Qpr-10yr = CIA = (0.90)(3.48 IN/HR)(11,534 SF)  
Qpr-10yr = 0.829 CFS

PERCENT REDUCTION IN PEAK FLOW  
% REDUCTION = 100 - ((0.829/0.675)\*100) = **-5.3%**

DUE TO IMPLEMENTATION OF DETENTION TANK/CISTERN:

**BUILDING A**  
DETENTION VOLUME REQUIRED FOR 25%+13.1% REDUCTION  
Vdet req'd (CF) = (0.25+0.131)\*(Qex-10yr)\*Tc  
Vdet req'd (CF) = (0.381)\*(1.293 CFS)\*(5 MIN)\*(60 SEC/MIN)  
Vdet req'd (CF) = 221.7 CF OR 1,661 GALLONS

DETENTION PIPE PROVIDED BY CISTERN IN BUILDING  
Vdet provided = 7'X7' X 5' TALL CISTERN = 245 CF  
CISTERN TO BE LOCATED INSIDE THE BUILDING.

**BUILDING B**  
DETENTION VOLUME REQUIRED FOR 25%+16.5% REDUCTION  
Vdet req'd (CF) = (0.25+0.165)\*(Qex-10yr)\*Tc  
Vdet req'd (CF) = (0.415)\*(0.709 CFS)\*(5 MIN)\*(60 SEC/MIN)  
Vdet req'd (CF) = 132.4 CF OR 992 GALLONS

DETENTION PIPE PROVIDED BY CISTERN OUTSIDE THE BUILDING  
Vdet provided = 6'X6' X 5' TALL CISTERN = 180 CF  
CISTERN TO BE LOCATED OUTSIDE THE BUILDING.

DUE TO IMPLEMENTATION OF DETENTION PIPE:

**WEST PLAZA**  
DETENTION VOLUME REQUIRED FOR 27.6%+5.3% REDUCTION  
Vdet req'd (CF) = (0.276+0.053)\*(Qex-10yr)\*Tc  
Vdet req'd (CF) = (0.329)\*(0.675 CFS)\*(5 MIN)\*(60 SEC/MIN)  
Vdet req'd (CF) = 100.0 CF OR 751 GALLONS

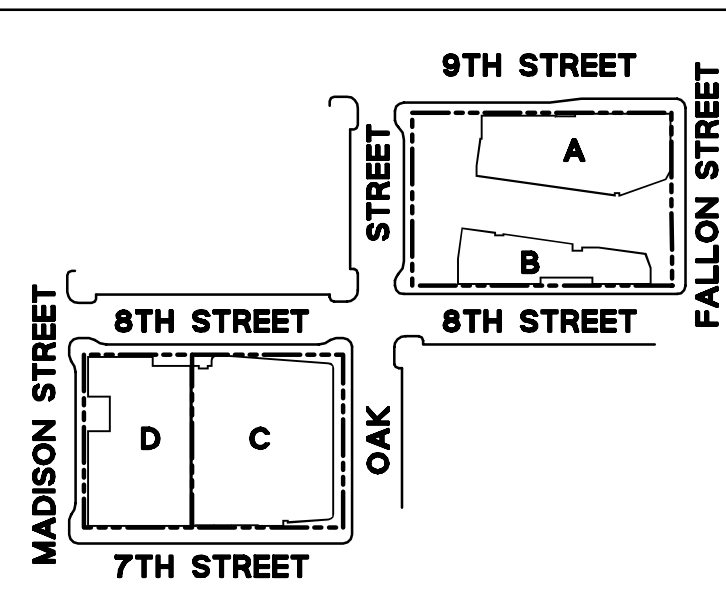
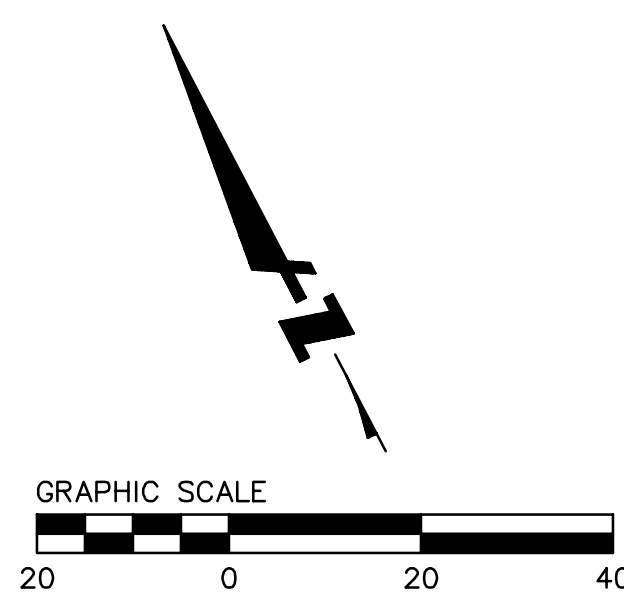
DETENTION PROVIDED BY 24" PIPE  
Vdet provided = π\*R<sup>2</sup>\*LENGTH = π\*(1.00FT)<sup>2</sup>\*8.7FT = 27 CF

DETENTION PROVIDED BY 8" PIPE  
Vdet provided = π\*R<sup>2</sup>\*LENGTH = π\*(0.33FT)<sup>2</sup>\*17FT = 9 CF

DETENTION PROVIDED BY 48" STORM BOX BASIN  
Vdet provided = LENGTH\*WIDTH\*HEIGHT = 4.0FT\*4.0FT\*4.0FT = 64 CF  
Vdet provided TOTAL = 100 CF  
DETENTION PIPE TO BE LOCATED ADJACENT TO BUILDING B, IN THE WEST PLAZA.

**LEGEND**

- STORMWATER FILTER UNIT
- FLOW DIRECTION
- [Hatched Box] IMPERVIOUS ROOF AREAS
- [Dotted Box] IMPERVIOUS PLAZA/PODIUM AREAS
- [Stippled Box] PERVIOUS LANDSCAPE AREAS
- - - DRAINAGE AREA BOUNDARY



JOB NUMBER: 20190110  
DATE: 04/26/2021  
SCALE: 1" = 20'

**PRELIMINARY STORMWATER PLAN (BLOCK 1)**

**C7.1**

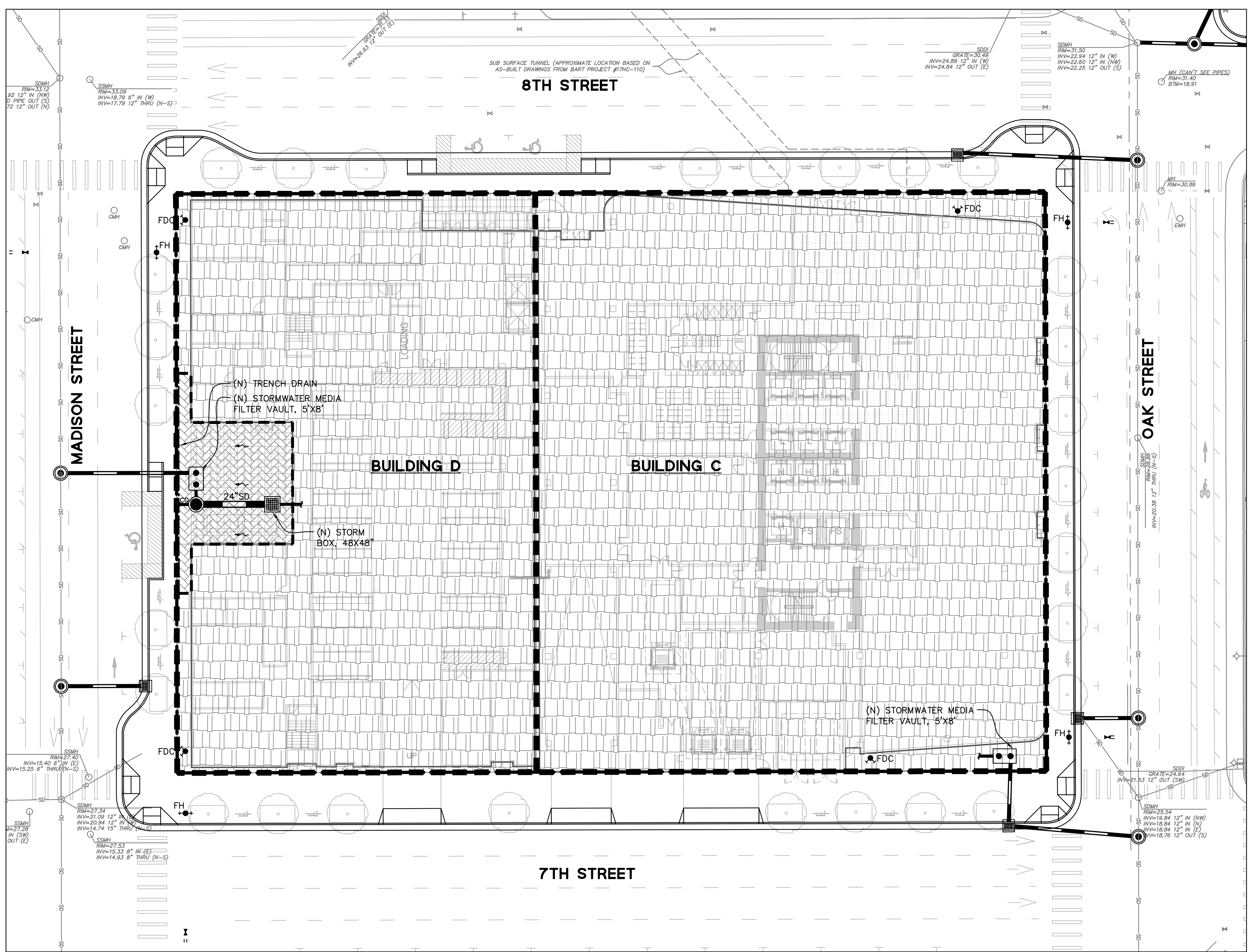
# LAKE MERRITT BART DEVELOPMENT

Oakland, CA 94607

## PRELIMINARY DEVELOPMENT PLAN PACKAGE

- PRELIMINARY - NOT FOR CONSTRUCTION

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021



### STORMWATER COMPLIANCE DATA (BLOCK 2)

PER THE MUNICIPAL REGIONAL STORMWATER PERMIT ORDER NO. R2-0074, CERTAIN DEVELOPMENT PROJECTS THAT QUALIFY AS "SPECIAL PROJECTS" ARE ELIGIBLE FOR LOW IMPACT DESIGN TREATMENT REDUCTION CREDITS. THE LID TREATMENT REDUCTION CREDIT IS THE MAXIMUM PERCENTAGE OF THE AMOUNT OF RUNOFF THAT MAY BE TREATED WITH EITHER TREE-BOX-TYPE HIGH FLOWRATE BIOFILTERS OR VAULT-BASED HIGH FLOWRATE MEDIA FILTERS. THIS PROJECT IS CLASSIFIED AS A CATEGORY C SPECIAL PROJECT (TRANSIT ORIENTED) AND QUALIFIES FOR A TOTAL LID TREATMENT REDUCTION CREDIT OF 100% AS DESCRIBED BELOW.

CRITERIA FOR CATEGORY C (TRANSIT ORIENTED DEVELOPMENT) SPECIAL PROJECTS TO BE CONSIDERED A CATEGORY C SPECIAL PROJECT, A PROVISION C.3 REGULATED PROJECT MUST MEET ALL OF THE FOLLOWING CRITERIA:

- BE CHARACTERIZED AS A NON AUTO-RELATED LAND USE PROJECT. THAT IS, CATEGORY C SPECIFICALLY EXCLUDES ANY REGULATED PROJECT THAT IS A STAND-ALONE SURFACE PARKING LOT; CAR DEALERSHIP; AUTO AND TRUCK RENTAL FACILITY WITH ONSITE SURFACE STORAGE; FAST-FOOD RESTAURANT, BANK OR PHARMACY WITH DRIVE-THROUGH LANES; GAS STATION, CAR WASH, AUTO REPAIR AND SERVICE FACILITY; OR OTHER AUTO RELATED PROJECT UNRELATED TO THE CONCEPT OF TRANSIT-ORIENTED DEVELOPMENT.
- IF A COMMERCIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1.
- IF A RESIDENTIAL DEVELOPMENT PROJECT, ACHIEVE AT LEAST A DENSITY OF 25 DU/AC.
- IF A MIXED-USE DEVELOPMENT PROJECT, ACHIEVE AT LEAST AN FAR OF 2:1 OR A DENSITY OF 25 DU/AC.

#### 100% LID TREATMENT REDUCTION CREDIT

- 50% REDUCTION CREDIT - PROJECT IS LOCATED WITHIN A 1/4 MILE RADIUS OF A TRANSIT HUB
- 30% REDUCTION CREDIT - MIXED USE PROJECT WITH DENSITY GREATER THAN 100 DU/ACRE
- 20% REDUCTION CREDIT - 0% OF TOTAL POST-PROJECT IMPERVIOUS SURFACE IS DEDICATED TO AT-GRADE, SURFACE PARKING

#### LEGEND

- ○ STORMWATER FILTER UNIT
- FLOW DIRECTION
- ▨ IMPERVIOUS ROOF AREAS
- ▩ IMPERVIOUS PLAZA/PODIUM AREAS
- ▤ PERVIOUS LANDSCAPE AREAS
- - - DRAINAGE AREA BOUNDARY

#### STORMWATER LID TREATMENT SIZING

TOTAL LID TREATMENT REDUCTION CREDIT = 100%

TOTAL BLOCK 2 IMPERVIOUS AREA OF 60,032 SF ALLOWED TO BE TREATED W/ NON-LID TREATMENT MEASURES (MEDIA FILTER UNITS)

**BUILDING C**  
 $Q_{c,3} = CIA = (0.9) \cdot (0.2 \text{ IN/HR}) \cdot (35,216 \text{ SF})$   
 $Q_{c,3} = 0.1455 \text{ CFS}$

A 5'x8' MEDIA FILTER VAULT WITH (3) 12"X12" STACKED CARTRIDGES TO BE PROVIDED WITHIN BUILDING. TREATMENT FLOW CAPACITY = 0.16 CFS.

**BUILDING D**  
 $Q_{c,3} = CIA = (0.9) \cdot (0.2 \text{ IN/HR}) \cdot (24,816 \text{ SF})$   
 $Q_{c,3} = 0.1025 \text{ CFS}$

A 5'x8' MEDIA FILTER VAULT WITH (3) 18" CARTRIDGES TO BE PROVIDED WITHIN BUILDING. TREATMENT FLOW CAPACITY = 0.12 CFS.

#### STORMWATER AREA SUMMARY

THE CITY OF OAKLAND STORM DRAINAGE DESIGN GUIDELINES ESTABLISHES A 25% GOAL FOR PEAK FLOW REDUCTION COMPARED TO EXISTING CONDITIONS, TO THE EXTENT POSSIBLE. DUE TO THE FACT THAT ALMOST 90% OF THE SITE IS COVERED BY PERMANENT STRUCTURE, THERE IS LIMITED SPACE FOR DETENTION MEASURES ONSITE. AS A RESULT, A 25% PEAK FLOW REDUCTION WAS ACCOMPLISHED AT BUILDING C, AND 16% FOR BUILDING D.

EXISTING IMPERVIOUS SURFACE	PROPOSED IMPERVIOUS SURFACE	PROPOSED PERVIOUS SURFACE
35,216 SF	0 SF	35,216 SF
24,816 SF	0 SF	24,816 SF
24,816 SF	0 SF	24,816 SF

#### STORMWATER PEAK FLOW REDUCTION

PEAK FLOW CALCULATED PER CITY OF OAKLAND STORM DRAINAGE DESIGN STANDARDS.

DESIGN STORM = 10-YEAR MEAN ANNUAL PRECIPITATION (MAP) = 21 INCHES  
TIME OF CONCENTRATION = 5 MINUTES

**BUILDING C**  
 $Q_{EX-10YR} = CIA = (0.90) \cdot (3.48 \text{ IN/HR}) \cdot (35,216 \text{ SF})$   
 $Q_{EX-10YR} = 2,532 \text{ CFS}$

$Q_{PR-10YR} = CIA = (0.90) \cdot (3.48 \text{ IN/HR}) \cdot (35,216 \text{ SF})$   
 $Q_{PR-10YR} = 2,532 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((2,532 / 2,532) \cdot 100) = 0.0\%$

#### DUE TO CHANGE IN PROPOSED VS EXISTING AREAS:

**BUILDING D**  
 $Q_{EX-10YR} = CIA = (0.90) \cdot (3.48 \text{ IN/HR}) \cdot (24,816 \text{ SF})$   
 $Q_{EX-10YR} = 1,784 \text{ CFS}$

$Q_{PR-10YR} = CIA = (0.90) \cdot (3.48 \text{ IN/HR}) \cdot (24,816 \text{ SF})$   
 $Q_{PR-10YR} = 1,784 \text{ CFS}$

PERCENT REDUCTION IN PEAK FLOW  
 $\% \text{ REDUCTION} = 100 - ((1,784 / 1,784) \cdot 100) = 0.0\%$

#### DUE TO IMPLEMENTATION OF DETENTION TANK/CISTERN:

**BUILDING C**  
 DETENTION VOLUME REQUIRED FOR 25% REDUCTION  
 $V_{DET REQ'D} (CF) = (0.25) \cdot (Q_{EX-10YR}) \cdot T_c$   
 $V_{DET REQ'D} (CF) = (0.25) \cdot (2,532 \text{ CFS}) \cdot (5 \text{ MIN}) \cdot (60 \text{ SEC/MIN})$   
 $V_{DET REQ'D} (CF) = 284.9 \text{ CF OR } 2,134 \text{ GALLONS}$

DETENTION PIPE PROVIDED BY CISTERN IN BUILDING  
 $V_{DET PROVIDED} = 7' \times 7' \times 6' \text{ TALL CISTERN} = 294 \text{ CF}$

CISTERN TO BE LOCATED INSIDE THE BUILDING.

#### DUE TO IMPLEMENTATION OF DETENTION PIPE:

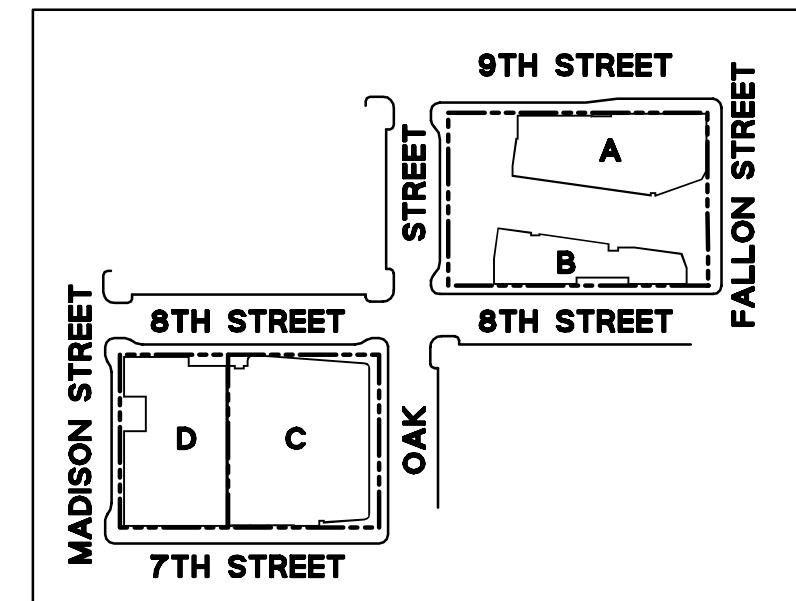
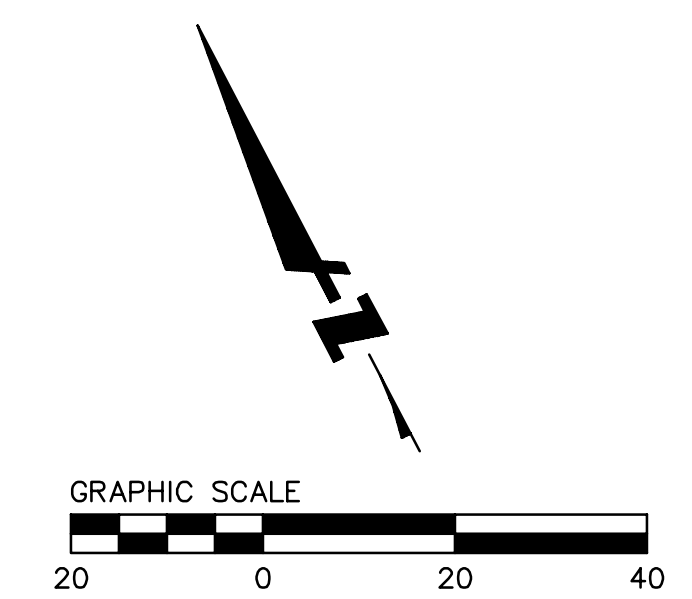
**BUILDING D**  
 DETENTION VOLUME REQUIRED FOR 16% REDUCTION  
 $V_{DET REQ'D} (CF) = (0.16) \cdot (Q_{EX-10YR}) \cdot T_c$   
 $V_{DET REQ'D} (CF) = (0.16) \cdot (1,784 \text{ CFS}) \cdot (5 \text{ MIN}) \cdot (60 \text{ SEC/MIN})$   
 $V_{DET REQ'D} (CF) = 128.0 \text{ CF OR } 962 \text{ GALLONS}$

DETENTION PROVIDED BY 24" PIPE  
 $V_{DET PROVIDED} = \pi \cdot R^2 \cdot LENGTH = \pi \cdot (1.00FT)^2 \cdot 21.0FT = 66 \text{ CF}$

DETENTION PROVIDED BY 48" STORM BOX BASIN  
 $V_{DET PROVIDED} = LENGTH \cdot WIDTH \cdot HEIGHT = 4.0FT \cdot 4.0FT \cdot 4.0FT = 64 \text{ CF}$

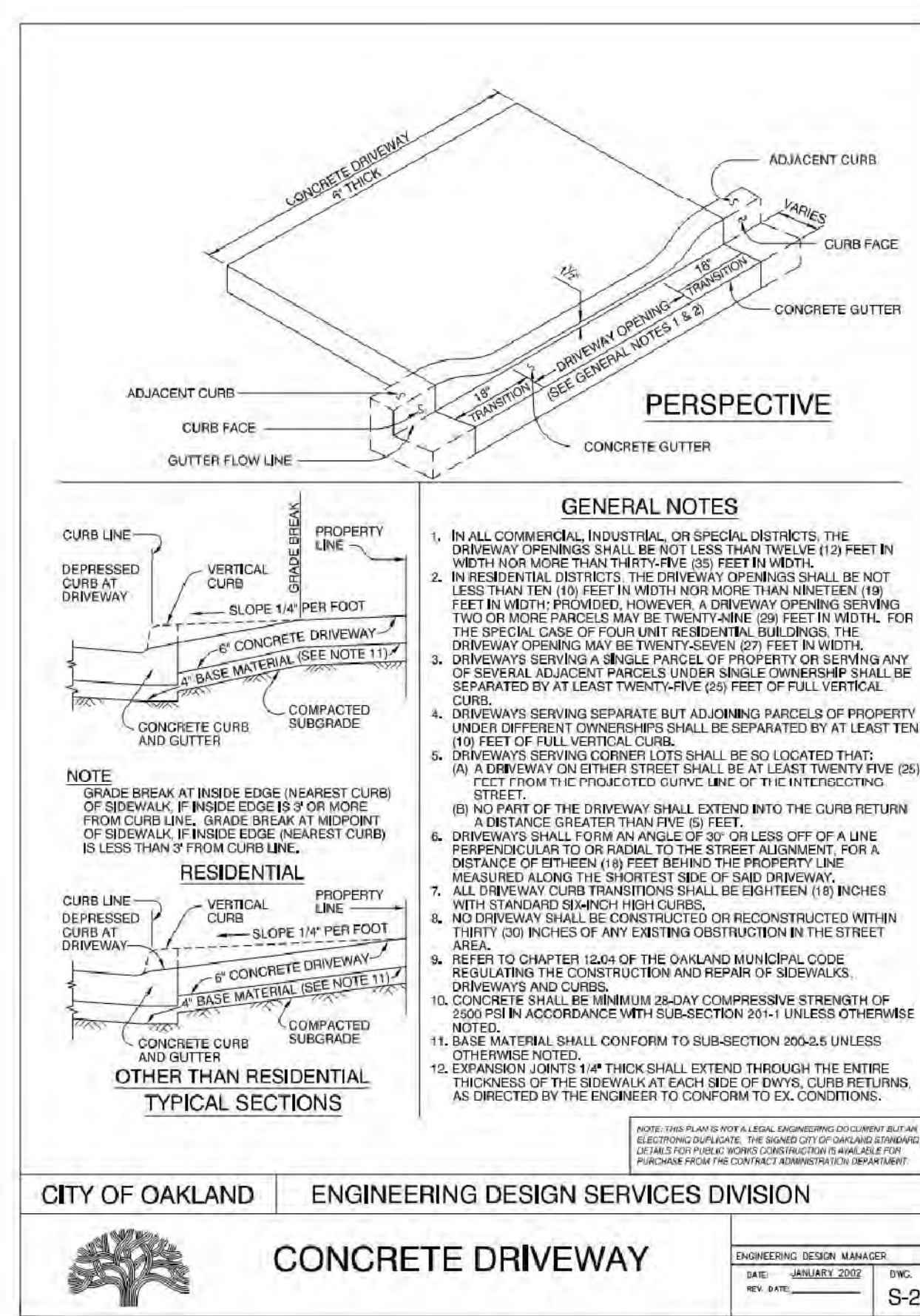
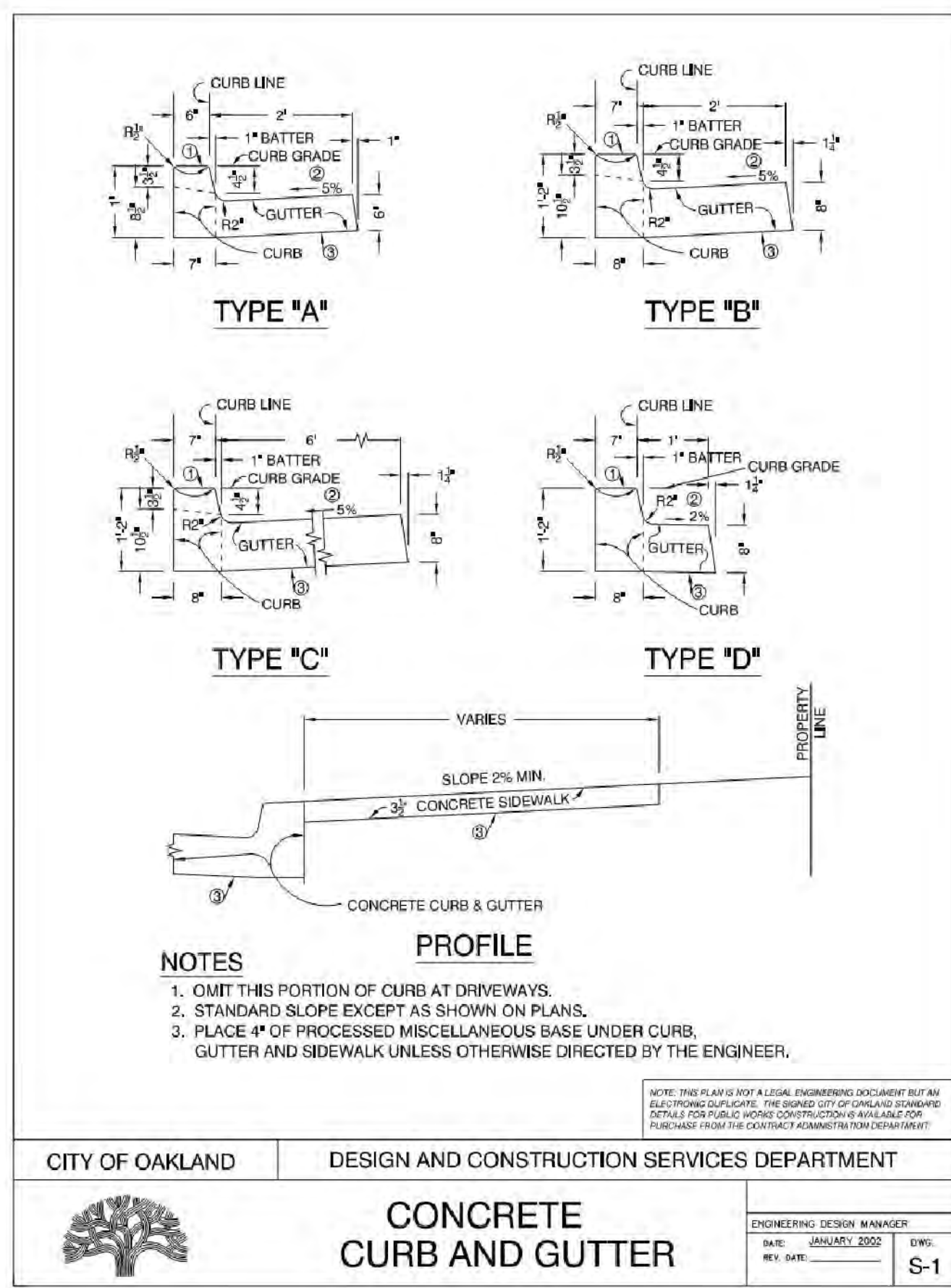
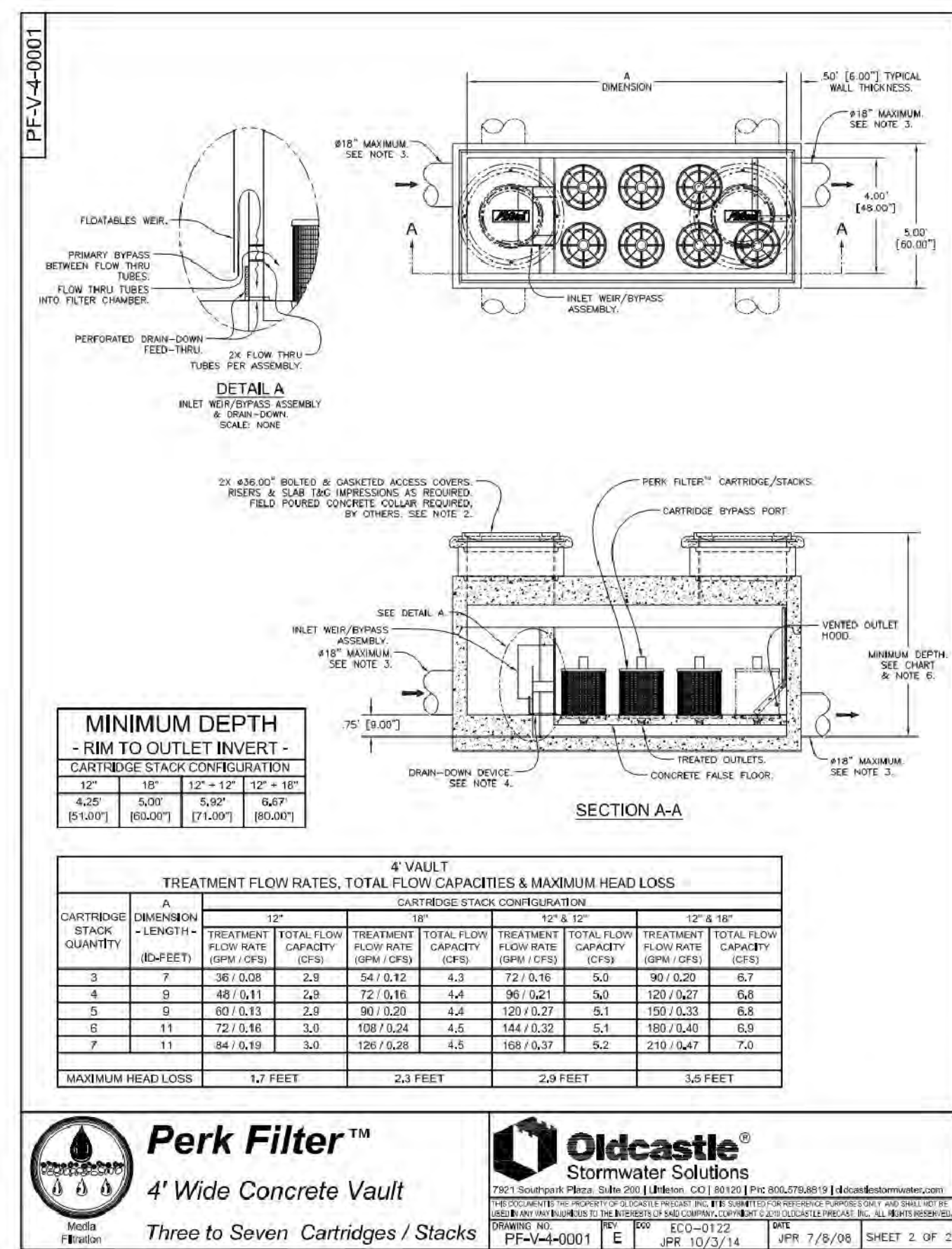
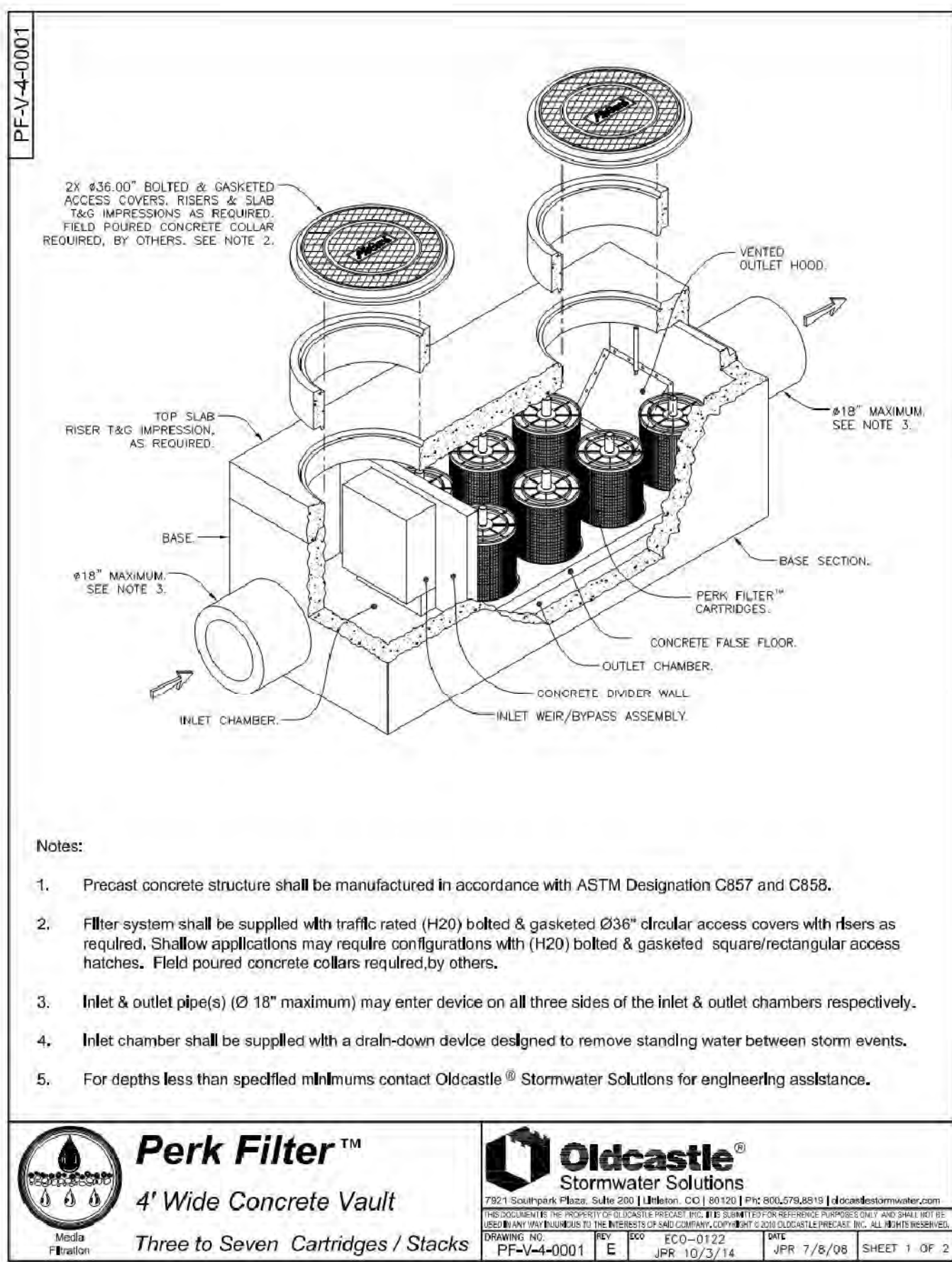
$V_{DET PROVIDED TOTAL} = 130 \text{ CF}$

DETENTION PIPE TO BE LOCATED ADJACENT TO THE BUILDING.



JOB NUMBER: 20190110  
DATE: 04/26/2021  
SCALE: 1" = 20'

# PRELIMINARY STORMWATER PLAN (BLOCK 2)

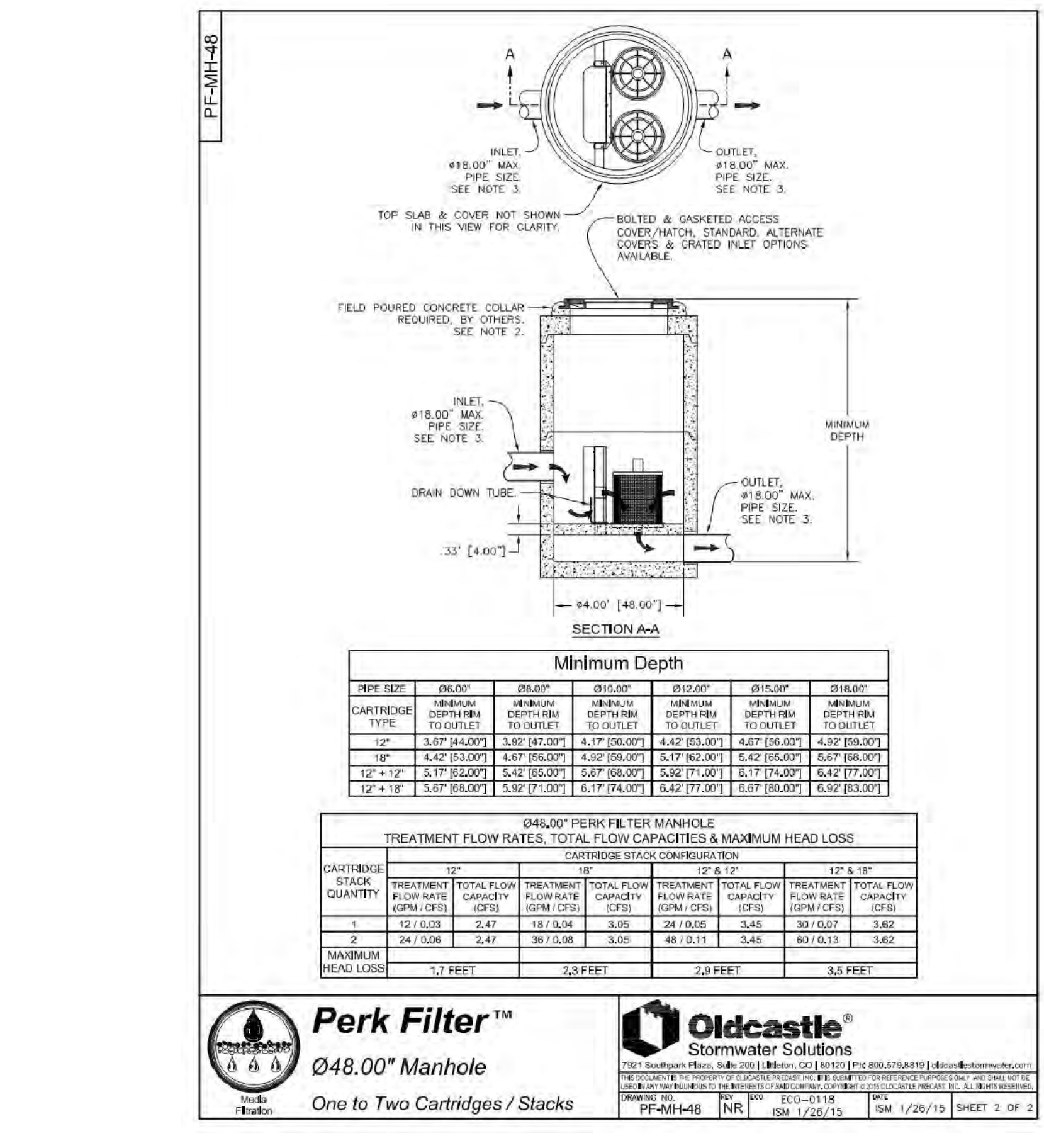
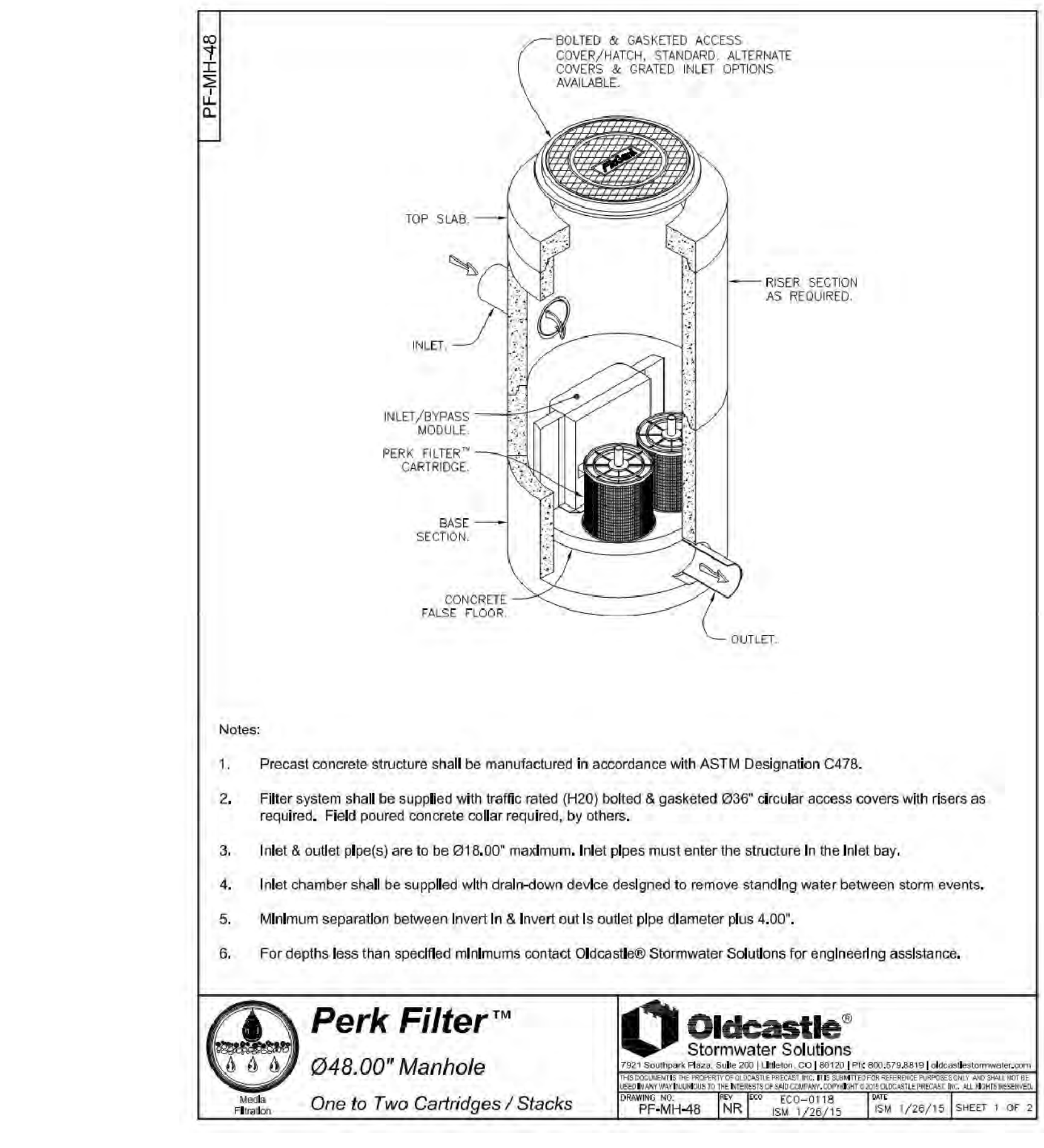
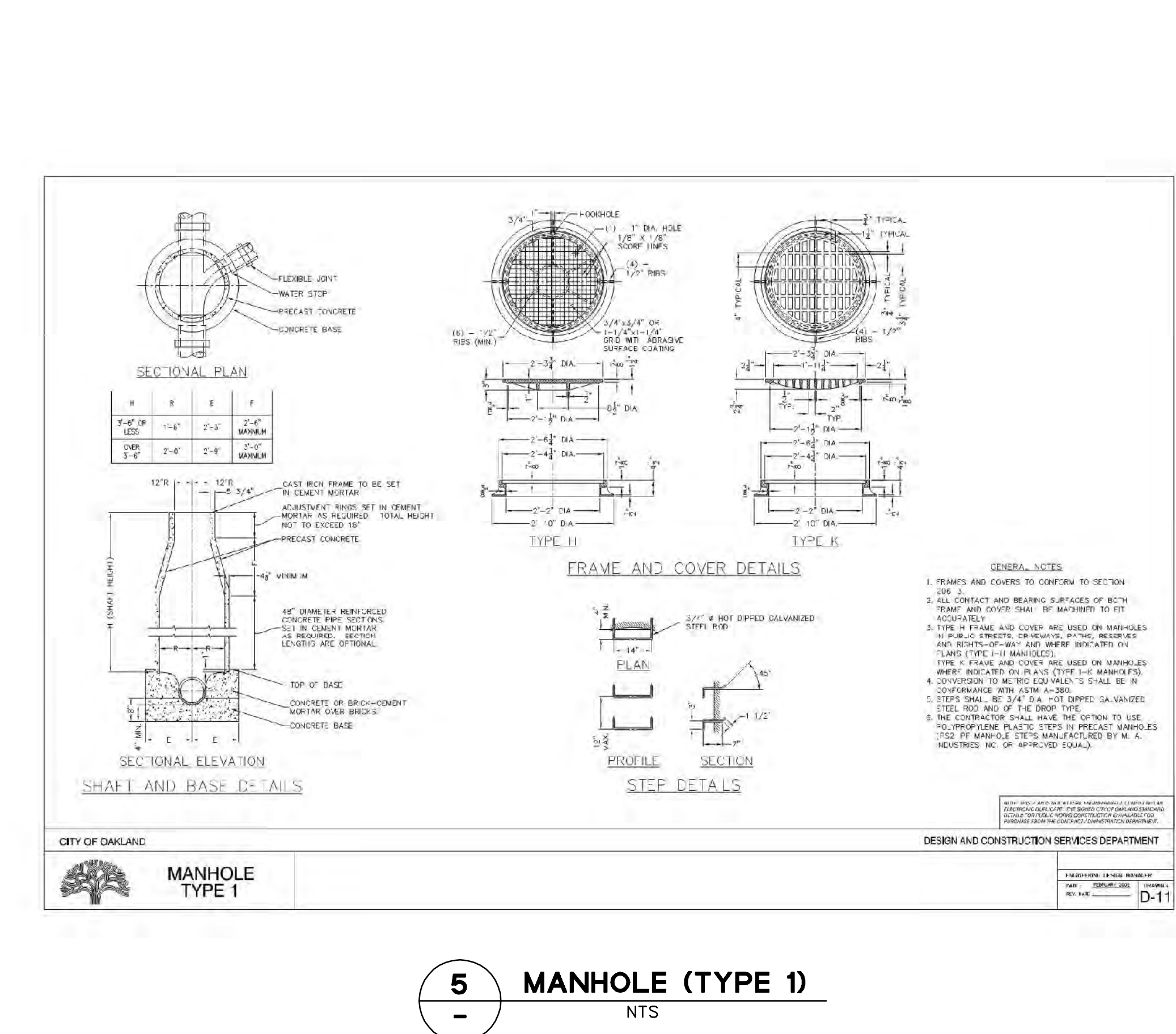


1 STORMWATER MEDIA FILTER VAULT (1 OF 2) NTS

2 STORMWATER MEDIA FILTER VAULT (2 OF 2) NTS

3 CONCRETE CURB & GUTTER NTS

4 CONCRETE DRIVEWAY NTS



**LAKE MERRITT BART DEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	01/20/2020
3	PDP SUBMITTAL #1 TO CITY	02/12/2020
4	REVISED RESPONSE TO PDP #1 COMMENTS	06/08/2020
5	REVISED RESPONSE TO PDP #2 COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDP #3 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP #4 COMMENTS	03/19/2021

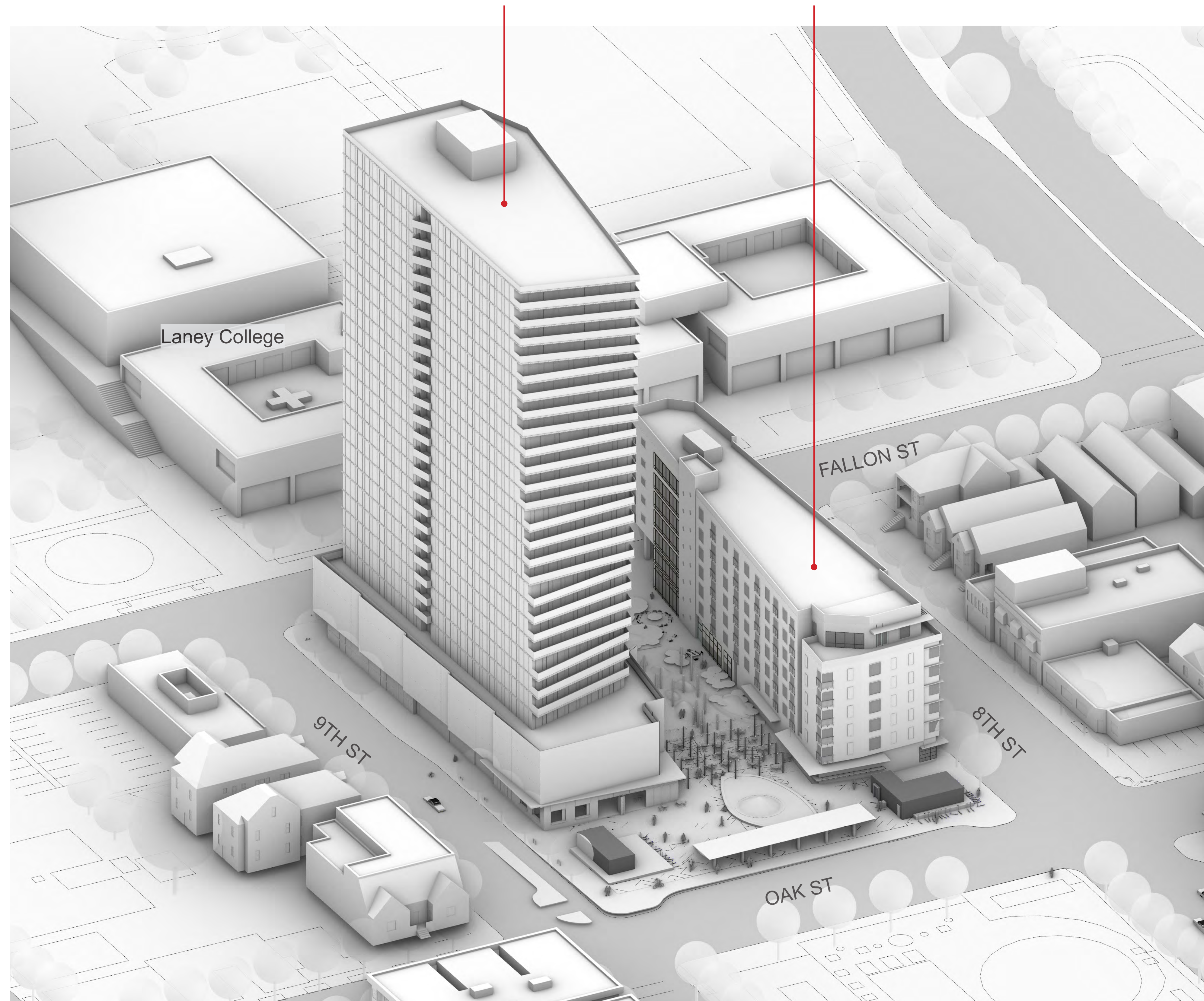
JOB NUMBER: 2019101  
DATE: 04/26/2021  
SCALE: AS NOTED

**PRELIMINARY DETAILS**

**C8.1**

**BUILDING A:**  
MARKET RATE  
RESIDENTIAL

**BUILDING B:**  
SENIOR HOUSING

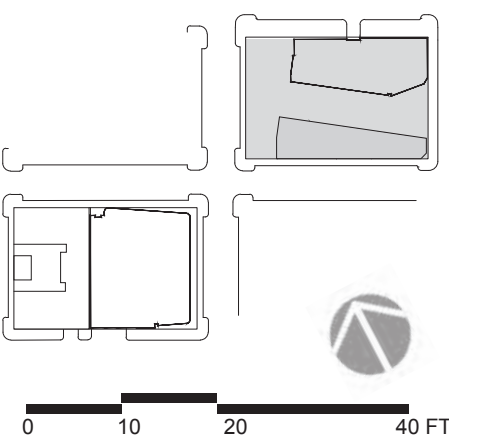


**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

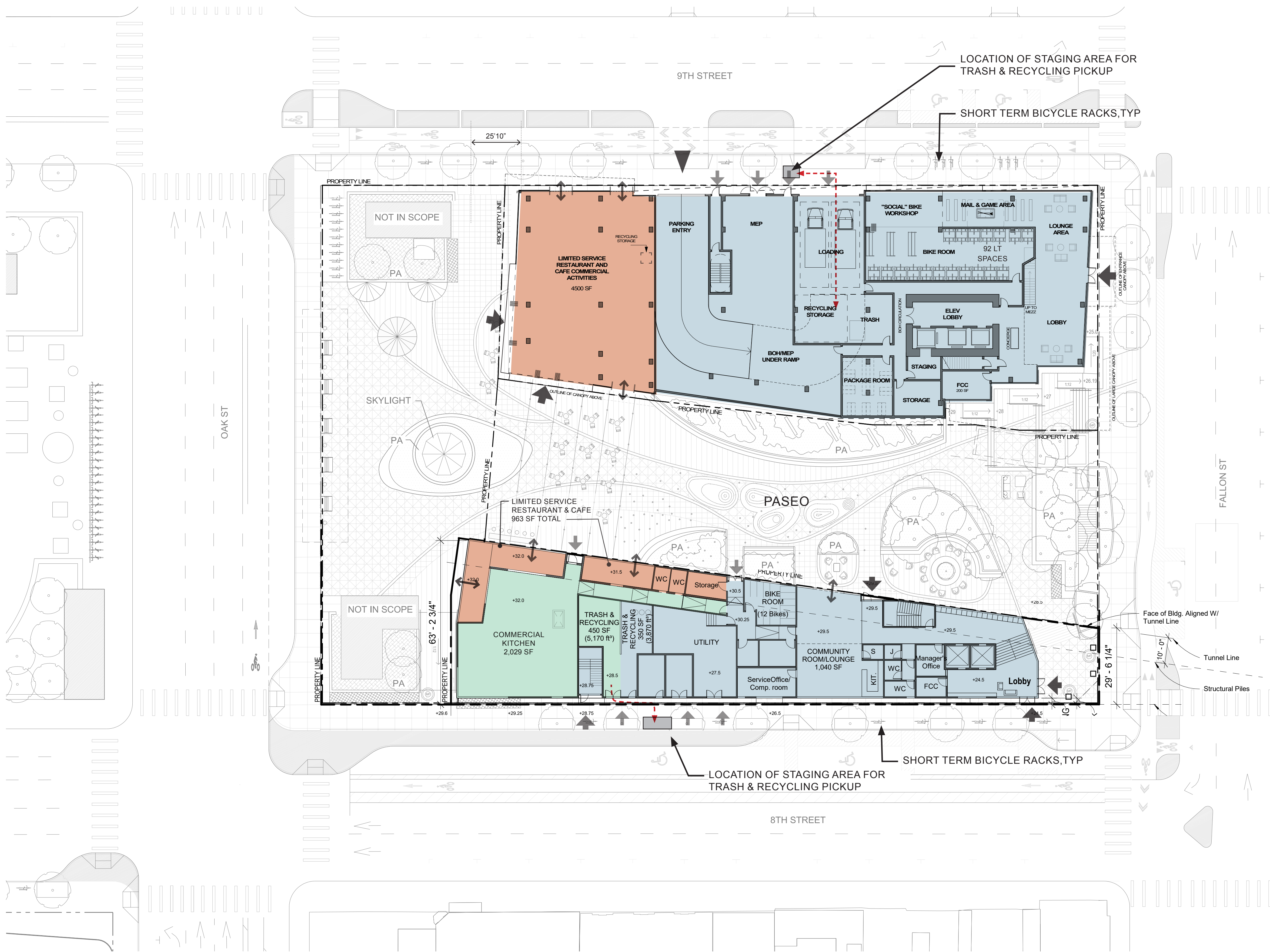
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPH COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPH COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPH COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPH COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1  
MASSING VIEWS**

**A2.10**



**KEY LEGEND**

- PRIMARY PEDESTRIAN ACCESS
- UTILITY/SERVICES OR EMERGENCY ACCESS
- PARKING ENTRANCE
- OPENINGS
- PATH OF TRAVEL

**LAND USE LEGEND**

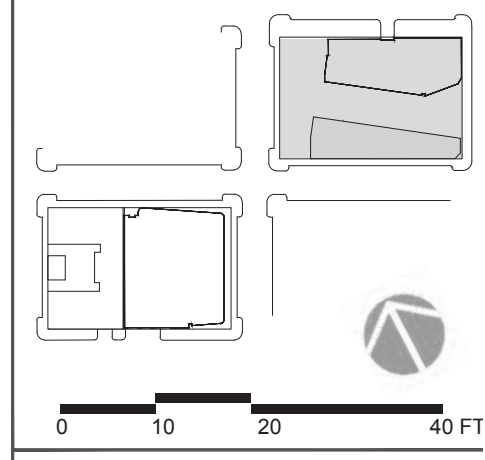
- PERMANENT RESIDENTIAL ACTIVITIES  
17.101G.01
- LIMITED-SERVICE RESTAURANT AND CAFE (COMMERCIAL ACTIVITIES)  
17.101G.01
- COMMERCIAL KITCHEN (CUSTOM MANUFACTURING)  
17.142.100.B

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

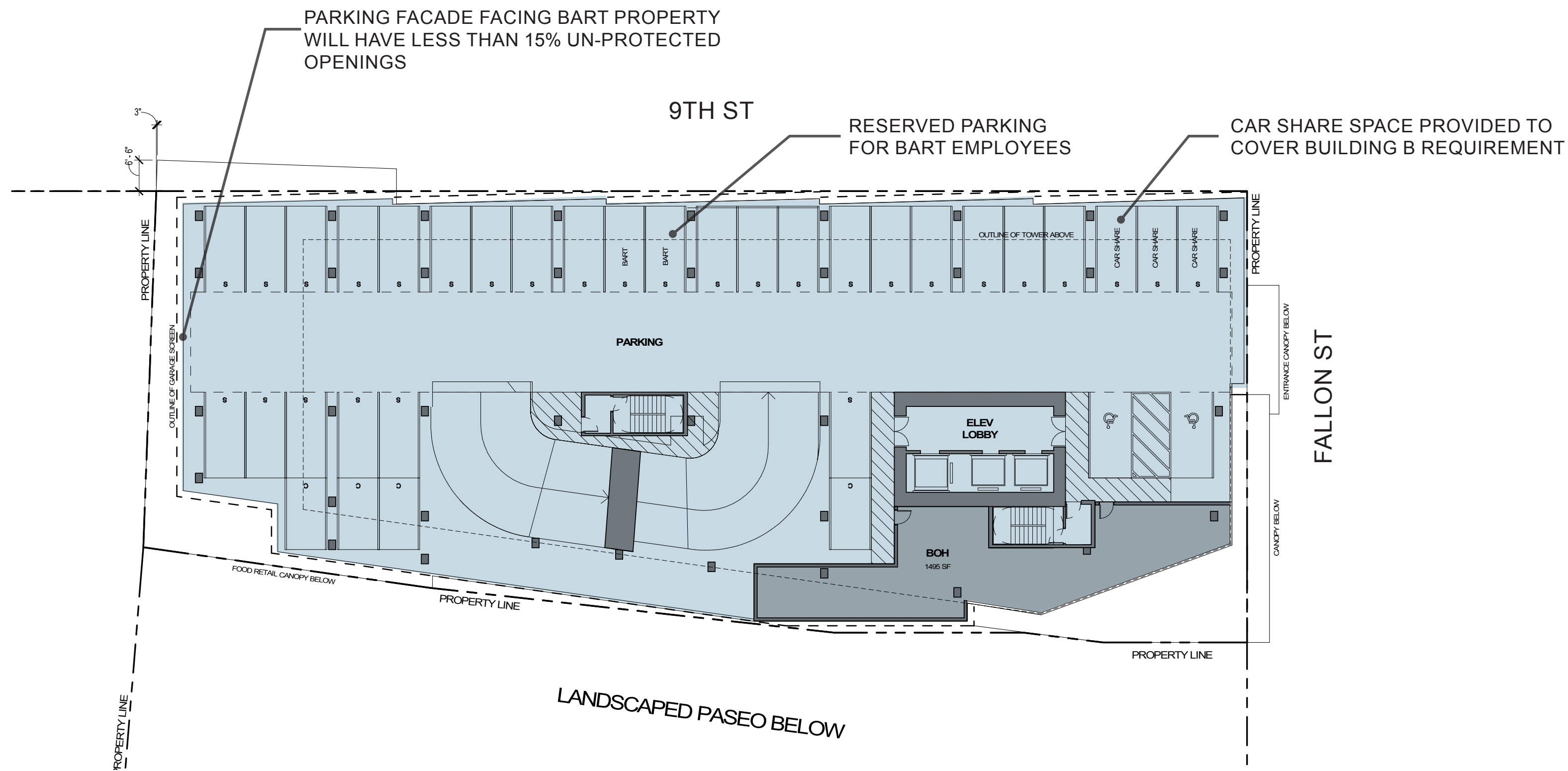
PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPA COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPA COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPA COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA COMMENTS	03/19/2021

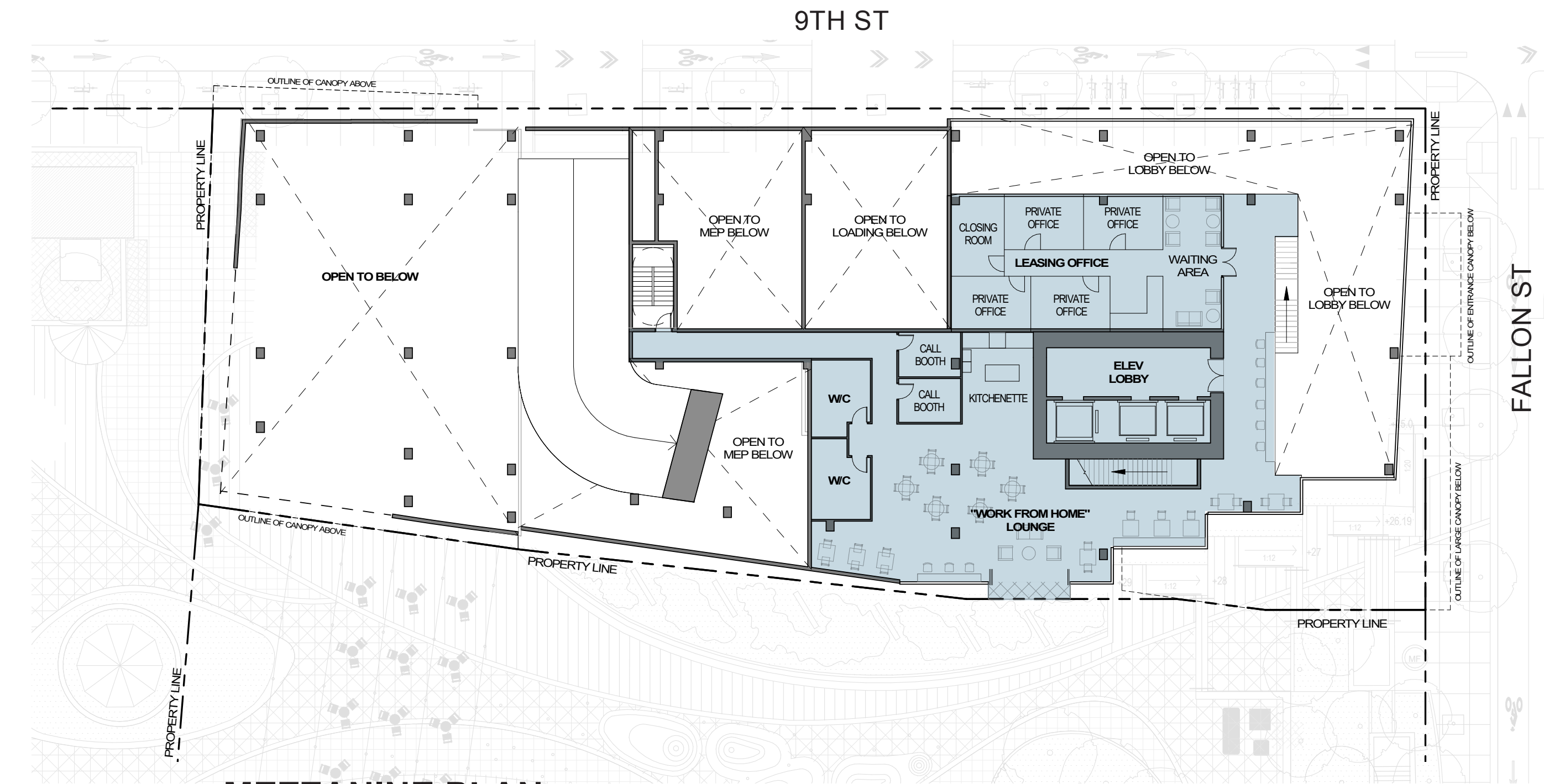


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

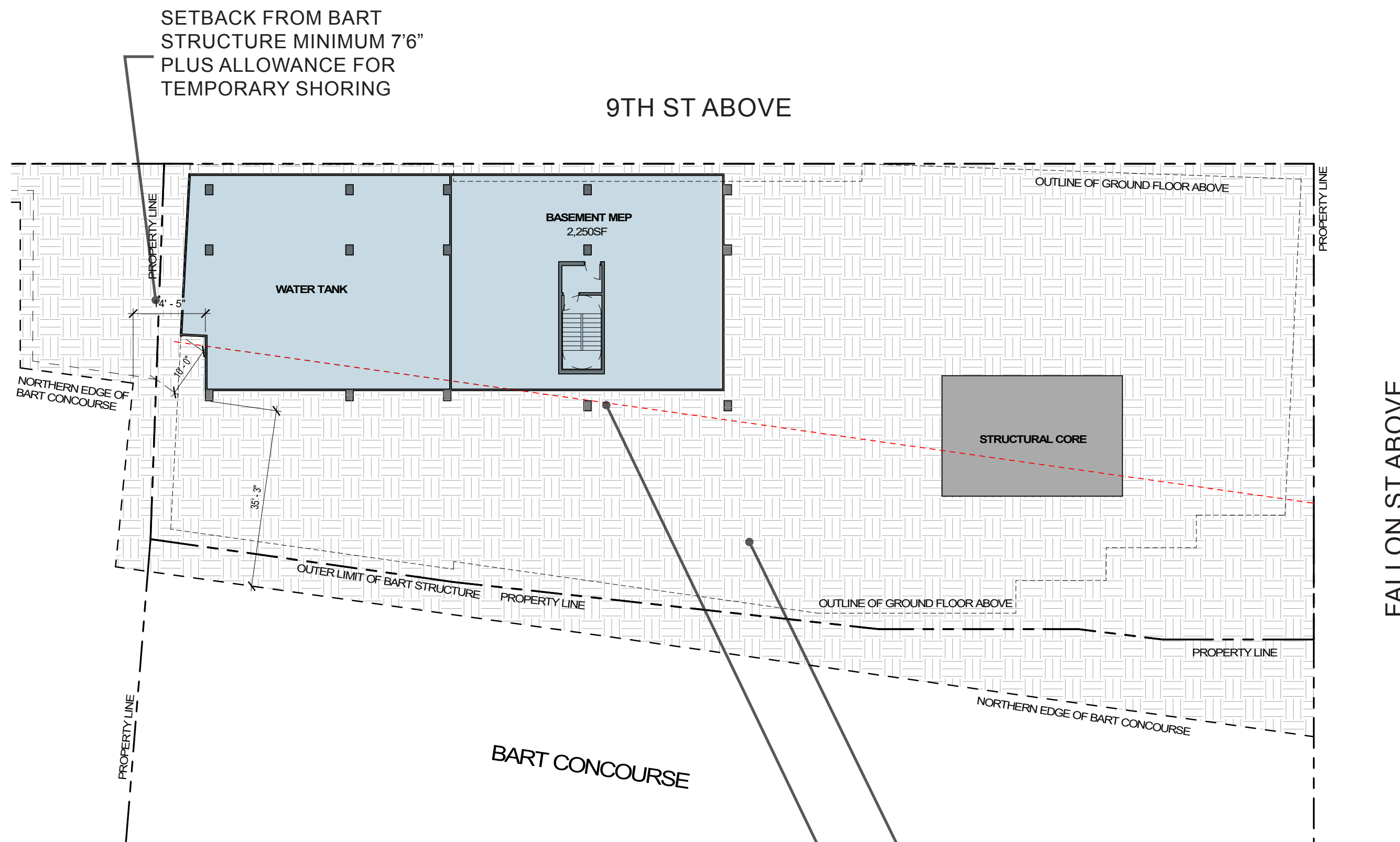
**BLOCK 1**  
Ground FLOOR PLAN



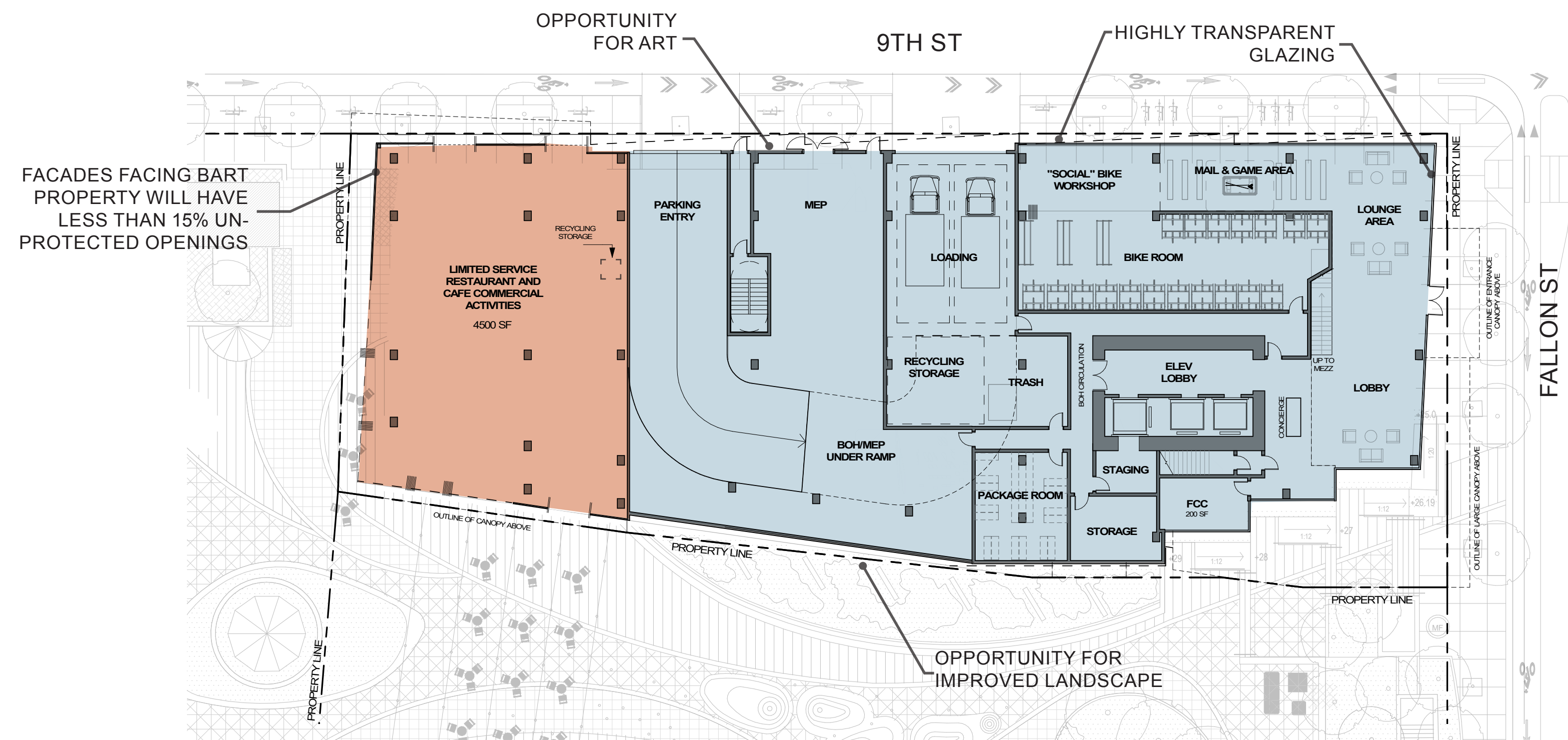
**TYPICAL PARKING PLAN (2-4)**



**MEZZANINE PLAN**



**BASEMENT PLAN**



**GROUND FLOOR PLAN**

**LAND USE LEGEND (PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES
- COMMERCIAL ACTIVITIES

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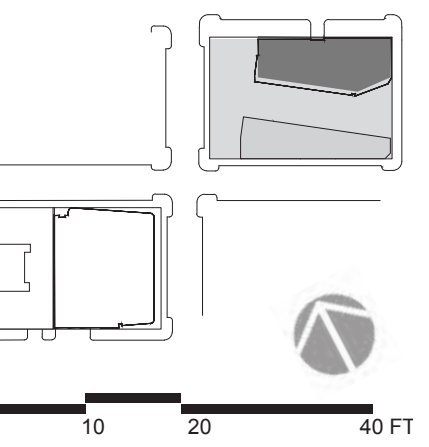
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

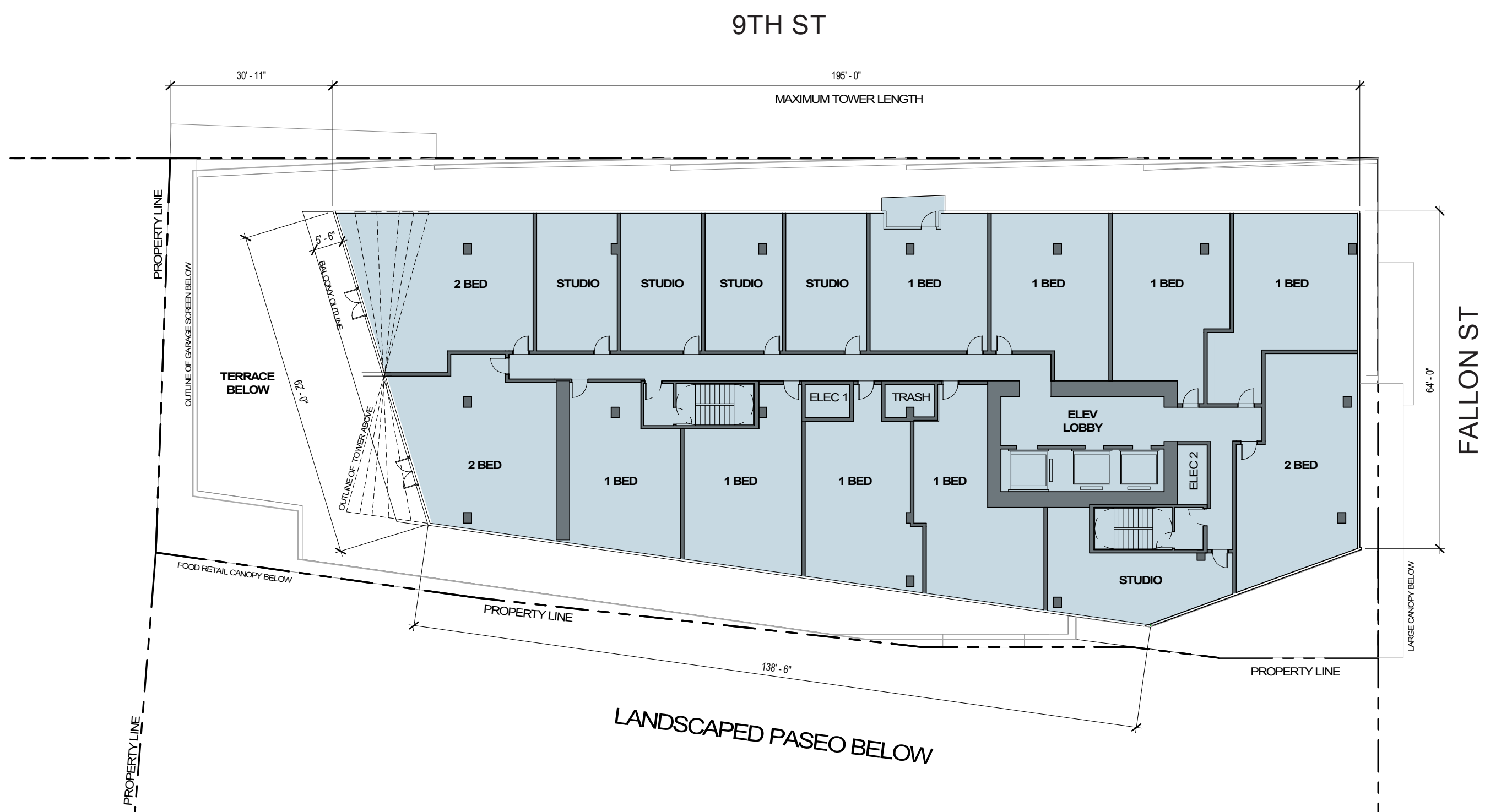
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

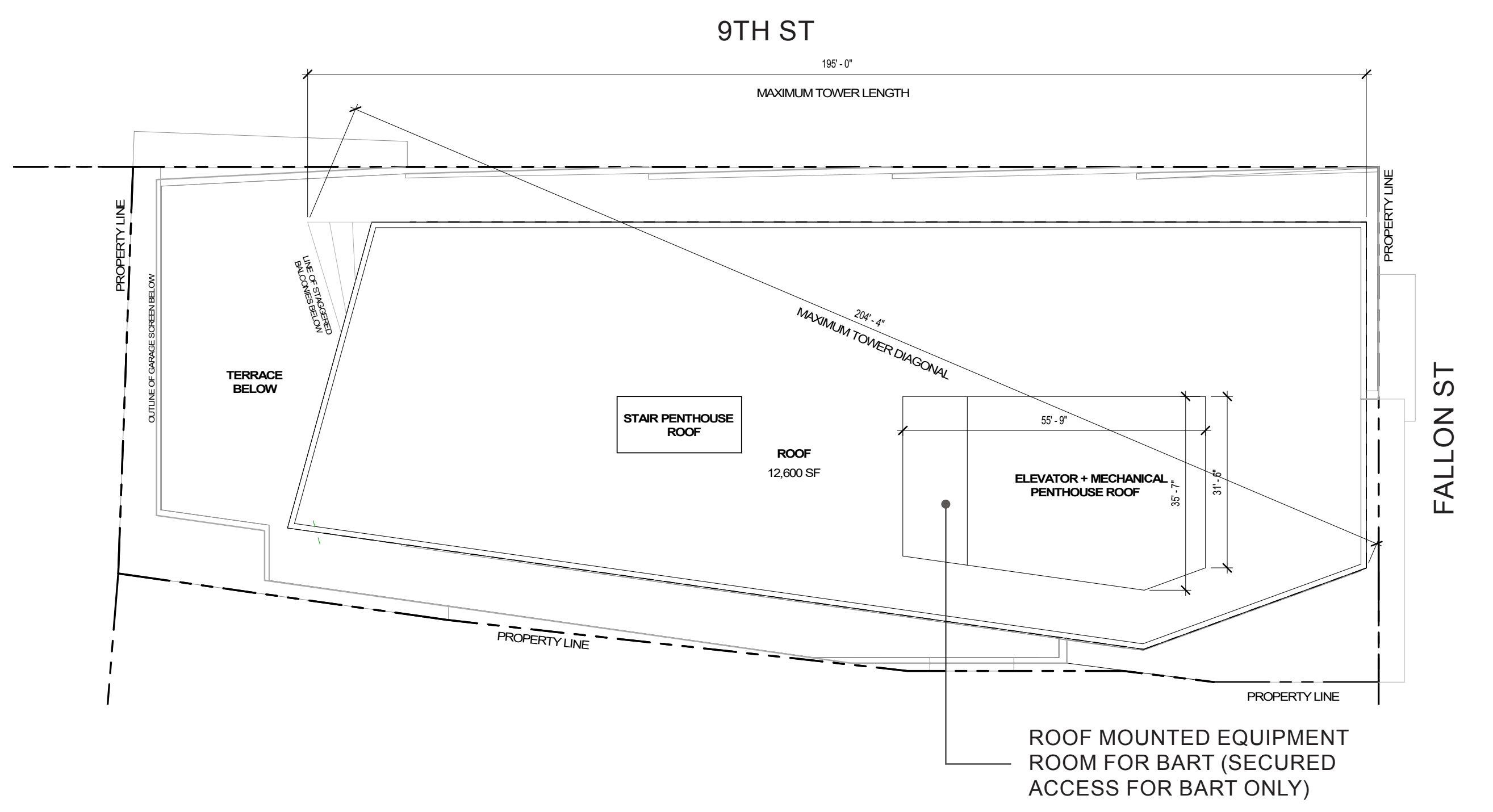
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NO.	ISSUE	DATE
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5	REVISED RESPONSE TO POP/R COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP/R COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP/R COMMENTS	03/19/2021



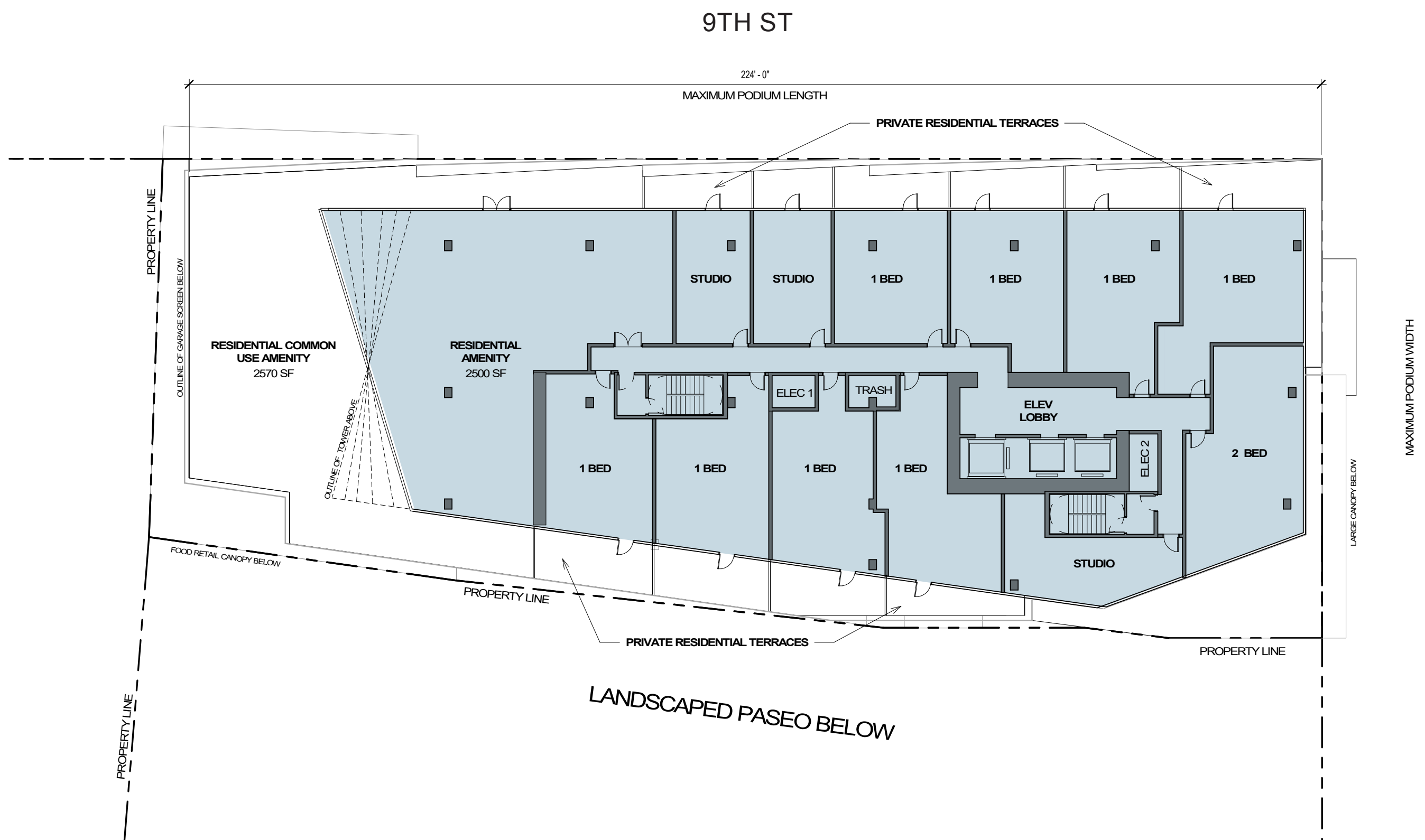
**BLOCK 1 / BLDG A LOWER FLOOR PLANS**



**TYPICAL RESIDENTIAL FLOOR PLAN (6-28)**



**ROOF PLAN**



**LEVEL 5 PLAN**



**LEVEL 28 PLAN**

**LAND USE LEGEND (PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES
- COMMERCIAL ACTIVITIES

NOTE: LOT SIZE IS 19,332 SF. AVERAGE RESIDENTIAL TOWER FLOOR AREA IS 12,665 SF OR 65% LOT COVERAGE (REF TABLE 17.101G.04)



**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

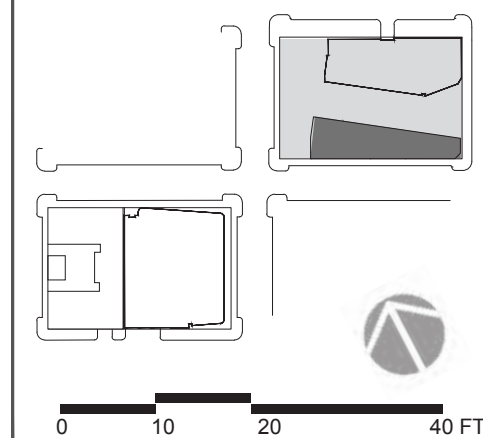
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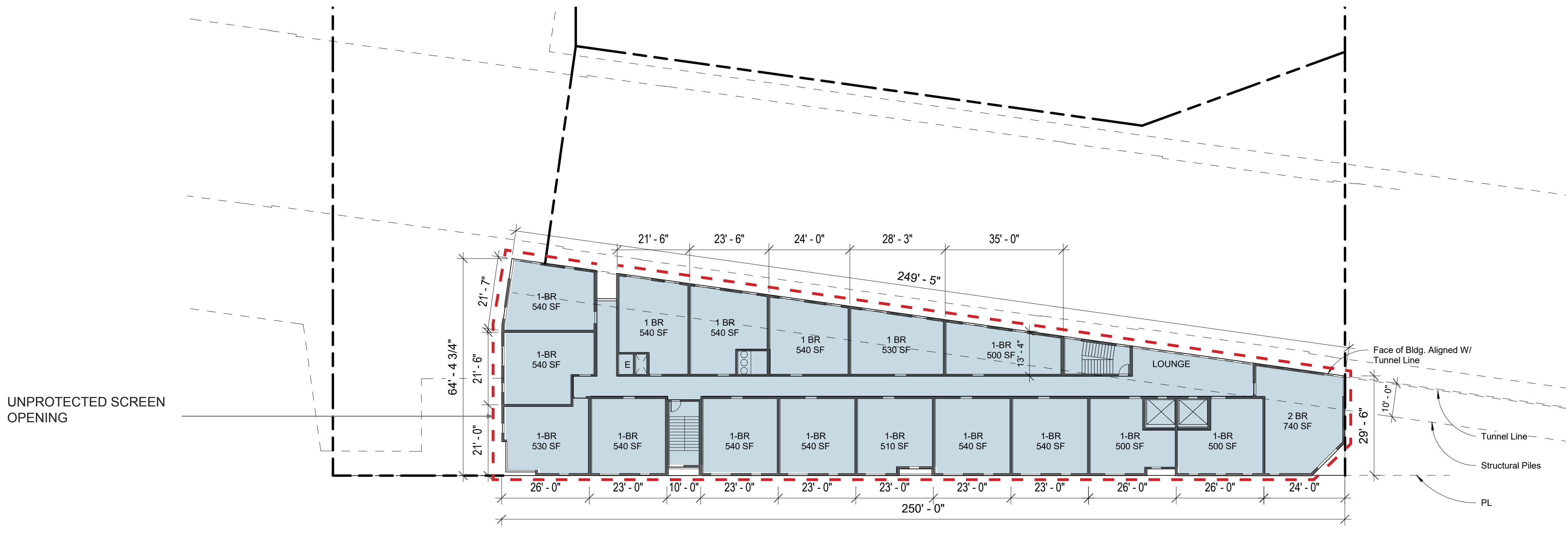
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6	REVISED RESPONSE TO POPF'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPF'S COMMENTS	03/19/2021

**LAND USE LEGEND (PER TABLE 17.101G.01)**

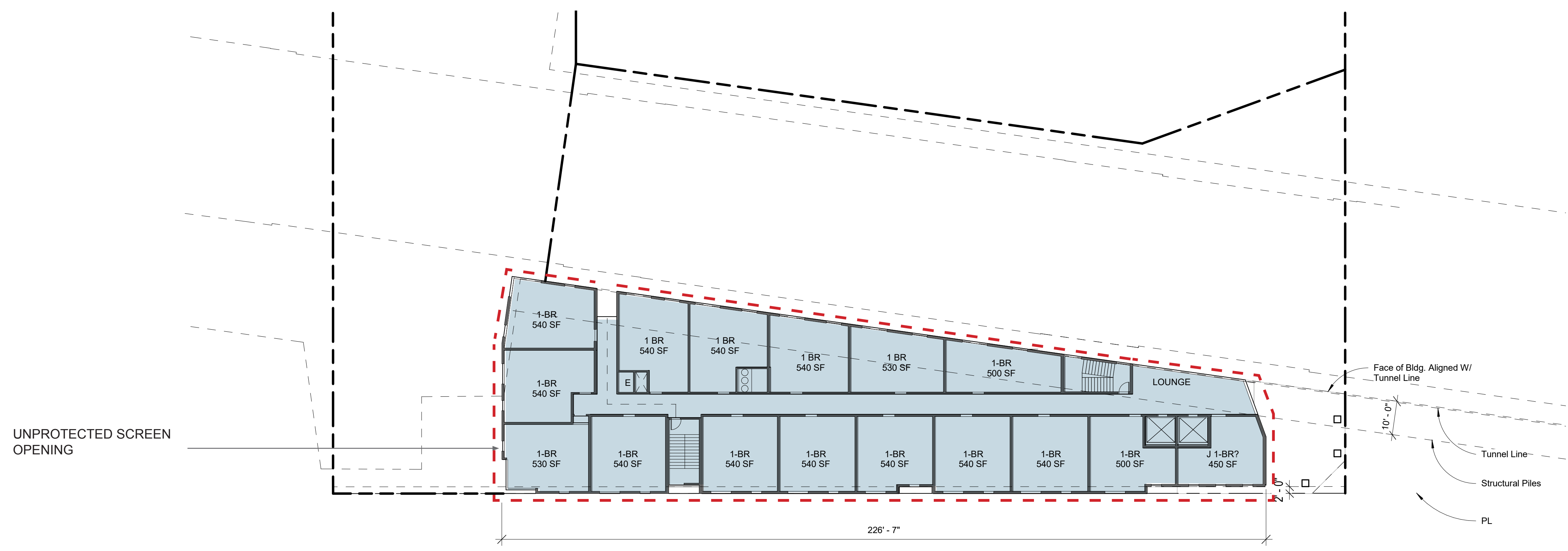
- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES



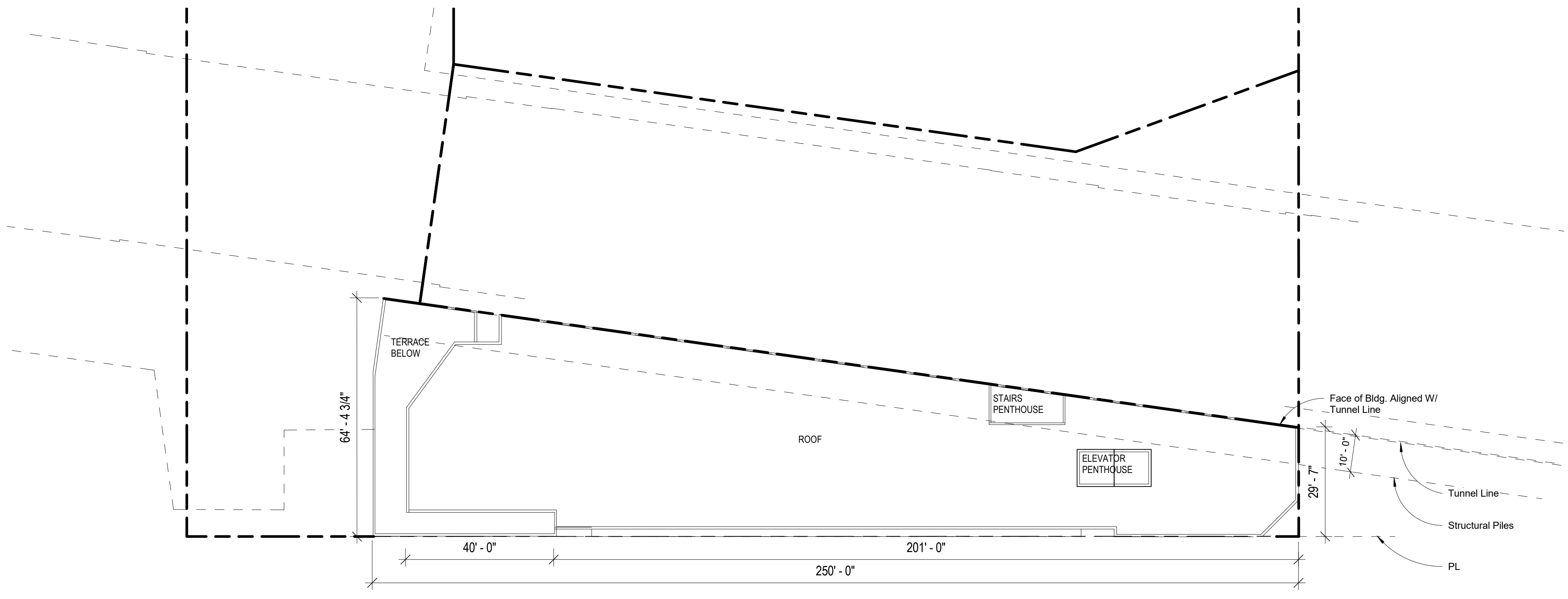
DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDG B UPPER FLOOR PLANS**



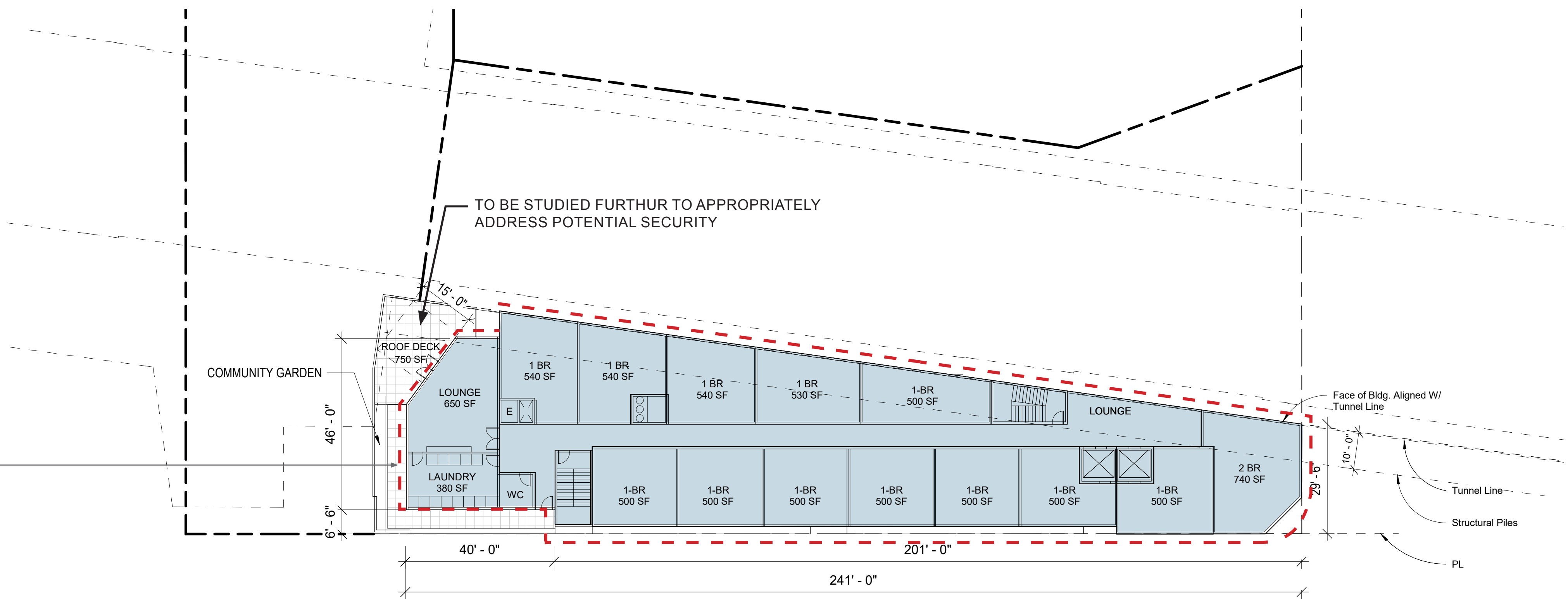
**TYPICAL RESIDENTIAL FLOOR PLAN (L3-L6)**



**RESIDENTIAL FLOOR PLAN (L2)**



**ROOF PLAN**



**RESIDENTIAL FLOOR PLAN (L7)**

**LAND USE LEGEND  
(PER TABLE 17.101G.01)**

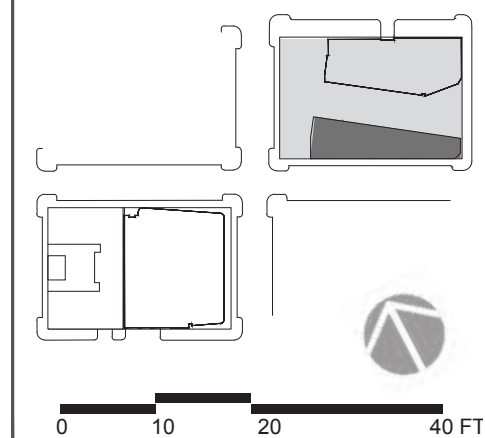
- PERMANENT RESIDENTIAL ACTIVITIES
- LIMITED-SERVICE RESTAURANT AND CAFE COMMERCIAL ACTIVITIES

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
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7	REVISED RESPONSE TO POPFZ COMMENTS	03/19/2021

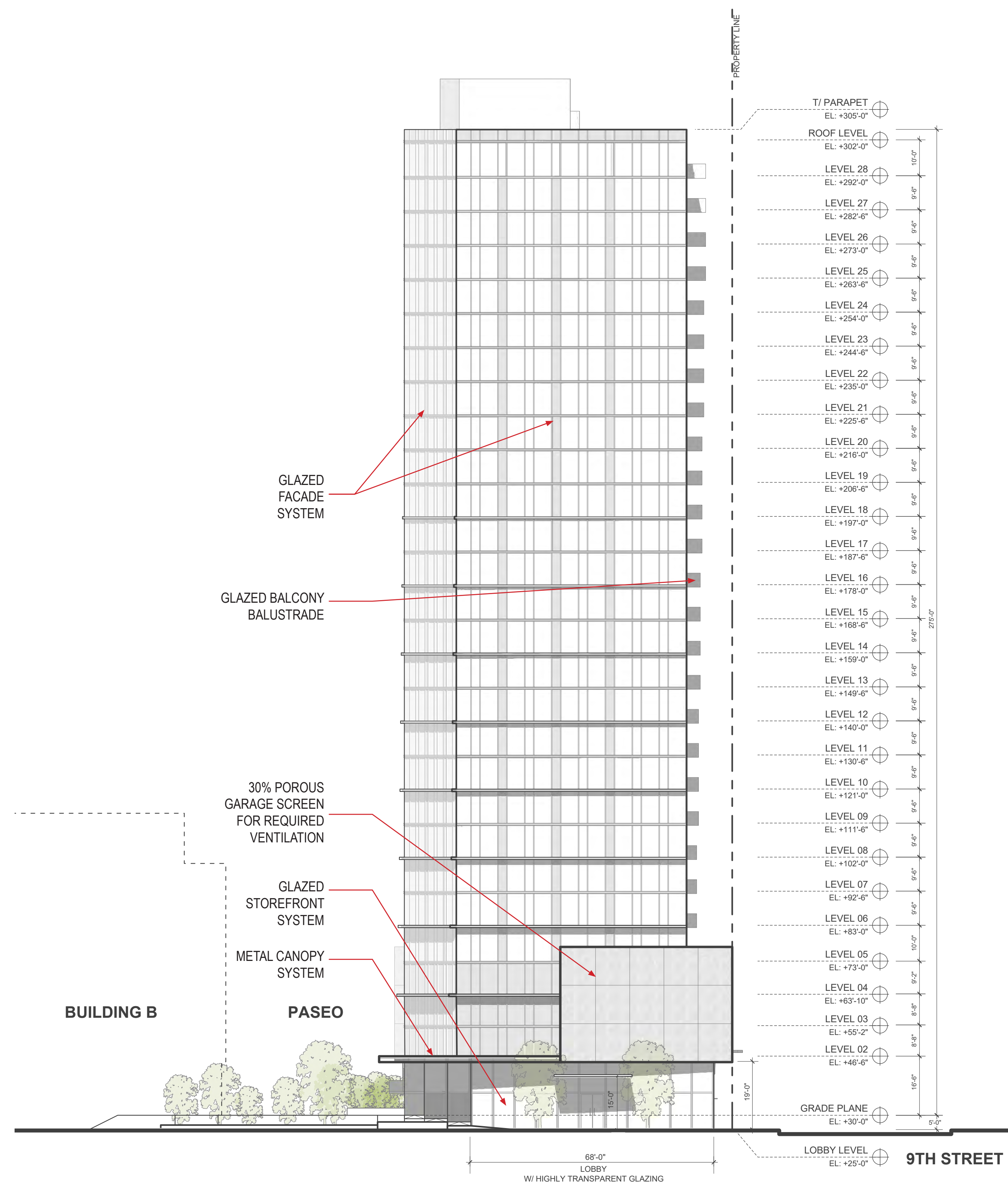


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**BLOCK 1 / BLDG B UPPER FLOOR PLANS**

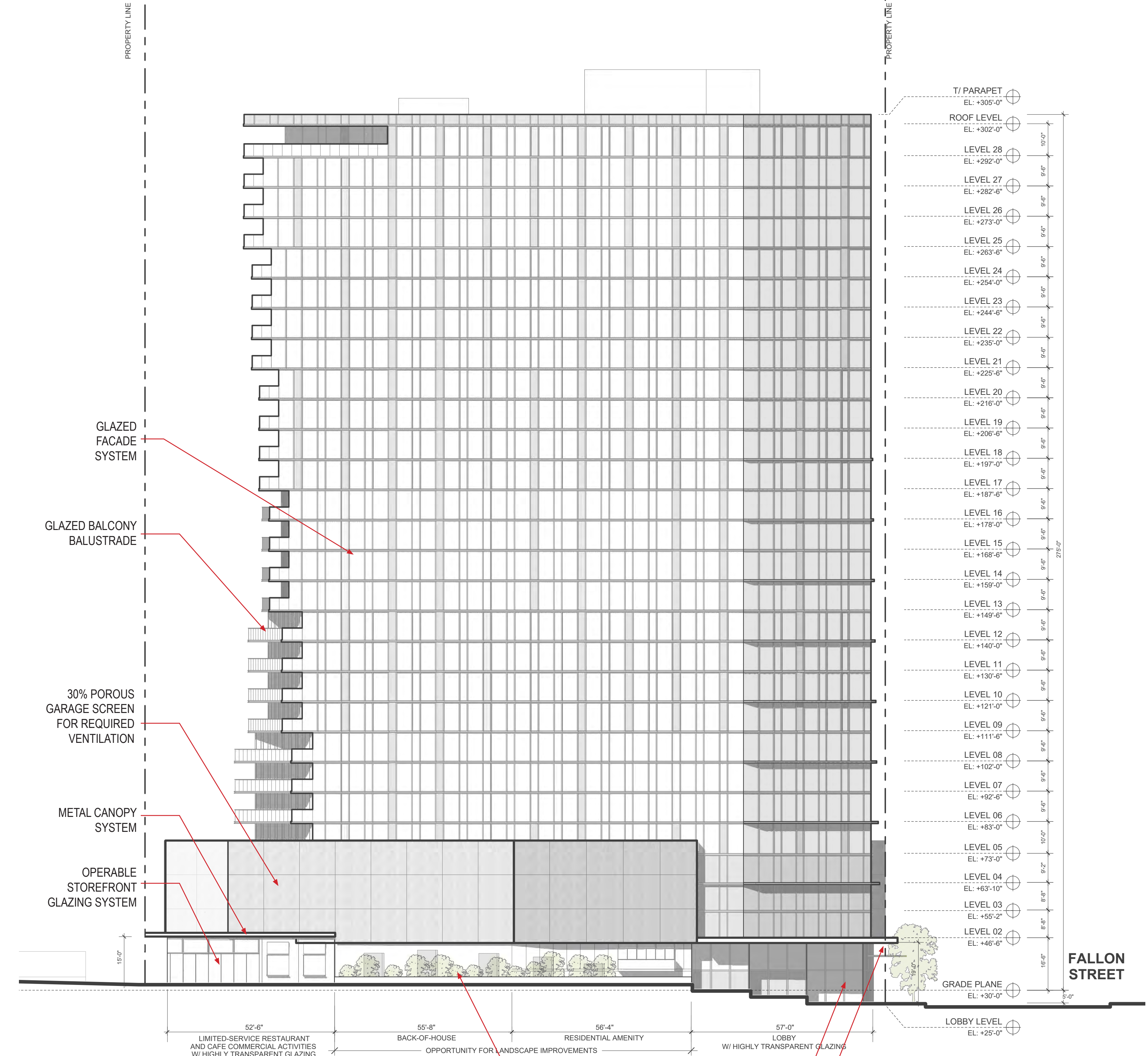
**NOTE:**

- WINDOWS AND BALCONY/DECKS FACING BART HEADHOUSE AREA NEED TO MITIGATE TO ENSURE THAT NO ITEMS CAN BE THROWN FROM THE ROOF OR WINDOW ONTO OR OVER THE HEADHOUSE AND SKYLIGHT AREA. DETAILS WILL BE PROVIDED LATER TO BE REVIEW AND APPROVED BY BART/CITY OF OAKLAND.
- COLORS AND MATERIALS TO BE FINALIZED AT FDP PHASE.

SIZE, SPECIFICATION, ACCESS, AND LOCATION OF ROOF MOUNTED EQUIPMENT TO BE APPROVED BY BART AT A LATER PHASE OF DESIGN



**EAST ELEVATION**



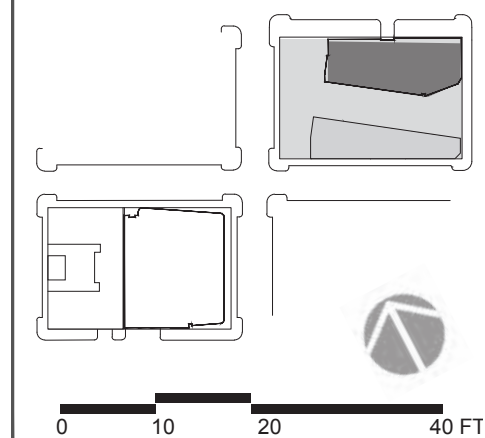
**SOUTH ELEVATION**

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

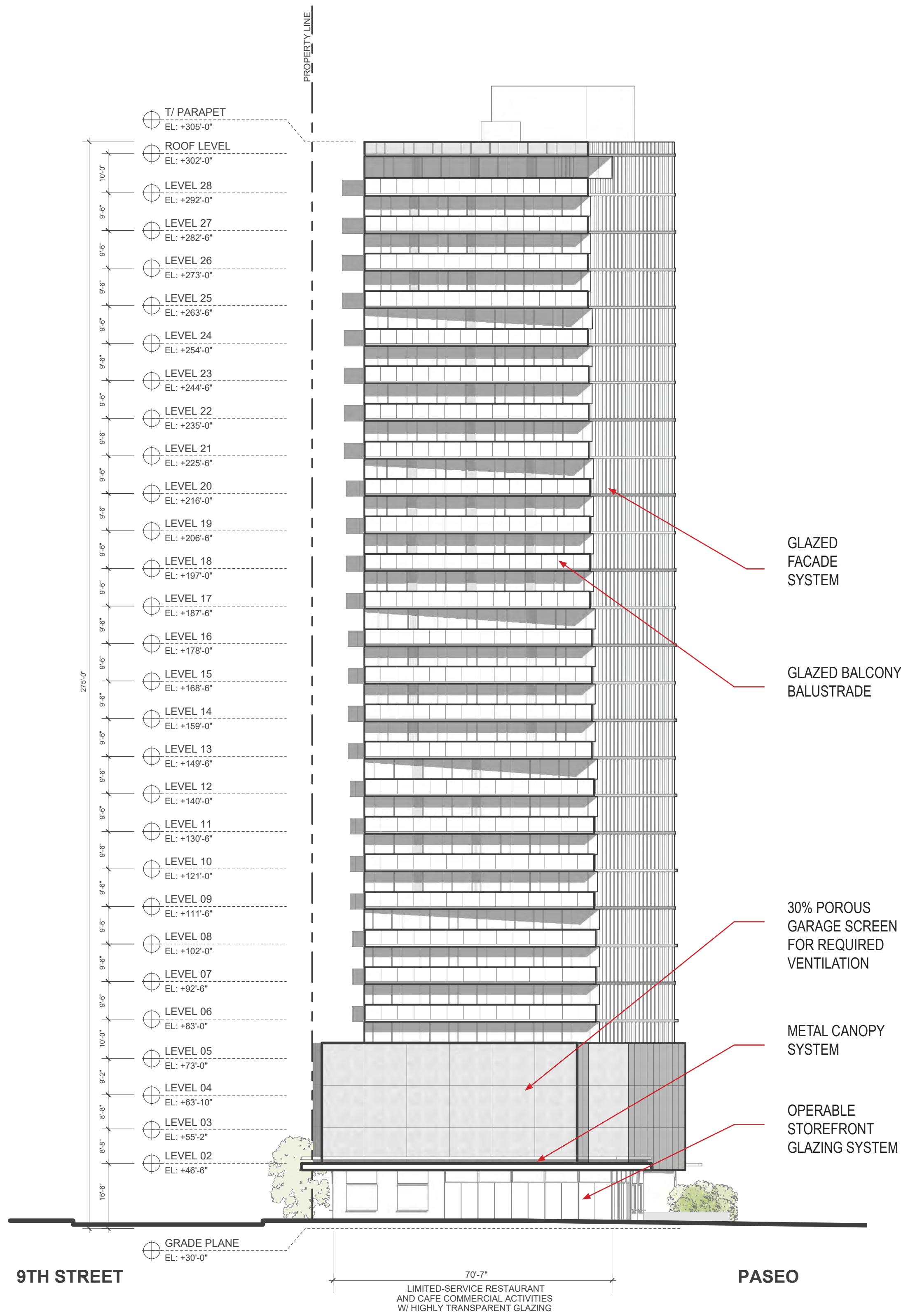
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION

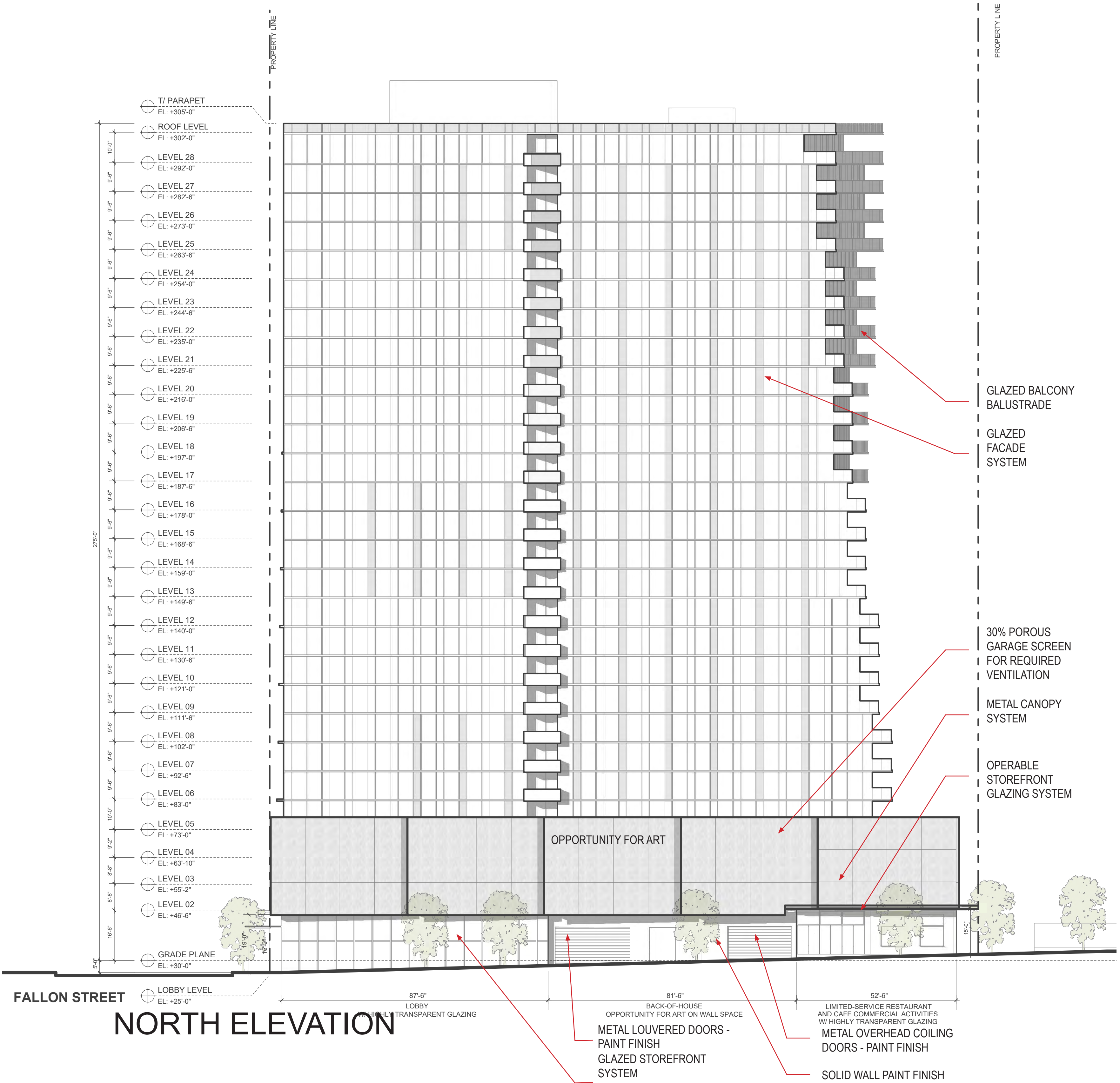
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7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021



DATE: 11/15/2019  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDGA EAST & SOUTH ELEVATIONS**



WEST ELEVATION



NORTH ELEVATION

**NOTE:**

- WINDOWS AND BALCONY/DECKS FACING BART HEADHOUSE AREA NEED TO MITIGATE TO ENSURE THAT NO ITEMS CAN BE THROWN FROM THE ROOF OR WINDOW ONTO OR OVER THE HEADHOUSE AND SKYLIGHT AREA. DETAILS WILL BE PROVIDED LATER TO BE REVIEW AND APPROVED BY BART/CITY OF OAKLAND.
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SIZE, SPECIFICATION, ACCESS, AND LOCATION OF ROOF MOUNTED EQUIPMENT TO BE APPROVED BY BART AT A LATER PHASE OF DESIGN

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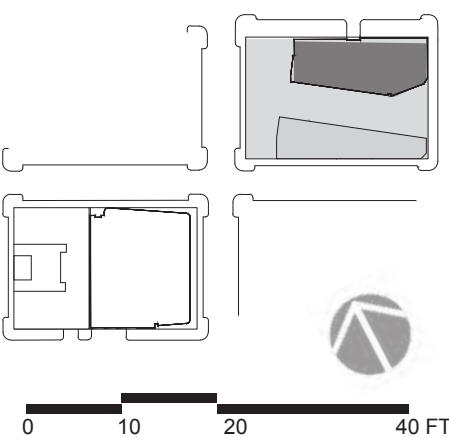
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

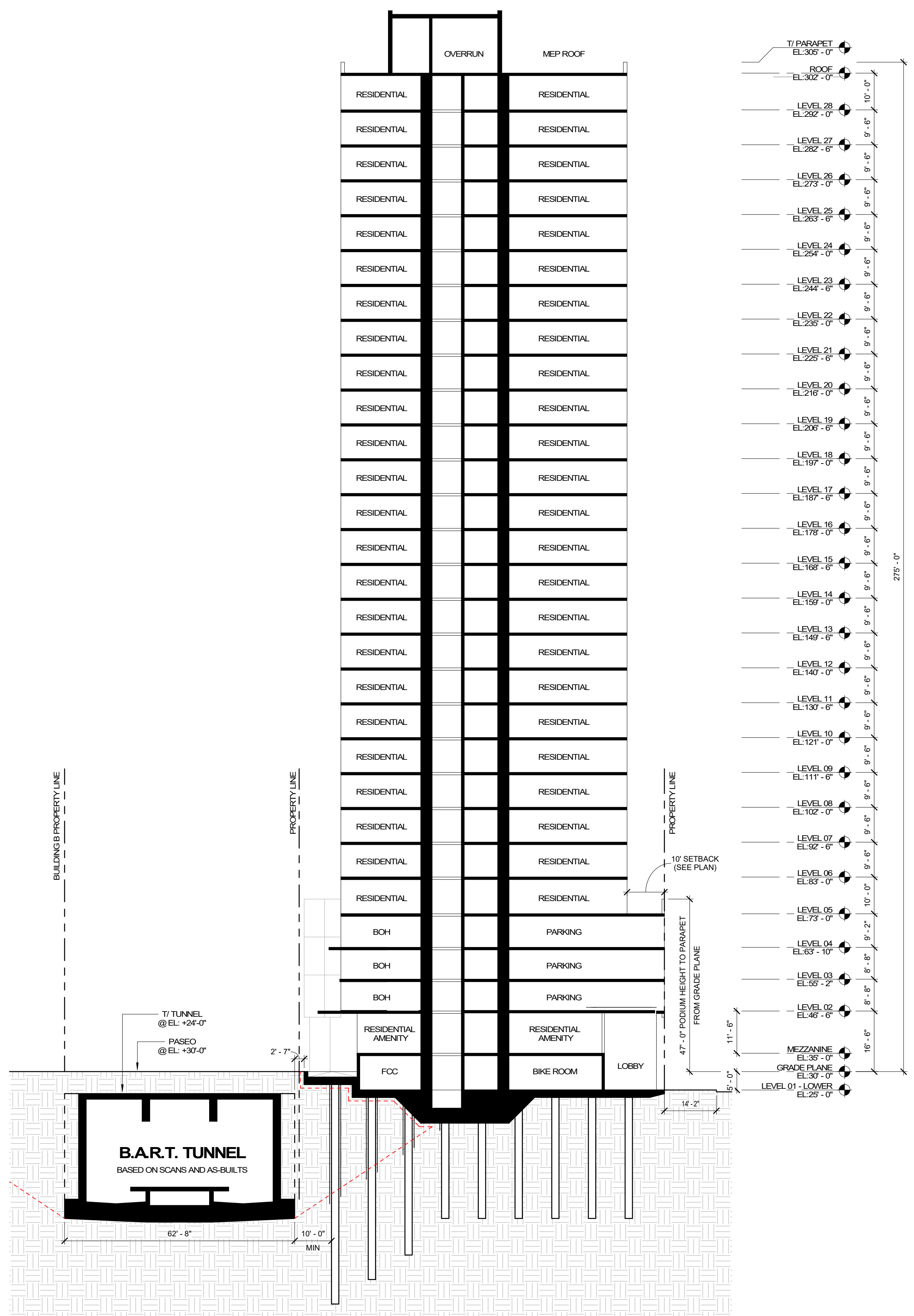
- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
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6	REVISED RESPONSE TO POPZ COMMENTS	02/22/2021
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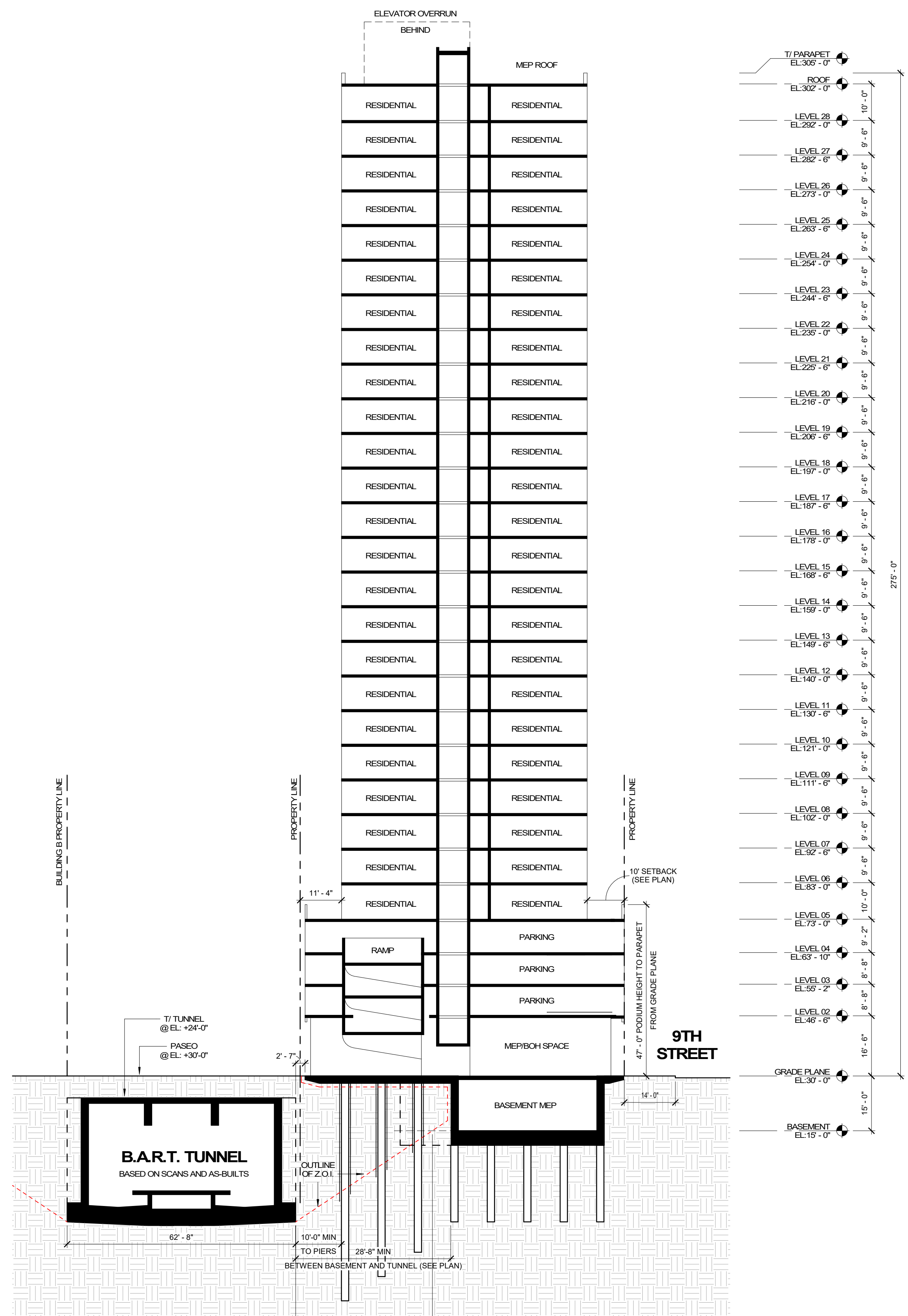


DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / BLDGA WEST & NORTH ELEVATIONS**

**NOTE:**  
 • FDP DESIGN MUST DEMONSTRATE PER BART'S REQUIREMENTS THAT THERE ARE NO STRUCTURAL IMPACTS TO THE TUNNEL PER BFS STANDARDS.



SECTION A: NORTH-SOUTH THROUGH LOBBY



SECTION B: NORTH-SOUTH THROUGH BASEMENT

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 318 HARRISON ST. STE 301  
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 www.inwilleraehl.com

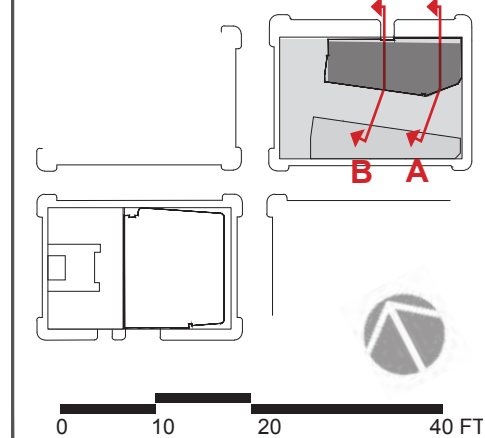
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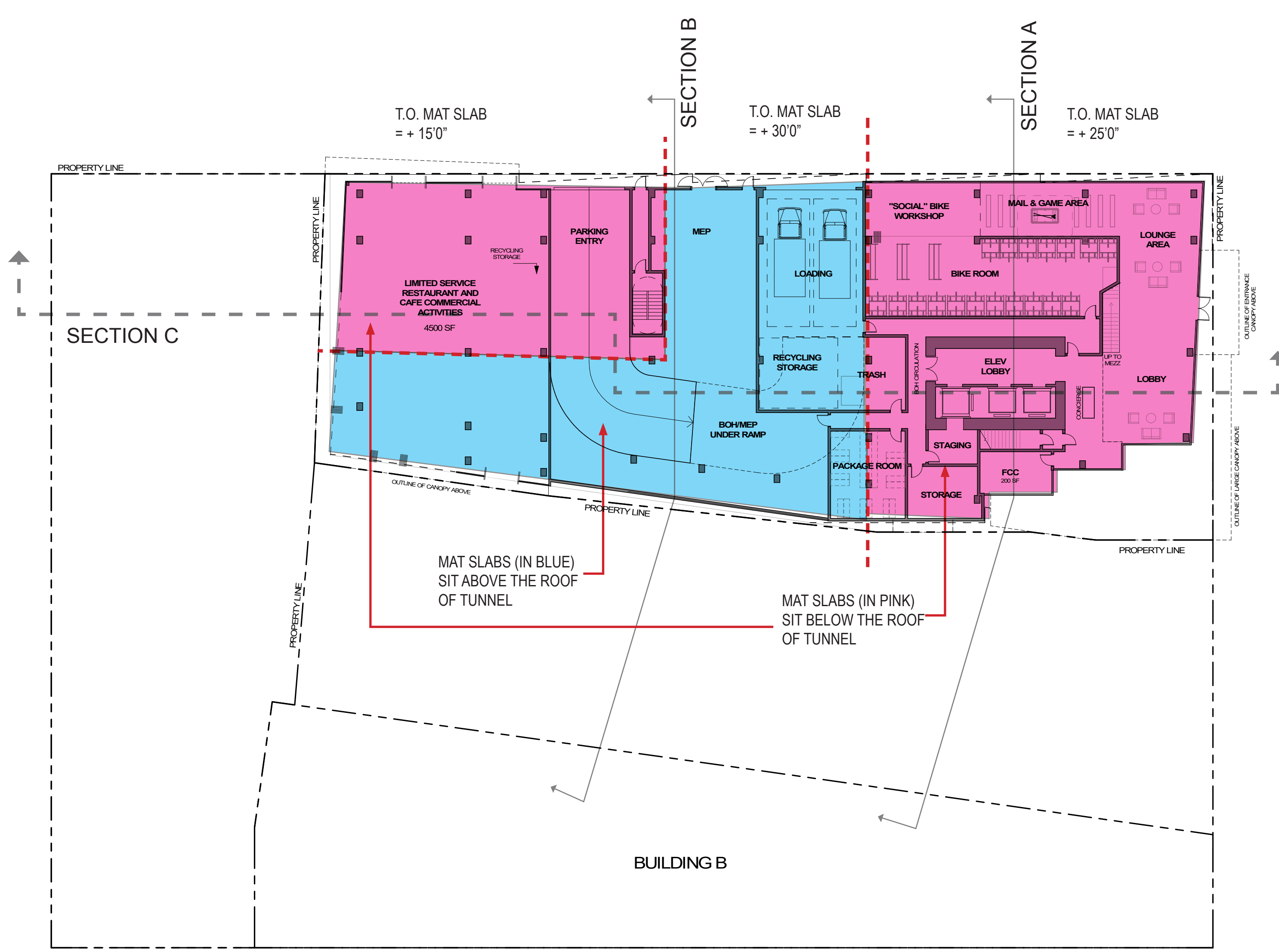
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PRELIMINARY - NOT FOR CONSTRUCTION

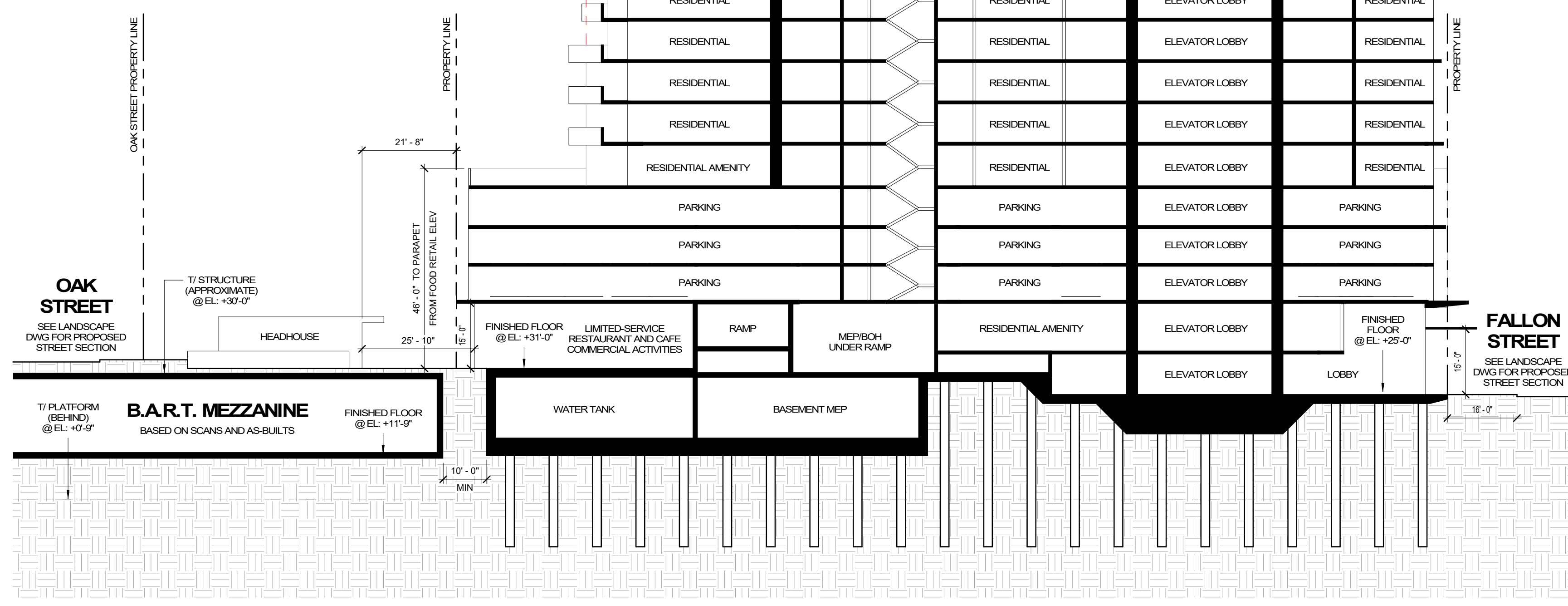
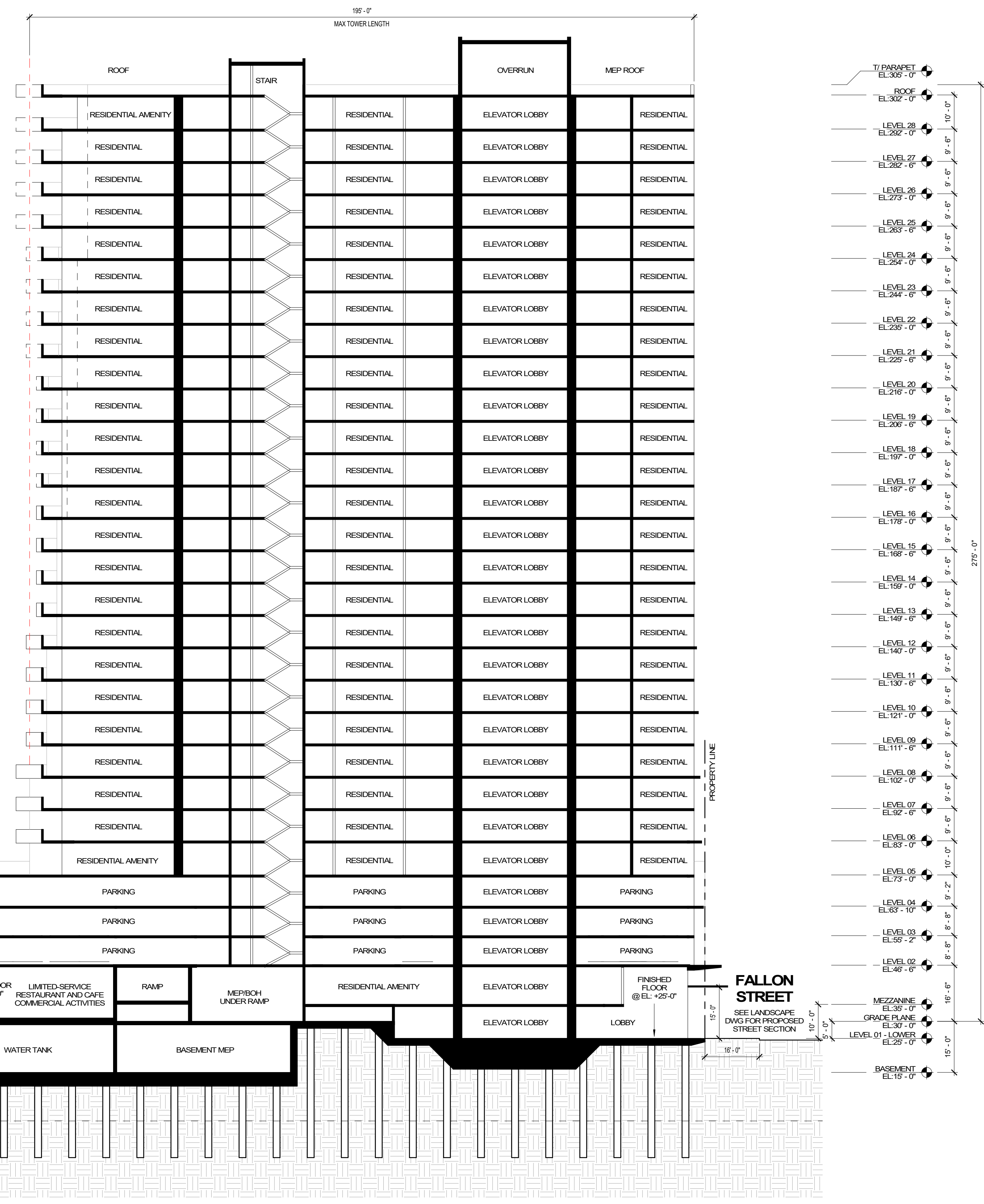
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6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021



DATE:  
 SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 BUILDING A NORTH - SOUTH SECTIONS**



**NOTE:**  
 • FDP DESIGN MUST DEMONSTRATE PER BART'S REQUIREMENTS THAT THERE ARE NO STRUCTURAL IMPACTS TO THE TUNNEL PER BFS STANDARDS.



**SECTION C: EAST-WEST**

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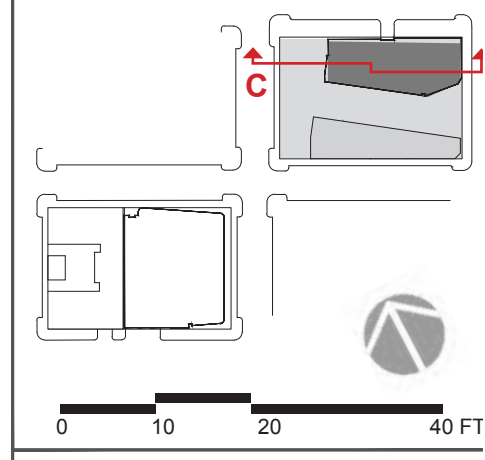
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**LAKE MERRITT BART REDEVELOPMENT**  
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**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

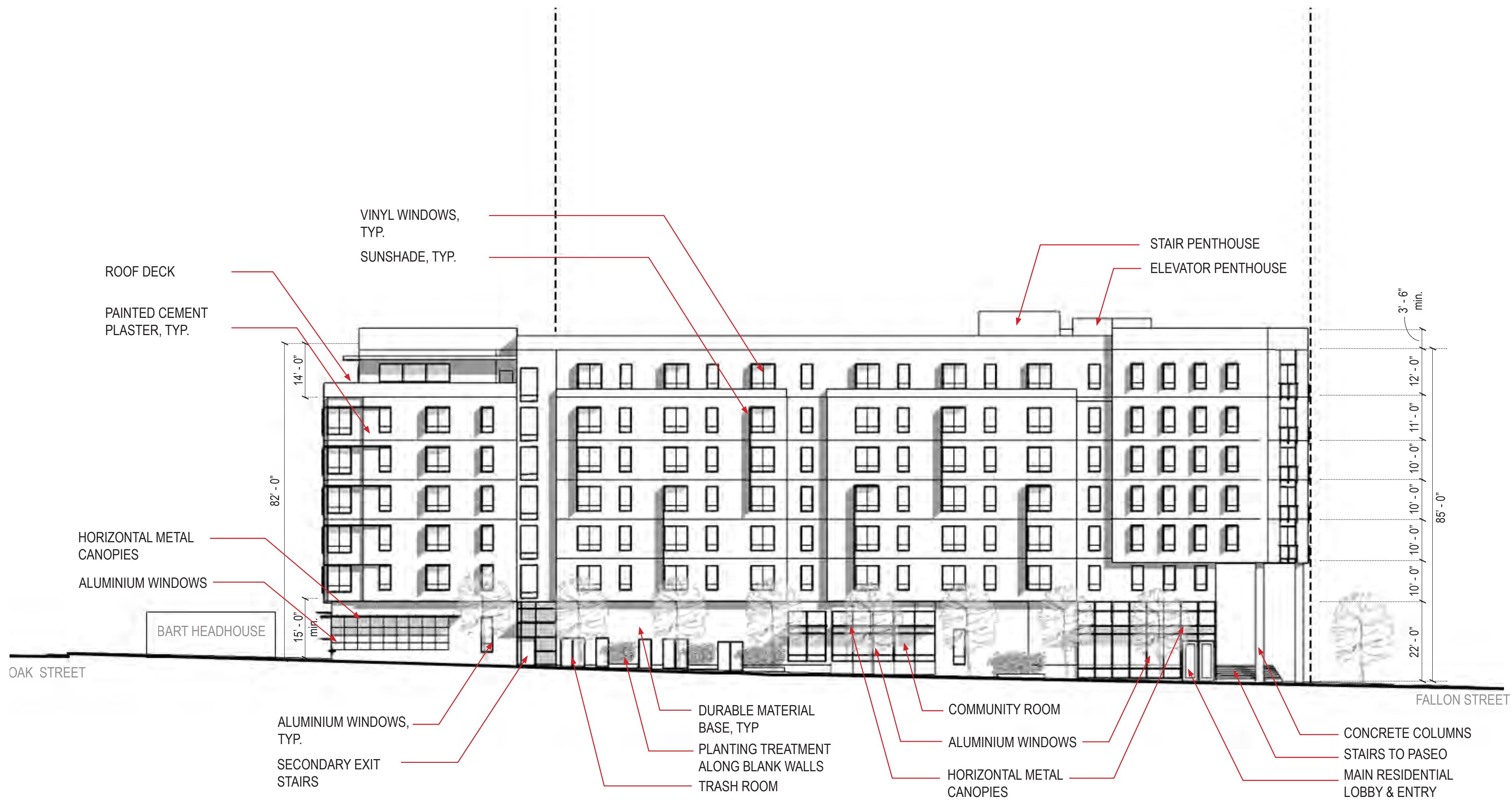
• PRELIMINARY - NOT FOR CONSTRUCTION •

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DATE:  
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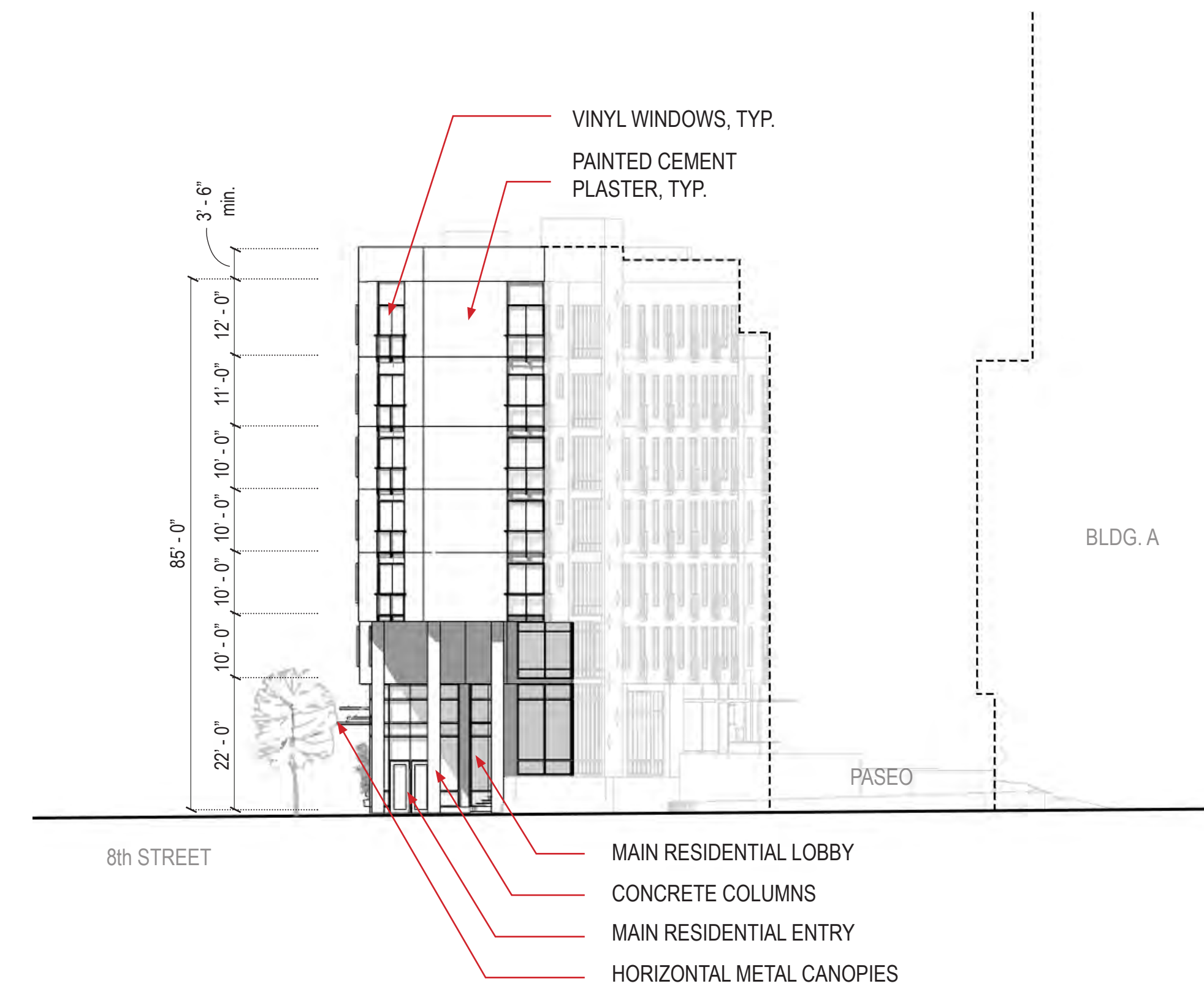
**BLOCK 1 BUILDING A EAST-WEST SECTION**



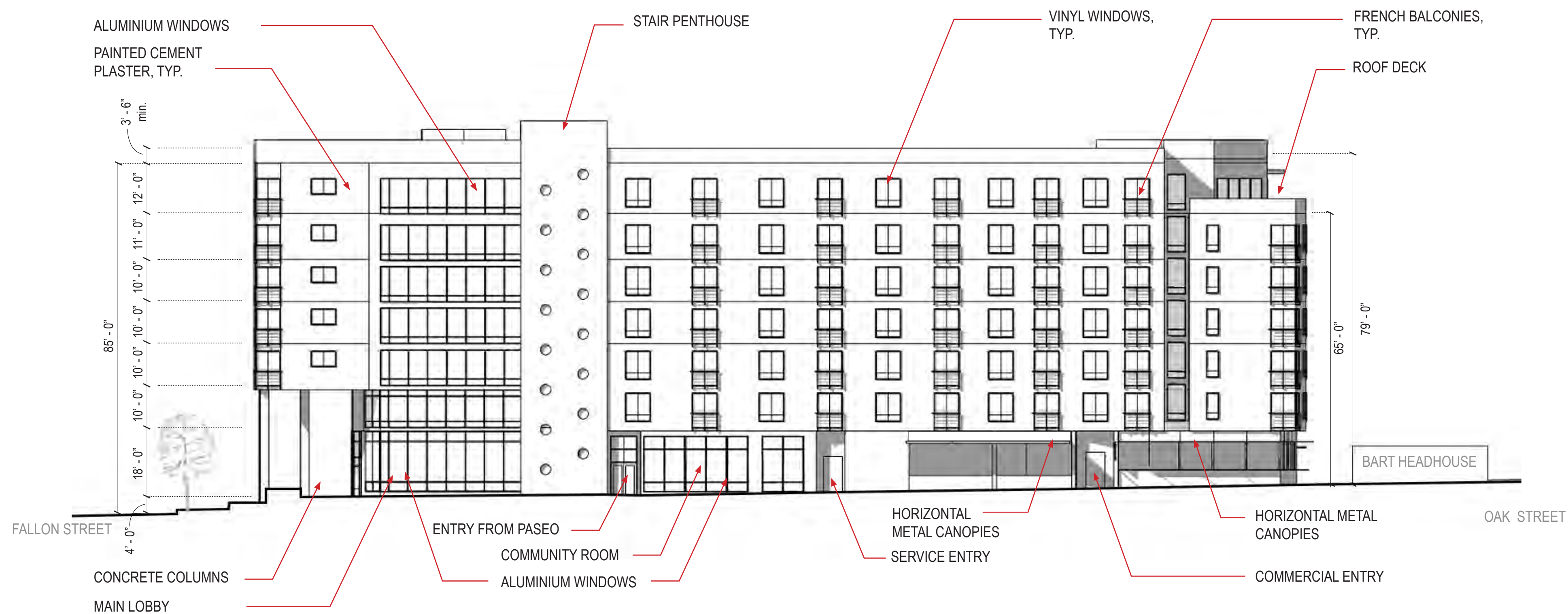
2. BUILDING B SOUTH ELEVATION

**NOTE:**

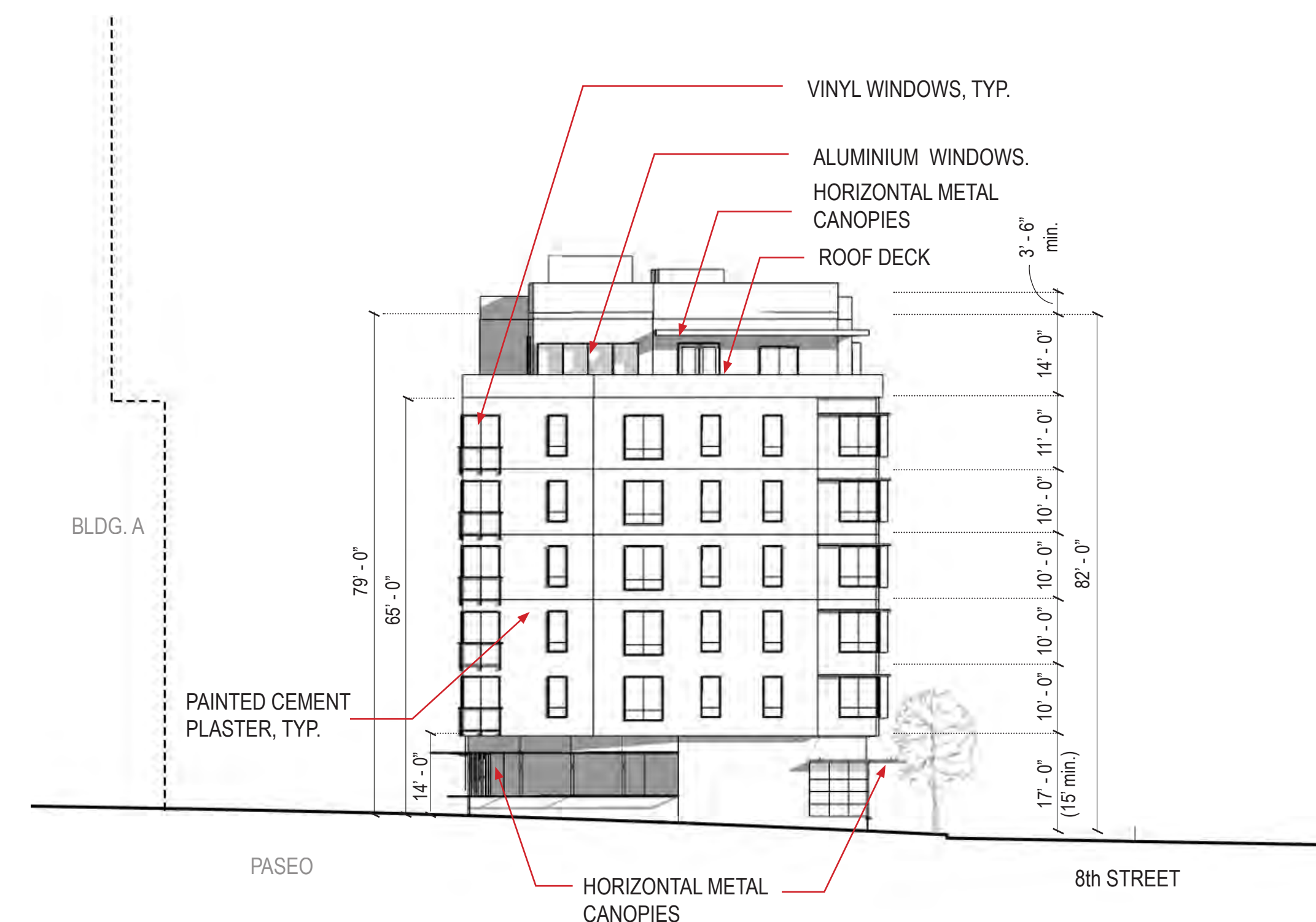
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1. BUILDING B EAST ELEVATION



4. BUILDING B NORTH ELEVATION



3. BUILDING B WEST ELEVATION

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
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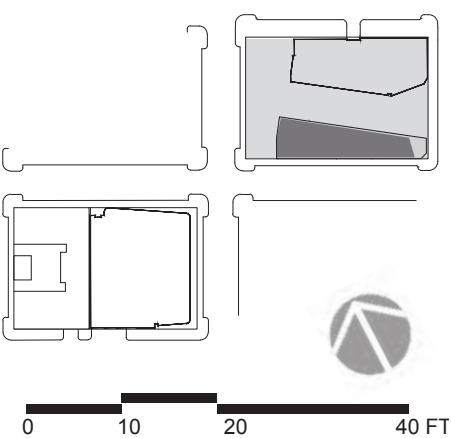
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

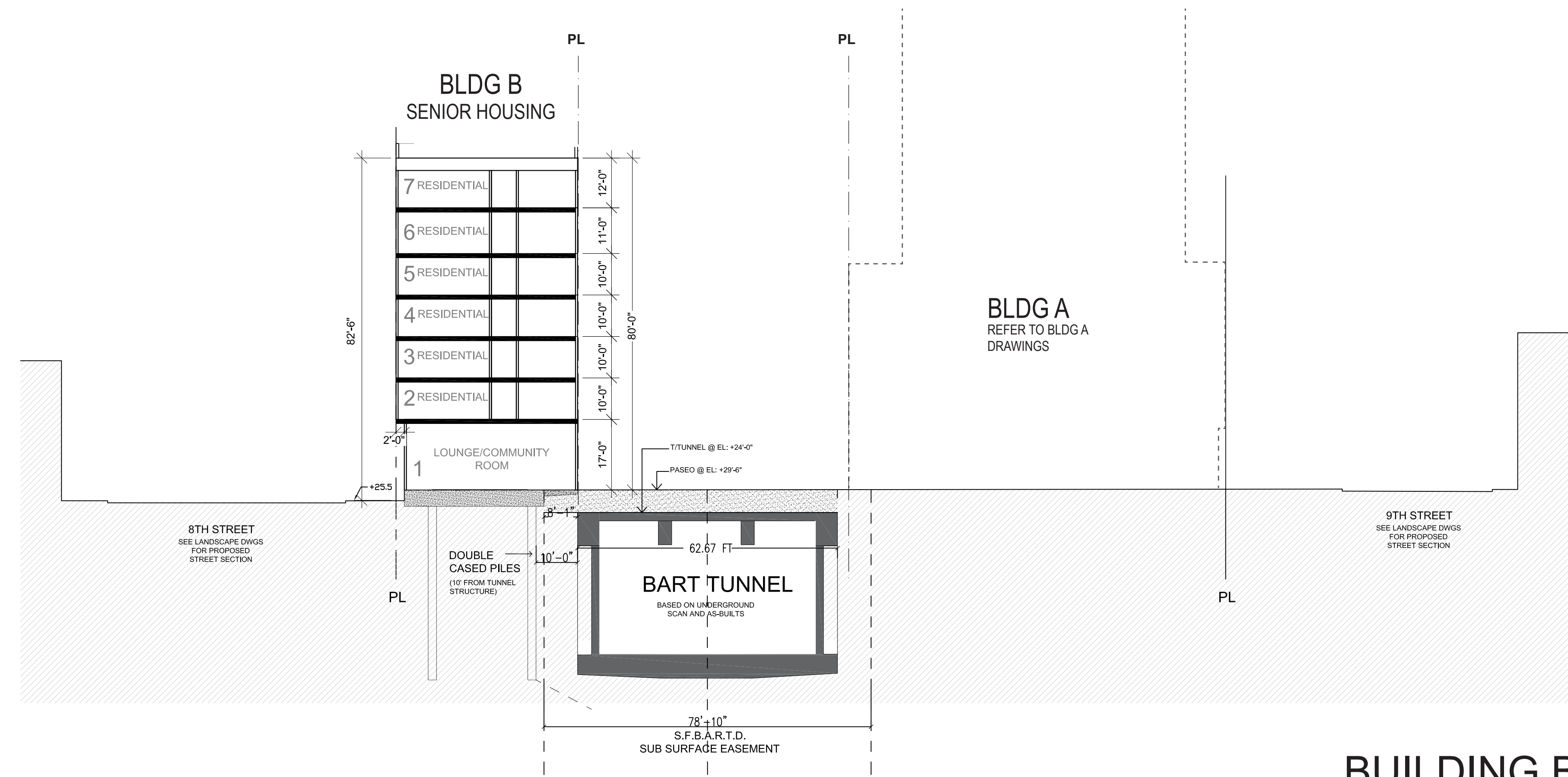
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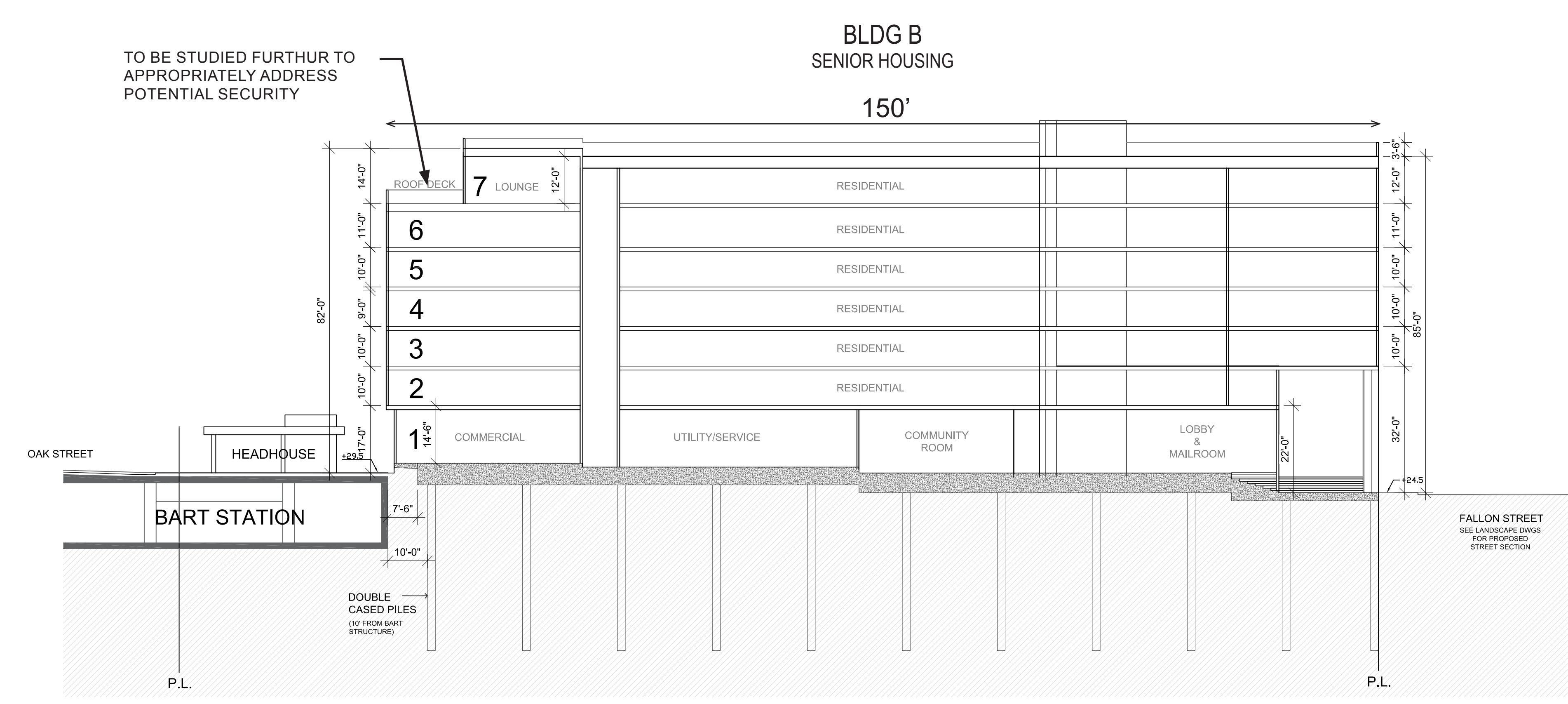
DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 BUILDING B ELEVATIONS**

**A2.20**



**BUILDING B - SECTION A**



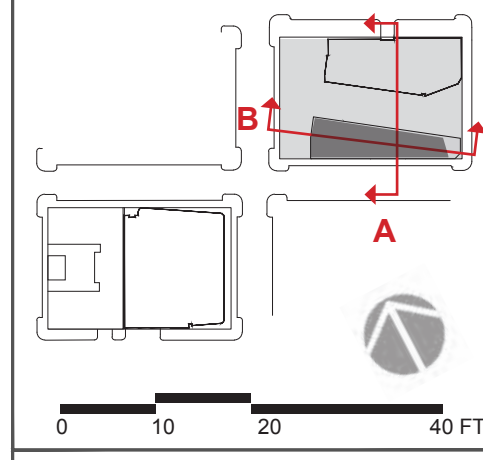
**BUILDING B - SECTION B**

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

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6	REVISED RESPONSE TO PDP#5 COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDP#4 COMMENTS	03/19/2021



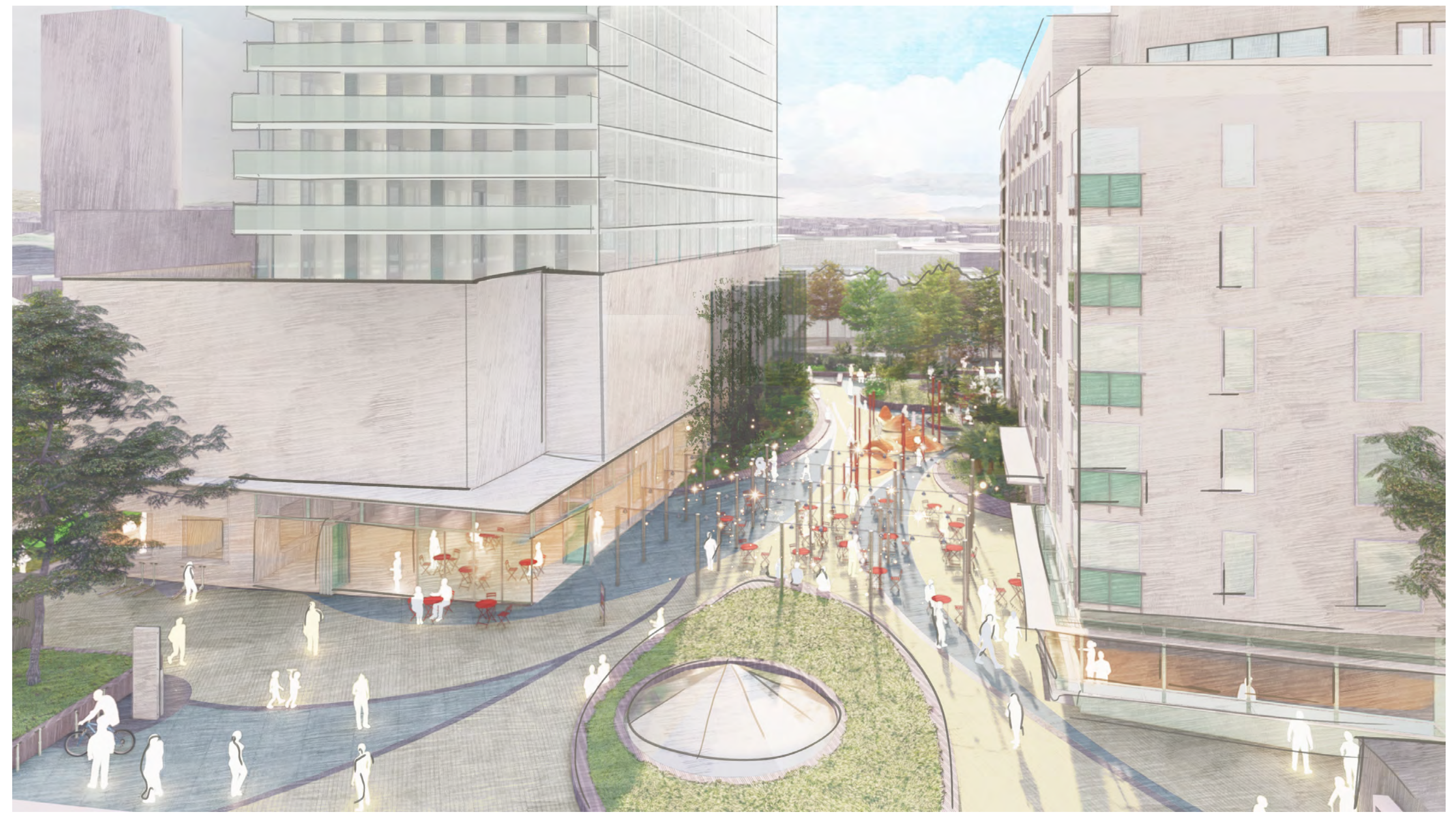
DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 BUILDING B SECTIONS**





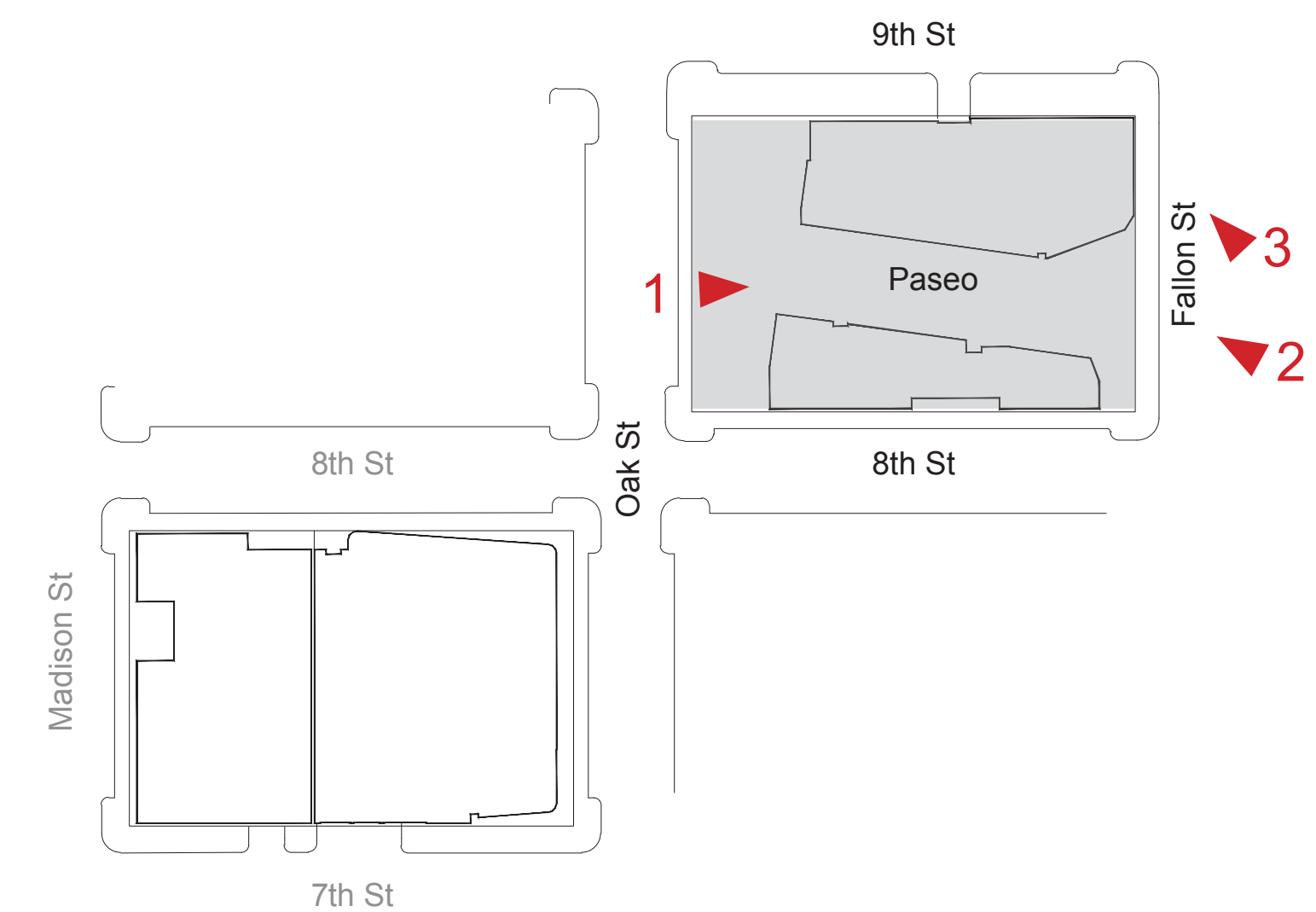
2. FALLON STREET  
VIEW TOWARDS THE PASEO



1. PASEO  
VIEW TOWARDS EAST



3. FALLON STREET  
VIEW TOWARDS BLDG A LOBBY



EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
INTEGRATED DESIGN  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com

SOLOMON CORDWELL BUENZ ARCHITECTS  
**SCB**  
335 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
(415) 776-2600  
www.scba.com

INWILLERUEHL  
ARCHITECTS  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
(510) 851-1698  
www.inwilleruehl.com

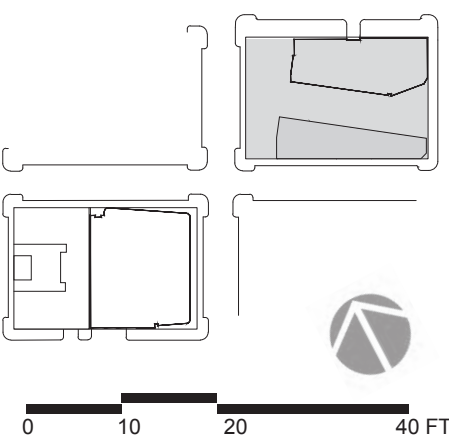
**BKF100**  
YEARS  
ENGINEERS, SURVEYORS, PLANNERS  
225 BROADWAY DR. SUITE 200  
REDWOOD CITY, CA 94063  
(650) 483-8300  
www.bkf.com

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE	NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019	
2	RESPONSES TO COMMENTS FROM BART	1/20/2020	
3	PDP SUBMITTAL #1 TO CITY	2/12/2020	
4	REVISED RESPONSE TO POPF COMMENTS	6/8/2020	
5	REVISED RESPONSE TO POPF COMMENTS	10/02/2020	
6	REVISED RESPONSE TO POPF COMMENTS	02/22/2021	
7	REVISED RESPONSE TO POPF COMMENTS	03/19/2021	



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 / CONCEPT 3D VIEWS**

**A2.22**

**NOTE:**

- SEE C2.1, C2.2, C6.1, C6.2 FOR FIRE HYDRANT LOCATIONS

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

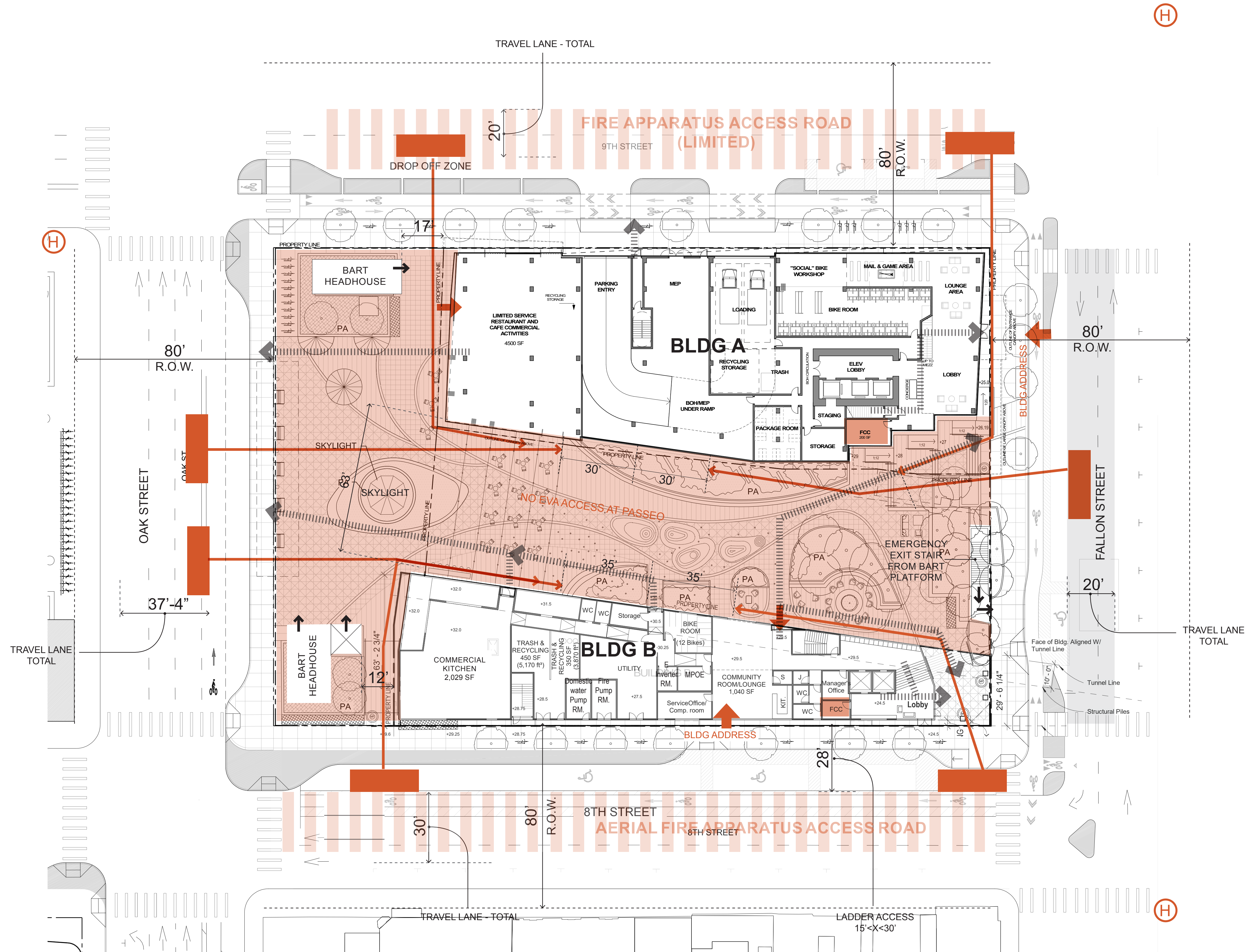
**STRADA**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
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SOLIMON CORDWELL BUENZ ARCHITECTS  
355 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
415/716-2600  
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INWILLERAUEHL  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/851-1686  
www.inwillerauehl.com

**BKF100**  
225 BUCKLAND DR. SUITE 200  
REDDING, CA 96001  
(530) 463-4300  
www.bkf.com



**BUILDING A:**  
27 STORIES/ 275' HEIGHT  
RESIDENTIAL/MIXED USE  
TYPE I  
FULLY SPRINKLERED

**BUILDING B:**  
7 STORIES/ 85' HEIGHT  
RESIDENTIAL/MIXED USE  
TYPE III OVER TYPE I  
FULLY SPRINKLERED

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

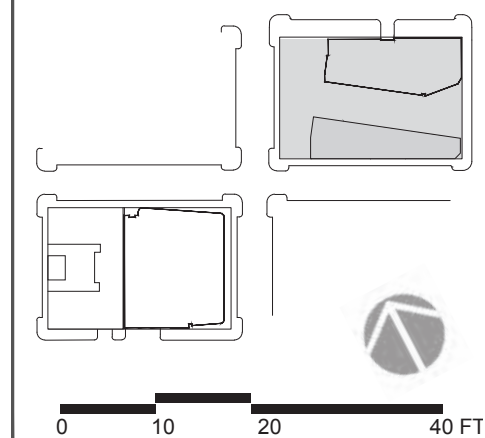
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	POP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POP'S COMMENTS	6/8/2020
5	REVISED RESPONSE TO POP'S COMMENTS	10/02/2020
6	REVISED RESPONSE TO POP'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO POP'S COMMENTS	03/19/2021

**KEY LEGEND**

- EXISTING HYDRANT LOCATION
- NO EVA ACCESS
- 150' HOSE DISTANCE
- FIRE TRUCK
- AERIAL FIRE APPARATUS ACCESS ROAD (26 FEET WIDE)
- NON-AERIAL FIRE APPARATUS ACCESS ROAD (20 FEET WIDE)
- PRIMARY BLDG ENTRANCE
- EXIT



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 1 FIRE ACCESS DIAGRAM**

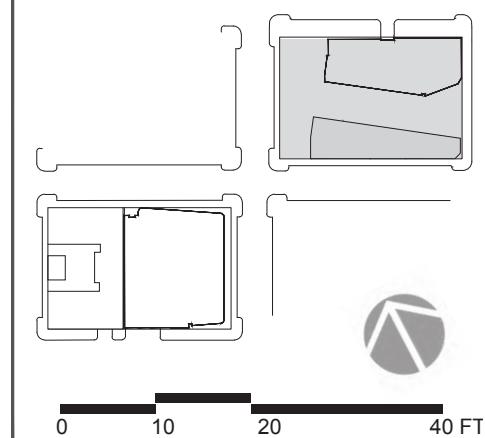
**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

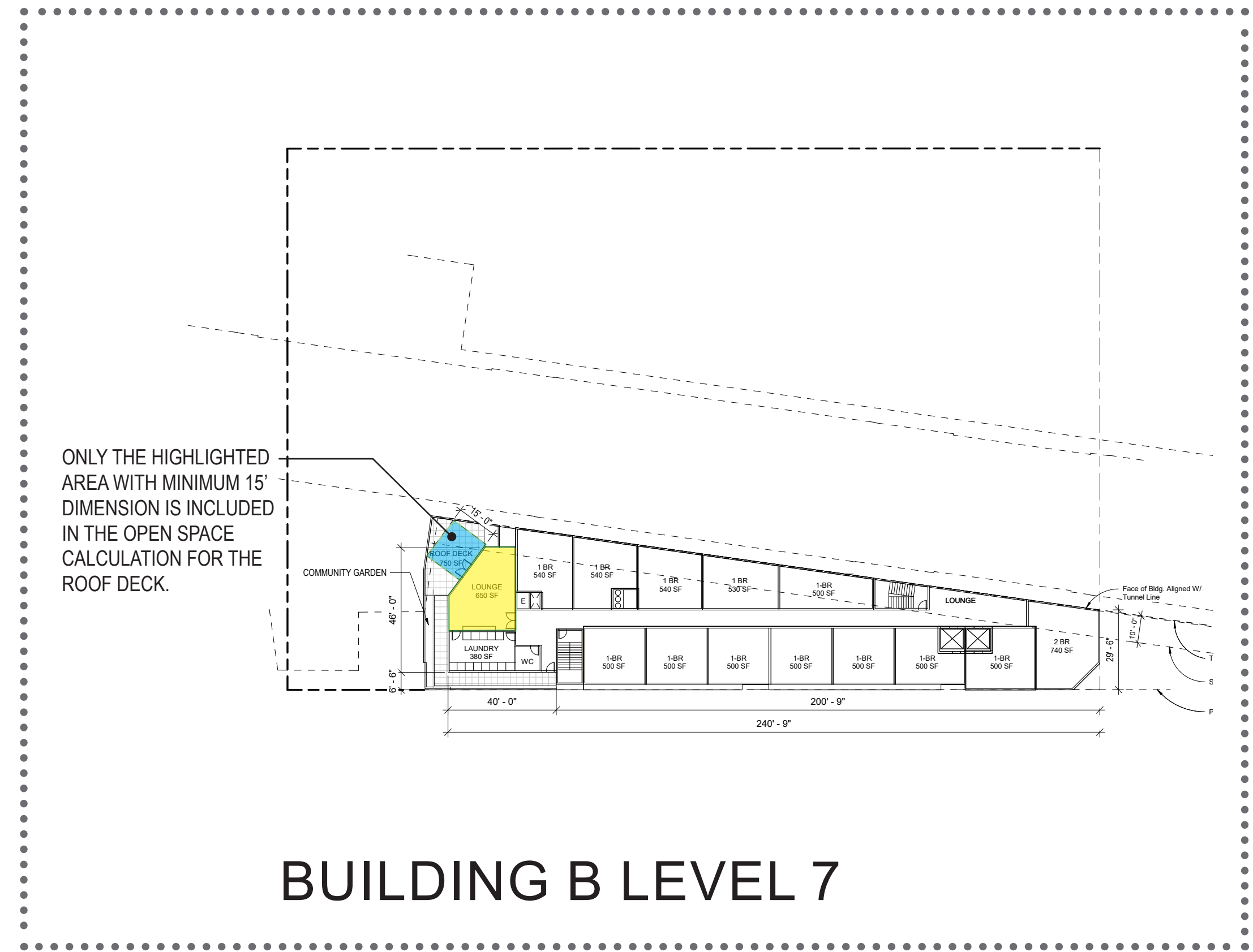
- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE

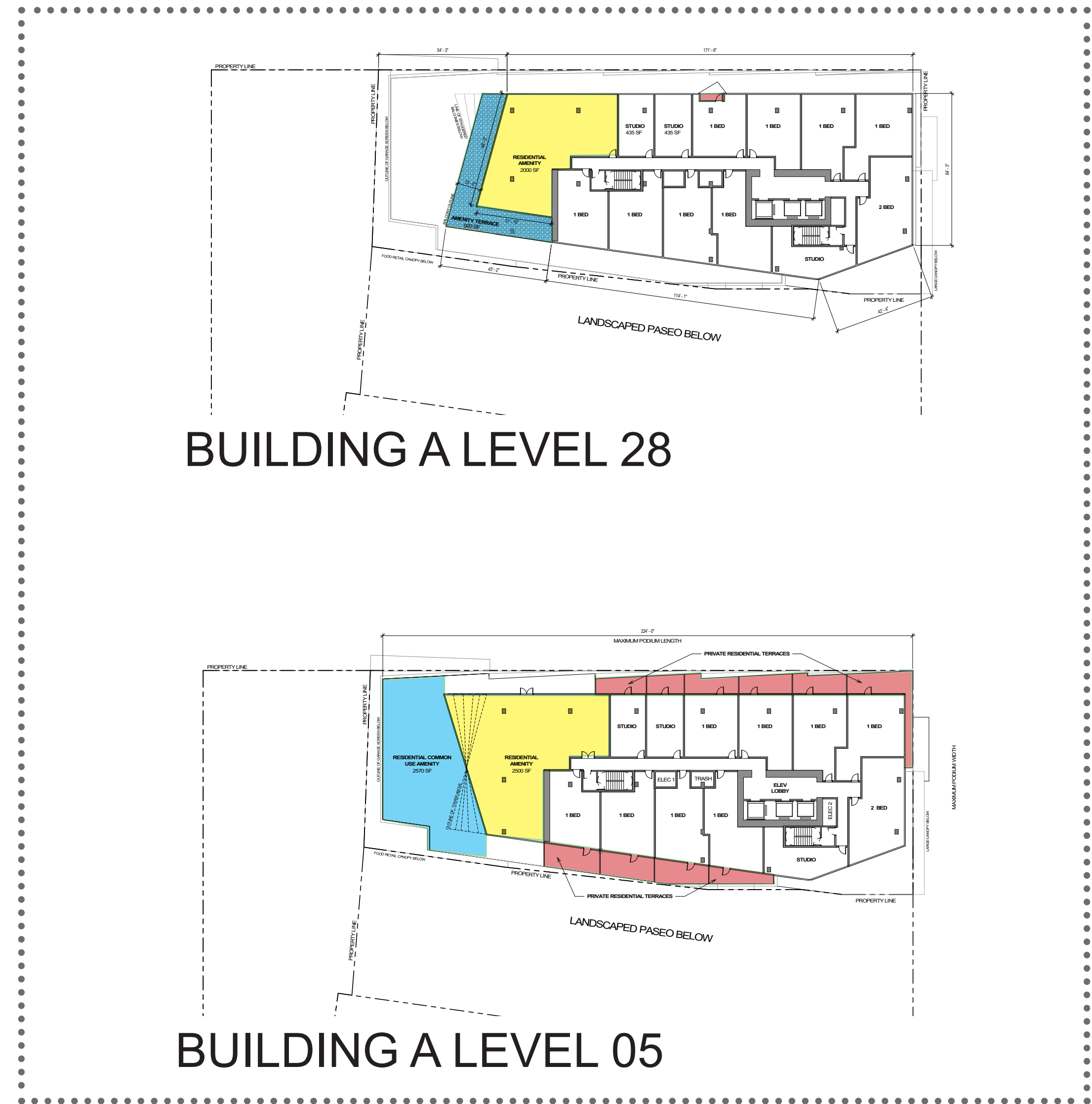
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPR COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPRZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPRFS COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPRNF COMMENTS	03/19/2021



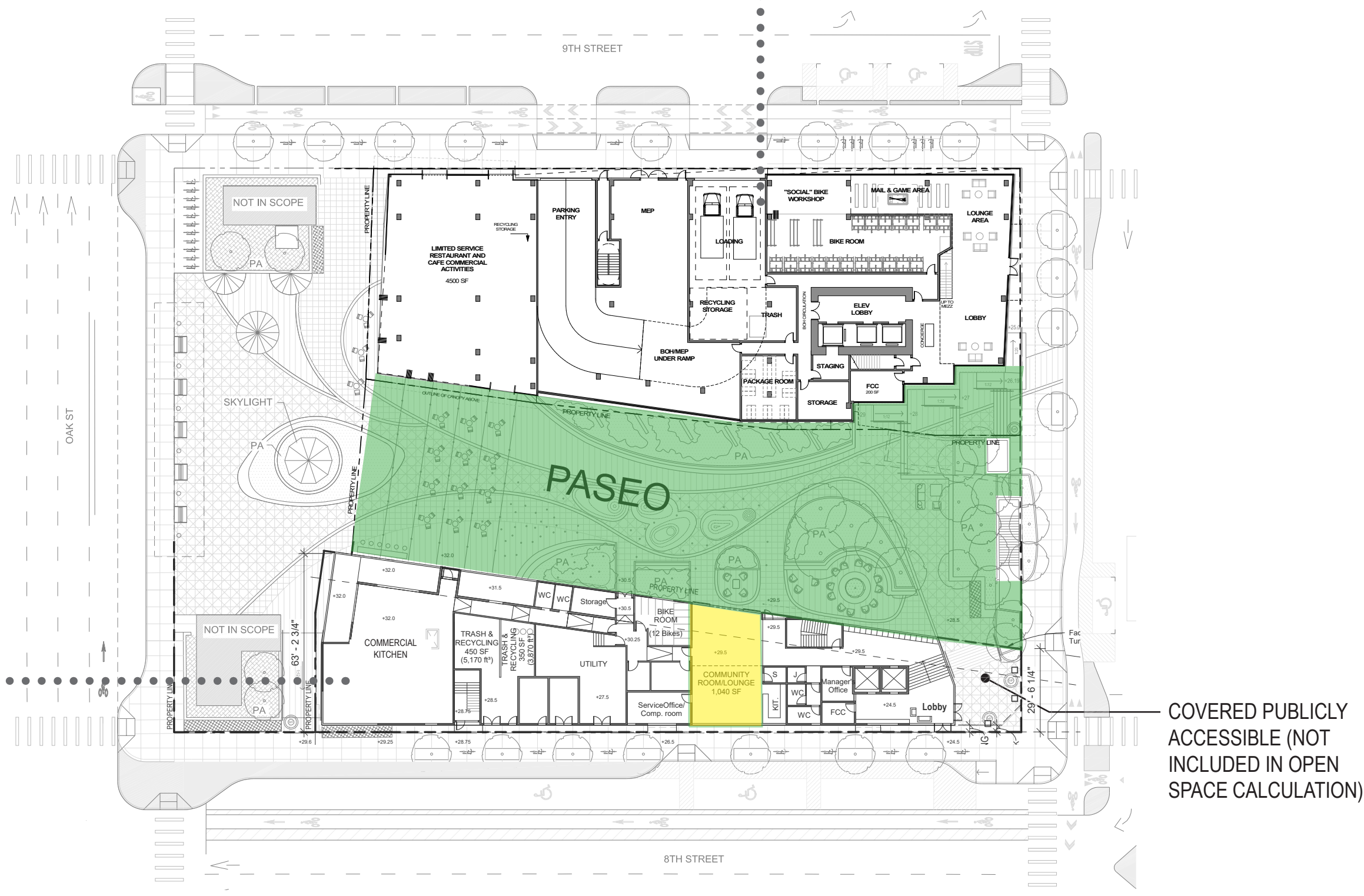
DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 1 OPEN SPACE EXHIBIT**



**BUILDING B LEVEL 7**



- PUBLICLY ACCESSIBLE OPEN SPACE
- GROUP USEABLE INDOOR OPEN SPACE (FOR BLDG OCCUPANTS)
- GROUP USEABLE OPEN SPACE (FOR BLDG OCCUPANTS)
- PRIVATE USEABLE OPEN SPACE (FOR UNIT OCCUPANTS)



		BUILDING A	BUILDING B	TOTAL
PUBLICLY ACCESSIBLE OPEN SPACE	A. BART Plaza (publicly owned)			11,610
	B. Paseo (minus exit stair & exhaust structure)	12,609	3,152	15,761
	C. Publicly Accessible Open Space on Building Parcel	305		305
GROUP USEABLE INDOOR OPEN SPACE (FOR BUILDING OCCUPANTS)		4,500	1,690	6,190
GROUP USEABLE OPEN SPACE (FOR BLDG OCCUPANTS)		3,490	250	3,740
PRIVATE USEABLE OPEN SPACE (FOR UNIT OCCUPANTS)		12,900	-	12,900
TOTAL		33,804	5,092	50,506

**NOTE:**

\*Publicly Accessible Open Space = Paseo (minus BART exit stair & Exhaust Shaft)

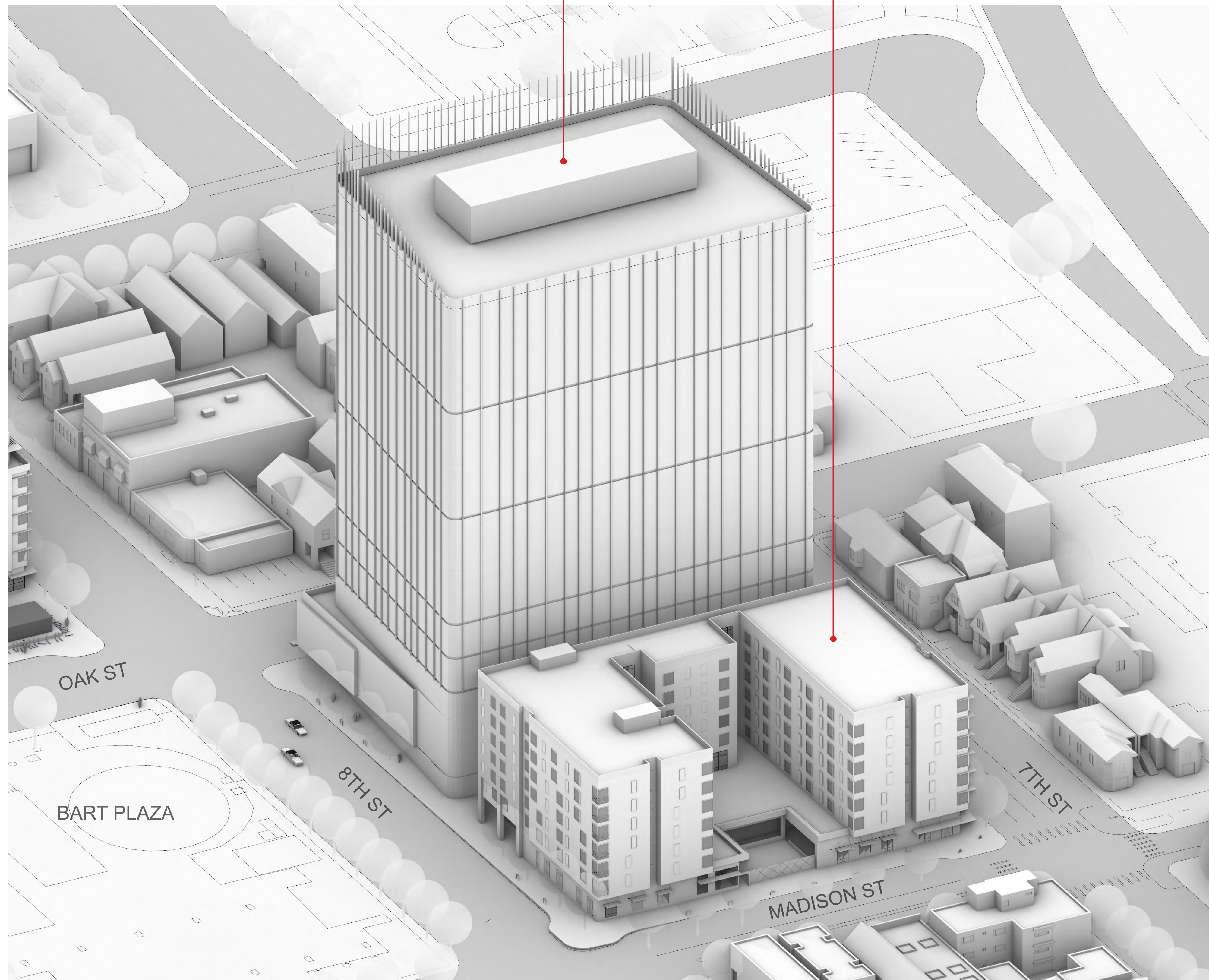
\*\* 20% of the Paseo is used for Bldg. B open space calc.

\*\*\* 80% of the Paseo is used for Bldg. A open space calc.

\*\*\*\* Covered entry plaza on Parcel B is not counted as Publicly Accessible Open Space.

BUILDING C:  
OFFICE

BUILDING D:  
AFFORDABLE  
HOUSING



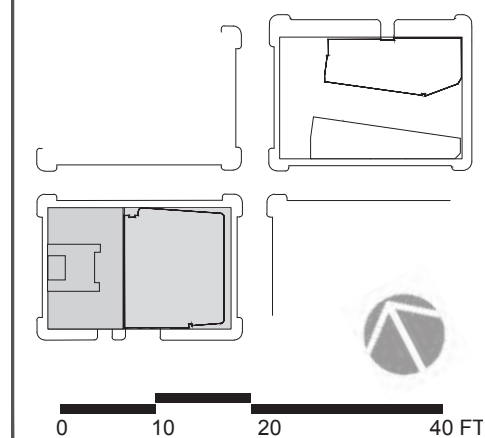
**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

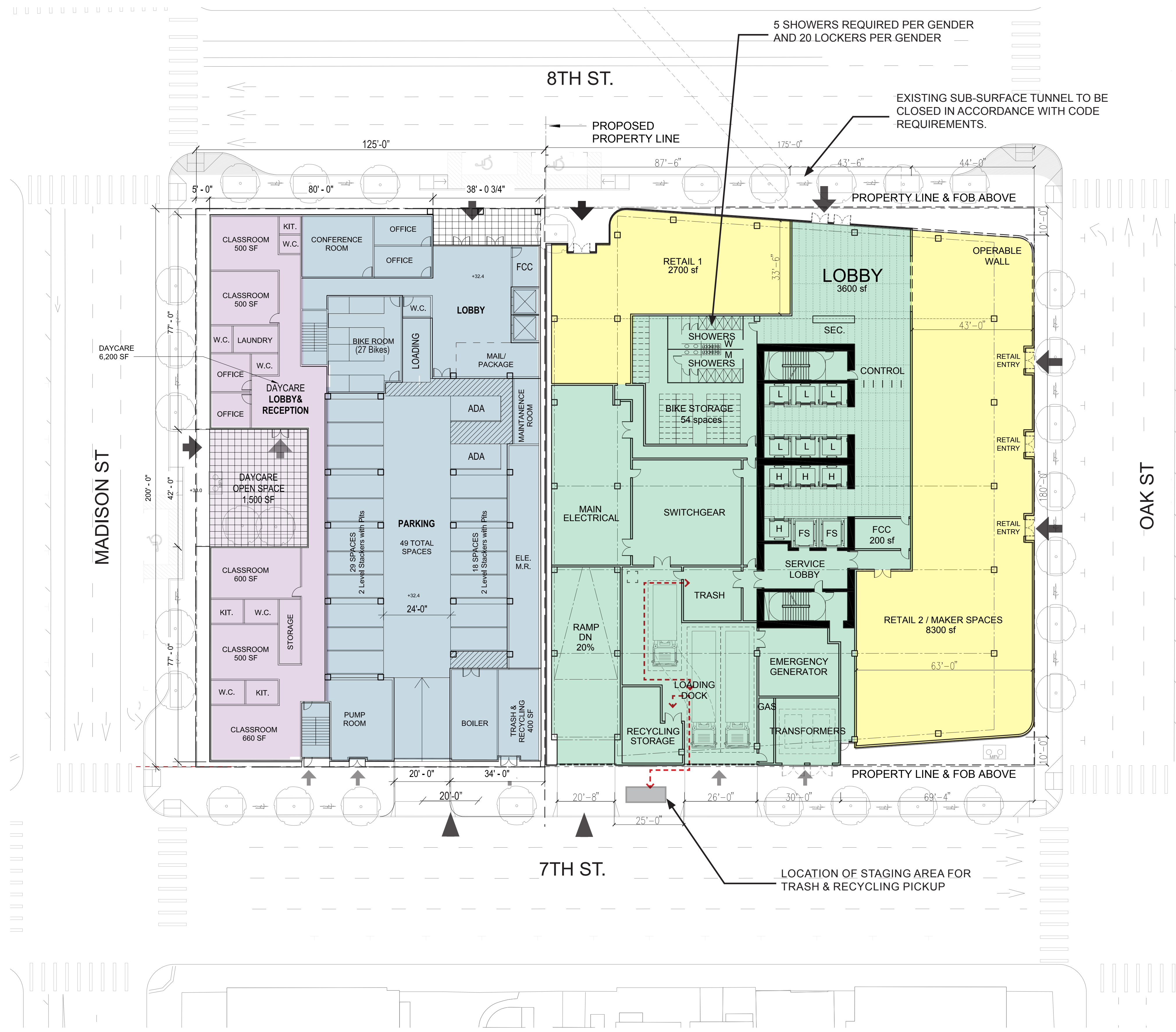
REVISION SCHEDULE

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPHZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPHZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPHZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPHZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 2  
MASSING VIEWS**



5 SHOWERS REQUIRED PER GENDER AND 20 LOCKERS PER GENDER

EXISTING SUB-SURFACE TUNNEL TO BE CLOSED IN ACCORDANCE WITH CODE REQUIREMENTS.

BIKES REQUIRED:  
OFFICE 52 LT  
OFFICE 26 ST  
RETAIL 2 LT  
RETAIL 2ST

- KEY LEGEND**
- ➡ PRIMARY PEDESTRIAN ACCESS
  - ➡ UTILITY/SERVICES OR EMERGENCY ACCESS
  - ➡ PARKING ENTRANCE

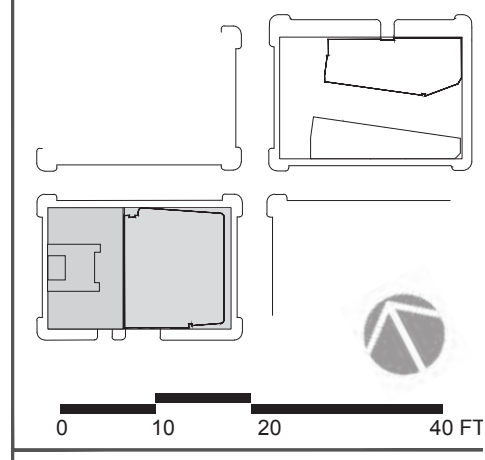
- LAND USE LEGEND**
- PERMANENT RESIDENTIAL ACTIVITIES 17.101G.01
  - BUSINESS ACTIVITIES 17.101G.01
  - GENERAL RETAIL & SALES ACTIVITIES 17.101G.01
  - DAYCARE (COMMUNITY EDUCATION CIVIC ACTIVITY) 17.142.100.A

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

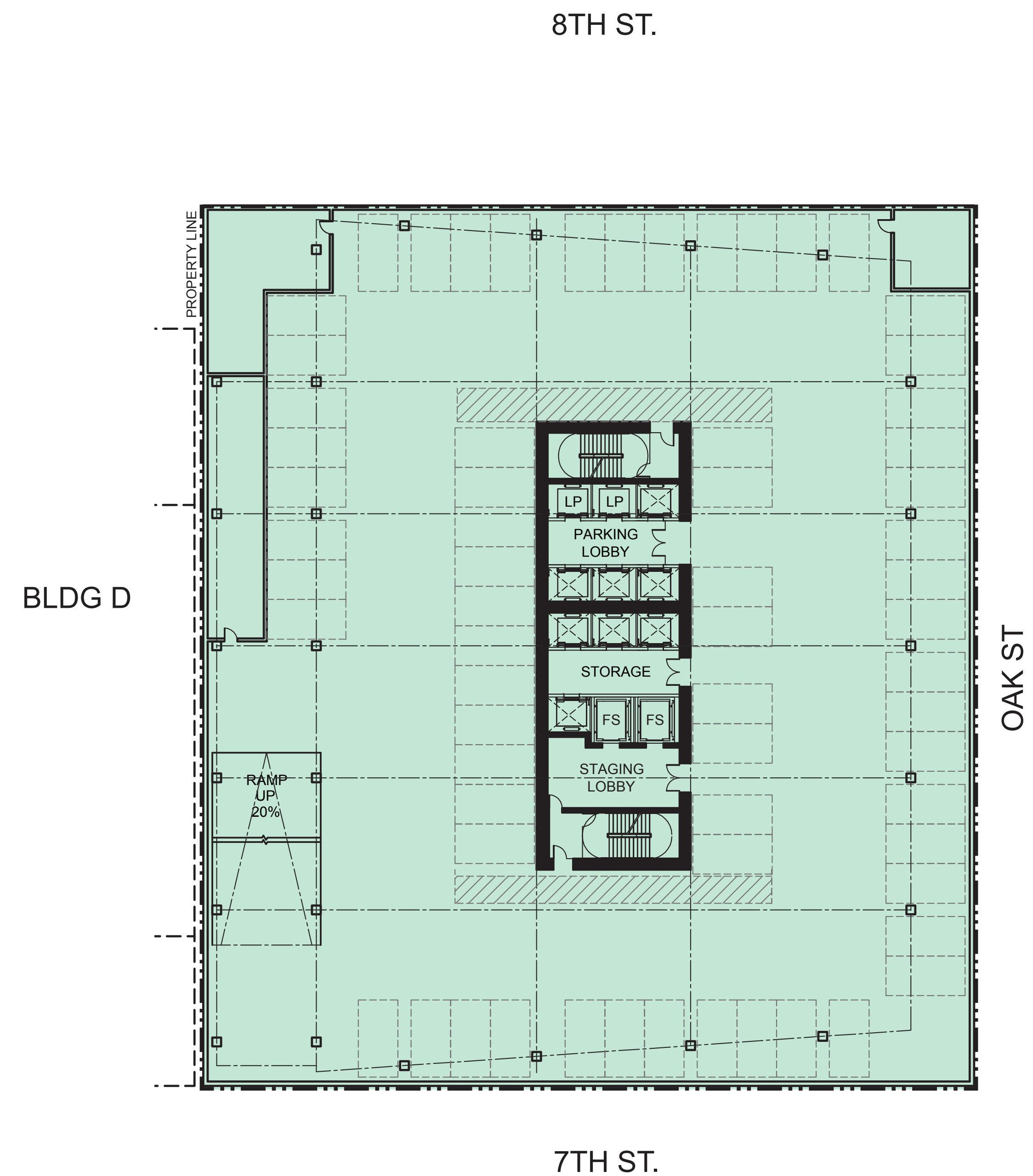
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

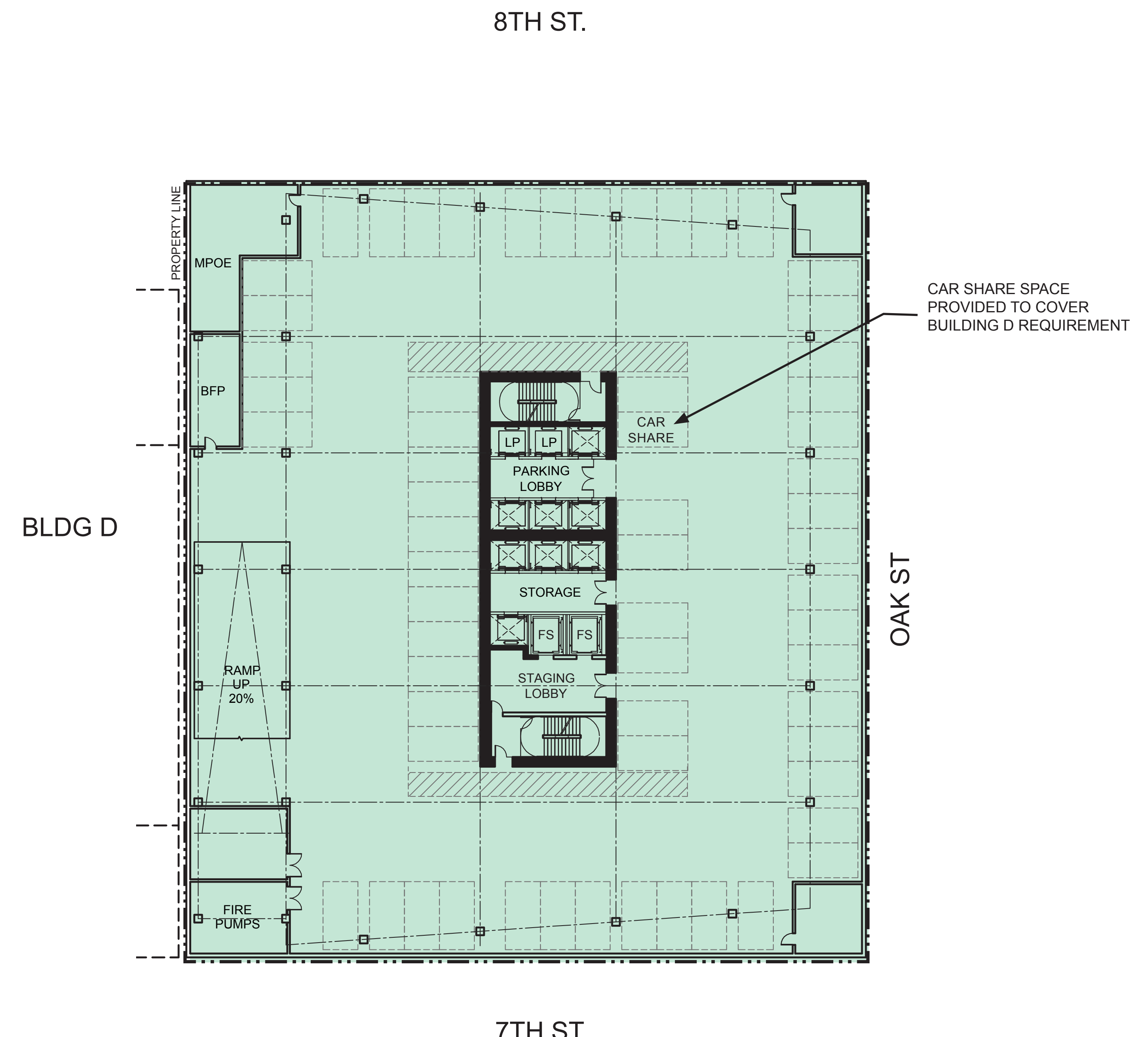
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPP COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPP COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPP COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPP COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 L1 FLOOR PLAN**



B2 PARKING PLAN



B1 PARKING PLAN

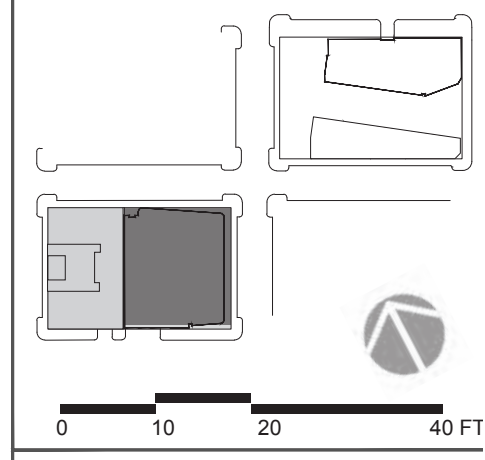
NOTE: MECHANICAL PARKING SYSTEMS TO BE EMPLOYED TO ACHIEVE 254 PARKING SPACES

**LAND USE LEGEND (PER TABLE 17.101G.01)**

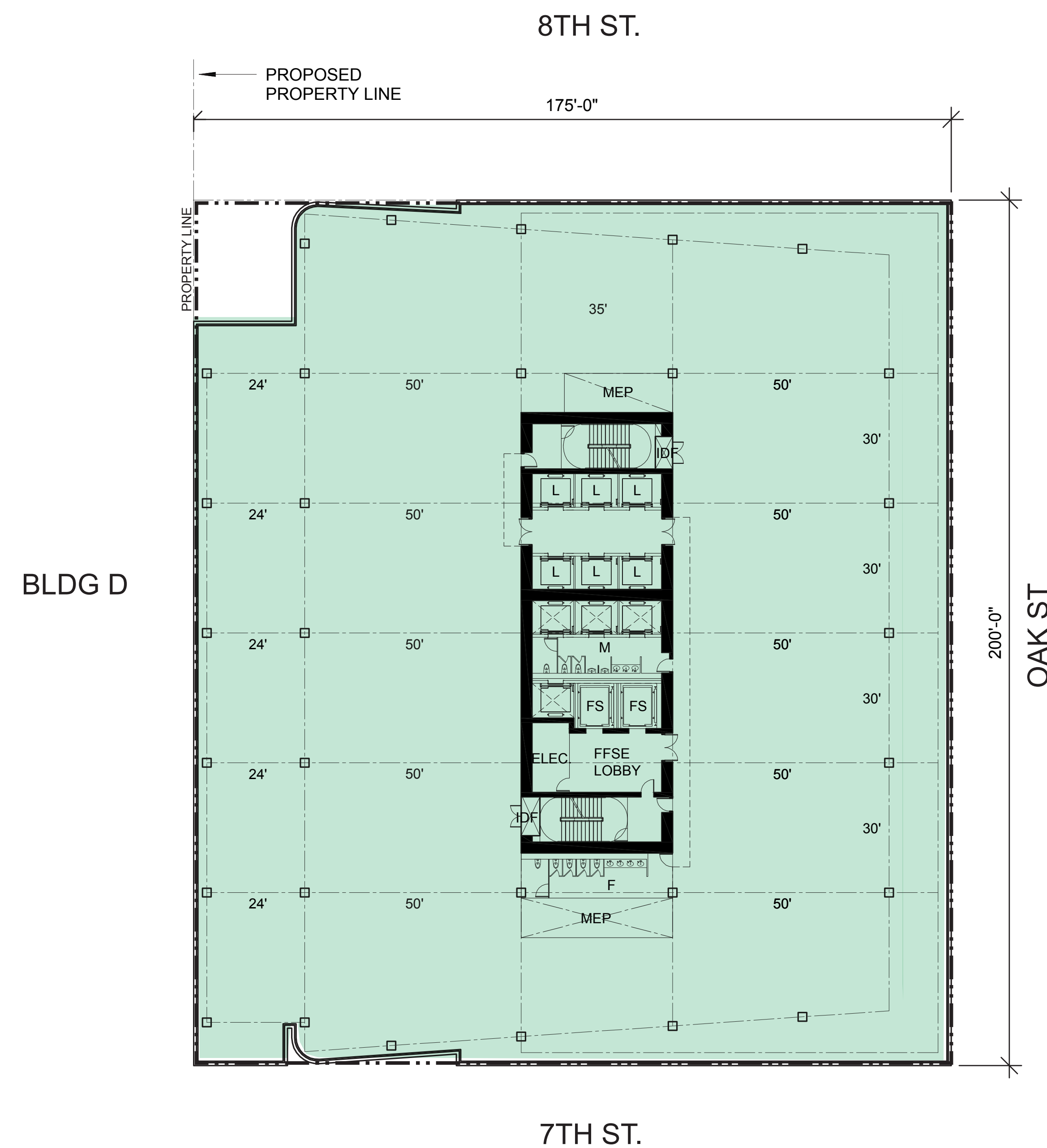
- PERMANENT RESIDENTIAL ACTIVITIES
- ADMINISTRATIVE/COMMERCIAL ACTIVITIES
- GENERAL RETAIL & SALES ACTIVITIES

PRELIMINARY - NOT FOR CONSTRUCTION -

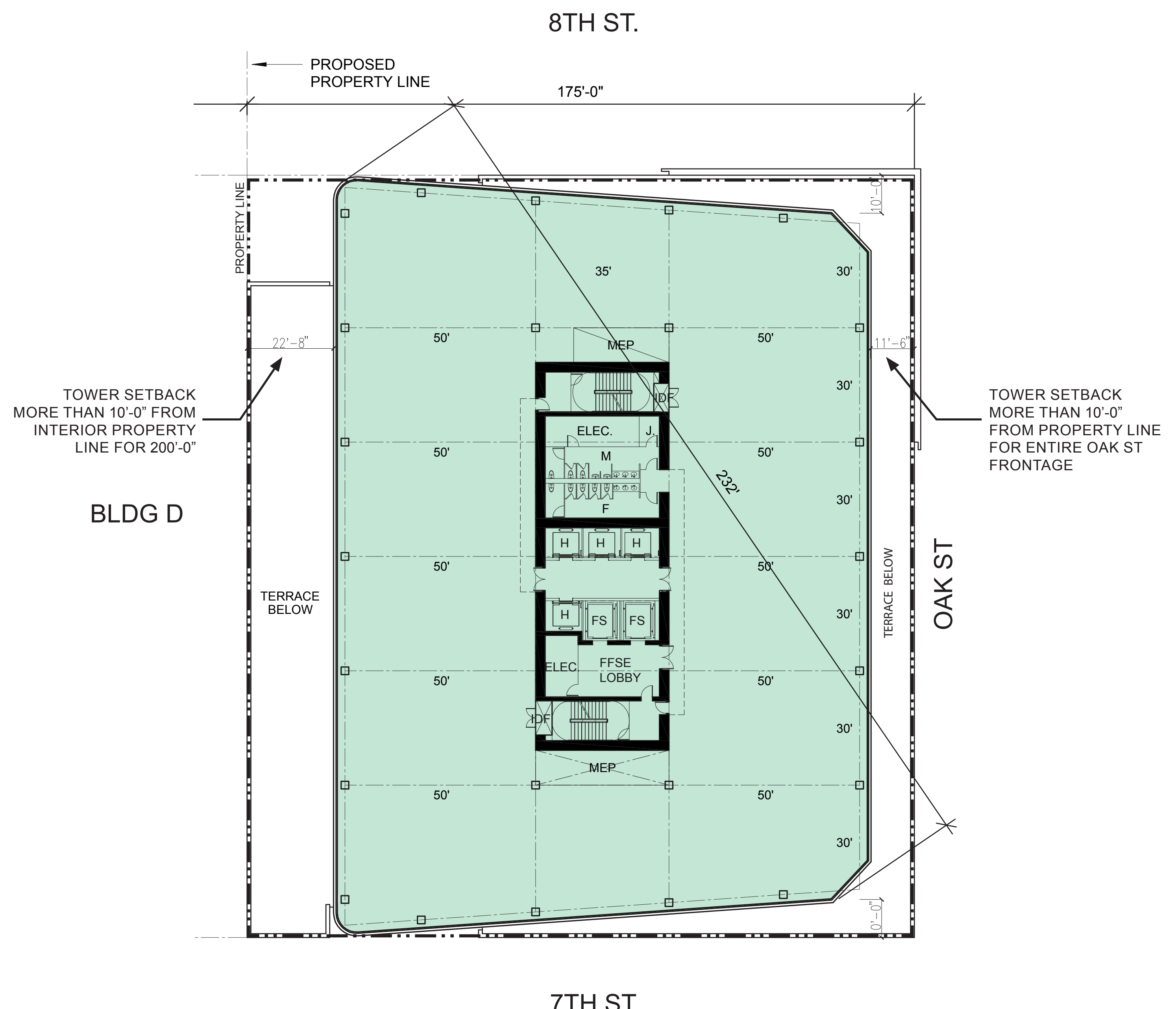
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPKZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021



BLOCK 2 / BLDG C  
TYPICAL FLOOR PLANS



FLOORS 2-3 TYPICAL PLAN



FLOORS 4-12 TYPICAL PLAN

TOWER SETBACKS AT INTERIOR LOT LINE (WEST) AND OAK STREET FRONTAGE (EAST) BY MINIMUM 10FT FOR MIN 50% OF PERIMETER LENGTH OF BUILDING BASED UPON GRANTING OF CUP.

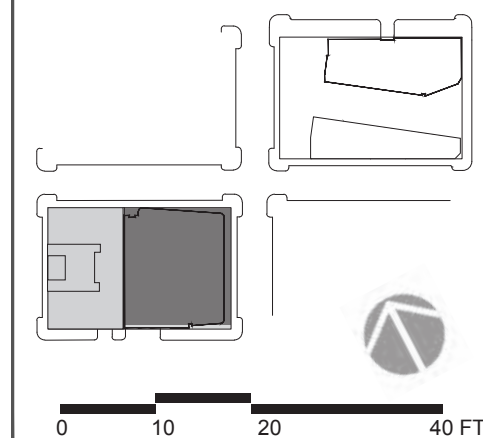
WEST SETBACK = 26.66% OF PERIMETER LENGTH OF BASE  
EAST SETBACK = 26.66% OF PERIMETER LENGTH OF BASE  
TOTAL SETBACK = 53.33% OF PERIMETER LENGTH OF BASE

**LAND USE LEGEND  
(PER TABLE 17.101G.01)**

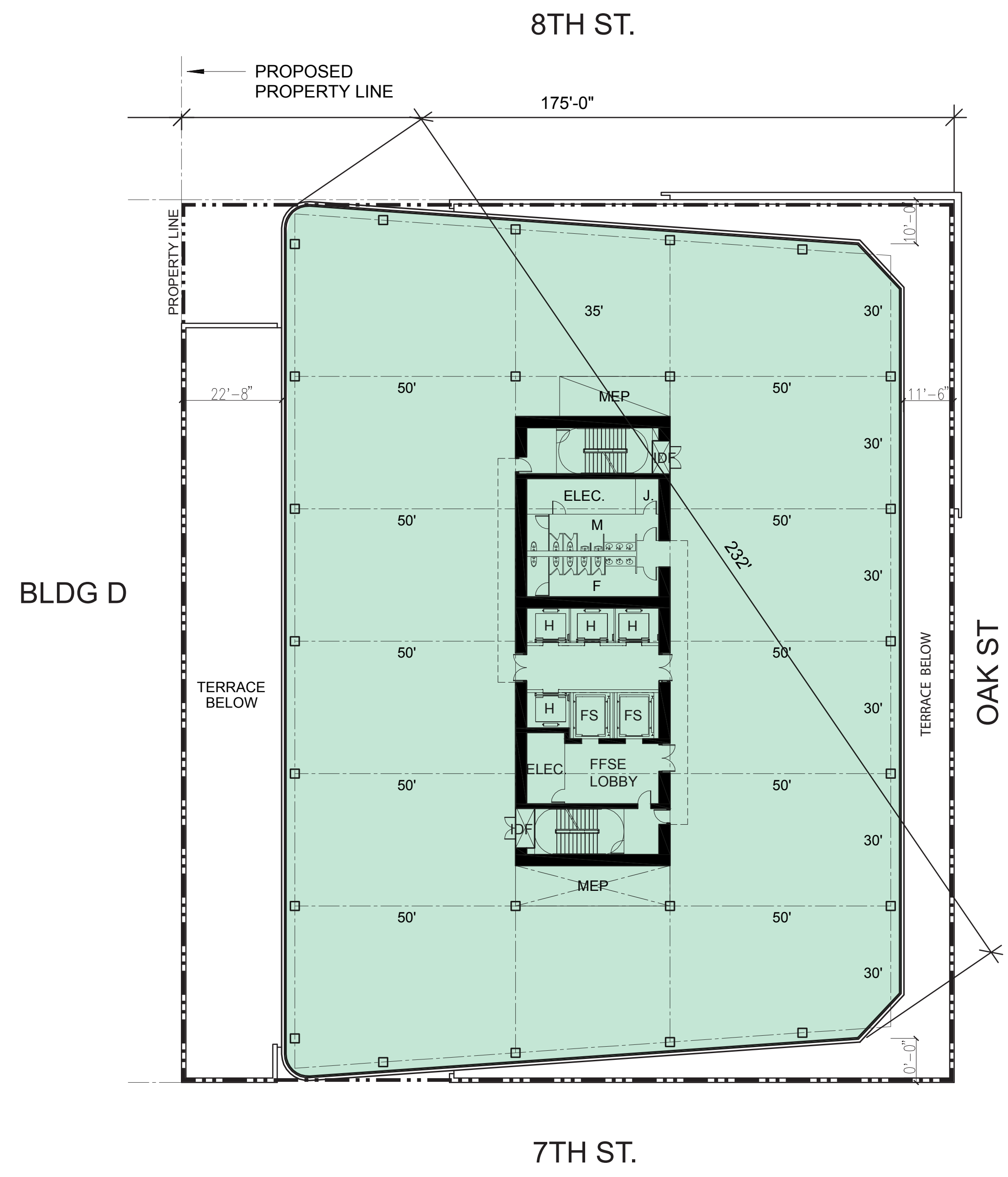
- PERMANENT RESIDENTIAL ACTIVITIES
- ADMINISTRATIVE/ COMMERCIAL ACTIVITIES
- GENERAL RETAIL & SALES ACTIVITIES

PRELIMINARY - NOT FOR CONSTRUCTION -

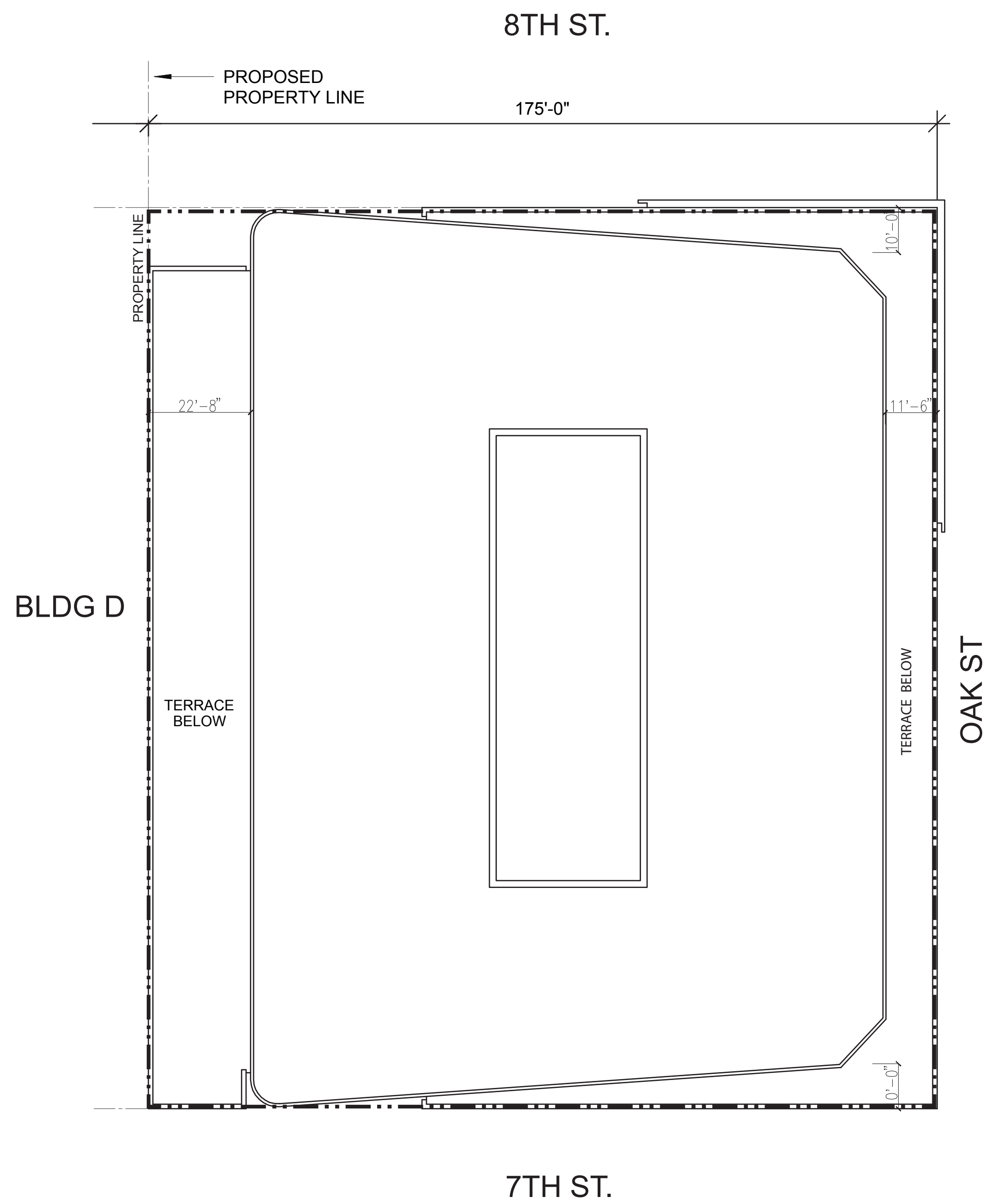
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO PDPA'S COMMENTS	6/8/2020
5	REVISED RESPONSE TO PDPA'S COMMENTS	10/02/2020
6	REVISED RESPONSE TO PDPA'S COMMENTS	02/22/2021
7	REVISED RESPONSE TO PDPA'S COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
BLOCK 2 / BLDG C  
TYPICAL FLOOR  
PLANS



FLOORS 13-19 TYPICAL PLAN



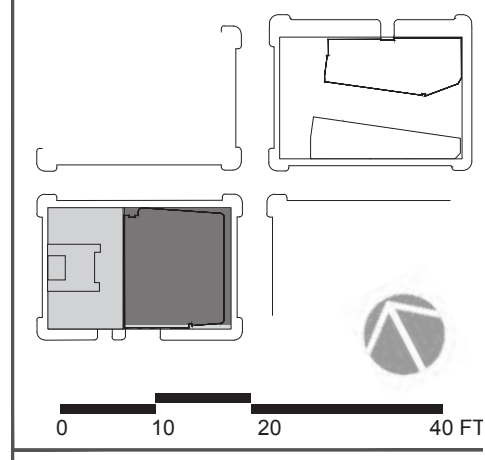
ROOF PLAN

**LAND USE LEGEND  
(PER TABLE 17.101G.01)**

- PERMANENT RESIDENTIAL ACTIVITIES
- ADMINISTRATIVE/  
COMMERCIAL ACTIVITIES
- GENERAL RETAIL & SALES ACTIVITIES

PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPKZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C  
FLOORS 13-19 &  
ROOF PLAN**

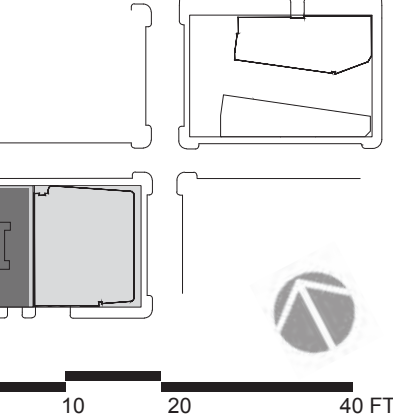


**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

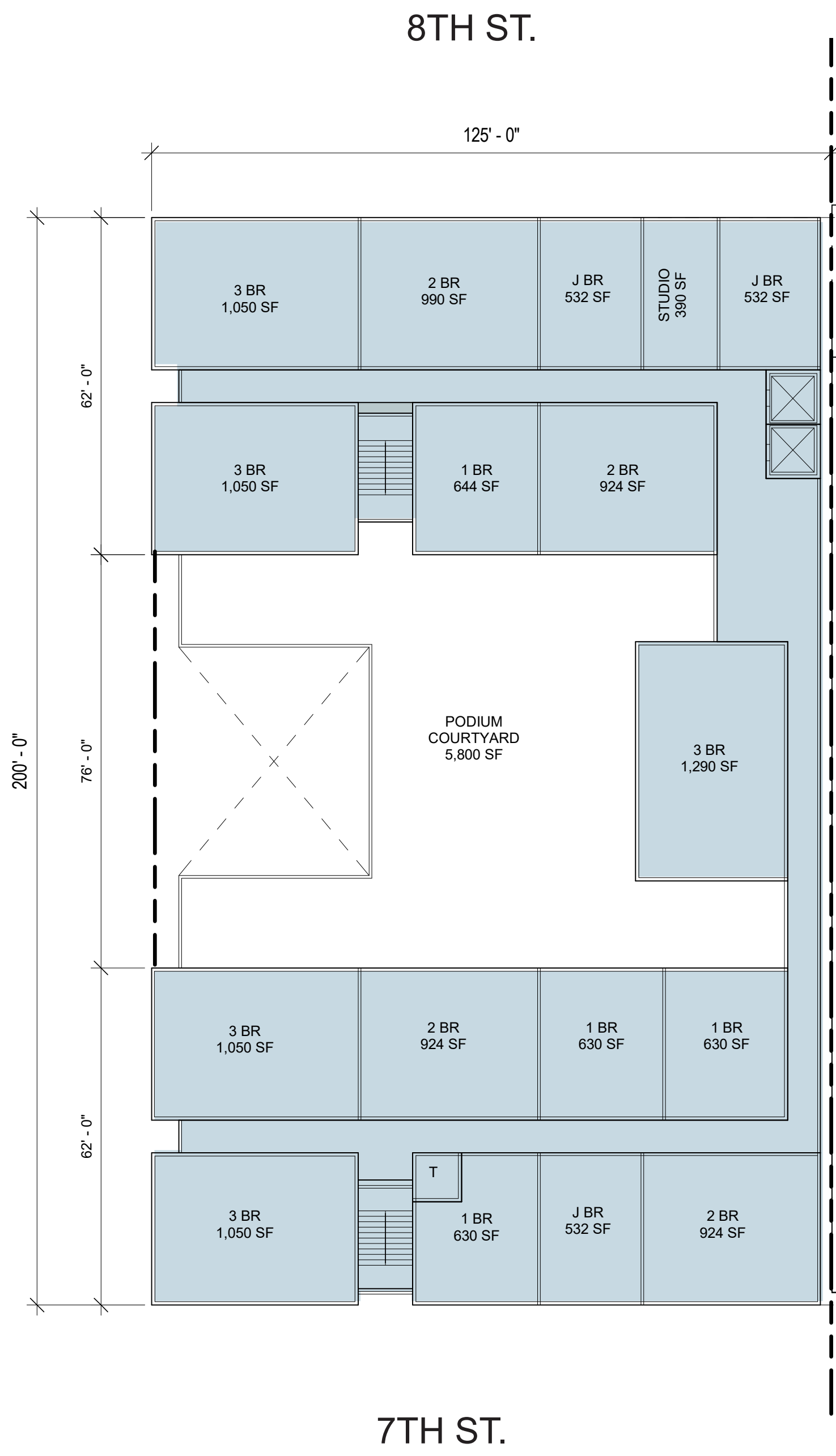
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

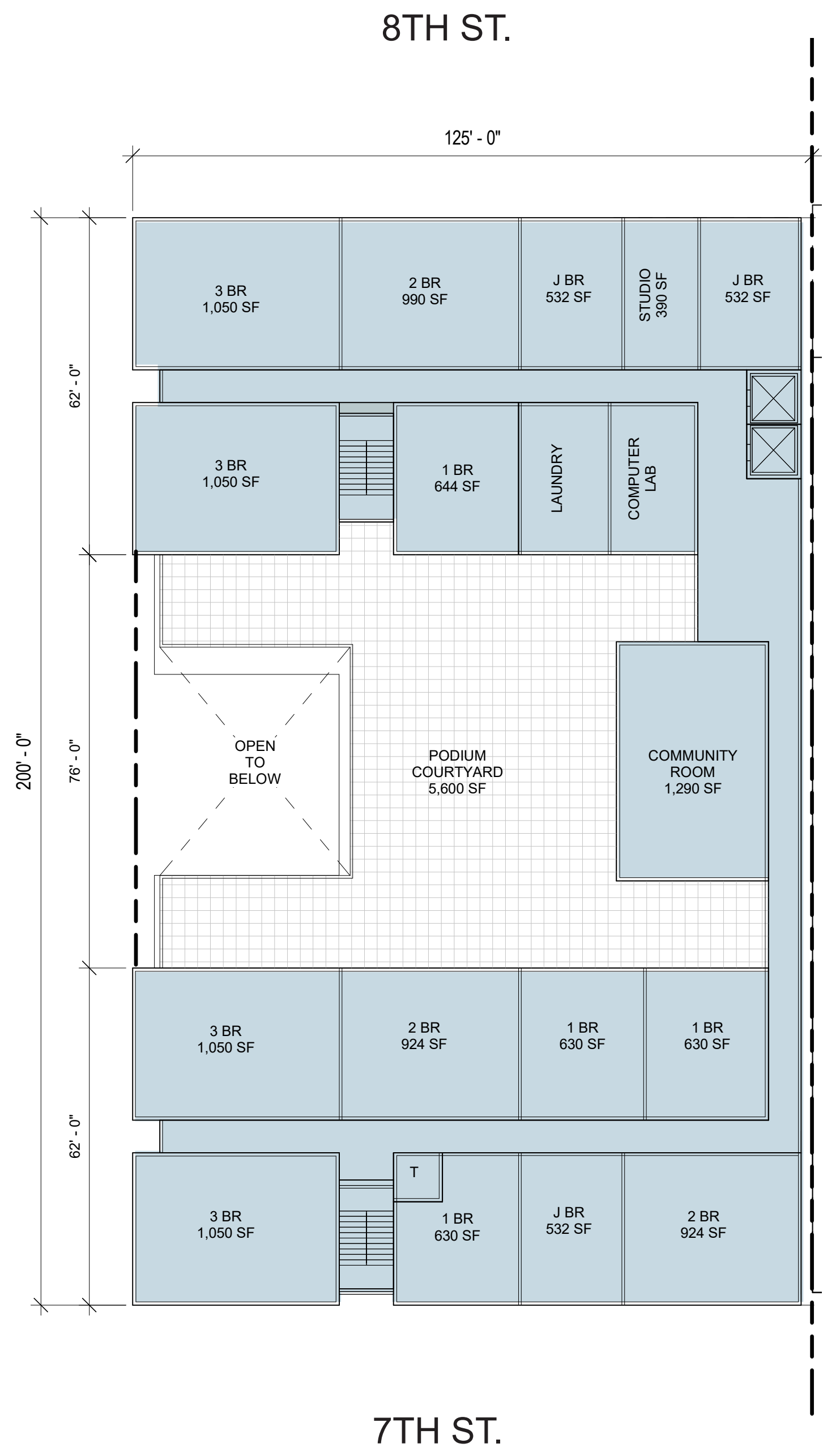
REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
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5	REVISED RESPONSE TO POPRZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPRZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPRZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG D UPPER FLOOR PLANS**



**TYPICAL RESIDENTIAL FLOOR PLAN (L3-7)**



**RESIDENTIAL FLOOR PLAN AT PODIUM LEVEL (L2)**

OFFICE BLDG

OFFICE BLDG

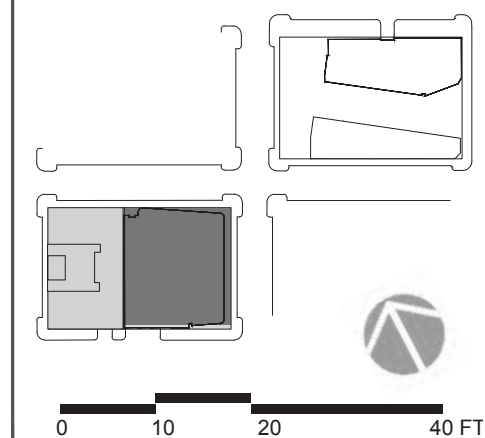
**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

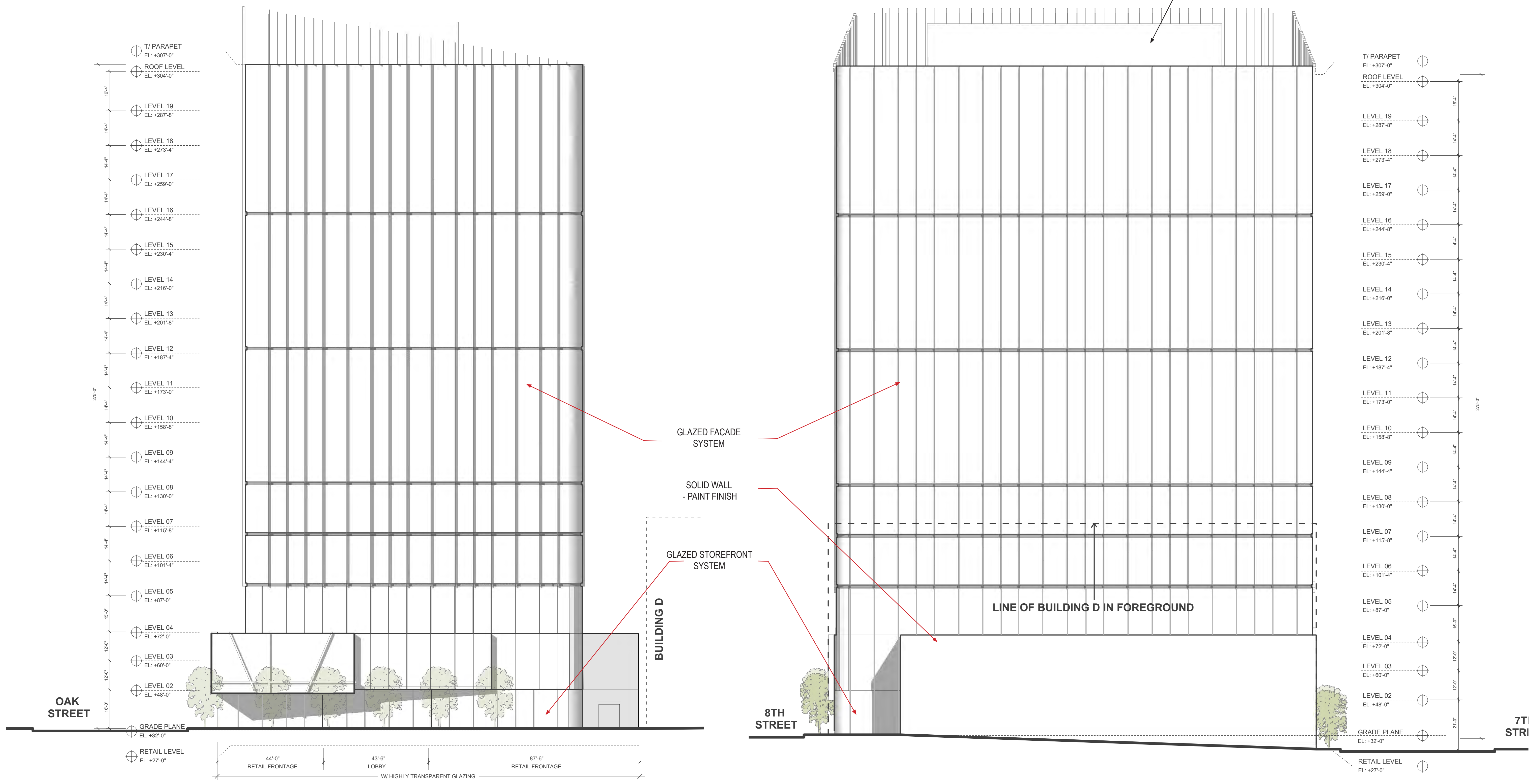
REVISION SCHEDULE

NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPKZ COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPKZ COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPKZ COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPKZ COMMENTS	03/19/2021



DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C ELEVATIONS**

SIZE, SPECIFICATION, ACCESS, AND LOCATION OF ROOF MOUNTED EQUIPMENT TO BE APPROVED BY BART AT A LATER PHASE OF DESIGN



**NORTH ELEVATION**

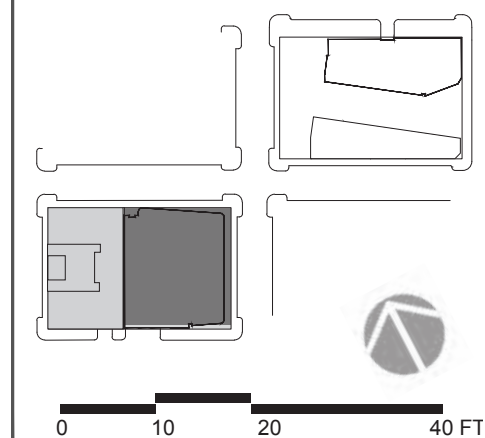
**WEST ELEVATION**

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

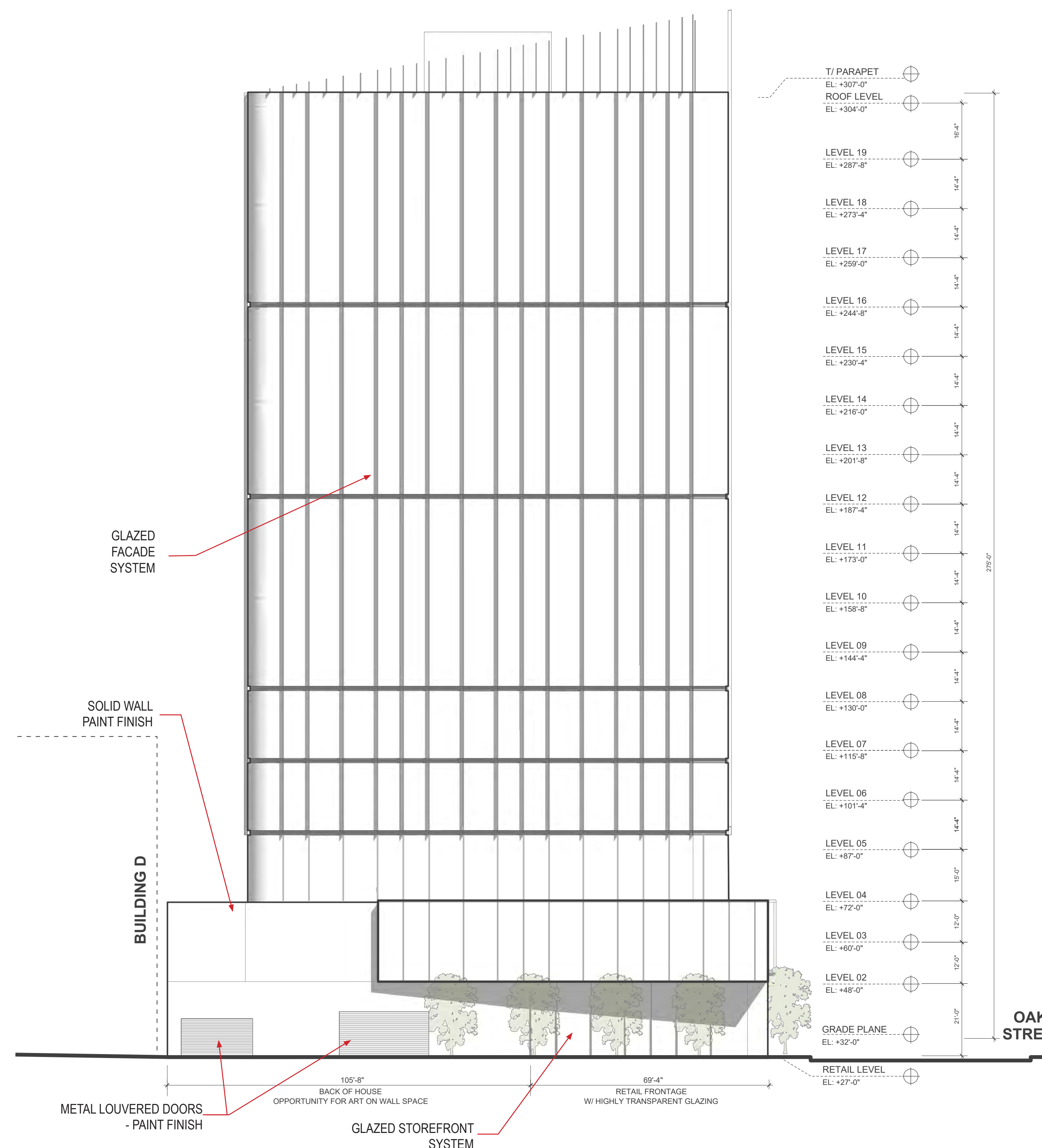
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

PRELIMINARY - NOT FOR CONSTRUCTION -

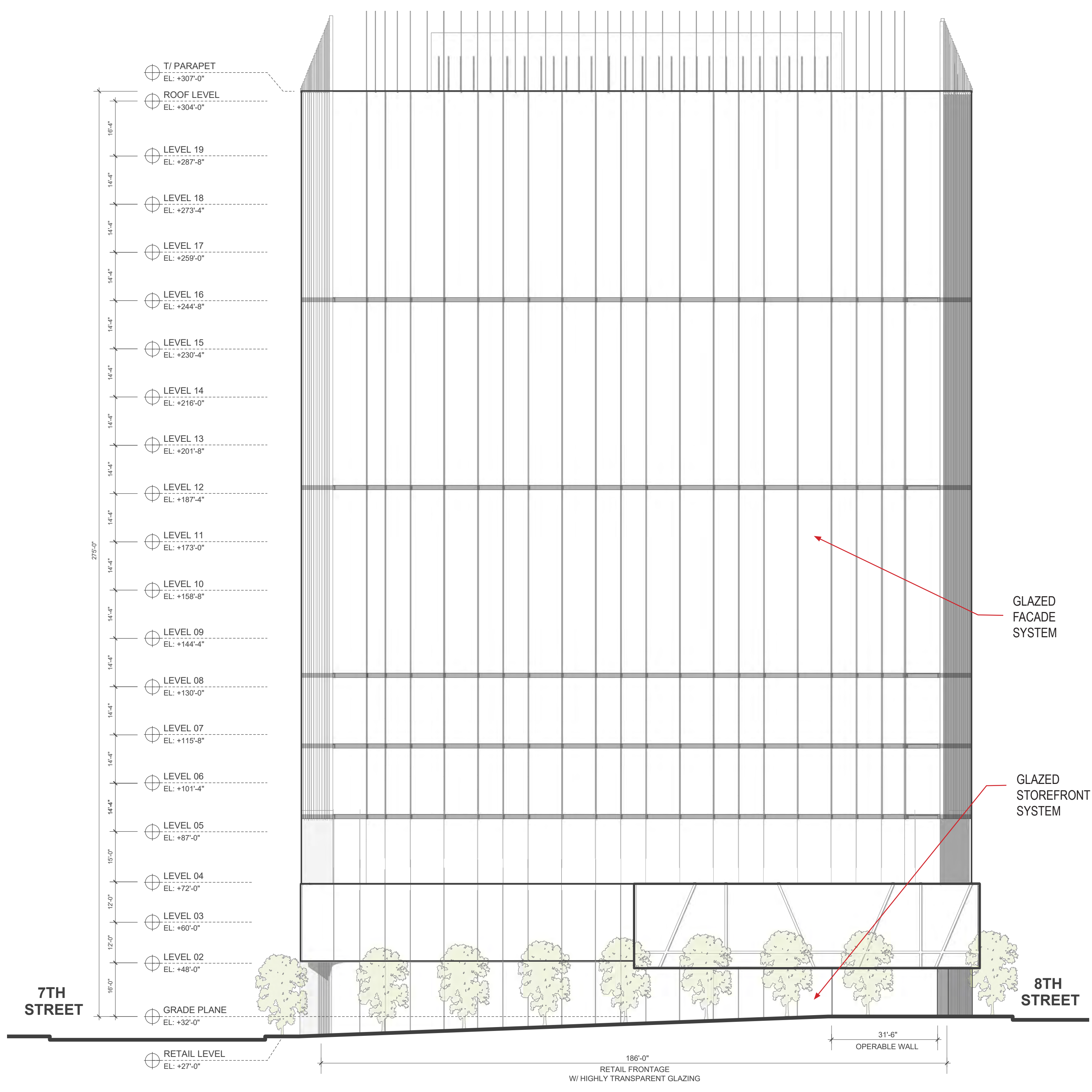
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
2	RESPONSES TO COMMENTS FROM BART	1/20/2020
3	PDP SUBMITTAL #1 TO CITY	2/12/2020
4	REVISED RESPONSE TO POPK COMMENTS	6/8/2020
5	REVISED RESPONSE TO POPK COMMENTS	10/02/2020
6	REVISED RESPONSE TO POPK COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPK COMMENTS	03/19/2021



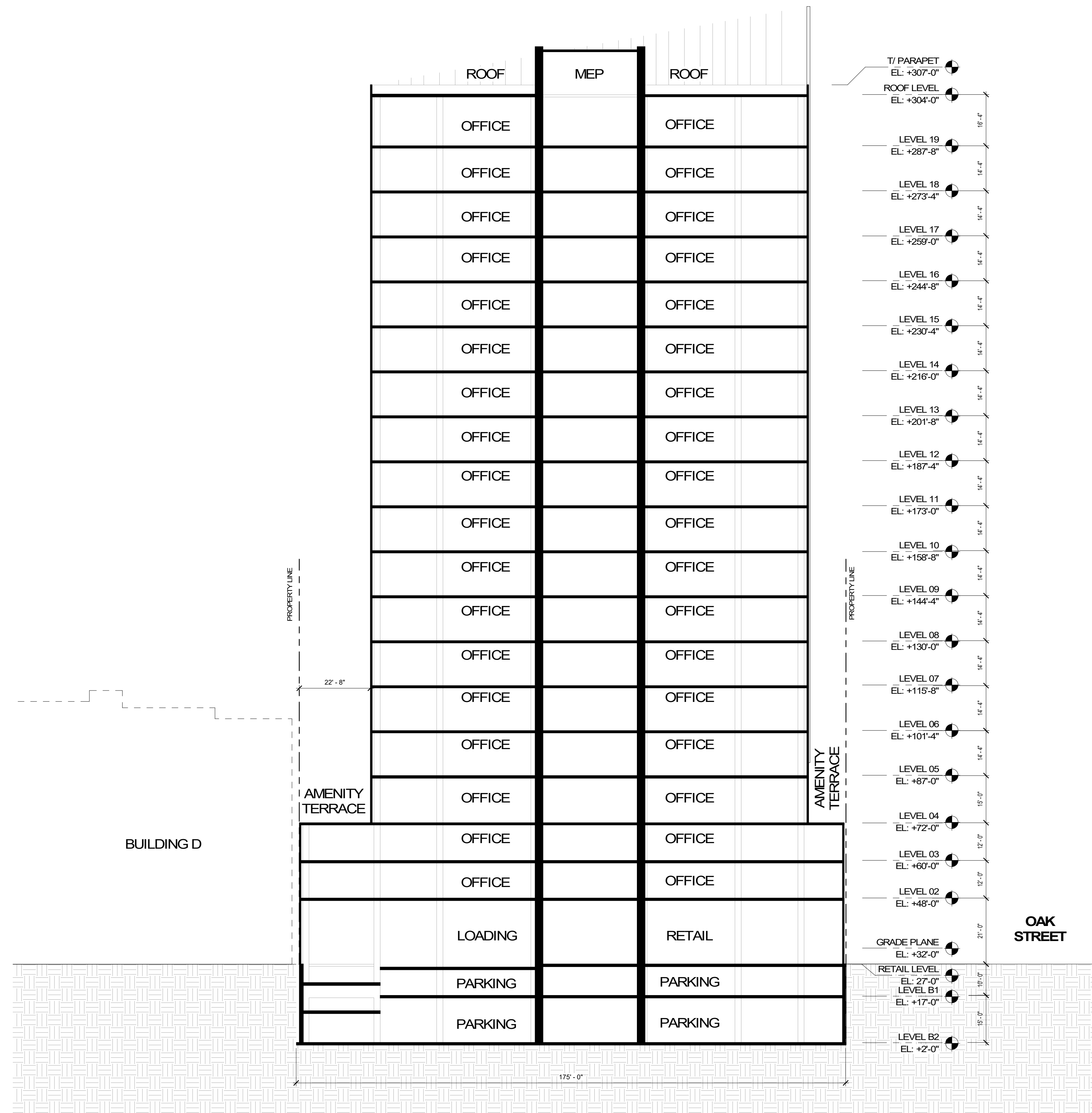
DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C ELEVATIONS**



**SOUTH ELEVATION**



**EAST ELEVATION**



EAST-WEST SECTION

**EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION**  
 1825 SAN PABLO AVE. #200  
 OAKLAND, CA 94612

**STRADA**  
 101 MISSION ST. #420  
 SAN FRANCISCO, CA 94105

**PYATOK**  
 1611 TELEGRAPH AVE. SUITE 200  
 OAKLAND, CA 94612  
 www.pyatok.com

**SCB**  
 SOLOMON CORDWELL BUENZ ARCHITECTS  
 335 CALIFORNIA ST.  
 SAN FRANCISCO, CA 94111  
 415/761-2600  
 www.scb.com

**INWILLERUEHL**  
 318 HARRISON ST. STE 301  
 OAKLAND, CA 94607  
 (510) 891-1696  
 www.inwilleruehl.com

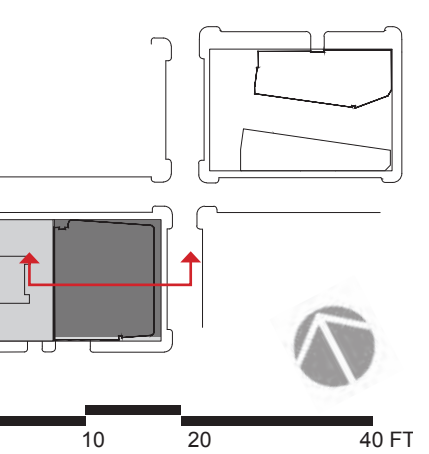
**BKF100+**  
 225 BUCKLAND DR. SUITE 200  
 REDWOOD CITY, CA 94063  
 (650) 483-6300  
 www.bkf.com

**LAKE MERRITT BART REDEVELOPMENT**  
 Oakland, CA 94607

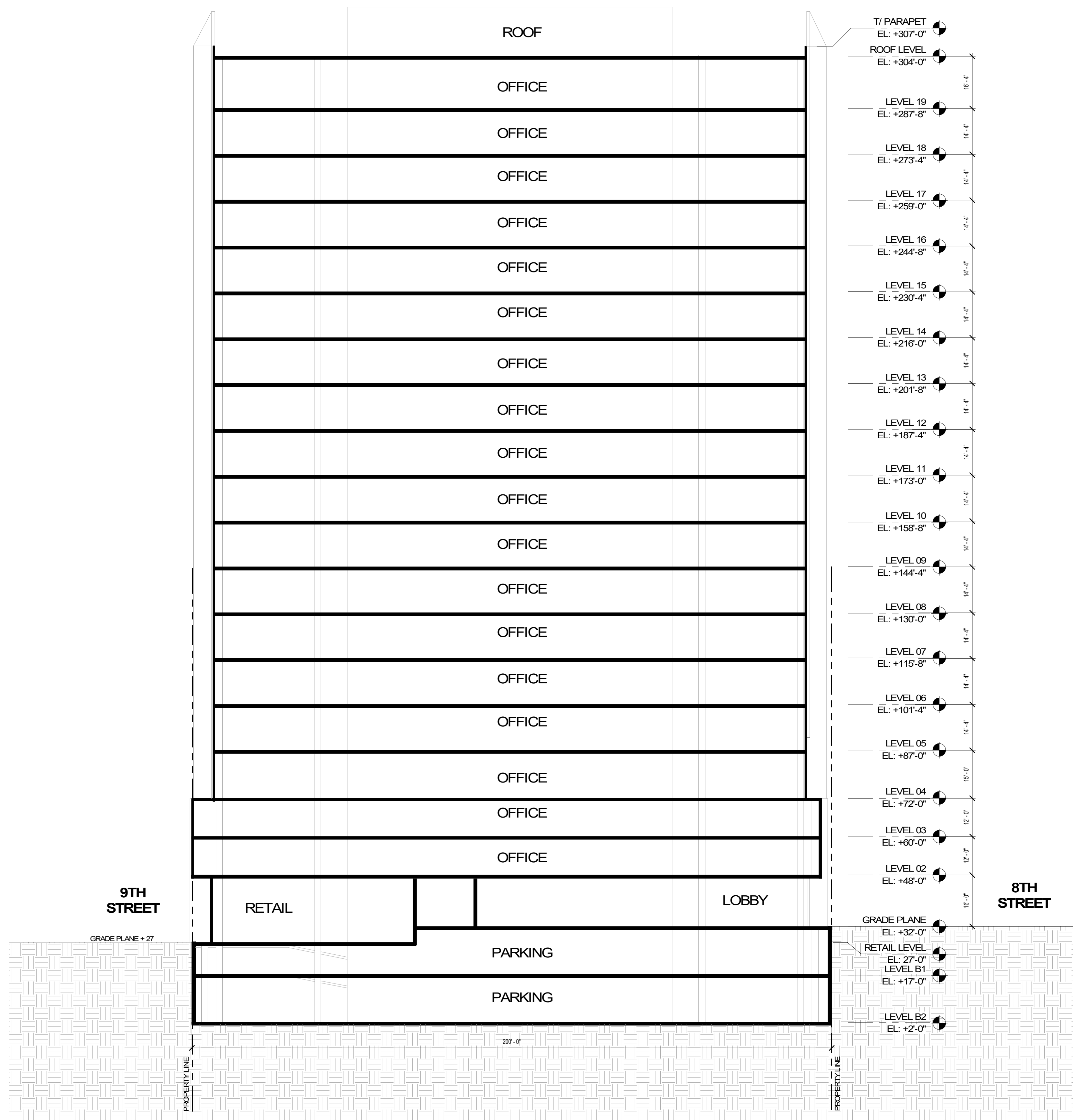
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

REVISION SCHEDULE		
NO.	ISSUE	DATE
1	RESPONSES TO COMMENTS FROM BART	11/15/2019
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6	REVISED RESPONSE TO POPH COMMENTS	02/22/2021
7	REVISED RESPONSE TO POPH COMMENTS	03/19/2021



DATE:  
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**BLOCK 2 EAST-WEST SECTION**



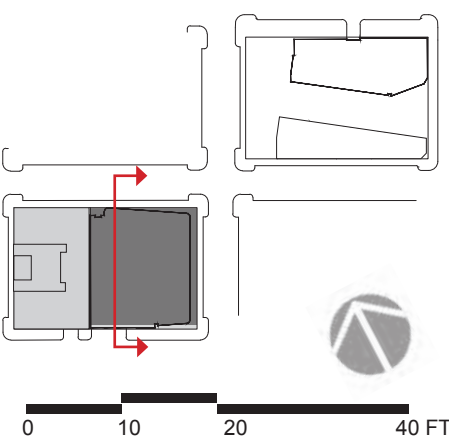
NORTH-SOUTH SECTION

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

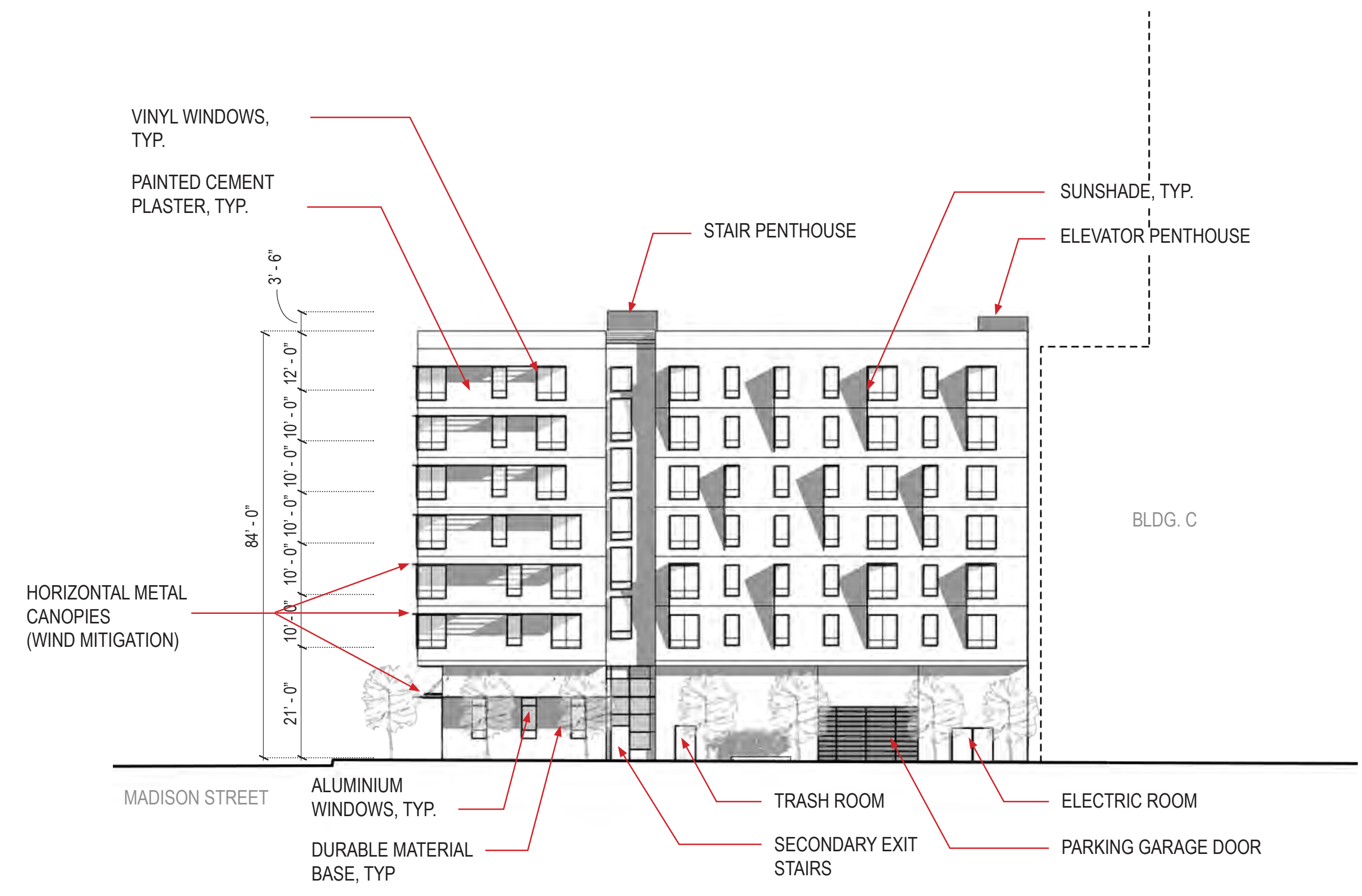
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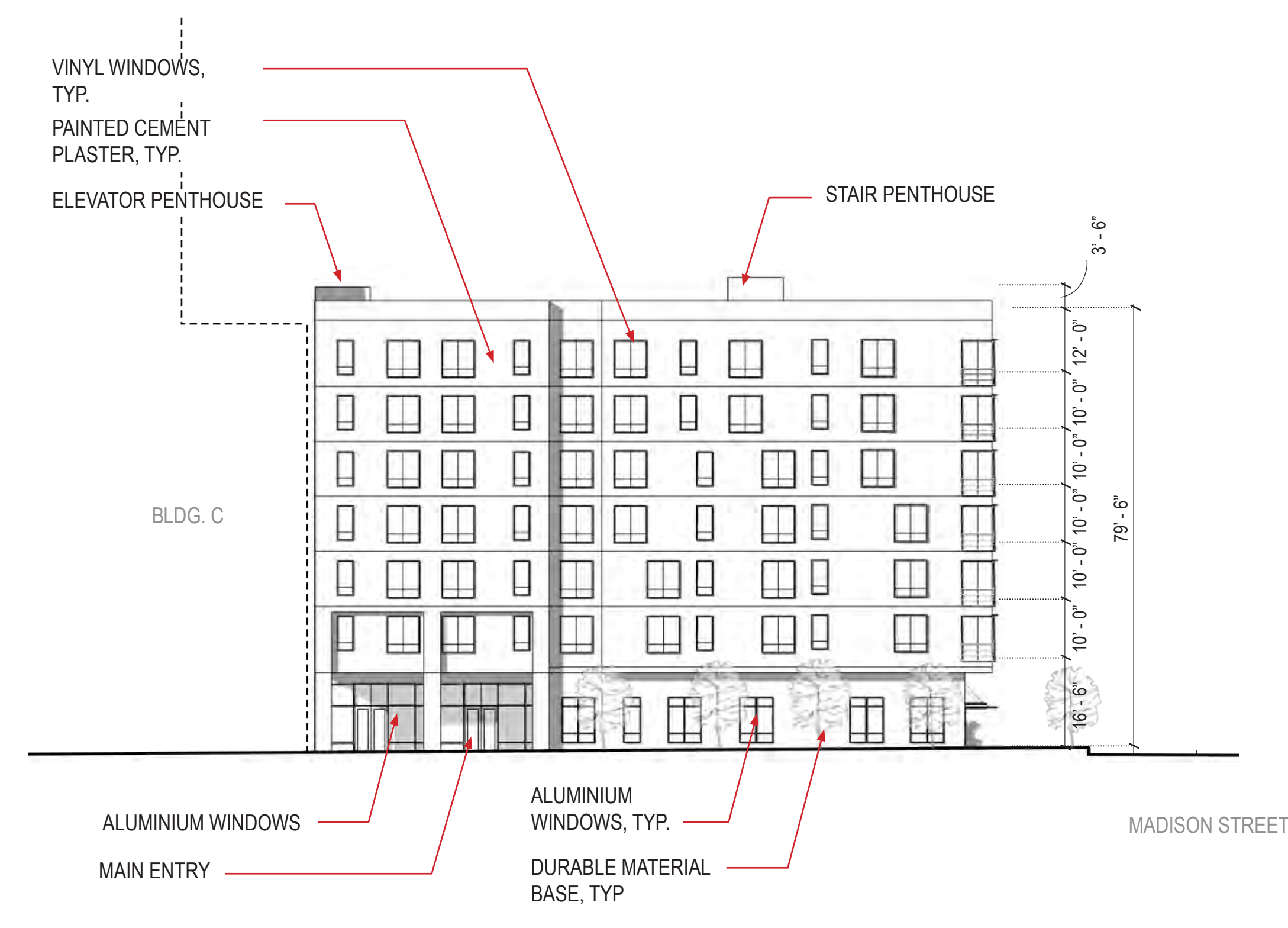


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SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG C NORTH-SOUTH SECTION**

NOTE: COLORS AND MATERIALS TO BE FINALIZED AT FDP PHASE.



2. BUILDING D SOUTH ELEVATION



1. BUILDING D NORTH ELEVATION



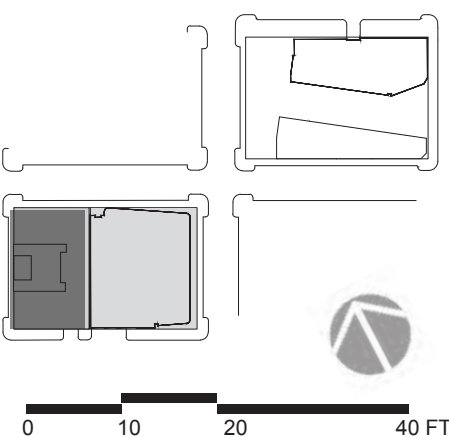
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**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

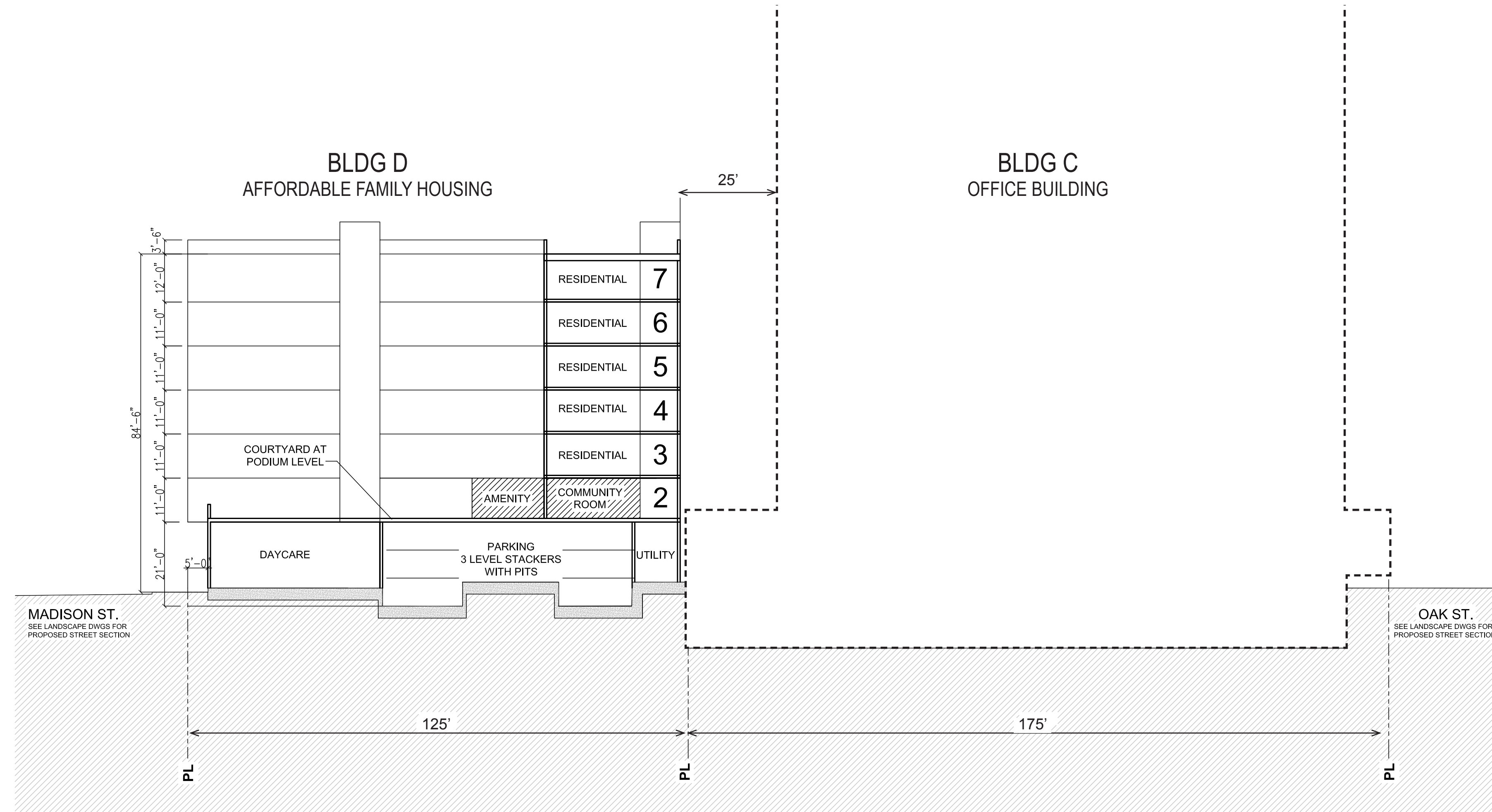
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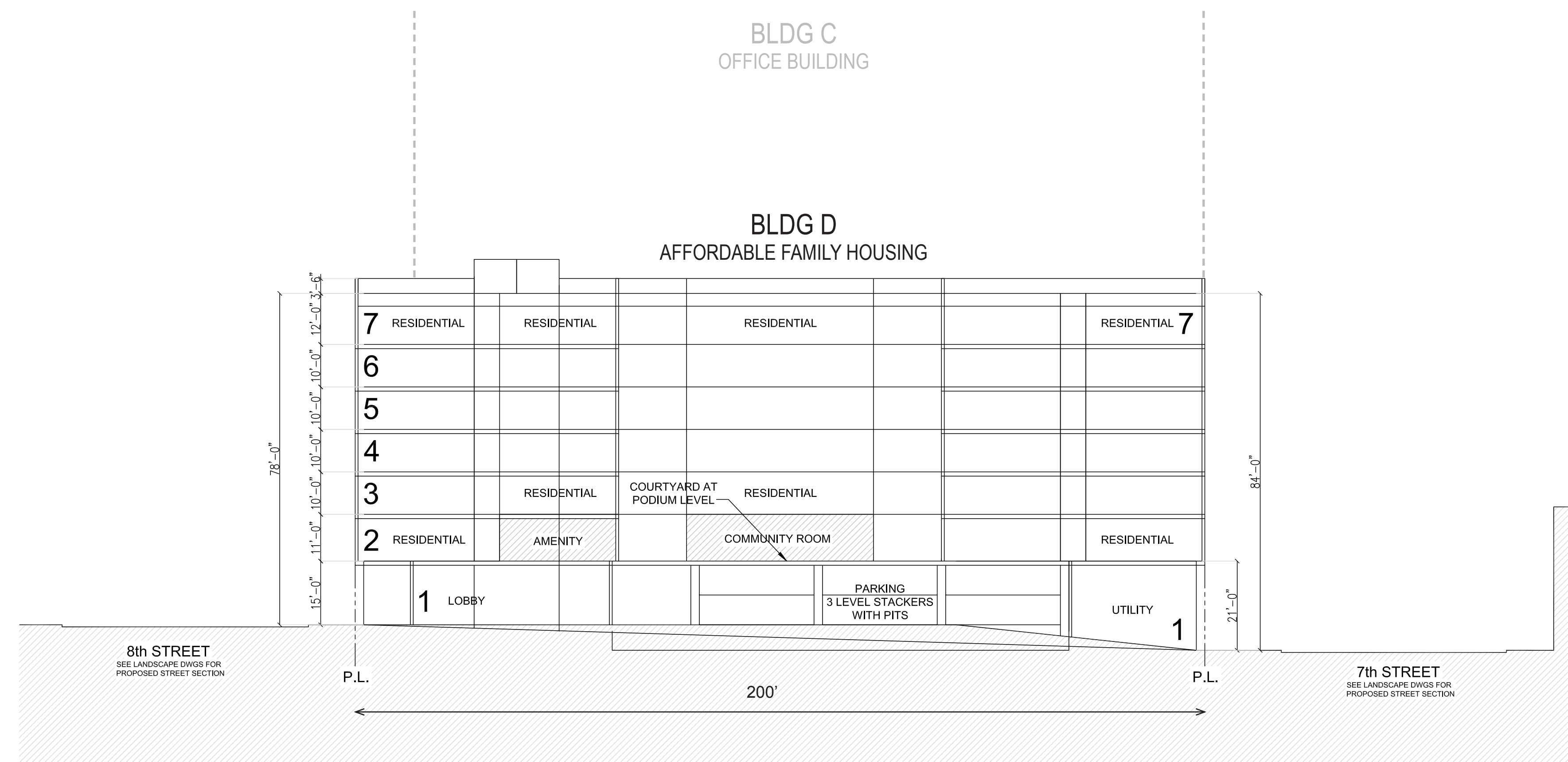
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DATE:  
SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 / BLDG D ELEVATIONS**



EAST-WEST SECTION



NORTH-SOUTH SECTION

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

**STRADA**  
101 MISSION ST. #420  
SAN FRANCISCO, CA 94105

**PYATOK**  
1611 TELEGRAPH AVE. SUITE 200  
OAKLAND, CA 94612  
www.pyatok.com

SOLIMON CORDWELL BUENZ ARCHITECTS  
335 CALIFORNIA ST.  
SAN FRANCISCO, CA 94111  
415/776-2400  
www.scb.com

**INWILLERUEHL**  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
510/851-1688  
www.inwilleruehl.com

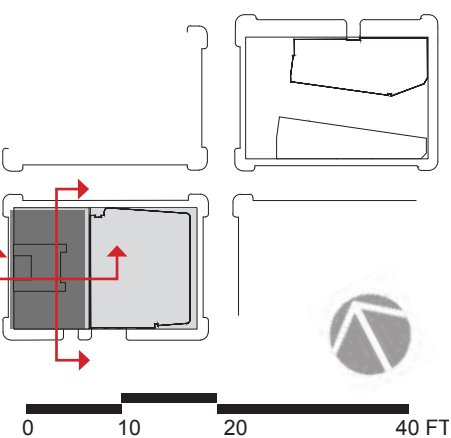
**BKF100+**  
225 BUCKLAND DR. SUITE 200  
REDWOOD CITY, CA 94063  
(650) 483-6300  
www.bkf.com

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

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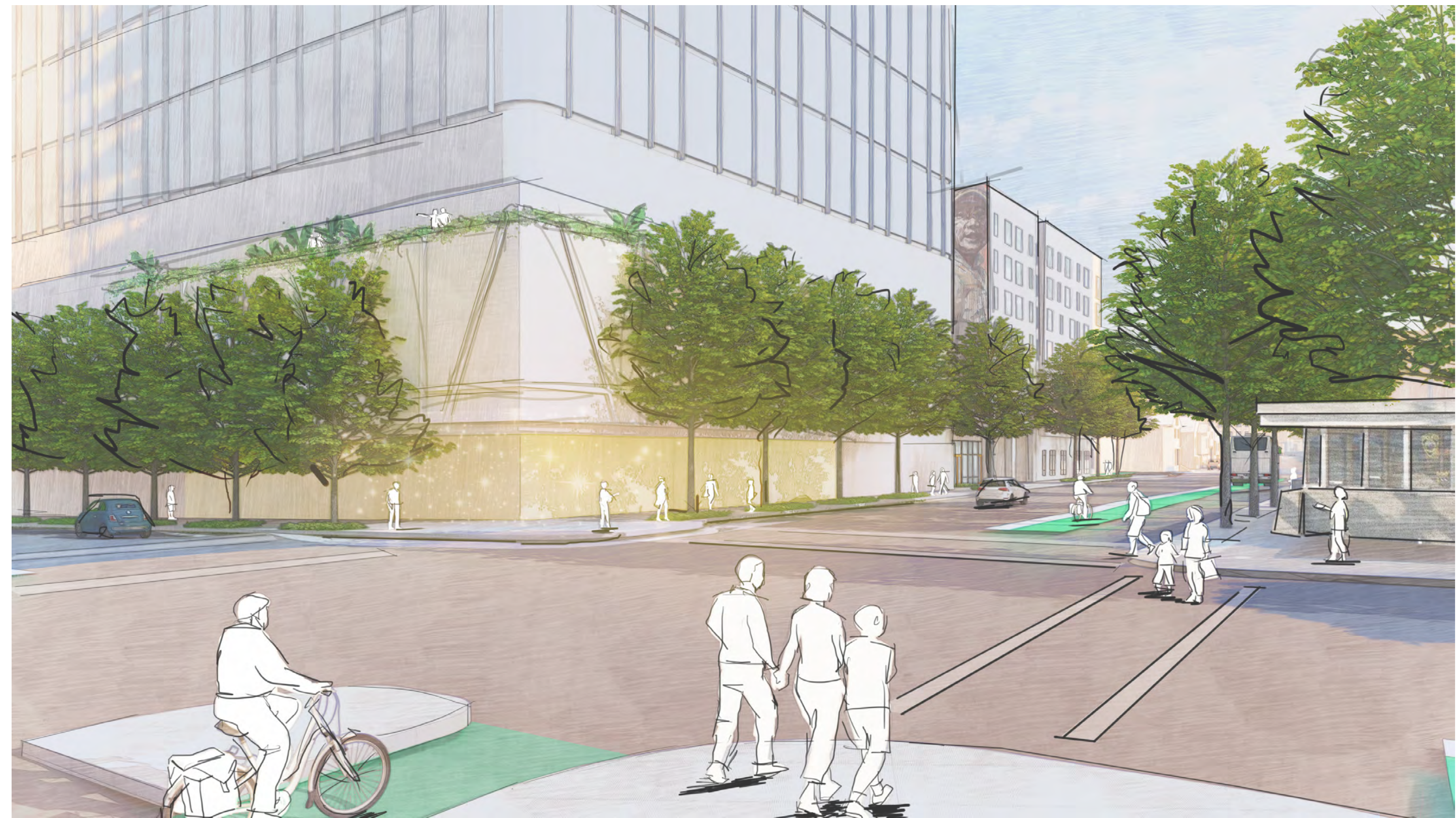
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**BLOCK 2 / BLDG D NORTH-SOUTH SECTION**



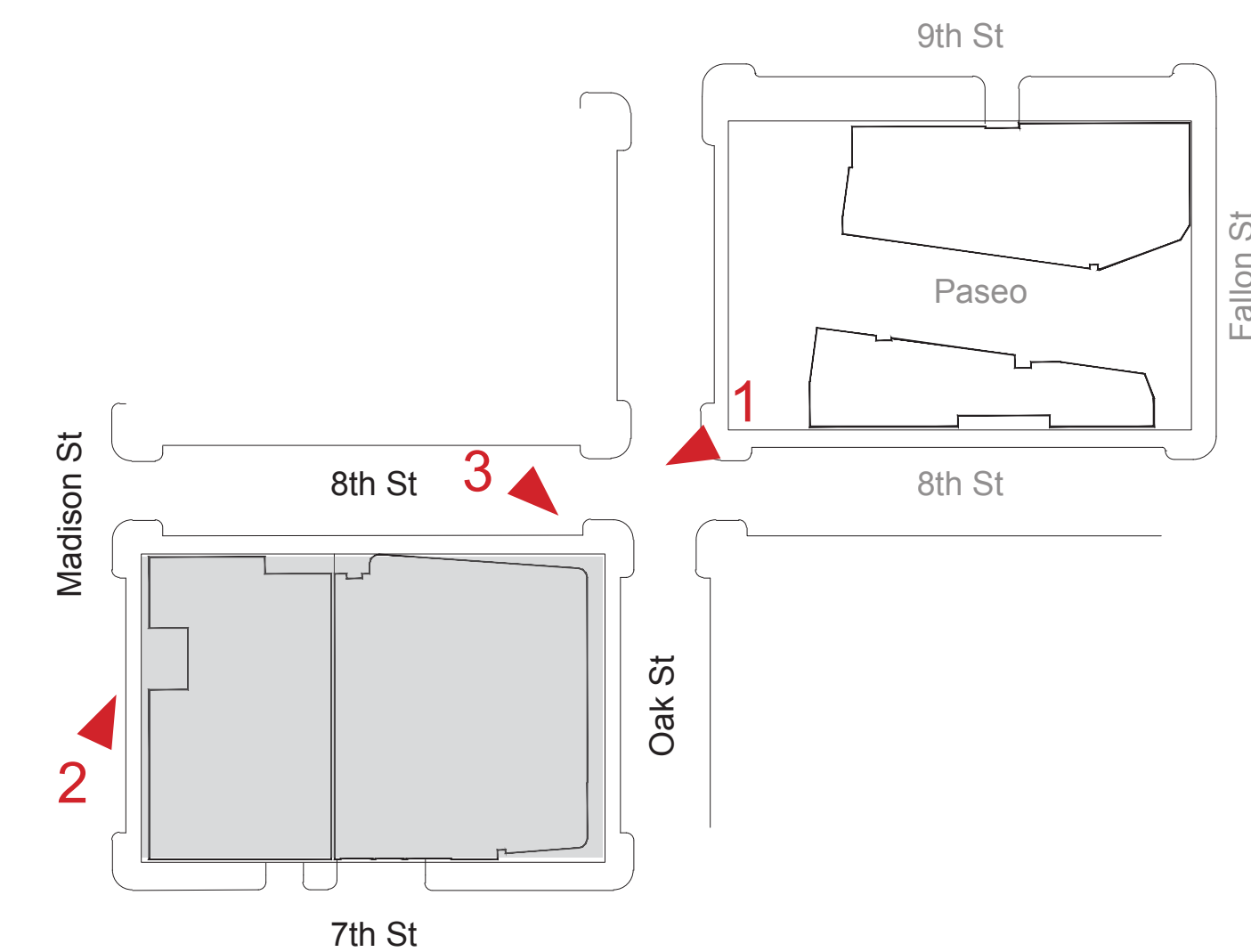
2. MADISON STREET  
VIEW TOWARDS THE DAYCARE



1. 8TH & OAK STREET  
VIEW TOWARDS SOUTH WEST



3. 8TH & OAK CORNER  
VIEW TOWARDS RETAIL SPACE



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DEVELOPMENT CORPORATION  
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OAKLAND, CA 94612

**STRADA**  
LANDSCAPE ARCHITECTS  
101 MISSION ST. #200  
SAN FRANCISCO, CA 94105

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ARCHITECTS  
318 HARRISON ST. STE 301  
OAKLAND, CA 94607  
(510) 891-1698  
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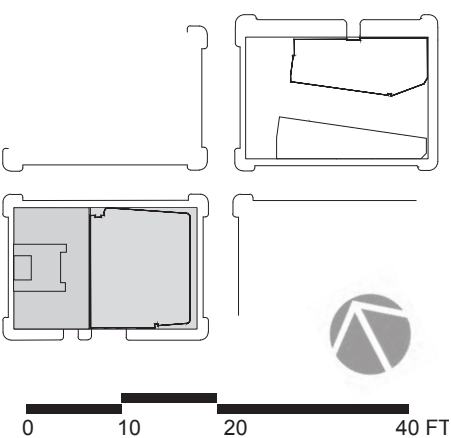
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YEARS  
ENGINEERS, SURVEYORS, PLANNERS  
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REDWOOD CITY, CA 94065  
(650) 485-8300  
www.bkf.com

**LAKE  
MERRITT  
BART  
REDEVELOPMENT**  
Oakland, CA 94607

**PRELIMINARY  
DEVELOPMENT  
PLAN PACKAGE**

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SCALE: 1" = 20' (VIEWED AT 24" X 36")  
**BLOCK 2 /  
CONCEPT 3D  
VIEWS**

**A2.42**



**NOTE:**

- SEE C2.1, C2.2, C6.1, C6.2 FOR FIRE HYDRANT LOCATIONS

EAST BAY ASIAN LOCAL DEVELOPMENT CORPORATION  
1825 SAN PABLO AVE. #200  
OAKLAND, CA 94612

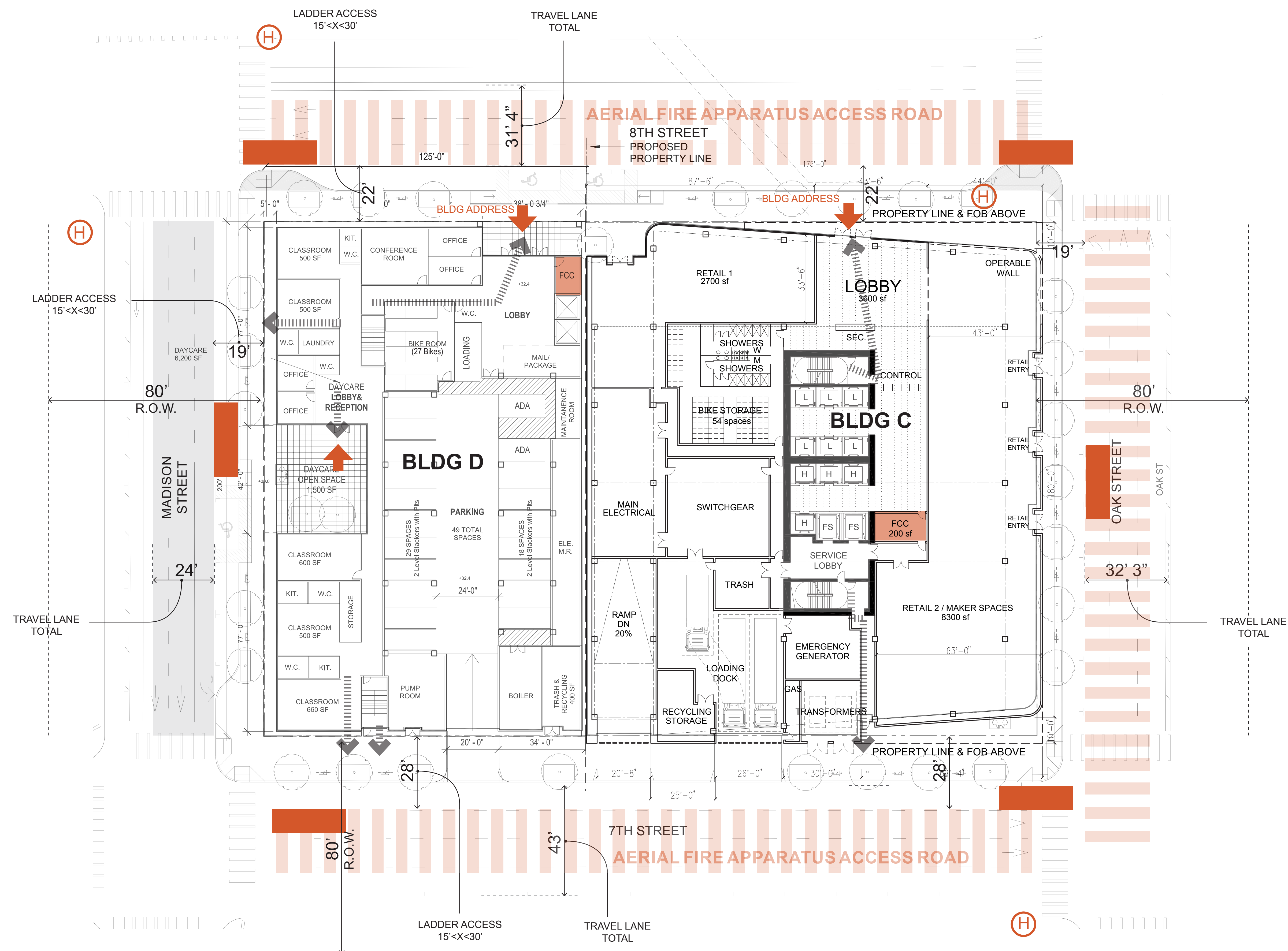
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REDDING, CA 96001  
(530) 483-4300  
www.bkf.com



**BUILDING C:**  
19 STORIES/ 275' HEIGHT  
OFFICE  
TYPE I  
FULLY SPRINKLERED

**BUILDING D:**  
7 STORIES/ 85' HEIGHT  
RESIDENTIAL/MIXED USE  
TYPE III OVER TYPE I  
FULLY SPRINKLERED

**KEY LEGEND**

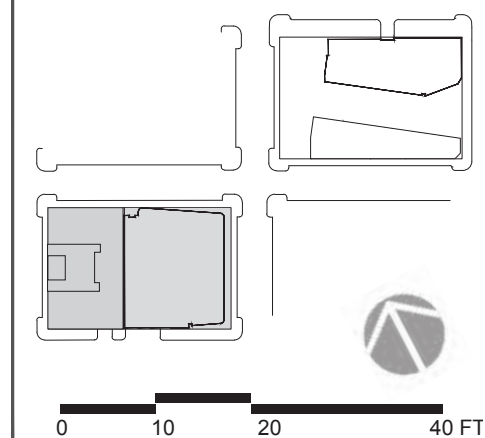
- (H) EXISTING HYDRANT LOCATION
- 150' HOSE DISTANCE
- ORANGE ARROW FIRE TRUCK
- ||||| AERIAL FIRE APPARATUS ACCESS ROAD (26 FEET WIDE)
- GRAY SHADING NON-AERIAL FIRE APPARATUS ACCESS ROAD (24 FEET WIDE)
- ORANGE ARROW PRIMARY BLDG ENTRANCE
- ||||| EXIT

**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

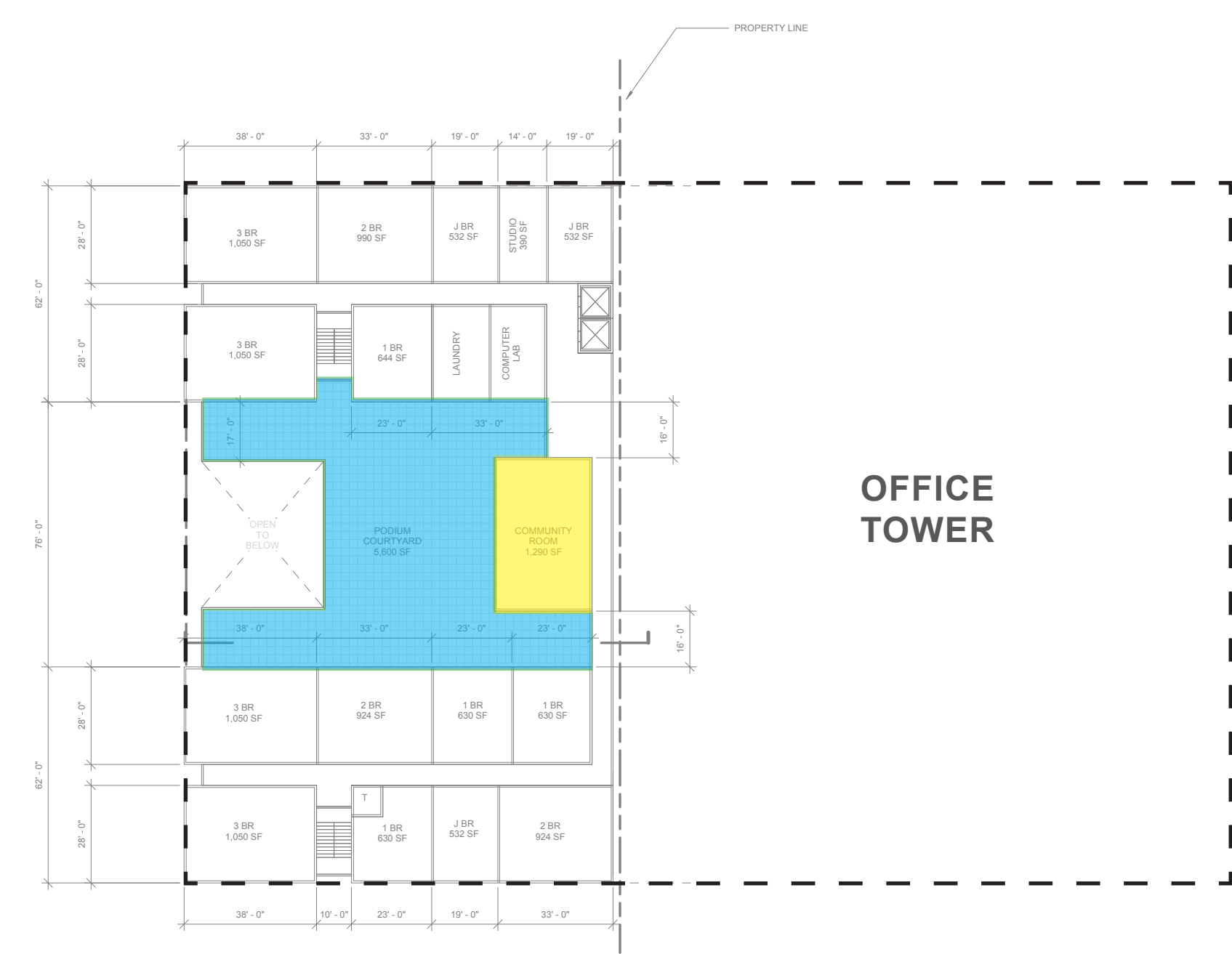
**PRELIMINARY DEVELOPMENT PLAN PACKAGE**

- PRELIMINARY - NOT FOR CONSTRUCTION -

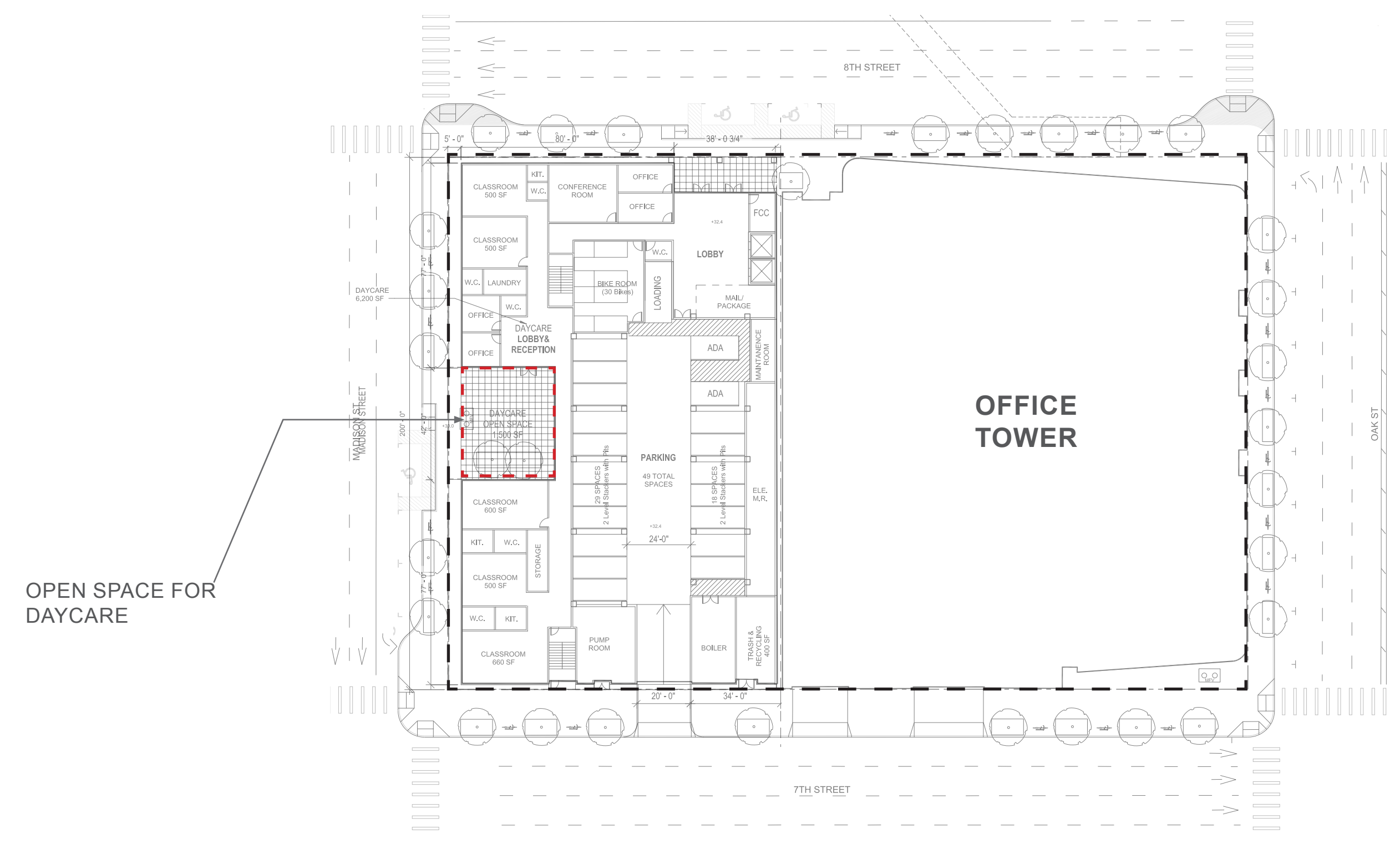
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**BLOCK 2 FIRE ACCESS DIAGRAM**



BUILDING D (COURTYARD/PODIUM)



BLOCK 2 - GROUND LEVEL

- PUBLICLY ACCESSIBLE OPEN SPACE
- GROUP USEABLE INDOOR OPEN SPACE (FOR BLDG OCCUPANTS)
- GROUP USEABLE OPEN SPACE (FOR BLDG OCCUPANTS)
- PRIVATE USEABLE OPEN SPACE (FOR UNIT OCCUPANTS)

	BUILDING D
PUBLICLY ACCESSIBLE OPEN SPACE	-
GROUP USEABLE INDOOR OPEN SPACE	1,200
GROUP USEABLE OPEN SPACE	5,600
PRIVATE USEABLE OPEN SPACE	-
<b>TOTAL</b>	<b>6,800</b>

**NOTE:**

- \* Covered entry plaza (setback at ground level) is not included in Publicly Accessible Open Space calculations
- \*\* open space is not required for Bldg C (office buildin)

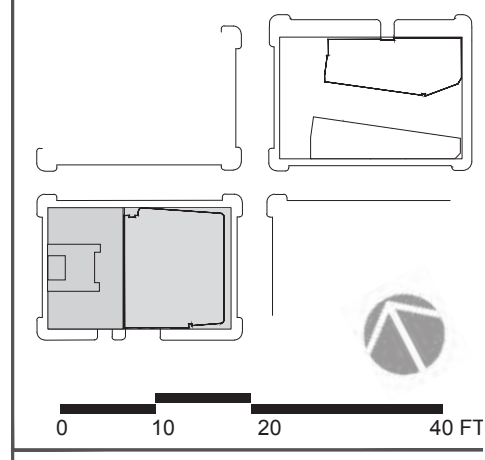
**LAKE MERRITT BART REDEVELOPMENT**  
Oakland, CA 94607

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SCALE: 1" = 20' (VIEWED AT 24" X 36")

**BLOCK 2 OPEN SPACE EXHIBIT**



**Design Guidelines for Lake Merritt BART**

May 3, 2021



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### Design Guidelines for Lake Merritt BART

#### APPLICABILITY

The Lake Merritt BART Design Guidelines (LMBDG), in conjunction with the development standards set forth in the D-LM-2 Lake Merritt Station Area Zoning District and the Lake Merritt Station Area Plan Design Guidelines (LMSAPDG), will serve as the basis for Design Review Findings for the Final Development Plans for the Lake Merritt BART Transit-Oriented Development (LMBTOD) Project. These guidelines build on the goals and intent set forth in The Lake Merritt Station Area Plan (LMSAP) and in LMSAPDG. The LMB Design Guidelines generally follow the format of the LMSAPDG, and provide more specific guidance for the design of this project site.

#### RELATION TO PRELIMINARY DEVELOPMENT PLAN (PDP)

The Lake Merritt BART Design Guidelines are one component of the Preliminary Development Plan (PDP) for the Lake Merritt BART project. The graphic components of the PDP establish the general arrangement and scale of building elements and the preliminary design for streets and public spaces. The LMB Design Guidelines provide supplementary guidance for the development of the detailed design of buildings, streetscape and open space.

#### 1. SITE DESCRIPTION

The Project spans two complete city blocks adjacent to the Lake Merritt BART Station. The Project's Block 1 is bounded by 9th Street, Fallon Street, 8th Street, and Oak Street. Currently this block serves the station as a surface parking lot owned and operated by BART, and the BART tunnel runs underneath the center of the site from Oak to Fallon. Additionally, this block has two station headhouse entrances on Oak, at the corners of 8th Street and 9th Street, which are not part of the LMBTOD project. The Project's Block 2 is bounded by 8th Street, Oak Street, 7th Street, and Madison Street. Block 2 is currently occupied by the existing office building and its private parking lot.

#### 2. COMMUNITY VISION

The Lake Merritt BART Station Transit-Oriented Development (the "Project") consists of high-rise and mid-rise buildings with a diverse mix of residential, retail, community space, and office uses, along with new publicly accessible open space. Each building brings a unique design while all working in concert to activate the ground-level experience and provide a common feeling of vibrancy, sense of arrival, and community. The Project is rooted in the design parameters and land use goals of the Lake Merritt Station Area Plan, and the City of Oakland's standards for quality, sustainable development. The Project aims to provide an equitable approach to Transit-Oriented Development, increasing transit ridership, delivering inclusive community benefits, and serving as a hub for the surrounding Chinatown, Jack London, Eastlake, and Lakeside neighborhoods.

The Bay Area Rapid Transit system (BART) moves hundreds of thousands of people across the Bay Area each day, including over 14,000 people coming in and out of the Lake Merritt BART Station. Over the past century, the Oakland Chinatown neighborhood has been a residential, commercial, and cultural home to a community, a place of local and international exchange.

The redevelopment of the Lake Merritt BART Station seeks to create a sense of arrival in this historic context, and to better connect the wider Bay Area region to Oakland Chinatown and the many other nearby neighborhood assets including Laney College, the Oakland Museum of California, Jack London Square, and Lake Merritt. Our development concept will enable the BART Station to become a dynamic TOD destination that complements, rather than competes with the commercial core of Oakland Chinatown, and will help it more equitably share in and contribute to the prosperity of Oakland and the entire Bay Area. The design of the public spaces will feature smooth lines that flow more organically, to encourage movement and remind visitors of the natural landscapes that are just a few blocks away.

The Project will include two phases and four buildings that feature a high-density mix of apartments across a wide range of income levels; office and community space; active ground-floor retail; and a reinvigorated and attractive public realm. The Project design also accommodates BART's existing infrastructure, including headhouse entrances to the station, as well as the underground tunnel and surrounding ventilation structures. The design must account for load restrictions, access needs, and other technical requirements to ensure safe operations for BART.

COMMUNITY VISION

The first phase of development will occur on Block 1, the current BART parking lot site. Block 1 includes:

- Market-rate and moderate-income rental housing in a high-rise building.
- Affordable senior housing in a mid-rise building.
- A generous mid-block pedestrian thoroughfare is anchored by transit uses, ground floor retail, interactive art or play, and a garden setting. Taken as a whole the publicly accessible space creates a layered identity for the site that connects to the surrounding neighborhoods. This publicly accessible space also presents an opportunity for public art that will contribute to the identity of the Project.

The second phase of development will occur on Block 2, after the existing office building is demolished. Block 2 will include:

- An iconic, high-rise, office building with ground floor retail and community-facing spaces.
- Affordable family housing in a mid-rise building. This building will also have a childcare center located at the corner of 8th and Madison Street, facing Madison Square Park.

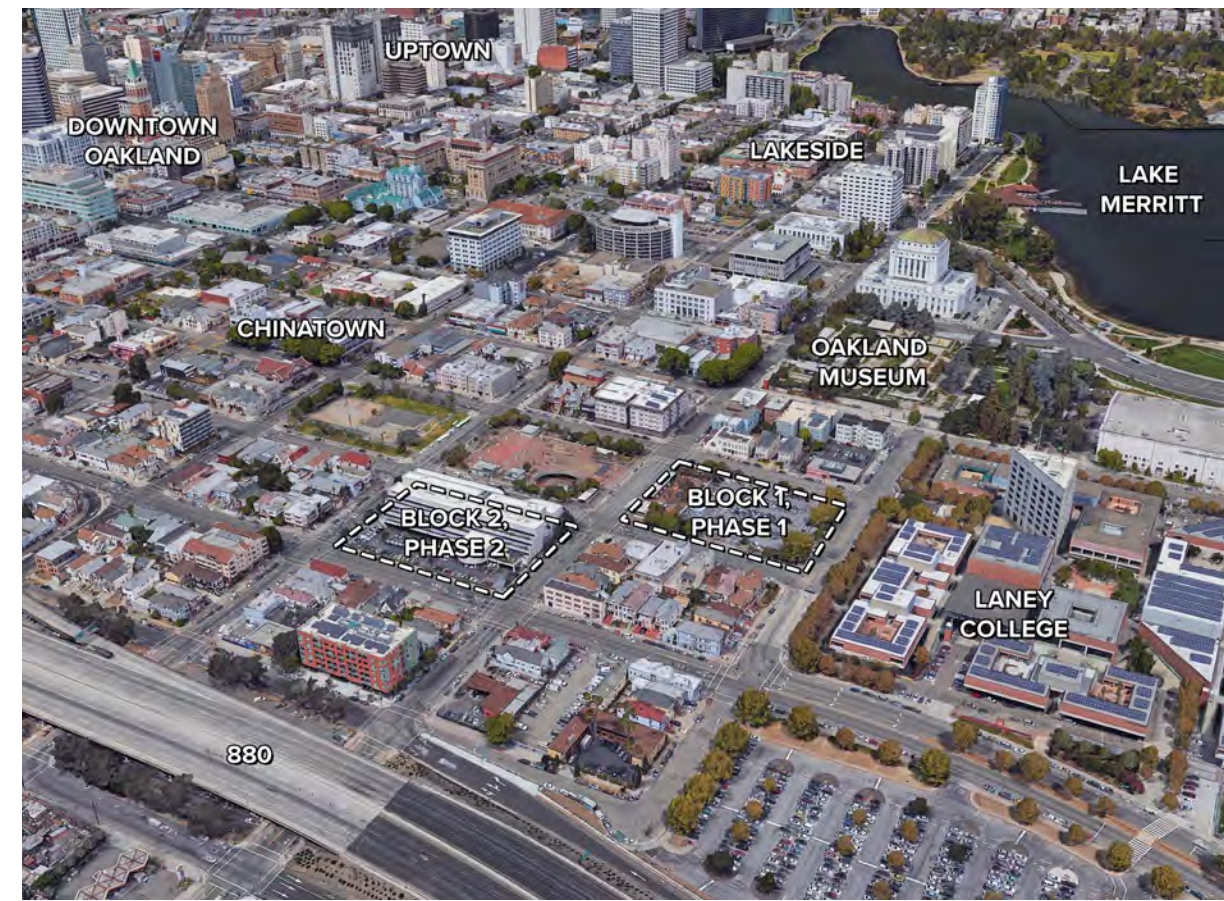


Figure 1: 3D Context View



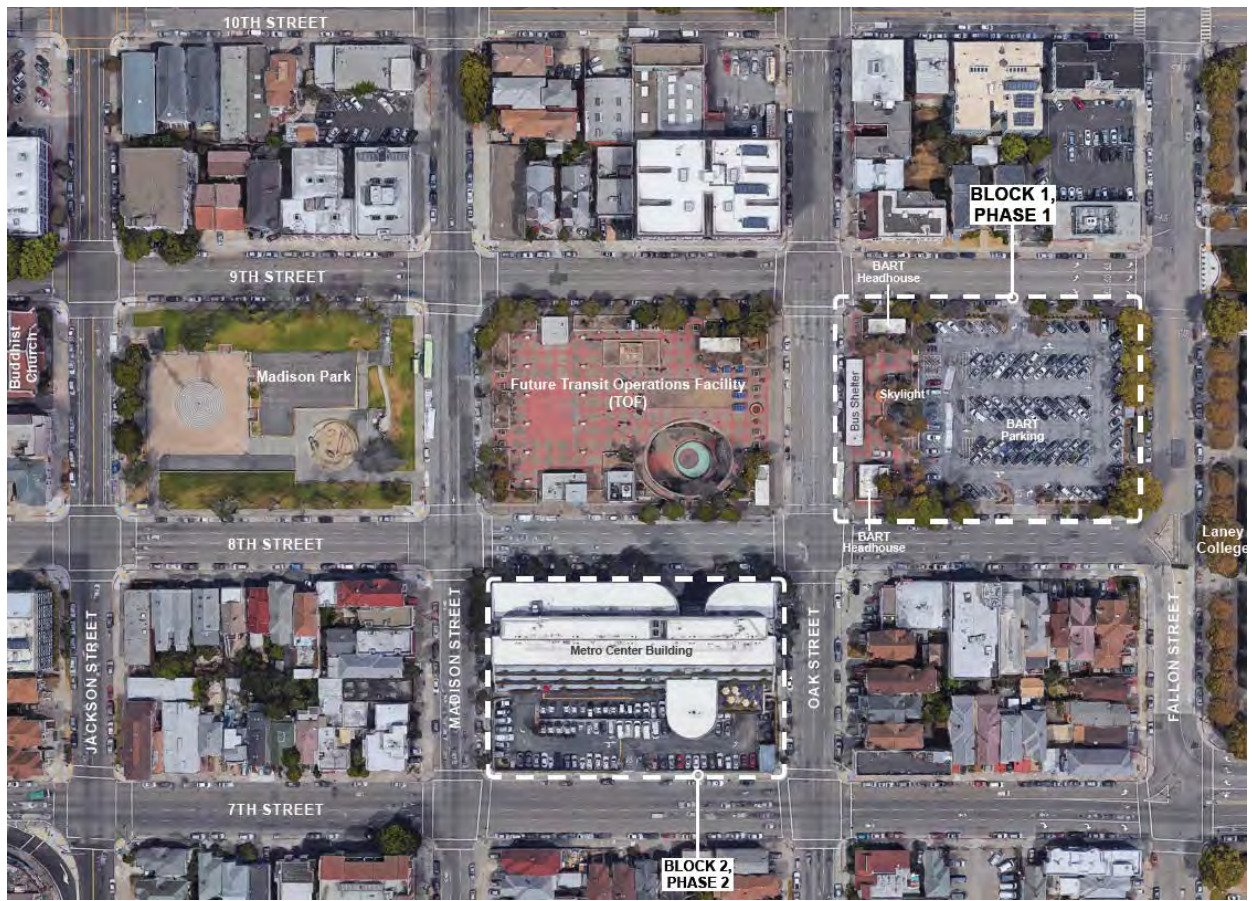


Figure 2: Context Map

### 3. GUIDING PRINCIPLES

The Project design will be guided by principles that address the unique opportunities presented by the redevelopment of the Lake Merritt BART Station. These principles build on the underlying goals set forth in the Lake Merritt Station Area Plan (LMSAP).

#### A. Cultural Identity and Sense of Arrival

The Project will create a regional destination that announces an arrival at the BART Station located in Oakland Chinatown, while also featuring prominent way-finding measures that highlight the surrounding civic and cultural assets. The Project seeks to celebrate Chinatown as a regional community destination and distinctive place, while also serving as a launching point to connect to the Oakland Museum of Art, Laney College, the Jack London Square Waterfront, Lake Merritt and the other nearby neighborhoods. A dynamic mix of high-rise and mid-rise improvements will create a new and identifiable mark on the skyline while simultaneously addressing the scale of the surrounding neighborhood context. Thoughtfully interspersed open space and an active pedestrian environment will promote a continuous flow between the Station and the surrounding neighborhood. The inflow and outflow at this core regional hub is a primary guiding principle of site programming.

#### B. Transit-Oriented Development

Transit-oriented development can enhance the arrival experience of riders and be a tool to connect people to places, cultures, and ecology. It also enhances sustainability by encouraging multimodal transportation and less reliance on automobile use. The Project's proposed design has been coordinated with a BART sponsored Access Study, which focused on better non-

#### DESIGN CONSIDERATIONS

automobile connections to this important transit node from the surrounding neighborhoods and institutions. The site is a 10-minute walk to the Amtrak Jack London Square Station, is surrounded by various AC Transit bus lines and bike lanes, and the Lake Merritt BART Station is at the center of the Project. The proposed Project program will deliver improvements that increase transit usage, improve pedestrian and bicycle safety, and provide a more sustainable transit environment for the community.

#### C. Publicly Accessible Gathering Space

The BART Station is already a hub of activity in Oakland, surrounded by formal and informal gathering spaces. A key goal for the Project is to enhance the attractiveness of existing spaces and add new opportunities for recreation, outdoor eating, contemplative space, public events, public art, wayfinding, and other communal activities. On Block 1, the engineering constraints and load limitations above BART's existing tunnel allow the Project to create an internal open space that connects to the BART Station entrances and will be a welcoming passageway for public access. The Project design focuses on making this public space a community destination, with landscaping, art, and neighborhood-scale retail. The Project will also include improvements to the public realm around the BART Station and on the surrounding streets and sidewalks to enhance the street-level experience for pedestrians and cyclists.

#### D. Community of Opportunity

The Project provides a mixture of market-rate and affordable housing units for a diverse mix of residents; brings a diverse job center to the neighborhood with office space targeted to community and non-profit

organizations; and creates new opportunities for community-focused retail and gathering spaces. Food retail has helped to spark a commercial renaissance in many parts of Oakland, and food is a historically important part of the identity of Chinatown. The project's retail scale, design, and orientation will be particularly well suited to local and "pop-up" restaurants, including new entrepreneurs who may have less access to startup capital.

#### 4. DESIGN CONSIDERATIONS

##### Block 1 Design Considerations

The BART tunnel structural requirements inform much of the site plan concept on Block 1. No primary building structures can be feasibly located over the BART tunnel and platform infrastructure because of load and access restrictions. However, this challenge also creates an opportunity to divide Block 1 into two separate buildings on opposing sides of the BART tunnel, allowing for variation in architecture and massing on the site. The north side of Block 1 adjacent to 9th Street is envisioned as a high-rise residential building with ground-floor commercial uses. The south side adjacent to 8th Street is envisioned as a mid-rise affordable housing building. This arrangement of building elements maximizes solar access at the public open spaces and provides appropriate spacing between the high-rise elements on Block 1 and Block 2. As a result of the mid-block passageway, both buildings on Block 1 have highly visible public frontages on all sides. In this circumstance special attention must be paid to creating an attractive ground level experience while accommodating normal building service functions. Residential lobbies are located further away from the BART headhouses to support

resident privacy and security, while commercial lobbies and retail spaces are oriented towards the BART headhouses.

The pedestrian walkway between the two buildings on Block 1 creates a new pedestrian space above the tunnel as a defining feature of the Project and key gathering space for the community. The Project connects pedestrian energy and activity to the center of the block, while also including active uses on key corners on Oak Street and Fallon Street. This orientation allows for a single sense of place with critical mass, as Buildings A and B can support each other with a cohesive retail program. It also allows for a more comfortable "outdoor room" for people to gather, with weather protection and active building frontages on both sides. The design draws on successful precedents throughout the world for two-sided retail or "night market" experiences with a series of small booths or stalls. The scale and diversity of uses – especially well-suited for food-related uses – encourages people to walk through, browse, and interact.

The publicly accessible open space will include movable outdoor seating for the retail at the west end and transition to interactive sculpture or play and quieter garden rooms at the east end towards Fallon Street. Furnishing and landscaping will redirect pedestrian flow and break the wide space into smaller "rooms", with active building uses at both the Oak Street and Fallon Street ends of the block. The open space will also include pedestrian-scaled lighting that may offer opportunities for hanging art or lanterns (similar to the cables that are used in Old Oakland between Broadway and Washington Streets). This will help to bring down the scale and create a more exciting visual and pedestrian-oriented environment as well as potentially providing a way to connect to art and culture, particularly during festivals and events.

#### Block 2 Design Considerations

The Block 2 plan will create a complementary mix of high-rise and mid-rise buildings, while adding a wider variety of affordable housing options focused on families, along with new high-rise office space. The office building will be located directly across from the BART headhouses on the eastern side of the block to support reverse-commute ridership for office tenants that will include community and non-profit organizations. The office building will also feature an active lobby and ground-floor retail uses to help activate the important corner at 8th and Oak Street. The mid-rise family affordable building will be located kitty corner from Madison Square Park, an amenity for families in the new building. Ground floor community-facing spaces (including potentially a daycare) will have direct visibility and access to the Park. Active and transparent frontages on 8th Street and Madison Street will support the larger area public realm improvement plan and build stronger connections to the Park.

#### Project Wide Design Considerations

##### • Landscape Design

Landscape design will enhance the pedestrian experience, and will also help to soften the hardness of the urban environment. Greening is an important part of improving the walking experience and some species provide practical benefits by removing particulate matter from the air (which the Chinatown Coalition has demonstrated using US EPA studies as a major health risk in the neighborhood), and reducing urban heat islands that will likely grow in severity over the coming decades. The Project design proposes a plant palette that evokes seasonal change and cultures.

##### • BART Station and BART Access

The Project aims to maintain strong visual connection and physical access to the BART Station, supporting an improved and safer transit rider experience. This includes improvements to the pedestrian experience and safety of the pedestrian walk-ways adjacent to the BART headhouses and Plaza, and incorporating these areas into active, safe and delightful urban places.

To accommodate the multitude of users of the Project (BART riders, residents, workers, visitors, etc.), careful planning for access to the site through various modes has been coordinated between BART, the City, AC Transit, and the Alameda County Transportation Commission (ACTC).

##### • Enhanced Pedestrian and Bicycle Access

The Project will provide improved pedestrian access to the BART Station Plaza and the development blocks from all directions, with activation on all sides. To accommodate the variety of one-way and two-way streets around our site, the Project will be designed to accommodate bike lanes, protected bike lanes, and two-way cycle tracks that will offer added safety for cyclist commuters. Secure bike parking will also be provided.

Key pedestrian improvements will include sidewalk upgrades such as repaving/regrading throughout the sites, planting of street trees, addition of corner bulb-outs, and sidewalk widening in some areas. Bicycle improvements include the incorporation of two-way protected bicycle lane on the south side of 9th Street at Block 1. This will serve as the major bicycle facility for

accessing the station area, and serve as a connection for neighborhoods to the east of the Development to the station area, Chinatown, and Downtown. The Project would also include a one-way protected bicycle lane on the west side of Fallon Street at Block 1.

##### • Relationship to the 7th Street/Harrison API

The 7th Street/Harrison Square Residential Historic District Area of Primary Importance (API) surrounds the Project, across the street on three sides of Block 2 and one side on Block 1, where height and residential character are defining features. Notable structures include detached one- and two-story wood frame Victorian and Colonial Revival cottages and houses, setback from the public way.

Where the Project abuts this district, care should be taken to design the buildings in relation to existing context. Elements such as modulation, the use of height datum, setbacks from the public way, and the use of similar textures or small-scale materials should be employed to generate a dialogue between the neighborhood and the Project. (LMSAP/DG-65: 7th Street/Harrison Square Residential Historic District API)

DESIGN CONSIDERATIONS

• Distinguished Tower Design

The two proposed towers will serve as a distinguished addition to the Oakland skyline, providing visual interest from vantage points throughout the city.

In accordance with the Lake Merritt Station Area Plan Design Guidelines, both Buildings A and C incorporate step-backs above the podium level to respond to existing context and to reduce the apparent mass from street level. In addition, tower shaping through sculpting and the provision of balconies should add visual interest to the tower forms. Both towers are located to minimize the amount of shadow cast on existing and proposed open spaces. (LMSAP/DG-23: Tower Spacing)

On Block 1, Building A is located north of the proposed Paseo to reduce the impact of shadows cast by the tower on the open space. The building footprint is dictated by BART infrastructure, with upper floors stepped back from the base. The tower is set back from the podium base to step down to the scale of the surrounding neighborhood context and to better define the expression of the slender tower above the podium. A signature visual move should be proposed for the west-facing facade to provide interest from downtown vantage points. On other facades, materiality, balconies and architectural details should be used to enhance visual interest. (LMSAP/DG-24: Distinguished Tower Design; LMSAP/DG-25: Skyline)

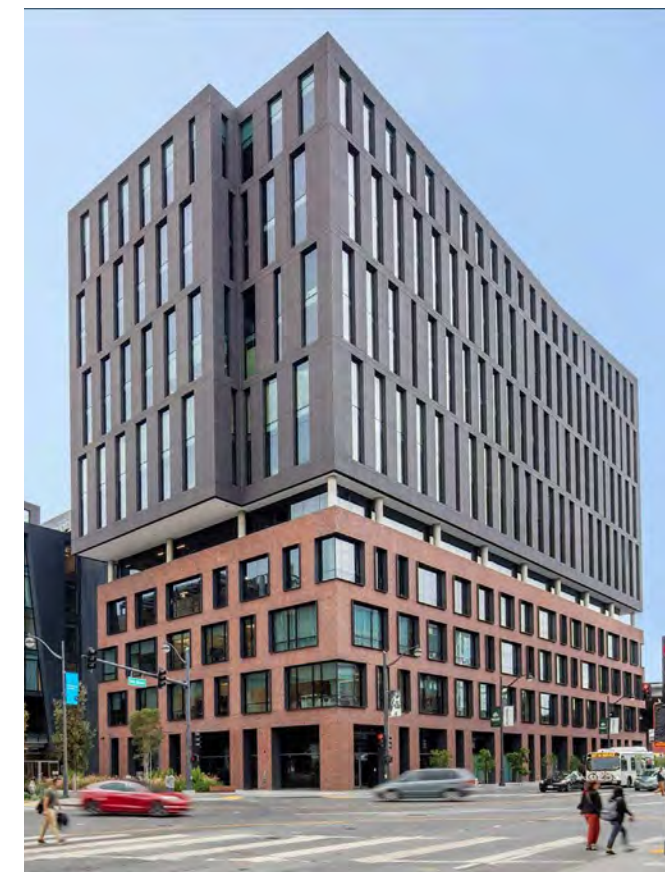


Figure 3: Example of Exterior Material

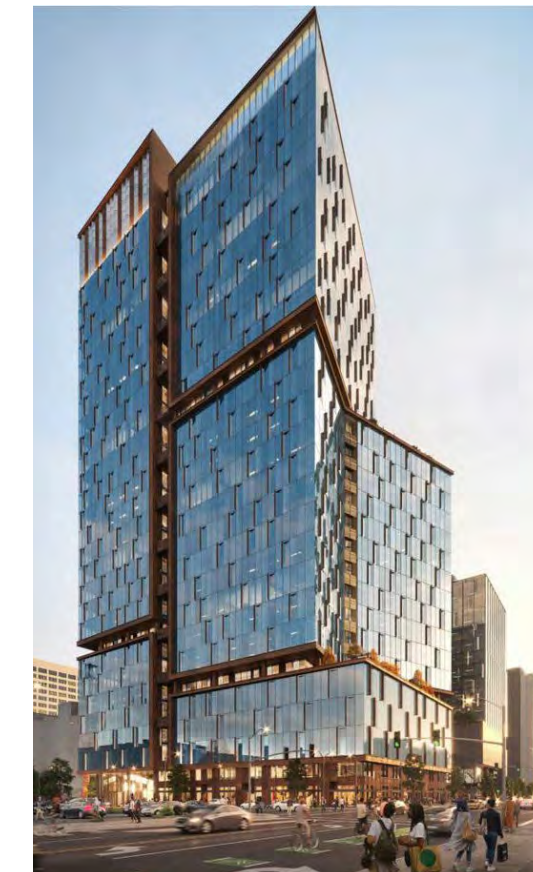


Figure 4: Example of an Iconic Form

DESIGN CONSIDERATIONS

On Block 2, Building C's podium should be appropriately scaled and detailed to respond to the neighborhood context, such as examples of the 7th Street / Harrison Square Residential Historic District Area of Primary Importance on Oak Street. The tower should be stepped back where appropriate to reduce the apparent mass in relation to existing context. The tower design should be distinguished through the use of high-quality materials, architectural details and sculpting of the form itself to create a distinguished addition to the Oakland skyline. Possible designs could incorporate large-scale, iconic features, large exterior balconies or geometric modifications of the tower form. (LMSAP/DG-24: Distinguished Tower Design; LMSAP/DG-25: Skyline)

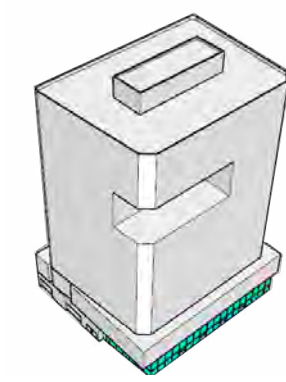


Figure 5: Example of iconic tower form

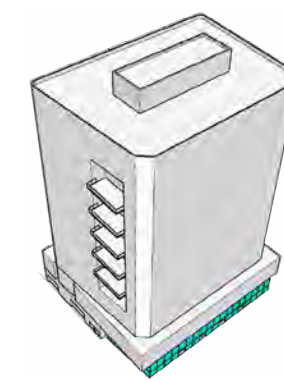


Figure 6: Example of super balconies of tower

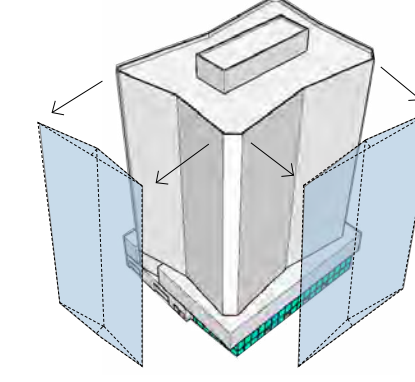


Figure 7: Example of sculpting of tower form



Figure 8: Example of iconic tower form



Figure 9: Example of super balconies



Figure 10: Example of sculpting of tower form

DESIGN CONSIDERATIONS

• Public Art/Programming

Public art, particularly located in the mid-block pedestrian walkway/public realm, will help create a sense of place and potentially serve as a landmark and wayfinding tool for the TOD Project and potentially the BART Station. Specific locations and concepts will be developed in consultation with local artists during the Final Development Phase of the Project.



Figure 11: Tommy Wong + Civic Design Center, Chinatown Banner, Oakland



Figure 13: Hung Liu: "Toke-off" SFO



Figure 12: Posts as Play Space Light Sculptural



Figure 14: Playful Furnishing

## 5. BUILDING DESIGN GUIDELINES

The Design Guidelines set forth in the following sections are based on the Guiding Principles and shall be applied and interpreted in the context of those Principles as Project Specific Guidelines for the Lake Merritt BART development. Where guidelines are similar in content to guidelines in the Design Guidelines for the Lake Merritt Station Area these guidelines are cross-referenced with the notation of (LMSAP/DG-#).

### A. Building Design – General Guidelines for Blocks 1 and 2

**1. Coordinated Design.** The design of the high-rise Buildings (A and C) and the mid-rise Buildings (B and D) should be coordinated to create a cohesive frontage and to reinforce the overall sense of identity for each Block. This coordinated design should take the form of a similar vocabulary of forms, openings, materials and colors. This should include consideration of how buildings work together to achieve the Design Principles for Lake Merritt BART, contribute to the public realm and the overall quality of life. (LMSAP/DG-1: Public Perception)

**2. Transition-in-Scale.** Mid-rise Buildings (B and D) on each block should be designed to provide a visual transition between the scale and rhythm of the neighboring buildings and high-rise Buildings (A and C). (LMSAP/DG-17: Reinforce the Existing Rhythm)

**3. Service Areas at Ground Level.** Service areas at the first level should be articulated with pedestrian-scaled facade articulation such as panels, contrasting textures, high-quality and interesting building materials, blind windows, doors, planting treatments, murals or other public art, and/or exterior detailing should create visual interest and diversity and to reinforce the pedestrian scale.



Figure 15: Artwork at Blank Walls

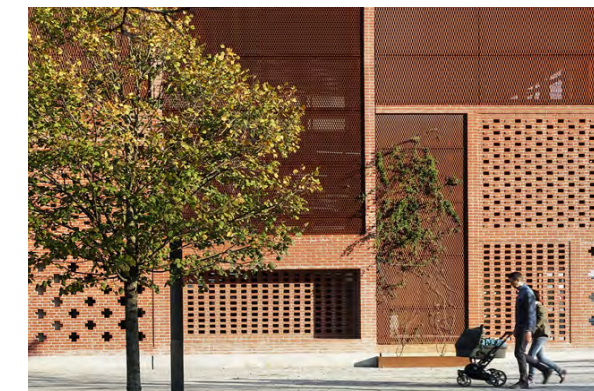


Figure 16: Contrasting Textures and Interesting Materials at Blank Walls

**4. Family Friendly Housing.** At residential units intended to accommodate families, especially in Building D as an affordable family building, the building design should include a variety of unit sizes to accommodate households of various sizes. Building design on both Block 1 and Block 2 should also incorporate other family friendly elements including outdoor play space designed to allow supervision and easy access. (LMSAP/DG-52: Family Friendly Housing)

**5. Shared Outdoor Spaces.** Shared outdoor spaces should include plantings, benches, lighting and other appropriate elements to create an inviting and useful space for the residents. Shared outdoor spaces should be designed to accommodate children where feasible by incorporating play structures and play areas. Shared outdoor spaces such as the publicly accessible open space Paseo, the courtyard, the open space at the

upper floor step backs and/or the rooftops, should be designed to have ample daylight and to be sheltered from the wind. (LMSAP/DG-57: Shared Spaces)

**6. Residential Community Rooms.** Where community rooms are provided they should be located to promote active use by residents. Community rooms are encouraged to be located adjacent to shared outdoor spaces or public open areas to create strong connections between indoor and outdoor activities. (LMSAP/DG-57: Shared Spaces)

**7. Exterior Color.** The exterior color palette should be coordinated to provide a cohesive overall appearance and to reinforce the design intent. The color palette at adjacent buildings should be coordinated to achieve the desired balance of cohesion and variety. (LMSAP/DG-73: Color)



Figure 17: Family Friendly Environment



Figure 18: Shared Outdoor Space



Figure 19: Residential Community Room



**B. Building Design –  
Street Frontage Guidelines for Block 1**

The guidelines below apply to portions of buildings fronting on the referenced street or public open space. These frontage guidelines are intended to supplement the Building Design General Guidelines. The character and design intent for each frontage is outlined at the beginning of each section.

**B.1. Oak Street Frontage (Buildings A and B)**

Oak Street is the “front door” to Block 1 and also provides the main access to Lake Merritt BART Station with BART entries located at the 8th Street and 9th Street corners. The east-west, mid-block Paseo above the BART tunnel creates an 80 foot-wide visual break between Buildings A and B. The wide BART Plaza at Oak Street accommodates the BART entries (headhouses) and creates a welcoming entry to the Paseo. The high-rise Building A and mid-rise Building B shall work together to create a cohesive sense of place and an active public realm at the BART Plaza and the Paseo. Oak Street is a highly visible frontage and its orientation toward Downtown Oakland provides an opportunity for active upper-stories with roof deck/gardens. Due to the adjacent BART Plaza, no service areas or parking access points are located on the Oak Street frontage.

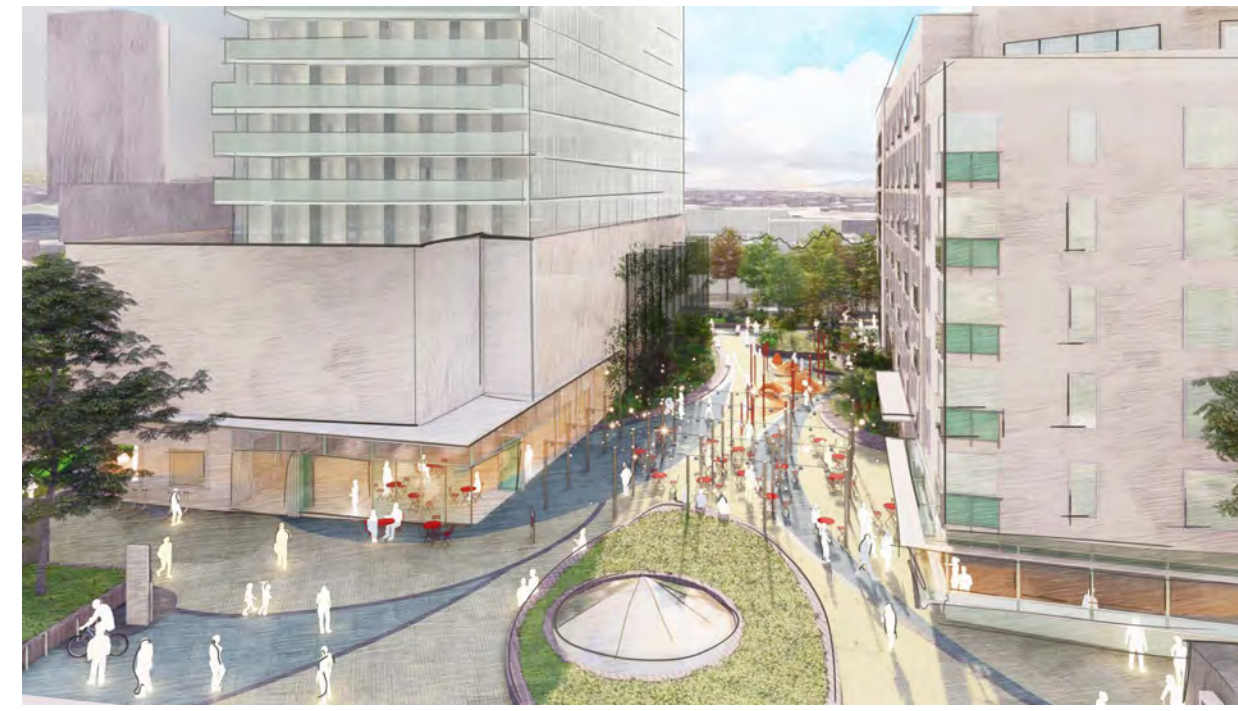
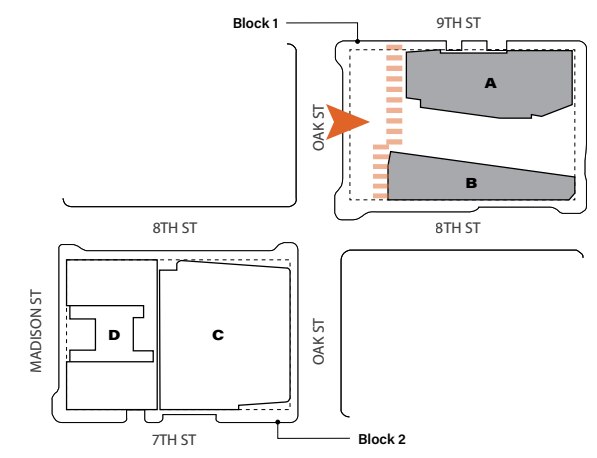


Figure 20: Oak Street Frontage

**Oak Street Frontage: Design Standards**

- a. The Building A tower shall be setback from the base building a minimum of 20 feet.
- b. Parking uses located in the podium at Building A shall include 25% openings for natural ventilation.
- c. The corner of the podium at the Paseo and Oak Street shall be notched a minimum of 10 feet by 10 feet.
- d. A canopy shall be provided over entrances to the ground-floor commercial spaces. See Figure 13.

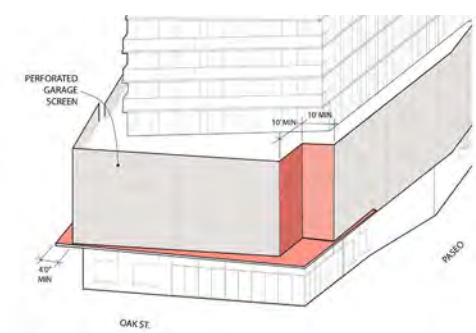


Figure 21: Building A Corner at Oak Street and the Paseo



Figure 22: Building Corners and Defined Open Space

**Oak Street Frontage: Design Guidelines**

- a. Architectural articulations and landmark features should be used to highlight and emphasize the building corners facing Oak Street, to visually define and animate the mid-block pedestrian thoroughfare entry and to facilitate pedestrian flow.
- b. Active commercial uses should be provided at the ground floor of all four building corners along Oak Street with visual transparency to create an active and inviting public realm and to connect the street life to the Paseo. (LMSAP/DG-39: Storefronts, DG-32: Views of Indoor Spaces)
- c. Open spaces and/or articulated roof shapes should be provided at step backs and the rooftop facing Oak Street to emphasize the gateway elements and to provide shared outdoor spaces overlooking the BART Plaza and Downtown.
- d. The design of the high-rise Building A and the mid-rise Building B should be coordinated to create a cohesive frontage and to reinforce the overall



Figure 23: Active Roof at Step Backs

- e. Identity for the block. This coordinated design should take the form of a similar vocabulary of forms, openings and materials.
- e. Use of canopies, awnings or other projecting elements should provide shelter and shade at west facing active uses. At Building B, horizontal awnings above the ground floor should wrap around both corners of Oak Street and the Paseo and at Oak and 8th Street. (LMSAP/DG-35: Awnings)
- f. The visual prominence of the west facade of the Building A tower suggests incorporating three-dimensional elements such as balconies, recesses and similar architectural features that create deep shadows and provide shading for interior uses. (LMSAP/DG-24: Distinguished Tower Design)
- g. Varying materials, texture and scale should be used to differentiate the building base from the tower above. (LMSAP/DG-18: Transitions in Building Height)



Figure 24: Active Commercial Uses



Figure 25: Shaping West Tower Facade

**Oak Street Block 1: Streetscape**

This block of Oak is a dynamic area for multi-modal transfers by passengers and should function for efficiency and safety.

**Business Frontage Zone**

- a. Located in part above the BART tunnel, the design in this area must comply with the BART Facilities Standards.
- b. Furniture should include benches which orient in multiple directions and provide a variety of ways to sit including tiered, backed, non-backed, communal, etc.
- c. Furniture should also accommodate all needs of transit users and include bike lockers, bike racks, lighting, waste stations, and information kiosks/maps.
- d. The Paseo pavements shall extend to surround the skylight.
- e. The skylight should be visible and iconic and at night be lit for interest.

**Pedestrian Pathway Zone**

- a. Sidewalk concrete should be colored with the maximum amount of lamp black allowable by the

City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.

- b. Corners of sidewalks should be stamped with letters to show name of street.
- c. Located in part above the BART tunnel, the furnishings in this area must comply with BART Facility Standards and should not block sightlines into the Paseo.
- d. Furniture should include benches which orient towards both the Paseo and Oak Street.
- e. The expression of Oak Street as a "green street" encourages emphasizing public transit and where possible planting that does not conflict with the transit program. (LMSAP/DG-128: Streets with Special Functions)
- f. Street furnishings should be provided in transit waiting areas and as needed to facilitate multi-modal transfers from bike or scooter or bus to BART. (LMSAP/DG-131: Furnishings)



Figure 26: Oak Street Block 1

### B.2. Fallon Street Frontage (Buildings A and B)

Fallon Street borders the Laney College campus on its east side and connects directly to the Oakland Museum of California (OMCA) on the north end. The mid-block Paseo creates a visual break through the site while providing a physical connection between Laney College and OMCA on the east and BART Plaza and Madison Square Park on the west. The primary residential entries and associated amenity space for Buildings A and B are located on Fallon Street to create an active frontage and a vibrant entry to the Paseo. Similar to the Oak Street frontage, the high-rise Building A and mid-rise Building B shall work together to create a cohesive sense of place and an active public realm on Fallon and at the Paseo. Due to the limited street frontage, no service areas or parking access points shall be located on the Fallon Street frontage.

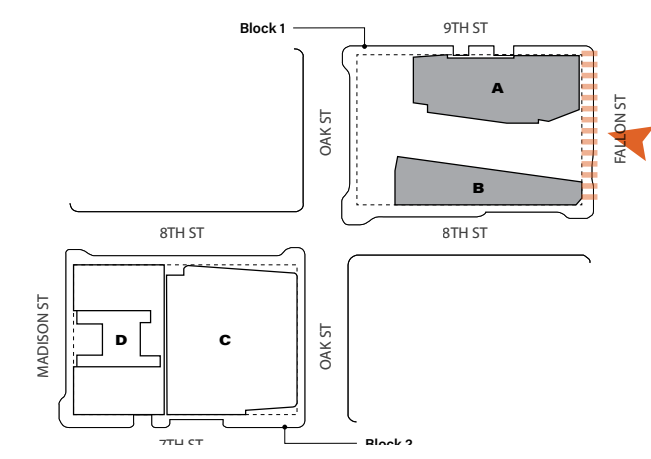


Figure 27: Fallon Street Frontage and the Paseo Entry

**Fallon Street Frontage: Design Standards**

- a. Parking uses shall include a minimum of 25% openings for natural ventilation.
- b. A minimum six-foot deep canopy shall be provided on Building A at the corner of Fallon and the Paseo above the pedestrian level.
- c. Horizontal fins with a minimum depth of one foot shall be provided on alternating upper floors. See Figure 21.

**Fallon Street Frontage: Design Guidelines**

- a. Emphasize the corner of 9th and Fallon Street as a prominent corner with architectural features and main residential entry. (LMSAP/DG-7: Corner Building Design, LMSAP/DG-30: Ground Floor Entries)
- b. Emphasize the corners of Fallon Street and 8th and 9th Streets as gateways to the Paseo and main residential entrances as well as providing architectural details to define the corner and a vibrant pedestrian entry. (LMSAP/DG-7: Corner Building Design, DG-30: Ground Floor Entries)
- c. Visual transparency should be used to create an active and inviting public realm and to connect the street life to the Paseo. (LMSAP/DG-32: Views of Indoor Spaces)
- d. Residential entries should be designed as prominent elements that reinforce and activate street frontage. (LMSAP/DG-30: Ground Floor Entries)
- e. The narrow corner at Building B creates a dramatic frontage that should be enhanced with a recessed entry or other significant and welcoming element that provides a visual landmark for Block 1 and relates to the scale of the Laney Campus.

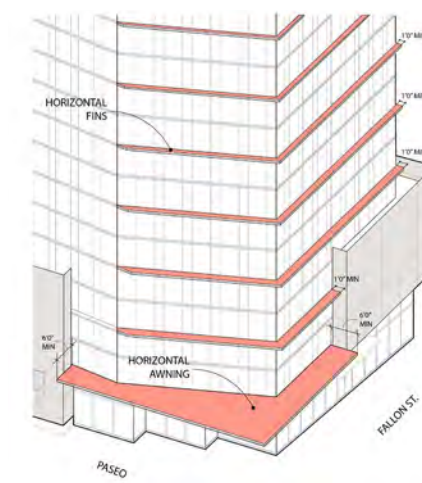


Figure 28: Building A Corner at Fallon Street and the Paseo



Figure 30: Robust Under Planting and Cohesive Street Tree Character



Figure 29: Residential Tower Entry



Figure 31: Building Corner at 8th and Fallon Streets

**Fallon Street: Streetscape**

Anchoring the Paseo's east end, this block connects to Laney and the Oakland Museum and embraces a campus character.

**Business Frontage Zone**

- a. Small urban furniture elements including benches, container plants, and extension of the lobby to the outside should be encouraged.
- b. Elegant vertical circulation with clear sitelines to the Paseo should be maintained at entry points mid-block in the business frontage zone.
- c. Plantings that screen BART structures and utilities are desirable within the constraints of the BART Facilities Standards.

**Pedestrian Pathway Zone**

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- b. Corners of sidewalks should be stamped with letters to show name of street or text connecting to cultural identity of site such as poetry or history.
- c. Public seating that can be located at back of sidewalk or near lobbies is encouraged.
- d. New street trees and lush plantings should be provided along Fallon to connect the Paseo with the campuses of Laney College and the OMCA. (LMSAP/ DG-110: Tree Planting and Preservation)

**Landscape Furniture Zone**

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- b. Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, and concrete.

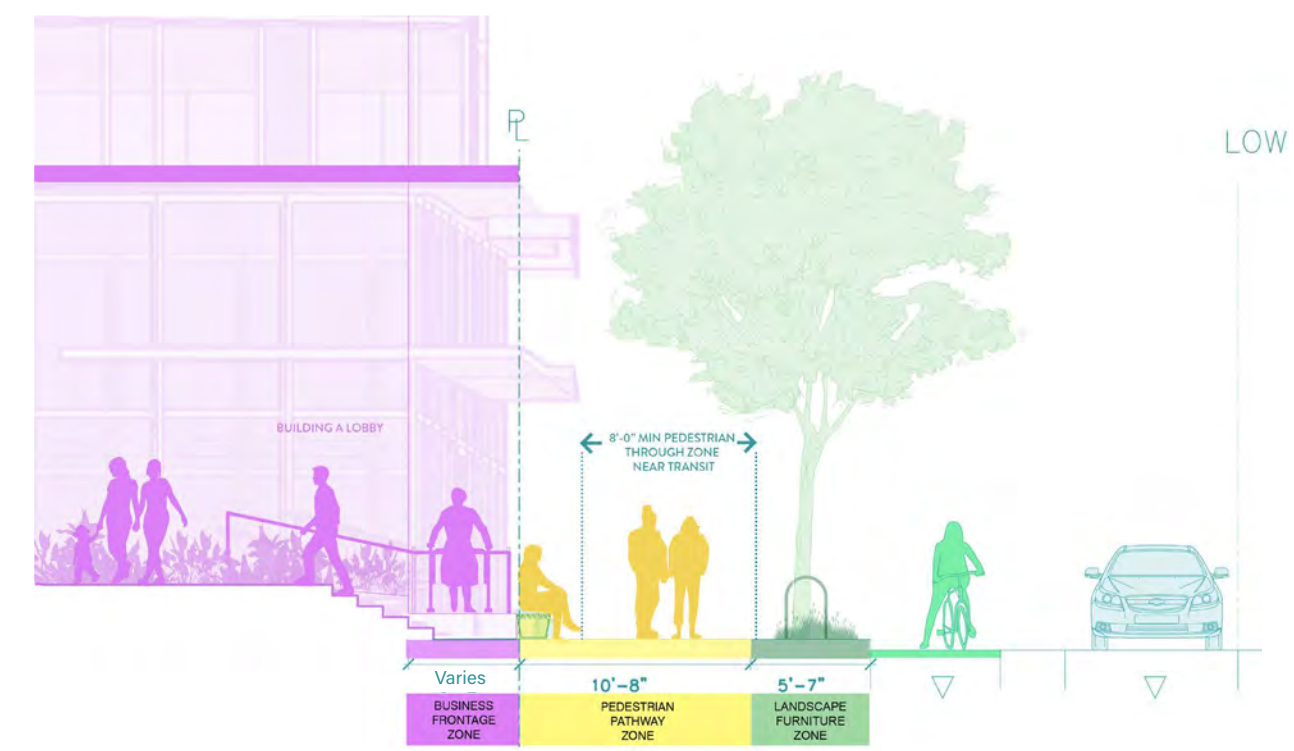


Figure 33: Fallon Street Block 1

### B.3. 9th Street Frontage (Building A)

9th Street is considered a Principal Street and serves as an important connector for auto, bicycle and pedestrian traffic traveling between Chinatown and Laney College. Active ground floor uses are encouraged on the 9th Street frontage with a focus on creating active corner elements. Access to parking and loading, and necessary ground floor services are located on the 9th Street frontage to avoid impacting the narrower Fallon Street frontage. Where non-active uses occur at the ground level street frontage, architectural and planting treatments should be used to maintain an attractive and lively pedestrian experience.



Figure 34: Special Paving between Tree Wells



Figure 35: Festival Lighting Infrastructure Cables

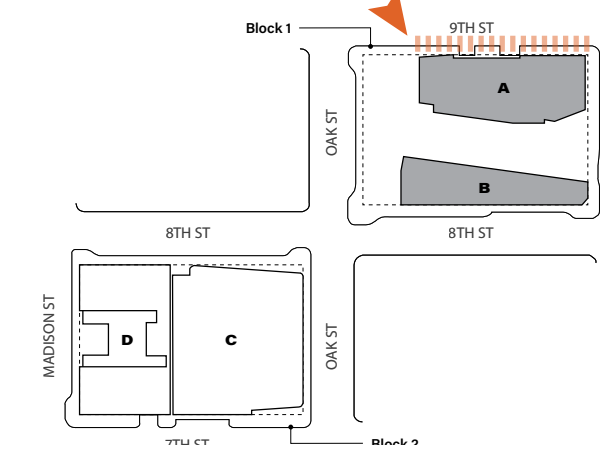


Figure 36: 9th Street and Oak Street Corner

**9th Street Frontage: Design Standards**

- a. The Building A tower shall be setback from the base building a minimum of 10 feet along the 9th Street frontage. See Figure 29.
- b. Parking uses located in the Building A podium shall be wrapped in a porous screen with minimum 25% openings to allow for natural ventilation. Screening element to be integrated with overall building design, or consist of high-quality, contrasting material to create an architectural feature. (LMSAP/DG-98: Integral Design)
- c. Active ground floor uses should be provided at the building corners at Fallon Street and Oak Street. The minimum length of the active frontage from each corner should be not less than 30 feet. (LMSAP/DG-8: Primary Lot Frontage)
- d. Transparent openings should be provided at active uses such that windows allow views of indoor space between two and nine feet above the sidewalk. (LMSAP/DG-32: Views of Indoor Space)
- e. The access for parking and loading areas should be as narrow as functionally possible. (LMSAP/DG-92: Vehicular Access)
- f. Non-active ground floor uses including service areas and parking facilities should be articulated with architectural elements such as a change in material and/or texture, screening elements, translucent windows and plantings (LMSAP/DG-93: Site Design)
- g. At the 9th Street frontage the tower should be set back 10 feet from the Building Base to reduce apparent scale and modulation of the podium facade. (LMSAP/DG-19: Step Back Above the Podium Height)
- h. Step backs above the Building Base should be utilized as roof gardens and active outdoor space. (LMSAP/DG-27: Active Upper Stories)

**9th Street Frontage: Design Guidelines**

- a. Set back the ground floor two to four feet from the public right-of-way to create a wider sidewalk with a minimum of eight foot clearance. Upper levels of the building may extend over the ground floor set back to the public right-of-way, maintaining a 15-foot minimum height from the sidewalk. (LMSAP/DG-125: Sidewalk Elements)

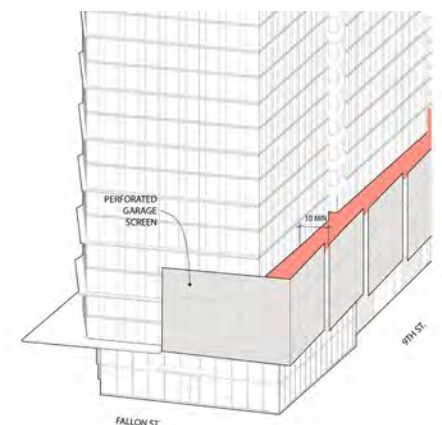


Figure 37: Building A Corner at 9th and Fallon Street.

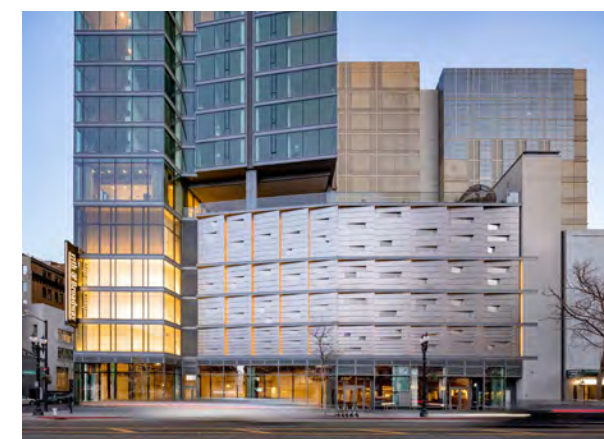


Figure 38: Porous Screen of Parking Uses



Figure 39: Active Use near Oak Street / BART



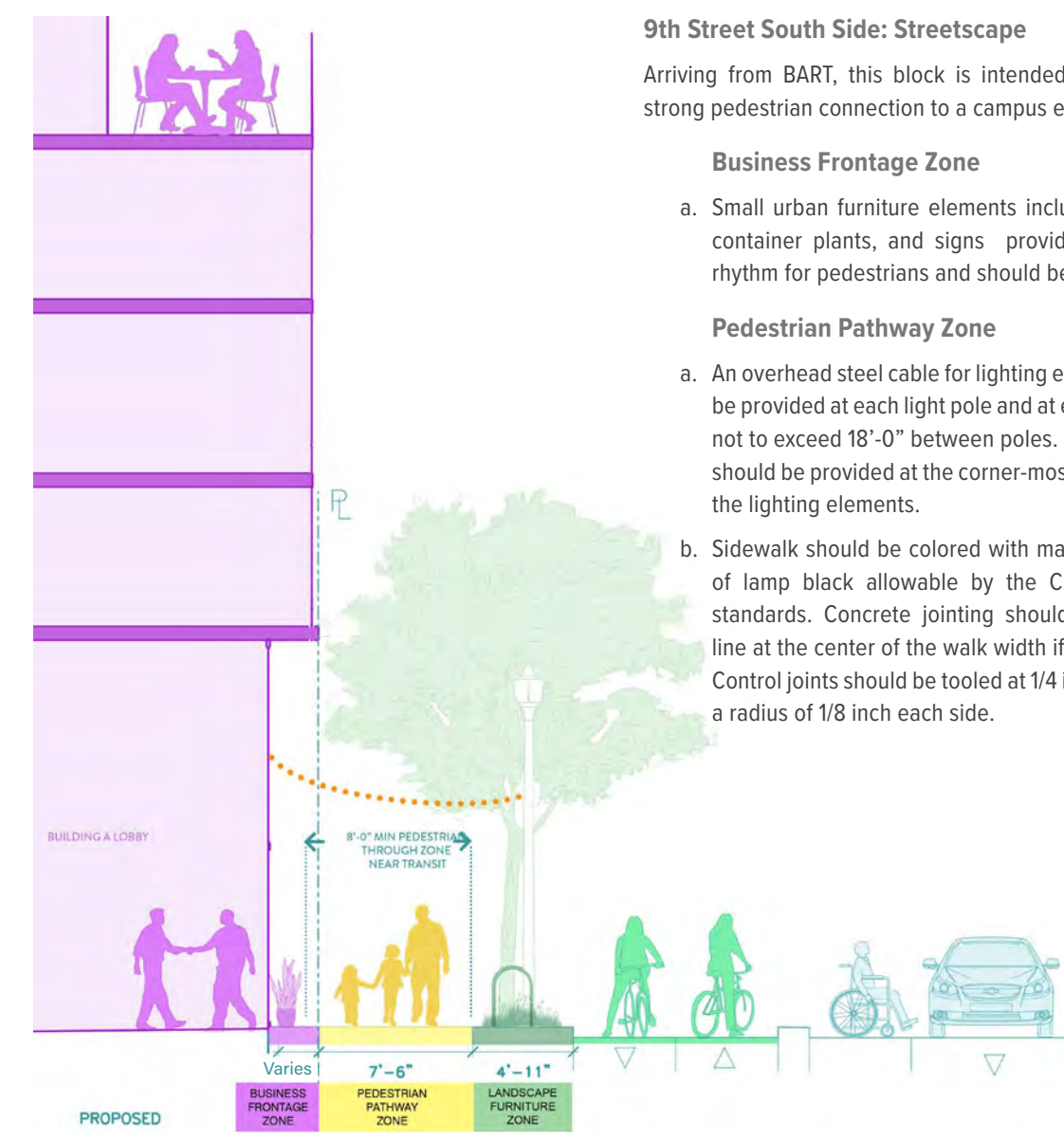


Figure 40: 9th Street Block 1

**9th Street South Side: Streetscape**

Arriving from BART, this block is intended to feel like a strong pedestrian connection to a campus experience.

**Business Frontage Zone**

- a. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

**Pedestrian Pathway Zone**

- a. An overhead steel cable for lighting elements should be provided at each light pole and at equal distances not to exceed 18'-0" between poles. Secured power should be provided at the corner-most light poles for the lighting elements.
- b. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.

- c. Corners of sidewalks should be stamped with letters to show name of street and or text connecting to cultural identity of site such as poetry or history.

- d. New street trees that could be carried west to Madison Park should be provided along 9th to connect to the campuses of Laney College and OMCA, strengthen its identity as a green street, and increase livability. (LMSAP/DG-110: Tree Planting and Preservation)

**Landscape Furniture Zone**

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- b. Between tree wells concrete unit pavers, stamped concrete, or cobblestones should be used to unify the three blocks of 8th and 9th at the curb edge. Stamp design could be created with an artist to be culturally relevant to Chinatown.
- c. Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials. For 8th and 9th these selections should extend to three block open space and should be colorful or otherwise stand out to reinforce the frame of the open space on 8th and 9th Streets.

**B.4. 8th Street Frontage (Building B)**

8th Street is considered a Principal Street and serves as an important connector for auto, bicycle and pedestrian traffic. It is designated as a pedestrian-oriented spine, a priority lighting corridor, a commercial corridor and transit preferential street. The Oak Street corner is the priority location for active retail uses on Block 1 and provides an important gateway to the public open spaces at the BART Plaza and the Paseo. The Fallon Street corner is also highly prominent due to the narrow building footprint and also provides an important gateway to the Paseo. Necessary ground floor services are located on 8th Street frontage to avoid impacting the Fallon Street frontage and public spaces at the Paseo and BART entrance areas.



Figure 41: Bus Bulb Out



Figure 42: Sculptural Skylight Wayfinding

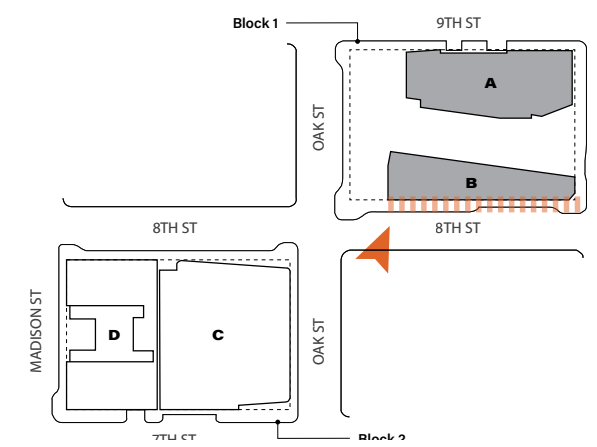


Figure 43: 8th Street Frontage

**8th Street Frontage: Design Guidelines**

- a. To allow for an eight-foot minimum clear width at sidewalk, the ground floor should be setback an average depth of two to four feet from the public right-of-way for the entire frontage length. Upper levels of the building may extend over the ground floor setback to the public right-of-way, maintaining a 15-foot-minimum height from the sidewalk. The ground floor setback may also provide room for planting treatments to reduce blank wall impacts where blank walls are unavoidable. See Figure 36. (LMSAP/DG-29: Distinct Ground Floor, DG-38: Blank Wall Limitations)
- b. Use massing breaks and architectural elements to reduce the apparent building bulk along 8th Street. The massing breaks could correspond to the internal function of the building. (LMSAP/DG-16: Three-dimensional Articulation)
- c. Building design should take advantage of the south facing frontage on 8th Street to create visually interesting patterns of lights and shadows. Provide regular rhythms of elements such as awnings, metal canopies, sunshades and bays, and/or recessed windows to create playful light and shadow patterns. Consider use of awnings, canopies, and other details over 'back of house' activities to create a more interesting facade. (LMSAP/DG-16: Three-dimensional Articulation)
- d. Active ground floor commercial spaces should be provided at the Oak Street corner with a recommended minimum length on 8th Street of not less than 30 feet, utilizing large glazing and horizontal awnings that fully wrap the corner of this façade. The minimum height of the ground floor commercial space from the 8th Street sidewalk should be no less than 15 feet and no colonnades are allowed in front of the commercial space. (LMSAP/DG-32: Views of Indoor Spaces, DG-7: Corner Building Design, DG-6: Avoid Colonnades)

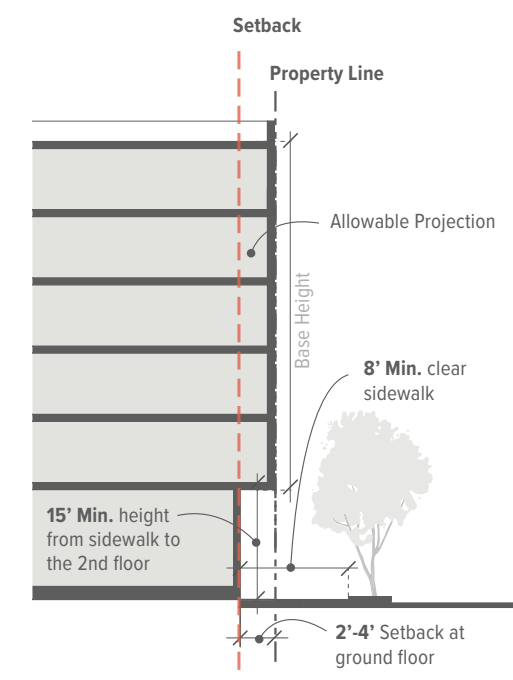


Figure 44: Ground Floor Setback



Figure 45: Planting at Ground Floor Setback



Figure 46: South Facing Frontage

BUILDING DESIGN GUIDELINES

- e. Emphasize and highlight the building corner at Oak Street and 8th Street by architectural forms and features such as change in the height, roof form, different material, and recessed or projected architectural elements, to visually define and animate the intersection. (LMSAP/DG-7: Corner Building Design)
- f. Provide active ground floor uses at Fallon Street corner with a minimum length on 8th Street of not less than 30 feet. Active uses at this location should include residential lobby entrance or residential amenity spaces. (LMSAP/DG-32: Views of Indoor Spaces)
- g. Provide windows on both sides of the community room and lobby to allow views from 8th Street frontage to the Paseo. This would activate the street frontage and also create visual connections between 8th Street and the Paseo. (LMSAP/DG-1: Public Perception, DG-32: Views of Indoor Spaces)
- h. Provide a minimum of 50% active ground floor frontage.
- i. Provide a ground floor setback at the Fallon Street corner to enhance public access and views to the Paseo from 8th Street. Upper levels of the building may extend over the corner setback to the public right-of-way. The corner ground floor setback should be a minimum of two stories. (LMSAP/DG-29: Distinct Ground Floor, DG-26: Pedestrian Scale)
- j. Limit blank walls, service doors and other non-active elements to a maximum of 30% of the linear ground floor frontage on 8th Street. Consider use of windows, architectural details, landscaping, or art details at these non-active elements. (LMSAP/DG-38: Blank Walls)
- k. New street trees should be provided along 8th to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)



Figure 47: Building Corner and Active Uses



Figure 48: Main Residential Entry at the Corner



Figure 49: Transparency of Active Frontage

**8th Street (North Side): Streetscape**

An important corridor for arriving in Oakland from the freeway and points east on bicycle, this block is an important threshold for arrival in Chinatown.

**Business Frontage Zone**

- a. The building wall of 8th Street is experienced head on by those arriving from the south and east and as such should be memorable.
- b. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

**Pedestrian Pathway Zone**

- a. An overhead steel cable for lantern elements should be provided at each light pole and at equal distances not to exceed 18'-0" between poles. Secured power should be provided at the corner-most light poles for the lantern elements
- b. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- c. Corners of sidewalks should be stamped with letters to show name of street.
- d. Extended space for pedestrians and transition from shuttles and cars to BART is provided outside the landscape furnishing zone

**Landscape Furniture Zone**

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- b. Between tree wells concrete unit pavers, stamped concrete, or cobblestones should be use to unify the three blocks of 8th and 9th at the curb edge. Stamp design could be created with an artist to be culturally relevant to Chinatown.
- c. Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials. For 8th and 9th these selections should extend to a three block open space and should be colorful or otherwise stand out to reinforce the frame of the open space on 8th and 9th.

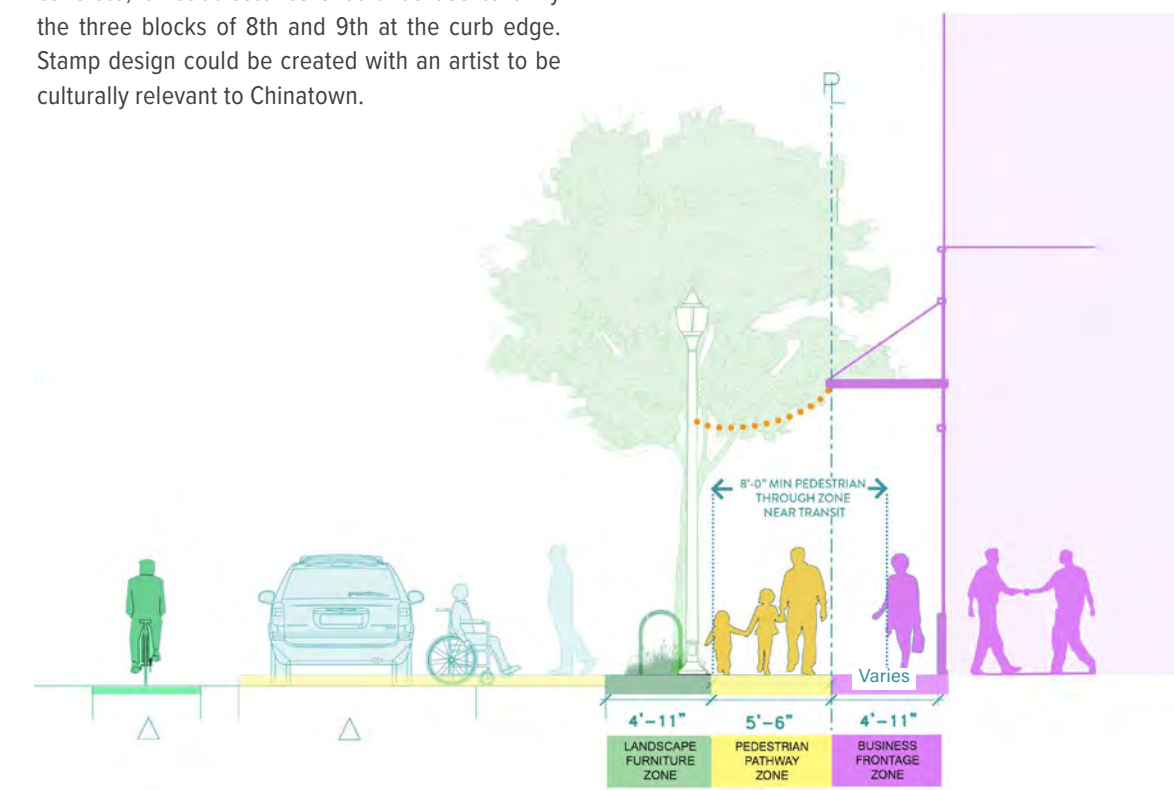


Figure 50: 8th Street Block 1

**B.5. Paseo Frontage (Buildings A and B)**

The Paseo will provide a new pedestrian-oriented connection between the BART entrances located on Oak Street and Laney College, Lake Merritt, and the Oakland Museum of California (OMCA) to the northeast. Active ground floor uses including retail and dining storefronts should be located near the BART headhouses and Oak Street. Building articulation, new landscape elements, and public art should enhance the pedestrian experience of this new public amenity. The high-rise Building A and mid-rise Building B should use a complementary and cohesive design language to create a vibrant sense of place.

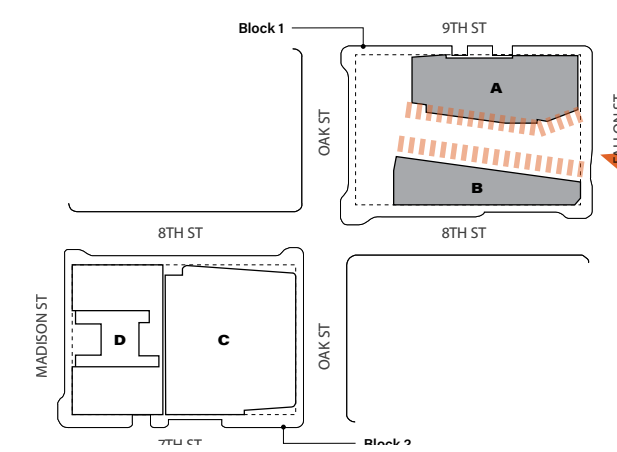


Figure 5f: Paseo Frontage

**Paseo Frontage: Design Standards**

- a. Building A tower shall be setback from the building base.
- b. Parking uses located in the Building A podium shall be wrapped in a porous screen with minimum 25% openings to allow for natural ventilation. Screening element to be integrated with overall building design, or consist of high-quality, contrasting material to create an architectural feature. (LMSAP/DG-98: Integral Design)
- c. A canopy at least four feet in width shall be located above pedestrian level at the corner of the Paseo and Oak Street. See Figure 13.

**Paseo Frontage: Design Guidelines**

- a. At the Paseo-fronting facades, visually interesting architectural features should be employed to encourage interactions with passersby and highlight focal points. (LMSAP/DG-26: Pedestrian Scale)
- b. Active uses such as storefronts, dining and building entrances should be located at the ground floor

frontage on both sides of the Paseo to create a destination and active public space. (LMSAP/DG-39: Storefronts)

- c. Use pedestrian-scale articulation such as overhangs, recesses, enhanced lighting, and increased transparency to create an active and inviting public space. (LMSAP/DG-26: Pedestrian Scale, DG-78: Building Lighting)
- d. Ensure that awnings and canopies are in scale with the building and complement the overall design while providing protection from weather and sun. (LMSAP/DG-35: Awnings)
- e. Where blank walls are unavoidable, their facades should be articulated with architectural elements such as a change in material and/or texture, screening elements, translucent windows, public art, and/or plantings. (LMSAP/DG-38: Blank Walls)
- f. Activate the roofs of the adjacent podium building(s) on the Paseo with shared amenities and other tenant uses to increase vibrancy of the pedestrian experience.



Figure 53: Active Corners along the Paseo



Figure 54: Community Room along Paseo



Figure 52: Use of Awnings and Indoor Outdoor Relation

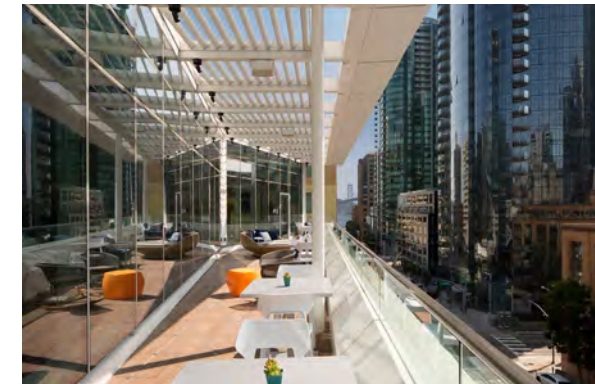


Figure 55: Active Use at Step Back

**C. Building Design – Street Frontage Guidelines for Block 2**

**C.1. 8th Street Frontage (Buildings C and D)**

8th Street is considered as a Principal Street and the “front door” for Block 2 as well as the primary address for both the high-rise Building C and the mid-rise Building D. 8th Street serves as an important connector for auto, bicycle and pedestrian traffic and it is designated as a pedestrian-oriented spine and Commercial Corridor. This segment of 8th Street links the commercial core of Chinatown with Madison Square Park, Lake Merritt BART Station, and Laney College. It is designated as a priority lighting corridor and transit preferential street. The BART Plaza on the north side of 8th Street is directly opposite Block 2. Active uses are encouraged to face 8th Street to enhance the pedestrian experience. Setbacks at the ground floor are encouraged to provide wider sidewalks and to provide a strong sense of arrival at the primary entries.

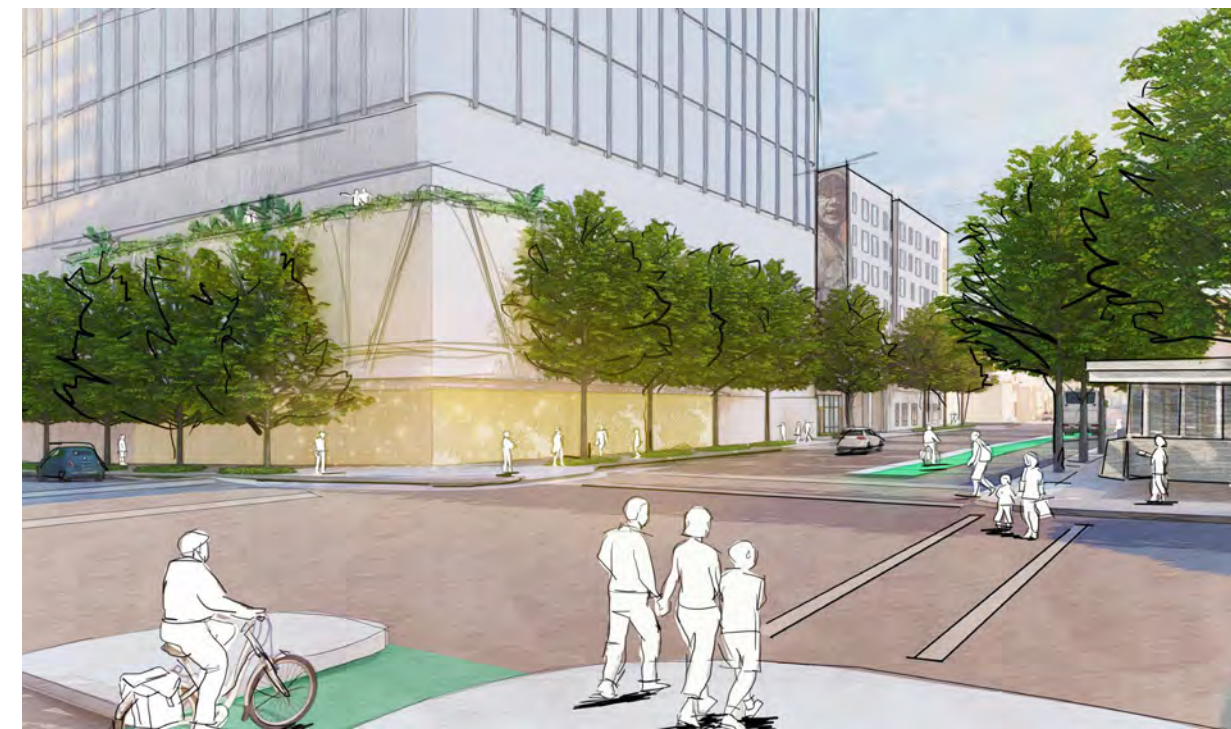
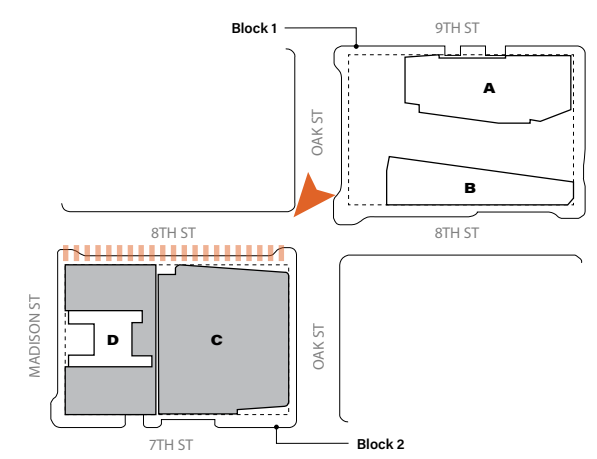


Figure 56: 8th Street Frontage



**8th Street Frontage: Design Guidelines**

- a. To allow for a five-foot six-inch minimum clear width at sidewalk, set back the ground floor an average depth of two to four feet from the public right-of-way for the entire frontage length. The depth of this ground floor step back may vary. Upper levels of the building may extend over the ground floor set back to the public right-of-way. (LMSAP/DG-29: Distinct Ground Floor)
- b. Architectural articulations and landmark features should be used to highlight and emphasize the building corners at Madison and Oak Street to define and animate the intersections. (LMSAP/DG-7: Corner Building Design)
- c. Active ground floor uses with high transparency should be provided for the entire 8th Street frontage. Active uses may include building entries, residential amenities, childcare space or other community serving uses and retail. Office and administrative uses at the ground floor should not exceed 25% of the length of the frontage. (LMSAP/DG-32: Views of Indoor Spaces)
- d. At the Oak Street corner, Building C should provide an active use that wraps the corner on to Oak Street with large windows and other architectural features that create a strong relationship to the BART entrances and to Block 1. A flexible retail use with possible connection to the office lobby should be explored. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)
- e. A small plaza, building recess or other street level open space should be provided at or near the juncture of Building C and Building D. This mid-block break should be designed as a welcoming “front porch” to accommodate a primary entry or entries for one or both buildings. (LMSAP/DG-32: Views of Indoor Spaces, DG-48: Entry)



Figure 57: Tower Articulation



Figure 58: Active Corners

BUILDING DESIGN GUIDELINES

- f. The Tower at Building C should be stepped back from the interior lot line at Building D to provide a mid-block massing break between Building C and Building D. This setback should be a minimum 10 feet for a minimum of 50% of tower perimeter, measured from the public right-of-way. It is recommended that this massing break be coordinated with the street level open space.
- g. The design of the high-rise Building C and the mid-rise Building D should be coordinated to create a cohesive frontage and to reinforce the overall sense of identity for the block. This coordinated design may take the form of a similar vocabulary of forms, openings and materials.
- h. At the 8th Street frontage the tower should be stepped back up to 10 feet minimum from the building base to reduce the apparent scale of the tower.
- i. The tower massing should employ articulation strategies such as massing breaks, tapering and sculpted corners and roofline to reduce apparent mass and create a distinctive profile on the skyline. (LMSAP/DG-22: Slender Towers, DG-24: Distinguished Tower Design)
- j. Open spaces should be provided at step backs and roof tops facing 8th Street to take advantage of views to BART Plaza, Lake Merritt and to downtown Oakland.



Figure 59: Mass Break between Buildings C and D

- k. Blank walls, service doors and other non-active elements should be limited to a maximum of 15% of the linear ground floor frontage on 8th Street. (LMSAP/DG-38: Blank Walls)
- l. Where blank walls or service doors occur at the ground level, consider incorporating artwork to enhance the 8th Street as the primary frontage for Block 2.



Figure 60: Prominent Residential Entry



Figure 61: Artwork on Blank Walls

**8th Street (South Side): Streetscape**

Located on an important corridor this block creates a street wall for framing the three block open space.

**Business Frontage Zone**

- a. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

**Pedestrian Pathway Zone**

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- b. Corners of sidewalks should be stamped with letters to show name of street.

**Landscape Furniture Zone**

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.
- b. Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

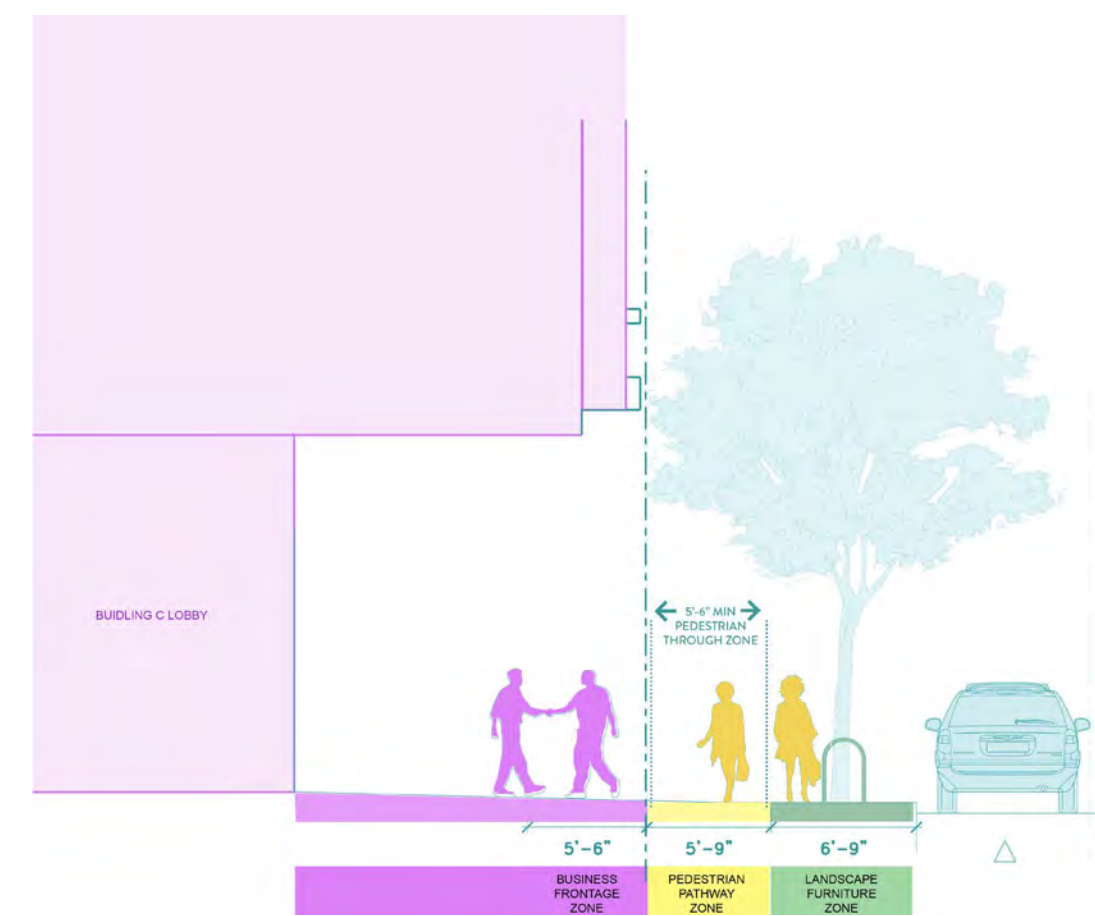


Figure 62: 8th Street Block 2

### C.2. Oak Street Frontage (Building C)

Oak Street is the southern gateway to the Lake Merritt BART Project and is identified as a green street in the LMSAP Design Guidelines. The three north bound lanes provide auto, bike and pedestrian connections from the Jack London neighborhood to Lake Merritt BART Station, the Oakland Museum and to Lake Merritt. Oak Street also provides a direct connection to the Lake Merritt BART Station from the I-880 Freeway. The east side of Oak Street is occupied by two-story residential buildings with varying setbacks. Oak Street slopes down approximately five feet from 8th to 7th Street. This slope will impact the arrangement of ground floor uses and the feasibility of entries to ground floor active uses. Wide sidewalks, active ground floor uses and upper floor step backs should be utilized to provide a welcoming pedestrian experience and a transition from the residential buildings on the south side of Oak Street to the high-rise Building C.

#### Oak Street Frontage: Design Guidelines

- a. Set back parts of the ground floor to allow for additional width at the sidewalk. The depth of the ground floor setback may vary but cannot exceed five feet. Where upper levels of the building extend over the ground floor setback, a generous 15-foot minimum clear height should be provided and the setback be free of columns to increase visibility between the interior and the public way. (LMSAP/DG-6: Avoid Colonnades, DG-32: Views of Indoor Spaces, DG-128: Streets with Special Functions)
- b. Active ground floor uses should be provided along the entire Oak Street frontage. Active uses may include building entries, a maker space or retail use. Multiple entries are encouraged where feasible. Office and administrative uses at the ground floor should not exceed 25% of the length of the frontage. (LMSAP/DG-30: Ground Floor Entrances)
- c. Architectural articulations and landmark features should be used to highlight and emphasize the building corners at 8th Street and 7th Street, including sculpted corners, changes in material and other features. (LMSAP/DG-7: Corner Building Design)
- d. At the 8th Street corner, Building C should provide an active ground floor use that wraps the corner onto 8th Street with large windows and other architectural features to create a strong relationship to the BART Plaza and to Block 1. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)
- e. At the 7th Street corner, Building C should provide an active ground floor use that wraps the corner onto 7th Street with architectural features to create a welcoming gateway element and a transition in scale to adjacent residential uses. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)
- f. The tower at Building C should be stepped back from the building base a minimum of 10 feet to reduce the apparent scale of the tower. (LMSAP/DG-19: Step Back Above the Podium Height)

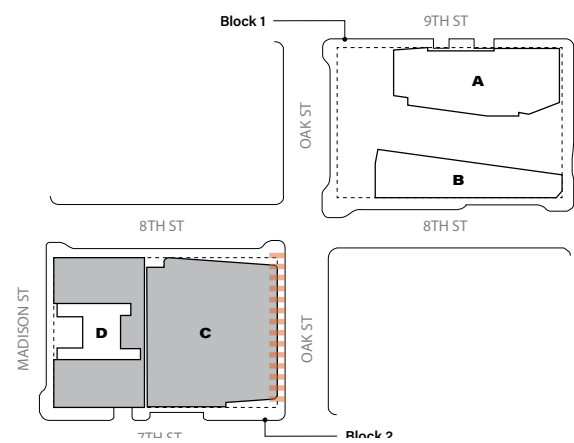


Figure 63: Active Corners

- g. Limit blank walls, service doors and other non-active elements to a maximum of 15% of the linear ground floor frontage on Oak Street. (LMSAP/DG-38: Blank Wall Limitation)
- h. Where blank walls or service doors are unavoidable at the ground level, consider incorporating artwork to reinforce Oak Street as a gateway to the Lake Merritt BART Project. (LMSAP/DG-38: Blank Wall Limitation)
- i. New street trees and lush planting should be provided along Oak to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)
- j. The Oak St. facade should be designed in response to existing buildings that form part of the 7th St/ Harrison API by considering the use of step-backs, texture and/or scale at the base of the building.



Figure 64: Step back above Building Base

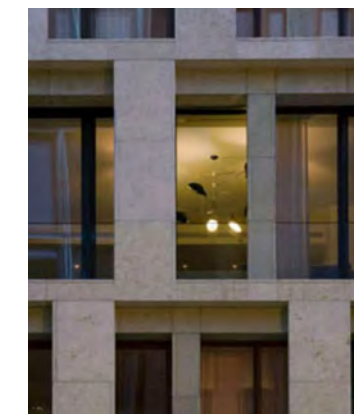


Figure 65: Example of materiality at building base

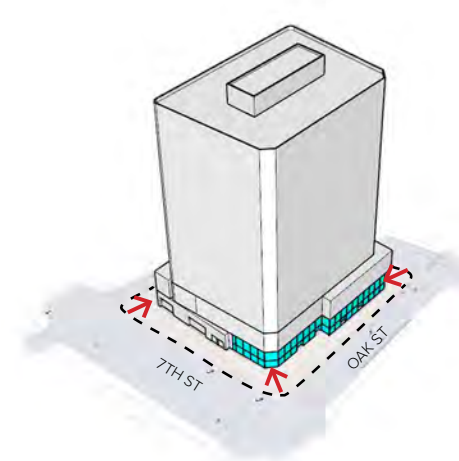


Figure 66: Example of step back at public way

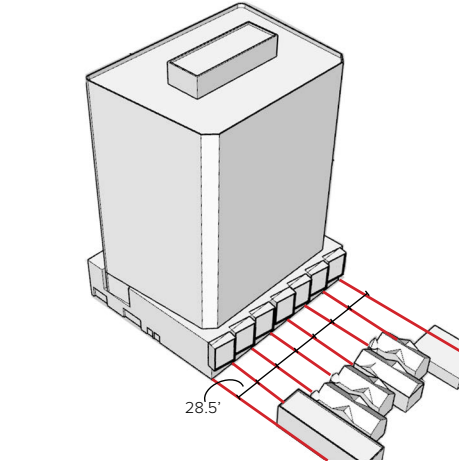


Figure 67: Example of modulation of building base

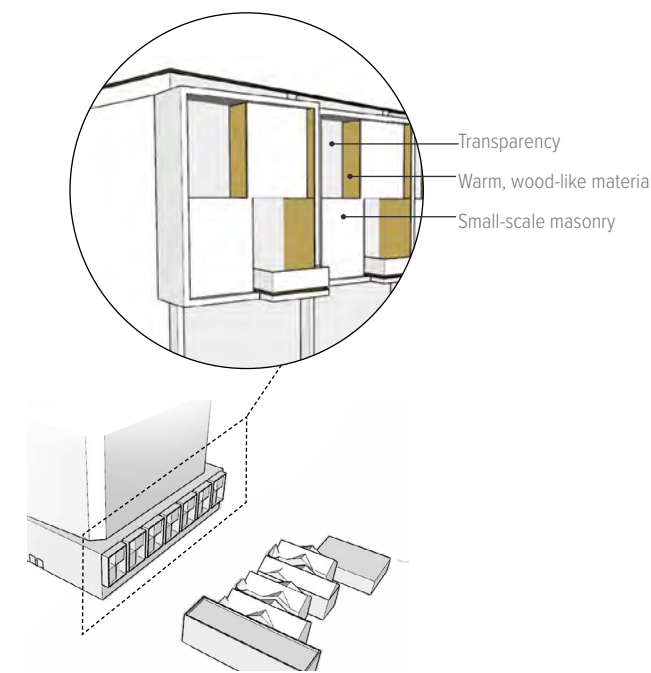


Figure 68: Example of materiality and texture at building base

**Oak Street (Block 2): Streetscape**

An important corridor for arriving in Oakland from the freeway and points south, this block is an important threshold for arrival in Chinatown and heading towards the Lake, the OMCA, or the Courthouse beyond.

**Business Frontage Zone**

- a. Setbacks at portions of the frontage of the ground floor provide space for the extension of the lobby program outside
- b. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

**Pedestrian Pathway Zone**

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- b. Corners of sidewalks should be stamped with letters to show name of street.

**Landscape Furniture Zone**

- c. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.

- d. Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

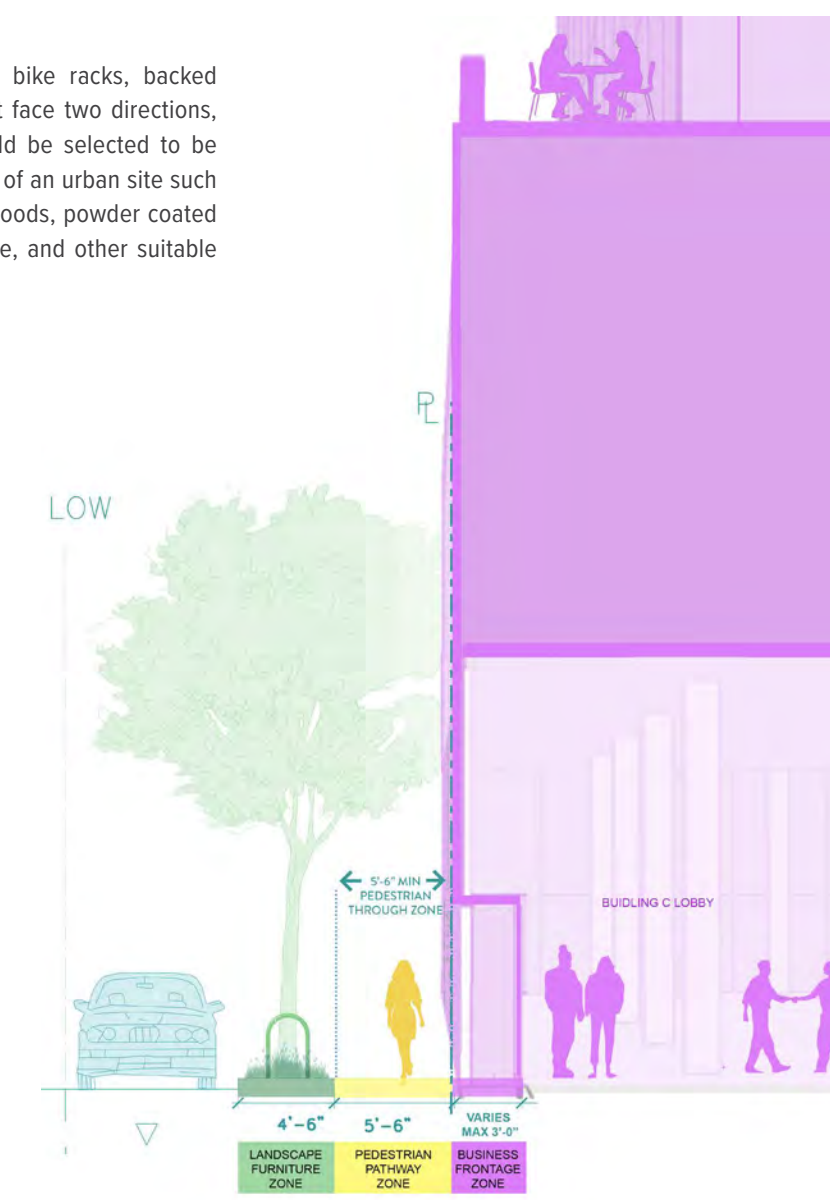


Figure 69: Oak Street Block 2

**C. 3. 7th Street Frontage (Buildings C and D)**

7th Street is a one-way street with three lanes of east bound traffic connecting the commercial and residential districts of Chinatown with south side of the Laney College campus and with the East Lake neighborhoods to the south. The south side of 7th Street is occupied by two- to three-story residential buildings with varying setbacks. Active uses should be provided at corner locations to create an attractive frontage. Parking entries and services for Building C and Building D should be consolidated mid-block at the 7th Street frontage to maximize the opportunity for active ground floor uses at the other frontages.

**7th Street Frontage: Design Guidelines**

- a. To allow for additional width at sidewalk, the ground floor should be setback an average depth of two feet from the public right-of-way for the entire frontage length. The depth of the ground floor setback may vary but cannot exceed five feet. Upper levels of the building may extend over the ground level setback. See Figure 36.
- b. Architectural articulation should be used to highlight the building corners and active ground floor uses at Oak Street and Madison Street.
- c. Active ground floor uses should be provided at the Oak and Madison Street corners with a minimum length on 7th Street of 30 feet. Active uses may include building entries, childcare classrooms, maker space or retail use. Entries to active space from 7th Street are encouraged where feasible.
- d. Parking access and service areas should be located mid-block. Curb cuts for parking access, loading and waste rooms should be consolidated where feasible. (LMSAP/DG-02: Vehicular Access)
- e. The design of parking entries and service areas at Building C and Building D should be coordinated to provide an integrated design and an attractive pedestrian frontage.
- f. Where the Building C tower is stepped back from the building base, occupied roof terraces should be provided to activate and improve the pedestrian experience. (LMSAP/DG-27: Active Upper-Stories)
- g. The tower at Building C should be stepped back from the interior lot line at Building D to provide a mid-block massing break between Building C and Building D. The setback should be at least 10 feet in width and at least 30 feet in depth, measured from the public right-of-way.

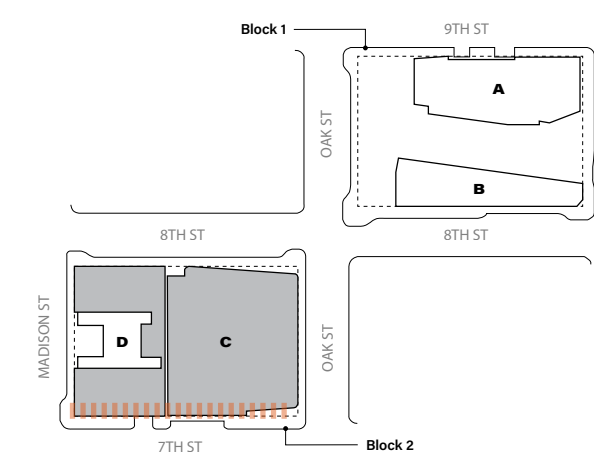


Figure 70: Active Roof Terrace at Step Backs



Figure 71: Highlight Building Corners

### 7th Street: Streetscape

A transition from larger buildings to smaller scaled houses, this street uses planting to soften and connect to neighbors gardens.

#### Business Frontage Zone

- a. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

#### Pedestrian Pathway Zone

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- b. Corners of sidewalks should be stamped with letters to show name of street.
- c. New street trees and lush planting along 7th are encouraged to reinforce its role as a green street and provide additional livability. (LMSAP/DG-10: Tree Planting and Preservation)

#### Landscape Furniture Zone

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.

- b. Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

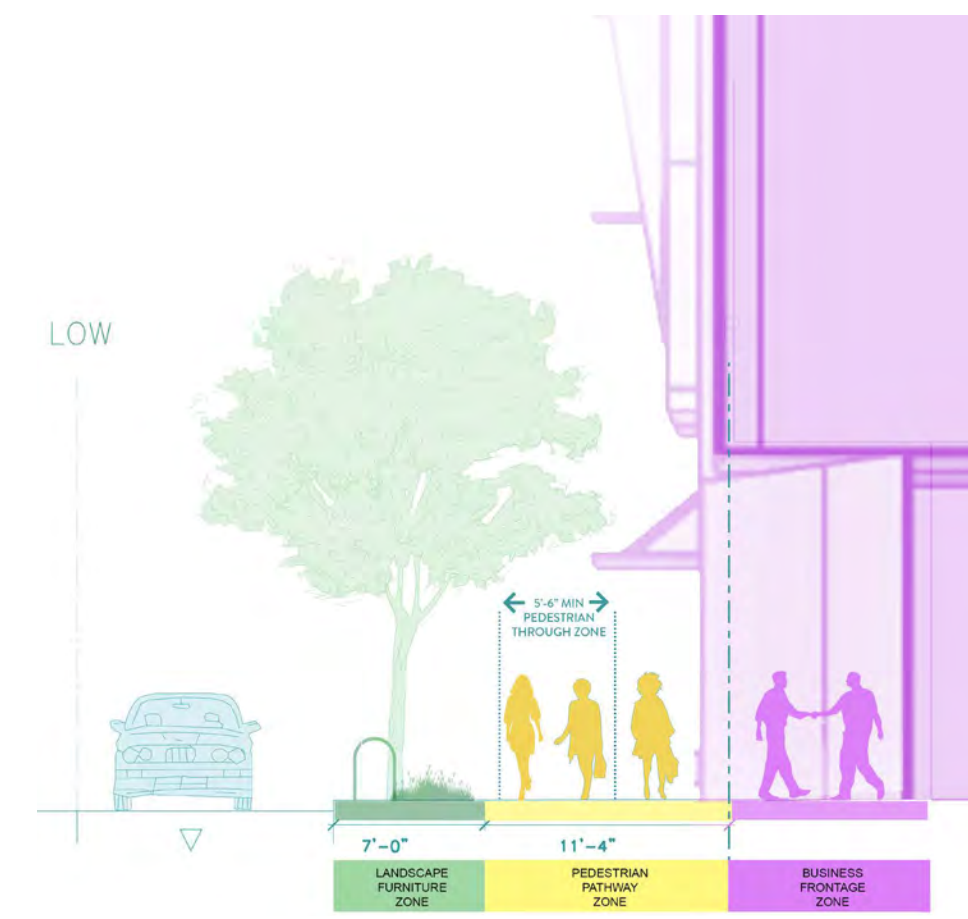


Figure 72: 7th Street Block 2



#### C.4. Madison Street Frontage (Building D)

Madison Street is the western boundary of the Lake Merritt BART Project and provides auto, bike and pedestrian connections from the Gold Coast neighborhood to Madison Square Park and the Jack London neighborhood. The west side of Madison Street is occupied by two- and three-story residential buildings with varying setbacks. Madison Street slopes down approximately five feet from 8th Street to 7th Street. This slope will impact arrangement of ground floor uses and the feasibility of street level entries. Wide sidewalks, active ground floor uses and significant massing breaks at the mid-rise building should be utilized to provide a transition from the existing residential scale on the west side of Madison Street to the mid-rise Building D.

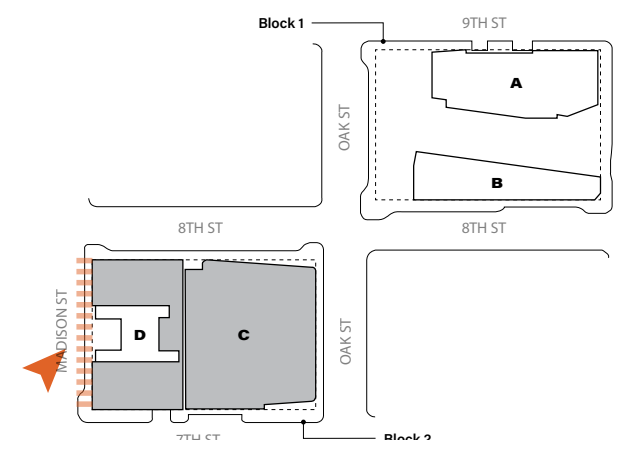


Figure 72: Madison Street Frontage

**Madison Street Frontage: Design Standards**

- a. A six-foot deep and a 40-foot long awning along 7th Street and a 10-foot long along Madison Street shall be provided at the southwest corner of Building D.
- b. Upper level horizontal awnings shall be included at the second and third floors of the 7th and Madison Street corners. These upper floor awnings shall be four-foot deep and 30-foot long along 7th Street from the 7th and Madison Street. Additional horizontal awnings above the third floor are not required but encouraged. See Figure 63.

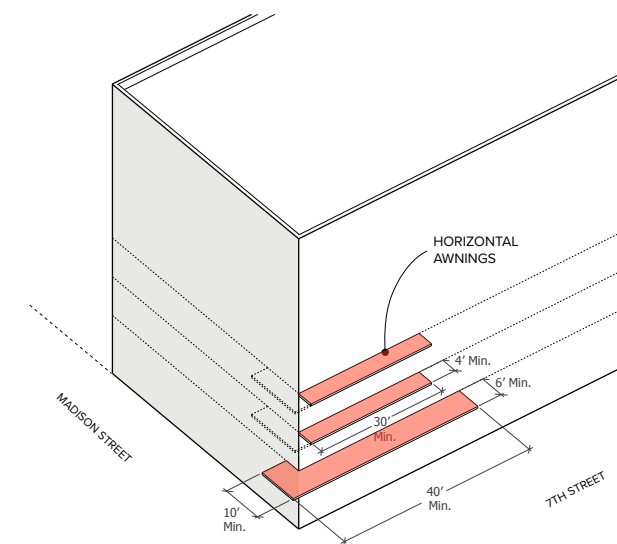


Figure 74: Awning at Building D, South-West Corner

**Madison Street Frontage: Design Guidelines**

- a. To allow for an eight foot minimum clear width at sidewalk, the ground floor should be setback a maximum depth of five feet from the public right-of-way for the entire frontage length. The depth of the ground floor setback may vary. Upper levels of the building may extend over the ground level set back to the public right-of-way. Architectural and landscape features such as awnings and planting should be used to enhance the pedestrian experience and to reduce the impact of upper floor overhang. See Figure 30. (LMSAP/DG-29: Distinct Ground Floor)
- b. Entries, transparent windows and glazing should be provided at the 8th Street and 7th Street corners with a minimum length on Madison Street of 30 feet. (LMSAP/DG-32: Views of Indoor Spaces)
- c. Active uses are encouraged at the remainder of the frontage. These uses may include offices, community amenity and childcare care facilities. (LMSAP/DG-42: Flexible Commercial Space)



Figure 75: Ground Floor Setback and Use of Awning

- d. Architectural articulations should be used to highlight the building corners and to active ground floor uses at 7th Street and 8th Street. (LMSAP DG-26: Pedestrian Scale)
- e. A street level plaza or courtyard is encouraged to create a mid-block break at the street level. This space should be at least 30 feet in width and at least 20 feet in depth. This space may serve as a building entry, outdoor space for residents or outdoor space for a potential childcare space or other community serving use. A decorative fence may be provided to secure street level open space provided the fence is at least 75% transparent with a maximum eight-foot height. (LMSAP/DG-46: Street Wall Openings)
- f. If a mid-block residential courtyard is provided at Building D at the street level or at an upper level, an opening to Madison Street is encouraged to provide additional sunlight into the courtyard and to provide a massing break at the upper floors. This opening should be at least 20 feet in width. Bridging elements are acceptable within this opening provided, they are open-air and allow significant visual transparency into the courtyard. (LMSAP/DG-46: Street Wall Openings)

**Madison Street: Streetscape**



Figure 76: Building Corner Design at 7th and Madison Streets



Figure 77: Massing Break and Street Level Plaza

A transition from larger buildings to smaller-scaled houses, this street uses planting to soften and connect to neighbors' gardens.

**Business Frontage Zone**

- a. Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.

**Pedestrian Pathway Zone**

- a. Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.
- b. Corners of sidewalks should be stamped with letters to show name of street.
- c. New street trees are encouraged along Madison to provide additional livability. (LMSAP/DG-10: Tree Planting and Preservation)

**Landscape Furniture Zone**

- a. Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet
- b. Furniture types include loop bike racks, backed

and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.

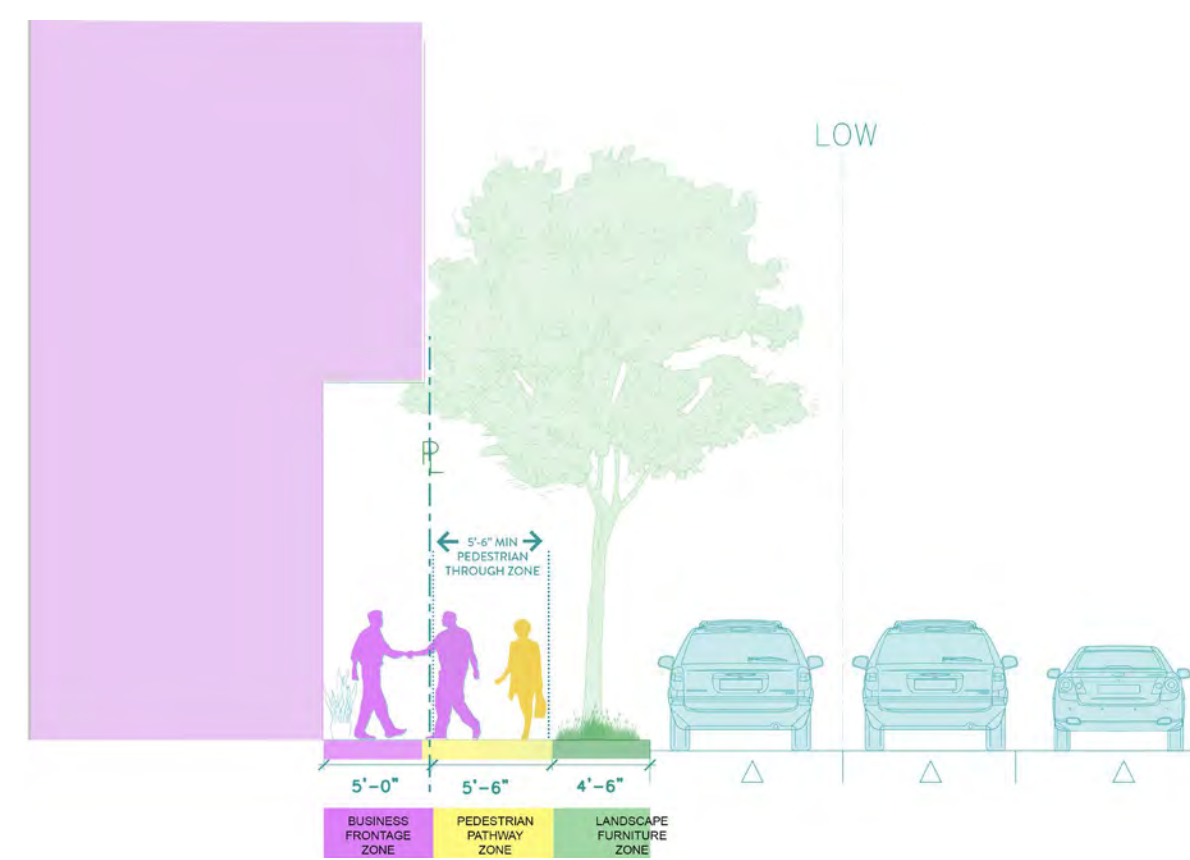


Figure 78: Madison Street Block 2

### 6. OPEN SPACE

The LMBTOD Project has an opportunity to extend the linear public realm that currently exists flowing from Madison Square Park to the BART Plaza, and into Block 1 of our redevelopment. This totally unique condition is one reason we sought to embrace the Paseo concept between Buildings A and B. It allows us to create a three block long publicly accessible open space, something that is quite uncommon in dense urban areas, and can be celebrated and highlighted by our project.

- a. The following recommendations are guidelines for the larger open spaces, not the sidewalk open space that is described above in the street frontages.
- b. In order to create a public realm that not only meets the needs of the community, but does so with the highest attention to health, safety, and environmental considerations the design should co-locate public or active ground floor programs and adjacent open space as an important partnership in the success of both. (LMSAP/DG-11: Crime Prevention through Environmental Design)
- c. Maximize planting opportunities are encouraged to create identity, buffer, to create strong edges and to generally increase greening that can be experienced by users of the open space. It is recommended that planting selections meet WELO criteria and emphasize native and drought tolerant tough urban plants. Plants should also be selected for cultural resonance with Chinatown. (LMSAP/DG-84 through DG-87: Landscaping)
- d. It is encouraged to maximize a variety of conditions that include, sunny, shady, partially sunny, partially shady in well-scaled and hospitable locations for seating. (LMSAP/DG-137: Sun Exposure)

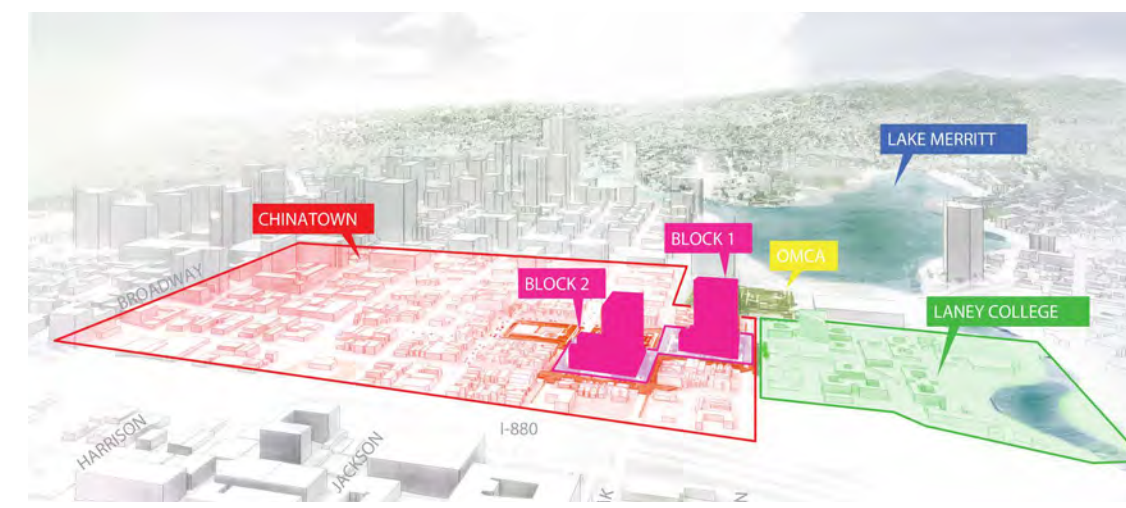


Figure 79: Adjacent Open Space Coordination

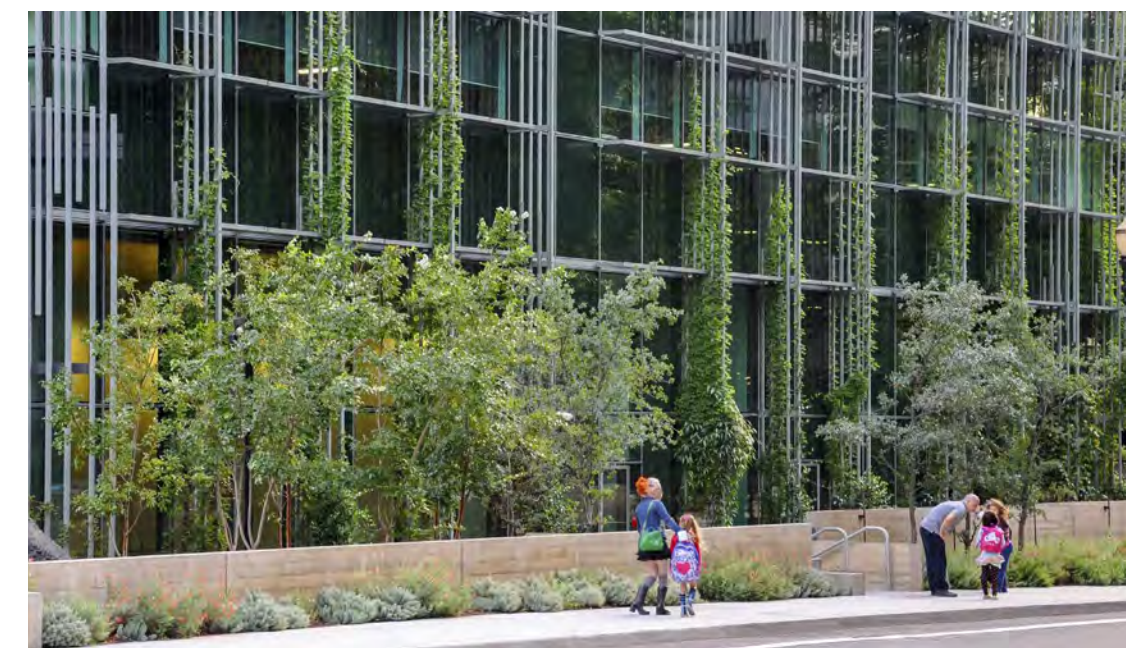


Figure 80: Maximize Greening by Utilizing the Vertical Plane

OPEN SPACE

### A. Block 1

#### A.1. The Paseo

- a. Concentrate outdoor dining at the west end of the Paseo to create a destination with multiple co-located options for dining.
- b. Create a layered program of use within the Paseo that is attractive at different times of day and attracts multigenerational users. (LMSAP/DG-142: Amenities and DG-146 through 148)
- c. Utilize materials that are appropriate to the civic and public nature of this location and meet BART's Facilities Standards. (LMSAP/DG-143: Surfaces)
- d. Paseo design shall accommodate BART maintenance truck accessibility for regular maintenance of BART infrastructure."



Figure 81: Dining



Figure 82: Interactive Art

#### Planting

- a. Planting in the Paseo must conform with BART Facilities Standards for work over existing BART tunnels. The planting must be hand watered and drainage must be directed off-site. Structural load criteria for the tunnel may limit soil volume and therefore the size or type of plants that could be planted here. The community process has resulted in a strong desire to maximize greening, so the design should try to achieve as much planting as possible within these above stated constraints.
- b. Plant selected should be low water, durable in a heavily used urban realm, and provide evergreen lush planting year round. Where possible with sun exposure, native plants, pollinator species, seasonal change, and culturally relevant plants should take priority in selections.
- c. Adequate soil volume and drainage should be provided for the long-term health of the plants.



Figure 83: Under-story Pollinator Planting



Figure 84: Culturally Relevant Planting as Seasonal Change



Figure 85: BART Facility Standards

**Furniture**

- a. Seating should be provided in a variety of types including: movable, fixed, communal, tiered, face to face, lounging, and back to back.
- b. Locations of seating should consider microclimate including wind, sun, and shade.
- c. Furniture should be high quality, durable, and beautiful. Color palette and material for furniture elements should be composed with both Paseo materials and architectural facade materials.
- d. Waste stations should provide for trash, recycling, and compost.
- e. No ash urns should be provided.

**Lighting**

- a. Site lighting should be designed to provide a consistent level of lighting for faces across the Project site and at transitions off the Project area.
- b. The Paseo identity during early evening gathering and dining should be strengthened with lighting that creates and celebrates the cultural identity of this site. Lanterns, color, and other lighting should be used to create a ceiling and sense of place.



Figure 86: Movable Furniture



Figure 87: Fixed Furniture



Figure 88: Modern Lighting

OPEN SPACE

**Pavement**

- a. Pavement in the Paseo must conform with BART Facilities Standards for work over existing BART tunnels.
- b. Pavement must be easily cleaned and durable for the heavy uses that are anticipated at most urban transit locations.
- c. Cast-in-place concrete with Lithocrete finish or other durable materials may be used for strong graphic pattern.
- d. Trench grate will integrate with ground floor pattern.

**A.2. Roof Deck**

- a. In consideration of local ecology of Lake Merritt and the open water, roof deck plantings should provide ecosystem services in the form of a pollinator garden. (LMSAP/DG-84 through DG-87)
- b. Program and utilization of roof decks should provide activation and diverse opportunities by multigenerational users.



Figure 89: Lithocrete Activates Ground Plane

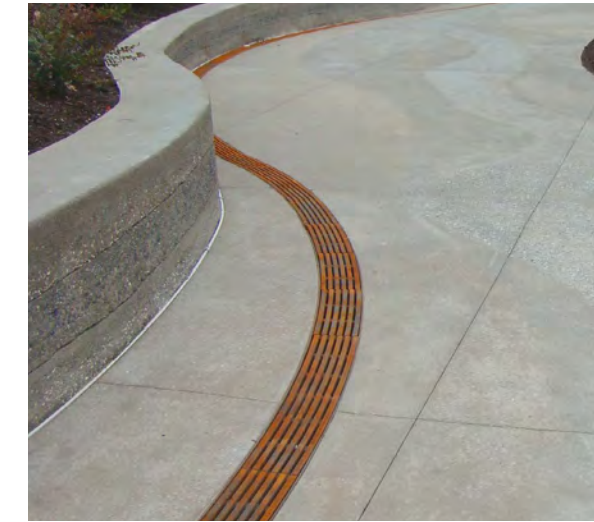


Figure 91: Trench Grate



Figure 90: Roof Top Pollinator Garden



Figure 92: Variety of Roof Top Programming



**B. Block 2**

**B.1. Entry Plaza**

- a. Entry plaza pavement should use high quality materials and be differentiated from adjacent sidewalks. (LMSAG/DG-145: High Quality Materials)
- b. Visual connection to interiors of the building should be maintained for people arriving or departing from the building. (LMSAP/DG-11: Crime Prevention through Environmental Design)

**B.2. Residential Courtyard**

- a. Create a layered program of use within the courtyard that is attractive at different times of day and attracts multigenerational users. (LMSAP/DG-142: Amenities, DG-146 through DG-148)
- b. Provide low-water and native planting that contributes to the local ecology. (LMSAP/DG-84 through DG-87)



Figure 93: Distinguished Plaza Pave Material

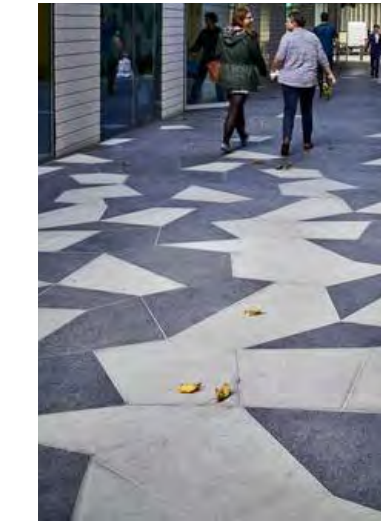


Figure 95: Distinguished Plaza Pave Material



Figure 94: Native Planting Low-Water Use



Figure 96: Space for Different Activities, Ages

### B.3. Daycare Open Space

- a. Children's play environments must meet the required codes of play spaces, but should also connect children with the natural world through materials, planting selections, and art.



Figure 97: Nature Exploration



Figure 98: Nature Exploration

## 7. SIGNAGE

- a. **Consistency.** Signage will be consistent with the guidelines set forth in the Lake Merritt Station Area Plan Design Guidelines (LMSAP/DG-79 through DG-83).
- b. **Integrated Design.** Signage should be designed to reinforce the overall design character of the Lake Merritt Project. Signs and mounting systems should be integrated into the exterior design and should be constructed of high quality materials that complement the exterior material and color palette. (LMSAP/DG-79 through DG-82)
- c. **Visibility and Illumination.** Signage should be located and designed to be readily visible by pedestrians. Graphics should be designed to be highly legible and consistent with the exterior design intent. Illumination should be provided to ensure signage is visible in the evening hours. (LMSAP/DG-79: Illumination, DG-83: Legibility and Readability)



Figure 99: Multilingual Graphic

**8. LIGHTING**

- a. Exterior site lighting will be consistent with most of the guidelines set forth in the Lake Merritt Station Area Plan Design Guidelines (LMSAP/ DG-123: Lighting and Safety, DG-124: Pedestrian-Oriented Lighting, DG-150: Lighting) However, the additional of light as a goal without consideration of overall levels is not recommended. Lighting design proposed is consistent with best practices and generally focused on evenness of transitions rather than increased brightness.
- b. Provide adequate lighting to provide a safe environment for pedestrian safety while conforming to current best practices to mitigate light pollution.
- c. The lighting plan will be designed to create well lit plazas and pedestrian pathways through the site including surrounding sidewalks.



Figure 100: Lighting of Local Oban Festival



Figure 101: Extend Festival Infrastructure



Figure 102: Modern Light Fixtures



Figure 105: Integrated Lighting Design



Figure 103: Existing Skylight



Figure 104: Skylight becomes a Beacon



Figure 106: Integrated Lighting Design

### 9. SUSTAINABLE DESIGN

This development is inherently sustainable in terms of land use due to the high density residential and commercial development near a major urban transit hub. Additional sustainable design measures can positively contribute to environmental benefits such as promotion of occupant comfort, water conservation, energy conservation, and healthy building environments. A few general sustainability measures are as follows:

#### Site Design Measures

The development shall employ architectural strategies that are responsive to the local climate including solar orientation, prevailing winds, and precipitation.

- a. Orient units and/or provide architectural shading treatments to maximize winter solar exposure and minimize summer exposure.
- b. Provide on-site stormwater treatment as appropriate to the scale of the buildings and available open space. The stormwater design shall comply with the Municipal Regional Permit Order No. R2-0074 and the City of Oakland Storm Drainage Design Guidelines, which establish a 25% goal for peak flow reduction compared to existing conditions, to the extent possible.
- c. Provide on-site secure bicycle parking and secure bicycle parking for the most biked to station of the BART network, reducing the carbon footprint.



Figure 107: Stormwater Management



Figure 108: Stormwater Management

- d. Reduce parking capacity to a reasonable minimum.
- e. Consider designated parking for carpool vans or car share vehicles.
- f. Use native, drought-tolerant and shade tolerant landscaping to minimize irrigation required.
- g. Provide more vegetated spaces and street trees to reduce the heat island effect.



Figure 109: Bike Rack



Figure 110: Bike Lockers



Figure 111: Scooter Corral



Figure 112: Kiss and Ride

#### Building Design Measures

- a. Use reflective roofing to minimize heat island effect.
- b. Use water-conserving fixtures and irrigation systems.
- c. Design building envelope, HVAC systems, lighting, and other systems to maximize energy efficiency. Consider fundamental commissioning of development systems.
- d. Consider on-site electrical generation or purchase of off-site renewable energy.
- e. Consider all-electrical building design and reduction of gas.
- f. Provide adequate facilities to allow for recycling by residents.
- g. Where possible, use recycled, salvaged, sustainably harvested, or locally produced materials.
- h. Use low- or no-VOC materials in interior spaces.
- i. The development should be designed and constructed in accordance with the recommendations of a recognized "Green" rating system such as GreenPoint, Enterprise Green Communities, and USGBC LEED rating.



**ATTACHMENT C:**

Vesting Tentative Tract Map No. 8560 and 8577

# LAKE MERRITT BART DEVELOPMENT

## VESTING TENTATIVE TRACT MAP NO. 8560 (BLOCK 1) AND 8577 (BLOCK 2) TRACT MAP NO. 8560, A 3 LOT SUBDIVISION AND REMAINDER LOT (2 RESIDENTIAL UNITS, 2 COMMERCIAL UNITS, AND 2 NON-CONDOMINIUM PARCELS) TRACT MAP NO. 8577, A 2 LOT SUBDIVISION (1 RESIDENTIAL UNIT AND 5 COMMERCIAL UNITS) CITY OF OAKLAND ALAMEDA COUNTY STATE OF CALIFORNIA

### PROJECT DESCRIPTION

#### BLOCK 1 (TRACT MAP NO. 8560)

THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 1, BOUND BY 8TH STREET, FALLON STREET, 9TH STREET, AND OAK STREET. A NEW MARKET RATE RESIDENTIAL BUILDING (BUILDING A), A NEW SENIOR HOUSING BUILDING (BUILDING B), AND A NEW PEDESTRIAN PASEO WILL BE CONSTRUCTED ON THE PROPERTY SITE AND WILL MAINTAIN AND IMPROVE ACCESS TO THE EXISTING LAKE MERRITT BART STATION. THE PROJECT WILL REMOVE THE 11 EXISTING LOTS AND WILL BE SUBDIVIDED INTO 3 NEW LOTS AND 1 REMAINDER PARCEL.

#### BLOCK 2 (TRACT MAP NO. 8577)

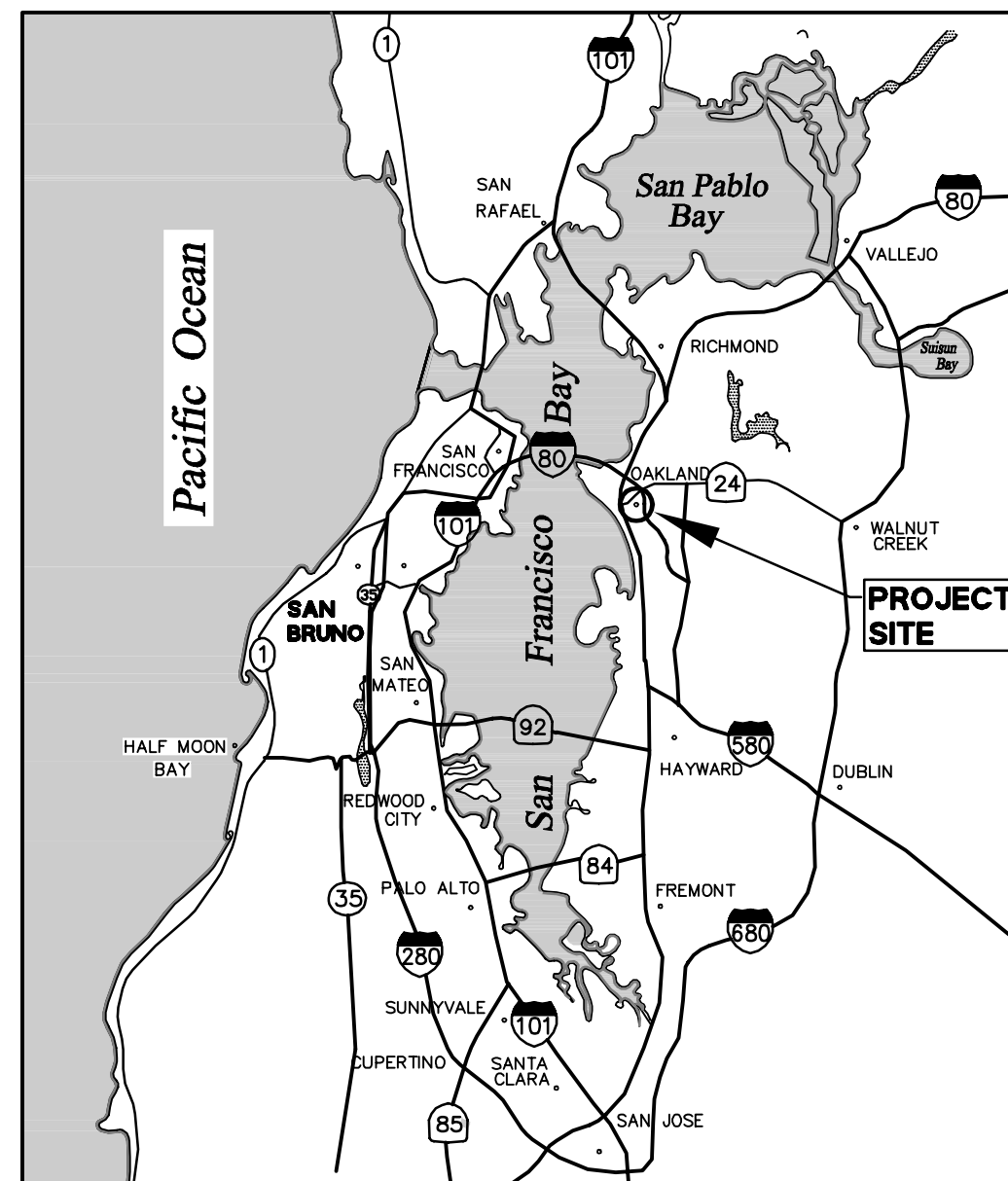
THE PROJECT IS PROPOSING TO DEMOLISH THE EXISTING PAVEMENT AND STRUCTURES ON BLOCK 2, BOUND BY 7TH STREET, OAK STREET, 8TH STREET, AND MADISON STREET. A NEW OFFICE BUILDING (BUILDING C) AND A NEW AFFORDABLE HOUSING BUILDING (BUILDING D) WILL BE CONSTRUCTED ON THE PROPERTY SITE. THE PROJECT WILL SUBDIVIDE THE EXISTING LOT INTO 2 NEW LOTS.

### GENERAL NOTES

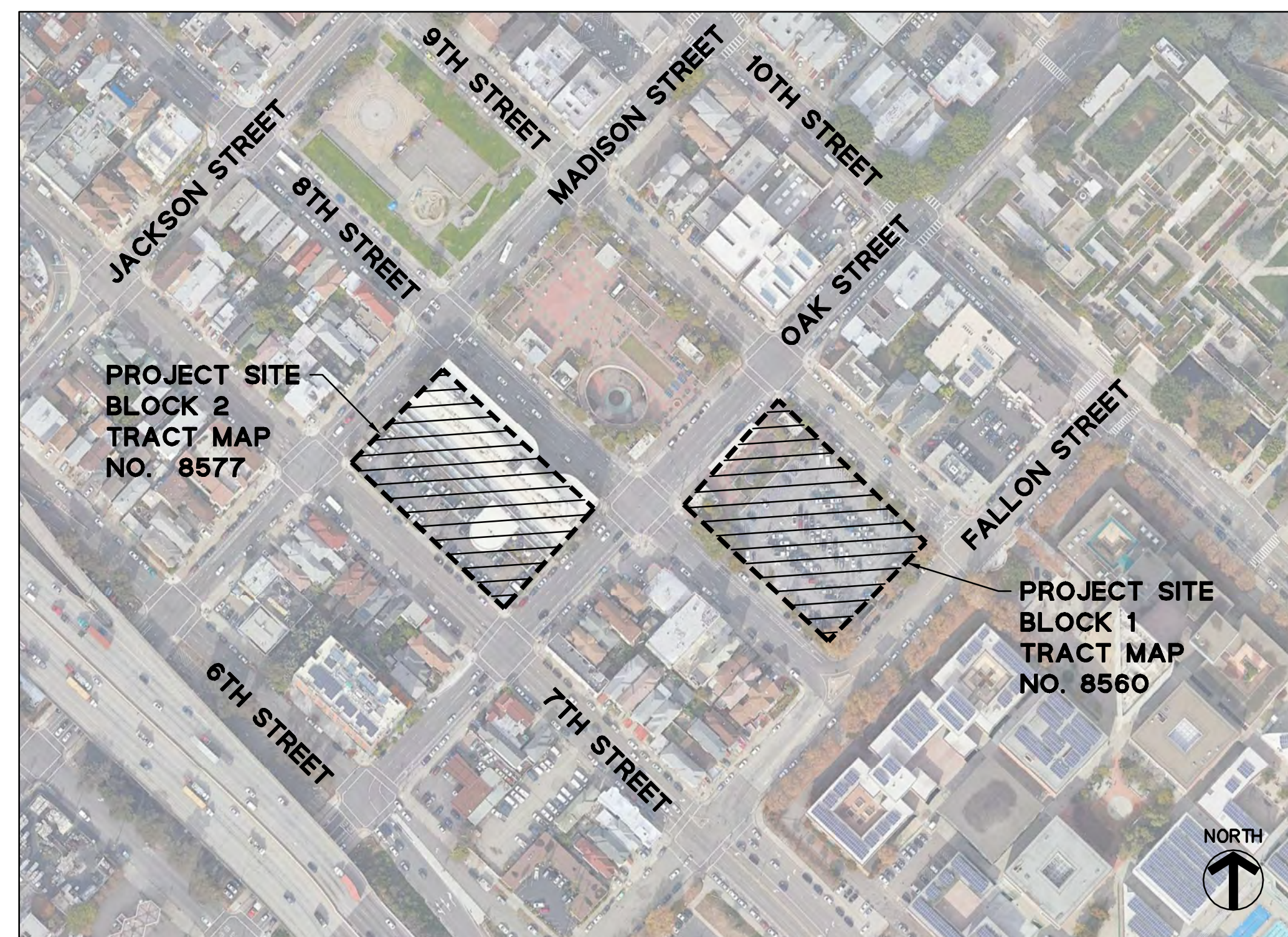
- VESTING TENTATIVE TRACT MAP:** THESE VESTING TENTATIVE TRACT MAPS ARE BEING FILED IN ACCORDANCE WITH CHAPTER 3, ARTICLE 2, SECTION 66452 AND WITH CHAPTER 2, ARTICLE 1, SECTION 66426 OF THE SUBDIVISION MAP ACT.
  - CONDOMINIUM UNITS AND NON-CONDOMINIUM PARCELS:**
    - VESTING TRACT MAP NO. 8560 (BLOCK 1) - A 3 LOT SUBDIVISION WITH 1 REMAINDER PARCEL BEING: 1 RESIDENTIAL CONDOMINIUM UNIT AND 1 COMMERCIAL CONDOMINIUM UNIT AS TO LOT 1, 1 RESIDENTIAL CONDOMINIUM UNIT AND COMMERCIAL CONDOMINIUM UNIT AS TO LOT 2, AND 2 NON-CONDOMINIUM PARCELS AS TO LOT 3 AND A REMAINDER PARCEL.
    - VESTING TRACT MAP NO. 8577 (BLOCK 2) - A 2 LOT SUBDIVISION BEING: 4 COMMERCIAL CONDOMINIUM UNITS AS TO LOT 1, AND 1 RESIDENTIAL CONDOMINIUM UNIT AND 1 COMMERCIAL CONDOMINIUM UNIT AS TO LOT 2.
  - MULTIPLE TRACT MAPS:** THE DEVELOPER INTENDS TO FILE TWO TRACT MAPS PURSUANT TO CHAPTER 3, ARTICLE 4, SECTION 66456.1 OF THE SUBDIVISION MAP ACT.
  - SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY UNDER THE SUPERVISION OF DAVIS THRESH, PLS #6868, PERFORMED ON MAY 13TH, MAY 15TH, AND MAY 22ND, 2019. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
  - FEMA DESIGNATED FLOOD ZONE:** PURSUANT TO THE FEDERAL EMERGENCY MANAGEMENT AGENCY, NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, COMMUNITY NO. 06001C0067H, EFFECTIVE DATE DECEMBER 21, 2018, THE SUBJECT PROPERTY LIES WITHIN FLOOD ZONE 'X' - AREAS DETERMINED OF MINIMAL FLOOD HAZARD.
  - UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES, AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
  - BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HERON IS BASED UPON RESOLUTIONS OF RECORD STREET AND LOT DIMENSIONS AND COLLECTED STREET MONUMENT LOCATIONS WITHIN THE SURROUNDING STREETS. MONUMENT COLLECTION WAS CONDUCTED ON APRIL 19, 2019. NO CURRENT MAP OR RECORD OF SURVEY CURRENTLY EXISTS FOR THE MAPPED BLOCKS; DEEDS MAKE REFERENCE TO KELLERSBERGER'S MAP OF OAKLAND FILED IN BOOK 7 OF MISCELLANEOUS MAPS AT PAGE 3, ALAMEDA COUNTY RECORDS.
  - HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.
  - BENCHMARK:** FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET. ELEVATION = 23.062 (NAVD88)
- ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COOVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COOVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COOVD ELEVATIONS. (ALL FUTURE IMPROVEMENT PERMIT PLANS WILL REFERENCE THE CITY OF OAKLAND VERTICAL DATUM)**

### PROJECT DATA

<b>OWNERS:</b>	STRADA INVESTMENT GROUP 101 MISSION STREET, SUITE 420 SAN FRANCISCO, CA 94105 PHONE: (415) 263-9151 CONTACT: WILLIAM GOODMAN	EAST BAY ASIAN LOCAL DEVELOPMENT CORP 1825 SAN PABLO AVENUE, SUITE 200 OAKLAND, CA 94612 PHONE: (510) 287-5353 CONTACT: ANDREW MATSAS
<b>ARCHITECTS:</b>	PYATOK ARCHITECTS 1611 TELEGRAPH AVENUE, SUITE 200 OAKLAND, CA 94612 PHONE: (510) 465-7010 CONTACT: PETER WALLER	SOLOMON CORDWELL BUENZ ARCHITECTS 255 CALIFORNIA STREET, 3RD FLOOR SAN FRANCISCO, CA 94111 PHONE: (415) 216-2450 CONTACT: CYRIL CHONG
<b>CIVIL ENGINEER:</b>	BKF ENGINEERS 255 SHORELINE DRIVE, SUITE 200 REDWOOD CITY, CA 94065 PHONE: (650) 482-6377 CONTACT: SIMON NORTH	
<b>LANDSCAPE ARCHITECT:</b>	EINWILLERKUEHL LANDSCAPE ARCHITECTURE 318 HARRISON STREET, SUITE 301 OAKLAND, CA 94607 PHONE: (510) 891-1696 CONTACT: SARAH KUEHL	
<b>ASSESSOR PARCEL NO.:</b>	001-0169-001 (BLOCK 1) 001-0171-002 (BLOCK 2)	
<b>EXISTING LAND USE:</b>	COMMERCIAL	
<b>PROPOSED LAND USE:</b>	MIXED USE	
<b>LAND AREA:</b>	BLOCK 1: 60,031 SF (1.378 ACRES) BLOCK 2: 60,028 SF (1.378 ACRES)	
<b>UTILITY INFORMATION:</b>	WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD) FIRE PROTECTION: CITY OF OAKLAND / EBMUD SEWAGE DISPOSAL: CITY OF OAKLAND STORM DRAIN: CITY OF OAKLAND GAS: PACIFIC GAS & ELECTRIC (PG&E) ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E) TELEPHONE: AT&T CABLE TELEVISION: COMCAST	



LOCATION MAP  
NTS



VICINITY MAP  
NTS

### ABBREVIATIONS

AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BACK OF WALK
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EV	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
HCR	=	HANDICAP RAMP
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
P	=	POST
PKM	=	PARKING METER
SDCO	=	STORM DRAIN CLEANOUT
SDDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UB	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	LOT LINE
---	---	EASEMENT LINE
---	---	RECORD LOT LINE TO BE REMOVED
---	---	CURB AND GUTTER
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWCUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	CATCH BASIN
---	---	SSMH
---	---	SDMH
---	---	SDDI
---	---	WATER VALVE
---	---	GAS VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DCDA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	STREET LIGHT
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE

### SHEET INDEX

SHEET NO	DESCRIPTION
C1.0	TITLE SHEET
C2.1	EXISTING PARCELIZATION (BLOCK 1)
C2.2	EXISTING PARCELIZATION (BLOCK 2)
C3.1	PROPOSED PARCELIZATION PLAN (BLOCK 1)
C3.2	PROPOSED PARCELIZATION PLAN (BLOCK 2)
C4.1	PROPOSED GRADING PLAN (BLOCK 1)
C4.2	PROPOSED GRADING PLAN (BLOCK 2)
C5.1	PROPOSED UTILITY PLAN (BLOCK 1)
C5.2	PROPOSED UTILITY PLAN (BLOCK 2)

### ENGINEER'S STATEMENT

THIS TENTATIVE MAP HAS BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

*Simon North*  
SIMON R. NORTH, P.E. CA 64657  
VICE PRESIDENT  
BKF ENGINEERS

04/26/2021  
DATE

### SURVEYOR'S STATEMENT

THIS TENTATIVE MAP HAS BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD SURVEY PRACTICE.

*Davis Thresh*  
DAVIS R. THRESH, P.L.S. CA 6868  
PRINCIPAL  
BKF ENGINEERS

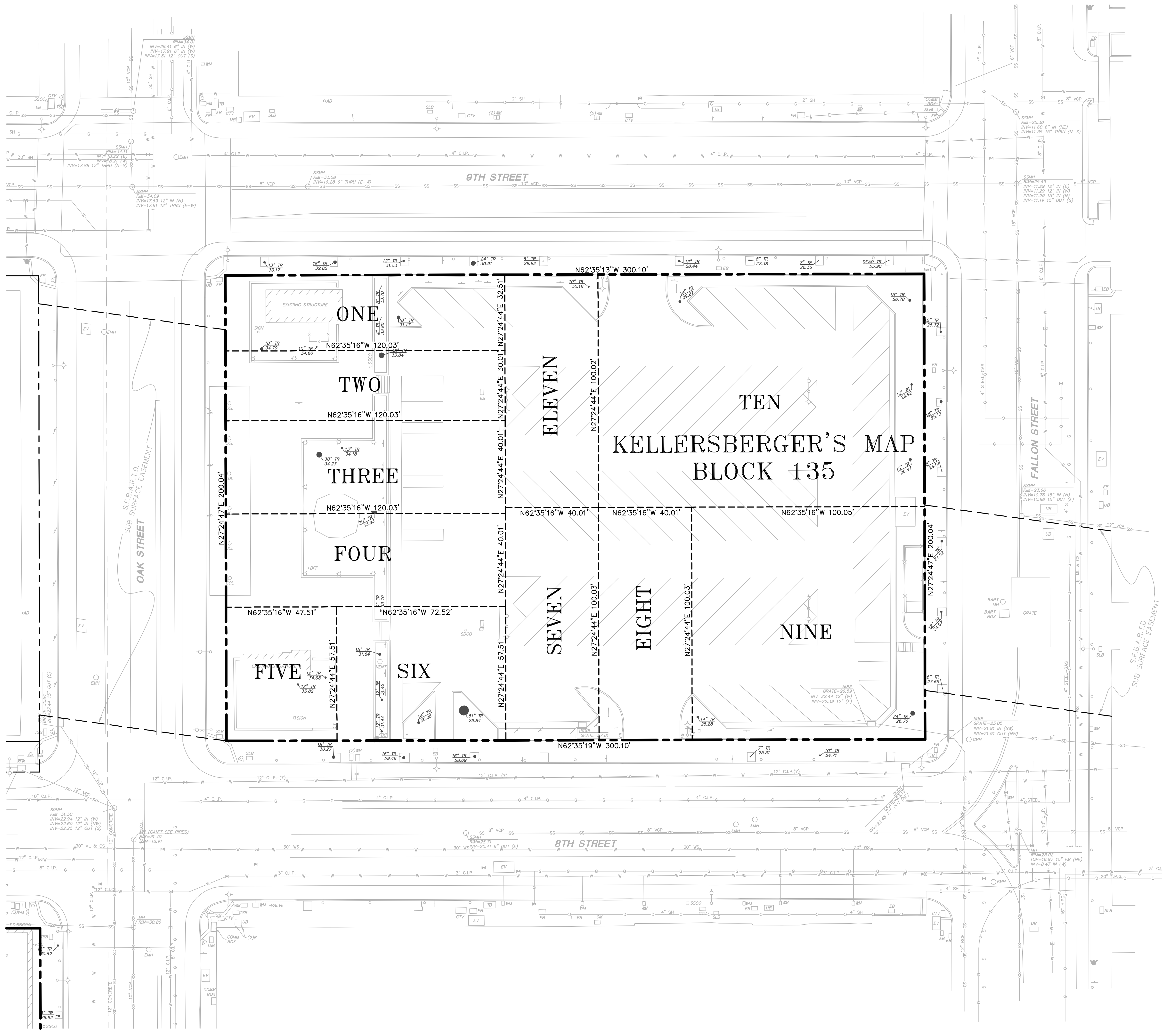
04/26/2021  
DATE

LAKE MERRITT BART DEVELOPMENT  
VESTING TENTATIVE TRACT MAP NO. 8560/8577  
TITLE SHEET  
ALAMEDA COUNTY  
CITY OF OAKLAND

Date	04/26/2021
Scale	AS NOTED
Design	DFS
Drawn	DFS
Approved SRN	
Job No	20190110
Drawing Number:	C1.0



DRAWING NAME: \\BKF-r-c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C2.1-C2.2\_ExistingParcelization.dwg  
PLOT DATE: 04-26-21



ANNOTATION & LEGEND

- AD = AREA DRAIN
- B = BOLLARD
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- WP = WATER PIPE
- ☆ = AREA/YARD LIGHT
- ⦿ = FIRE HYDRANT
- ⊙ = SIGN
- ⊕ = STREET LIGHT
- ⊖ = GAS VALVE
- ⊗ = WATER VALVE
- ⊘ = SIGNAL LIGHT
- E—E— = ELECTRICAL LINE
- G—G— = GAS LINE
- SD—SD— = STORM DRAIN LINE
- SS—SS— = SANITARY SEWER LINE
- UN—UN— = UNKNOWN UTILITY LINE
- W—W— = DOMESTIC WATER LINE
- — — — = BOUNDARY LINE
- - - - - = RECORD LOT LINE TO BE REMOVED
- · - · - · = EASEMENT LINE

NOTES

FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019  
ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF

HORIZONTAL CONTROL

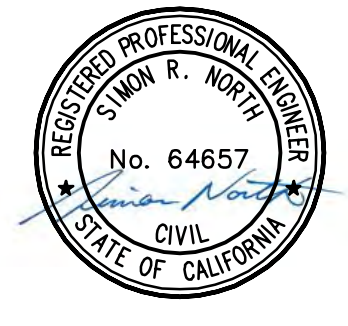
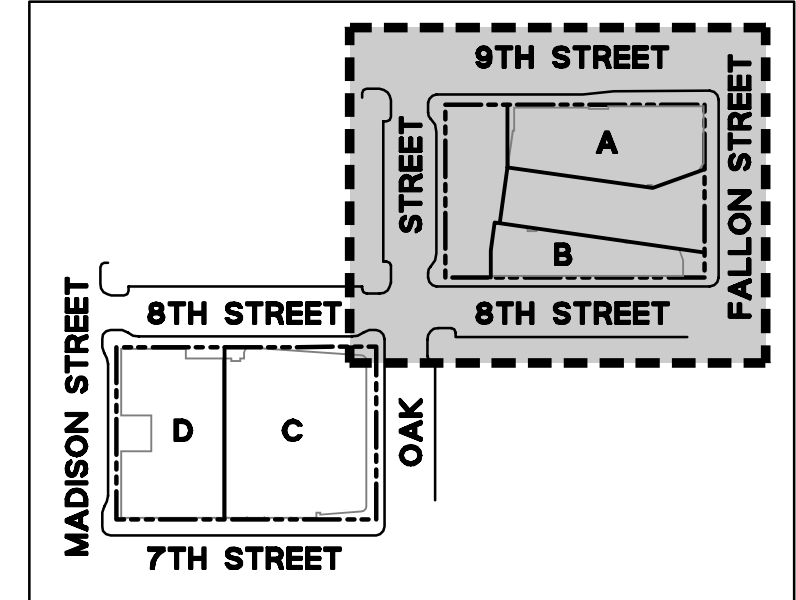
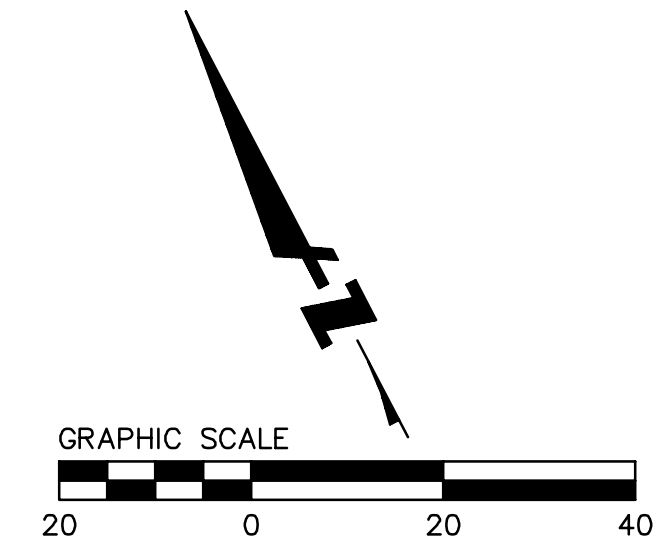
HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS85), EPOCH 2017.00

BENCHMARK

FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET.

ELEVATION = 23.062 (NAVD88)

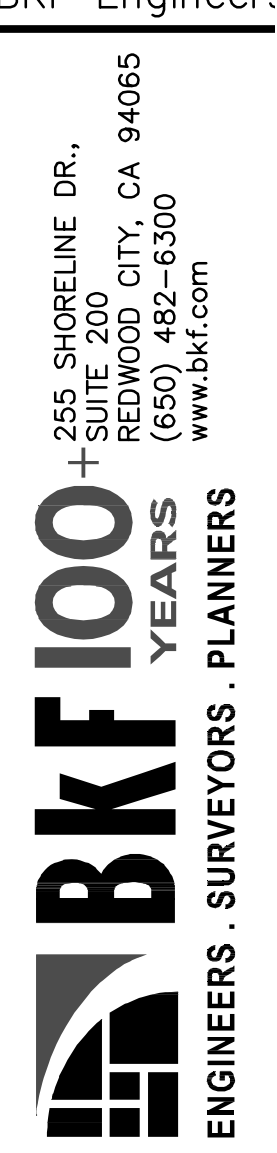
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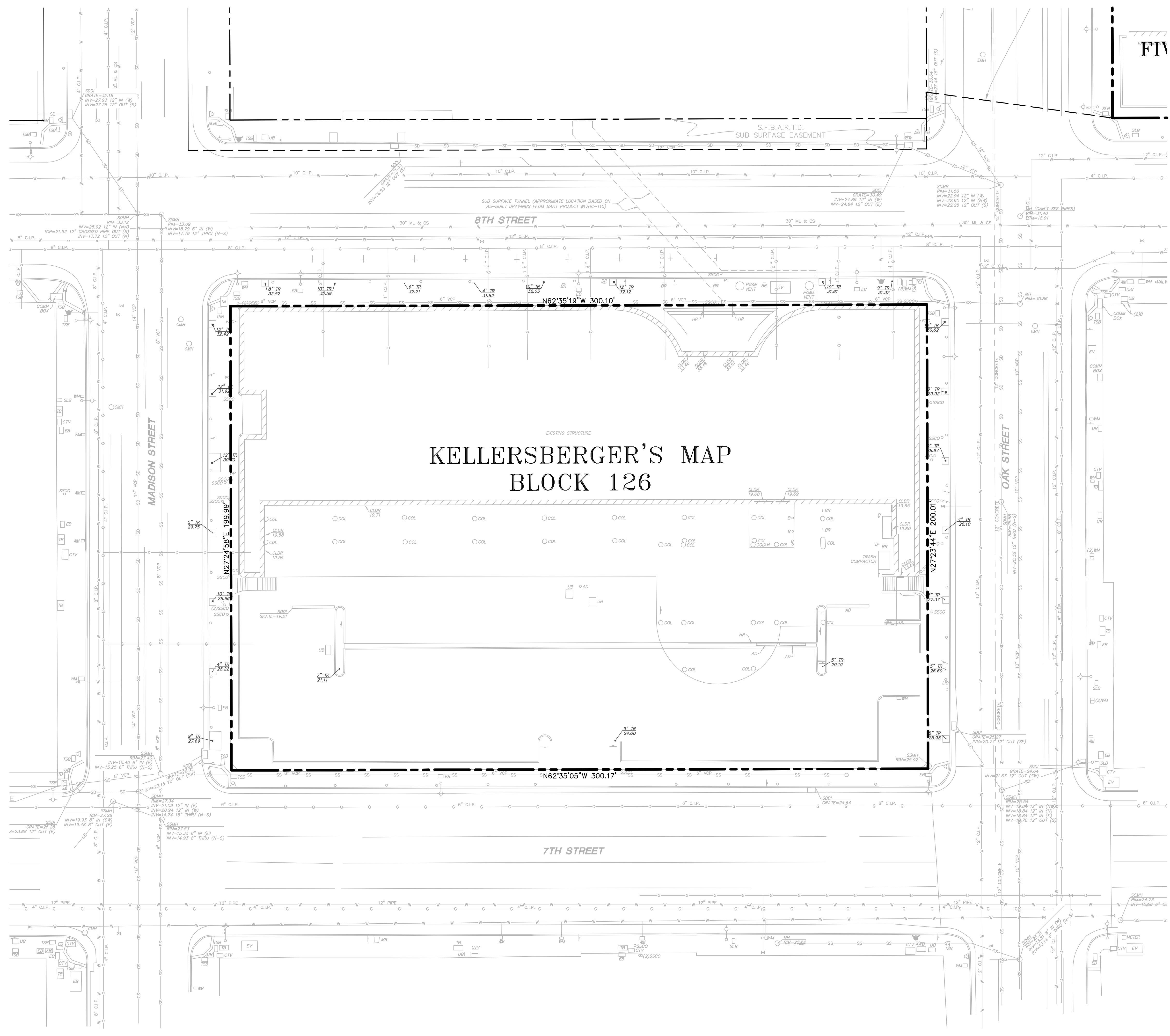
LAKE MERRITT BART DEVELOPMENT (BLOCK 1)  
VESTING TENTATIVE TRACT MAP NO. 8560  
EXISTING PARCELIZATION (BLOCK 1)  
CITY OF OAKLAND  
ALAMEDA COUNTY  
CALIFORNIA

Revisions	Date	By	No.
	02/22/2021		

Scale 1" = 20'  
Design DFS  
Drawn DFS  
Approved SRN  
Job No 20190110

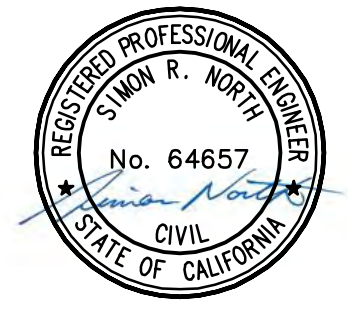
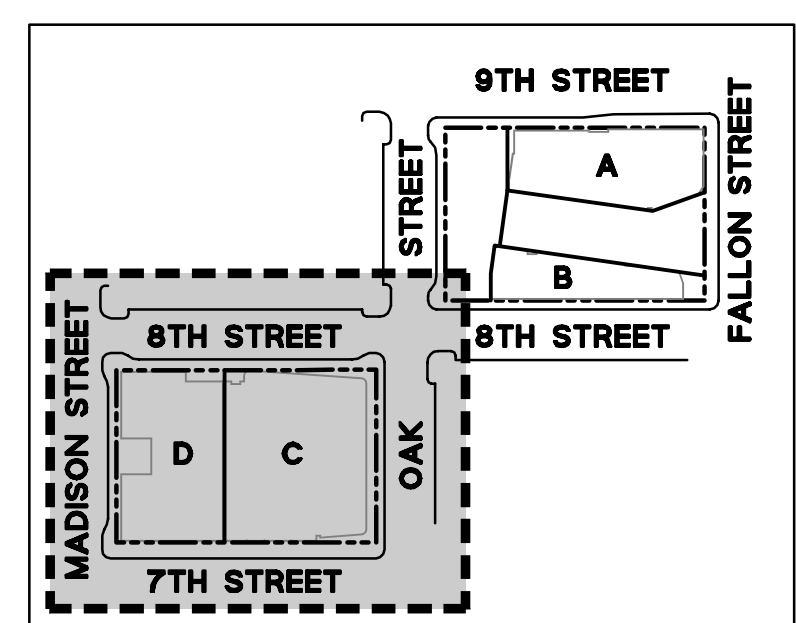
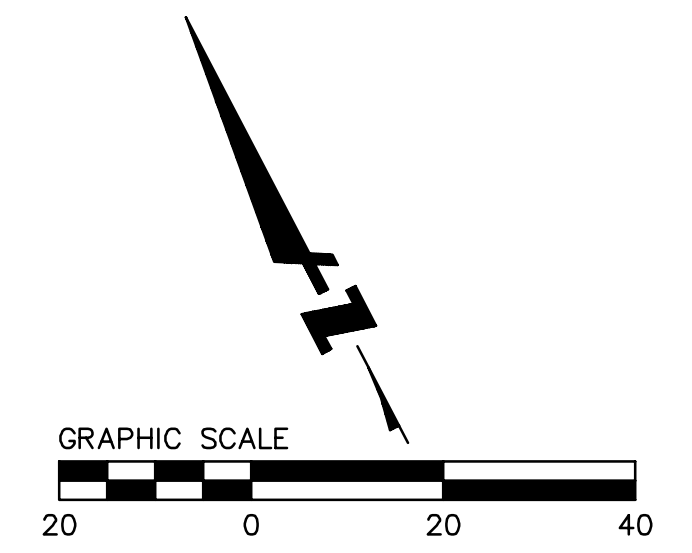


DRAWING NAME: \\BKF-r\cda\o\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C2.1-C2.2\_ExistingParcelization.dwg  
PLOT DATE: 04-26-21

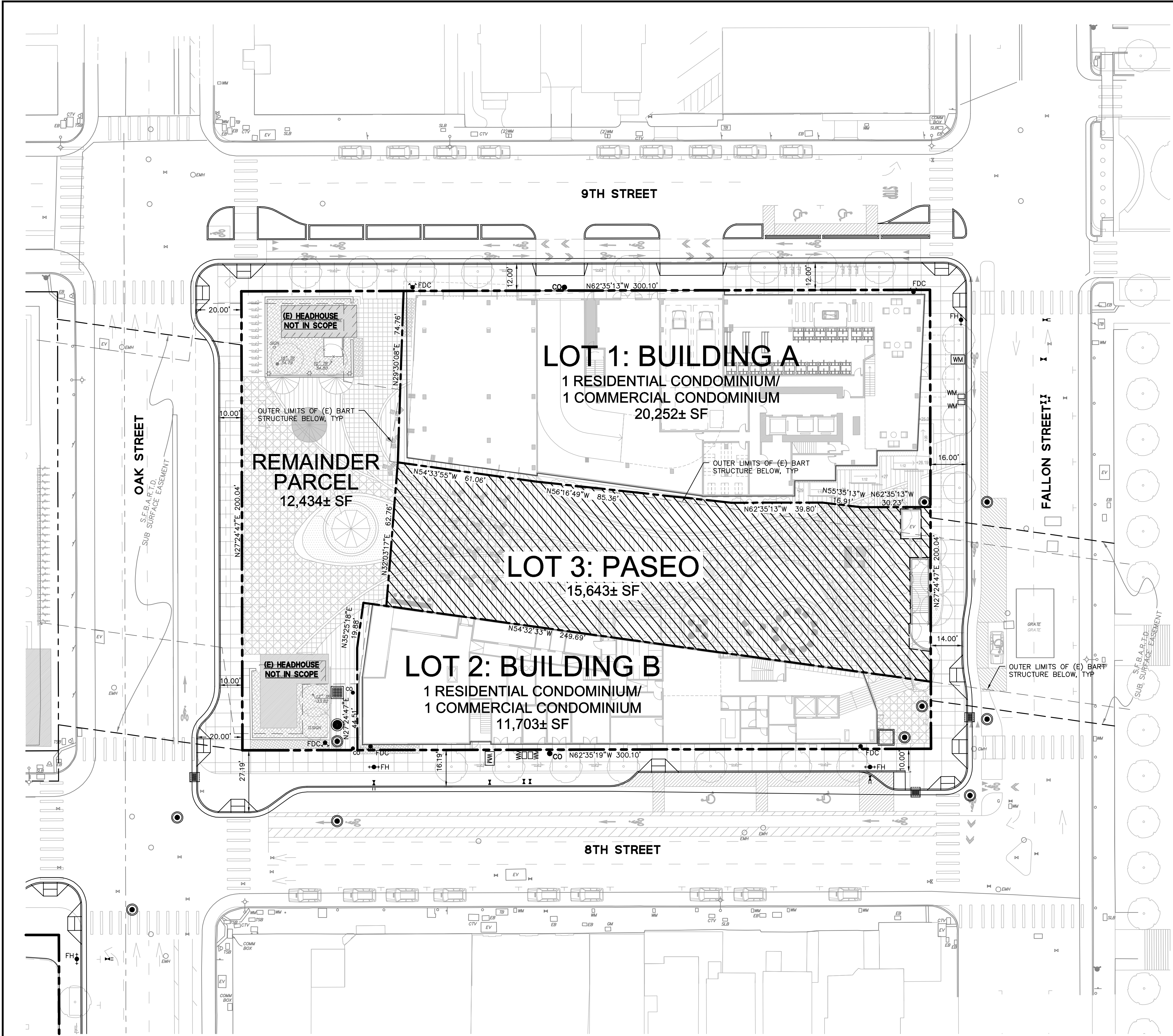


- ### ANNOTATION & LEGEND
- AD = AREA DRAIN
  - B = BOLLARD
  - BFP = BACKFLOW PREVENTOR
  - BR = BIKE RACK
  - BW = BACK OF WALK
  - CLDR = CENTERLINE OF DOOR
  - CMH = COMMUNICATION MANHOLE
  - COL = COLUMN
  - COMM = COMMUNICATION
  - CONC = CONCRETE
  - CTV = CABLE TELEVISION
  - DW = DRIVEWAY
  - EB = ELECTRICAL BOX
  - EMH = ELECTRICAL MANHOLE
  - EP = EDGE OF PAVEMENT
  - EV = ELECTRICAL VAULT
  - FDC = FIRE DEPARTMENT CONNECTION
  - FL = FLOWLINE
  - HCR = HANDICAP RAMP
  - LG = LIP OF CUTTER
  - LB = MAIL BOX
  - MH = MANHOLE
  - P = POST
  - PKM = PARKING METER
  - SDCO = STORM DRAIN CLEANOUT
  - SDDI = STORM DRAIN DROP INLET
  - SDMH = STORM DRAIN MANHOLE
  - SLB = STREET LIGHTING BOX
  - SSCO = SANITARY SEWER CLEANOUT
  - SSMH = SANITARY SEWER MANHOLE
  - TB = TELEPHONE BOX
  - TC = TOP OF CURB
  - TR = TREE
  - TSB = TRAFFIC SIGNAL BOX
  - TW = TOP OF WALL
  - UB = UTILITY BOX
  - UV = UTILITY VAULT
  - WM = WATER METER
  - WP = WATER PIPE
  - ☆ = AREA/YARD LIGHT
  - ⦿ = FIRE HYDRANT
  - ⊙ = SIGN
  - ⊙ = STREET LIGHT
  - ⊙ = GAS VALVE
  - ⊙ = WATER VALVE
  - ⊙ = SIGNAL LIGHT
  - E—E— = ELECTRICAL LINE
  - G—G— = GAS LINE
  - SD—SD— = STORM DRAIN LINE
  - SS—SS— = SANITARY SEWER LINE
  - UN—UN— = UNKNOWN UTILITY LINE
  - W—W— = DOMESTIC WATER LINE
  - — — — = BOUNDARY LINE
  - - - - - = RECORD LOT LINE TO BE REMOVED
  - - - - - = EASEMENT LINE

- ### NOTES
- FIELD DATES OF TOPOGRAPHIC SURVEY WERE MAY 13, 15, AND 22 2019
- ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF
- ### HORIZONTAL CONTROL
- HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLAN COORDINATE SYSTEM (CCS85), EPOCH 2017.00
- ### BENCHMARK
- FOUND BRASS PIN IN MONUMENT WELL ON MEDIAN ISLAND AT THE CENTERLINE OF FALLON STREET AND 8TH STREET.
- ELEVATION = 23.062 (NAVD88)
- ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.**



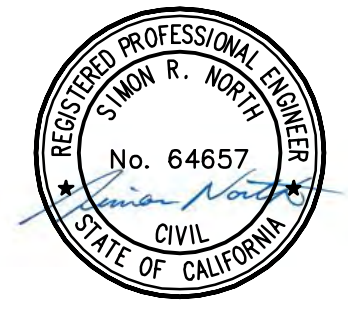
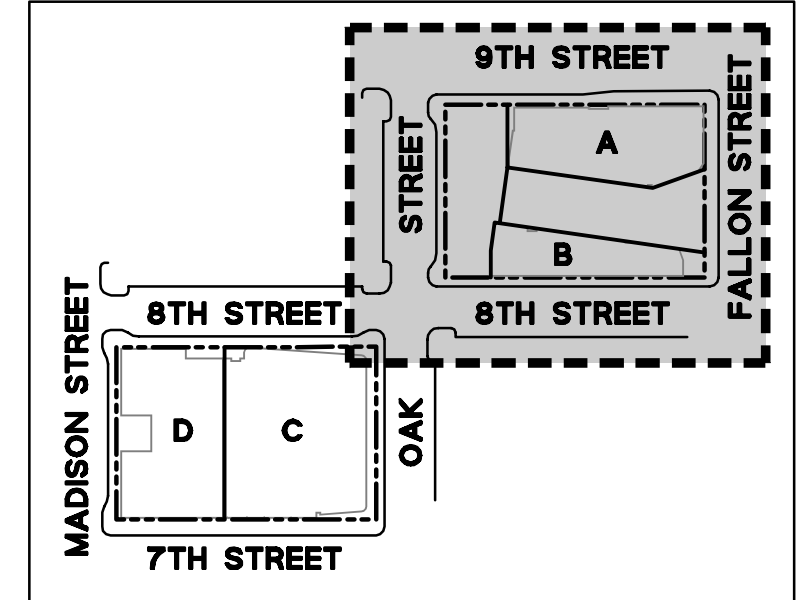
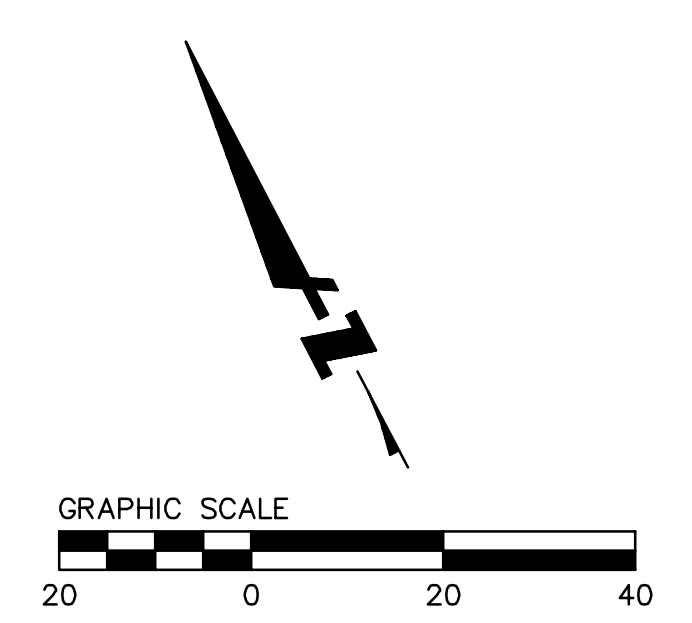
DRAWING NAME: \\bkf-c\da\2019\190110\_BART\_Lake\_Merritt\TPM\PLOTTED\_SHEETS\C3.1-C3.2\_ProposedParcelization.dwg  
 PLOT DATE: 04-26-21  
 PLOTTED BY: suid



- SITE PLAN NOTES:**
- ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
  - SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
  - THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
  - ONSITE PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS. PAVING WITHIN THE PUBLISH RIGHT OF WAY SHALL BE PER THE LATEST STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION ("GREENBOOK") AND CITY OF OAKLAND STANDARDS.
  - COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
  - SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
  - FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

- LEGEND:**
- BOUNDARY LINE
  - LOT LINE
  - EASEMENT LINE
  - PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

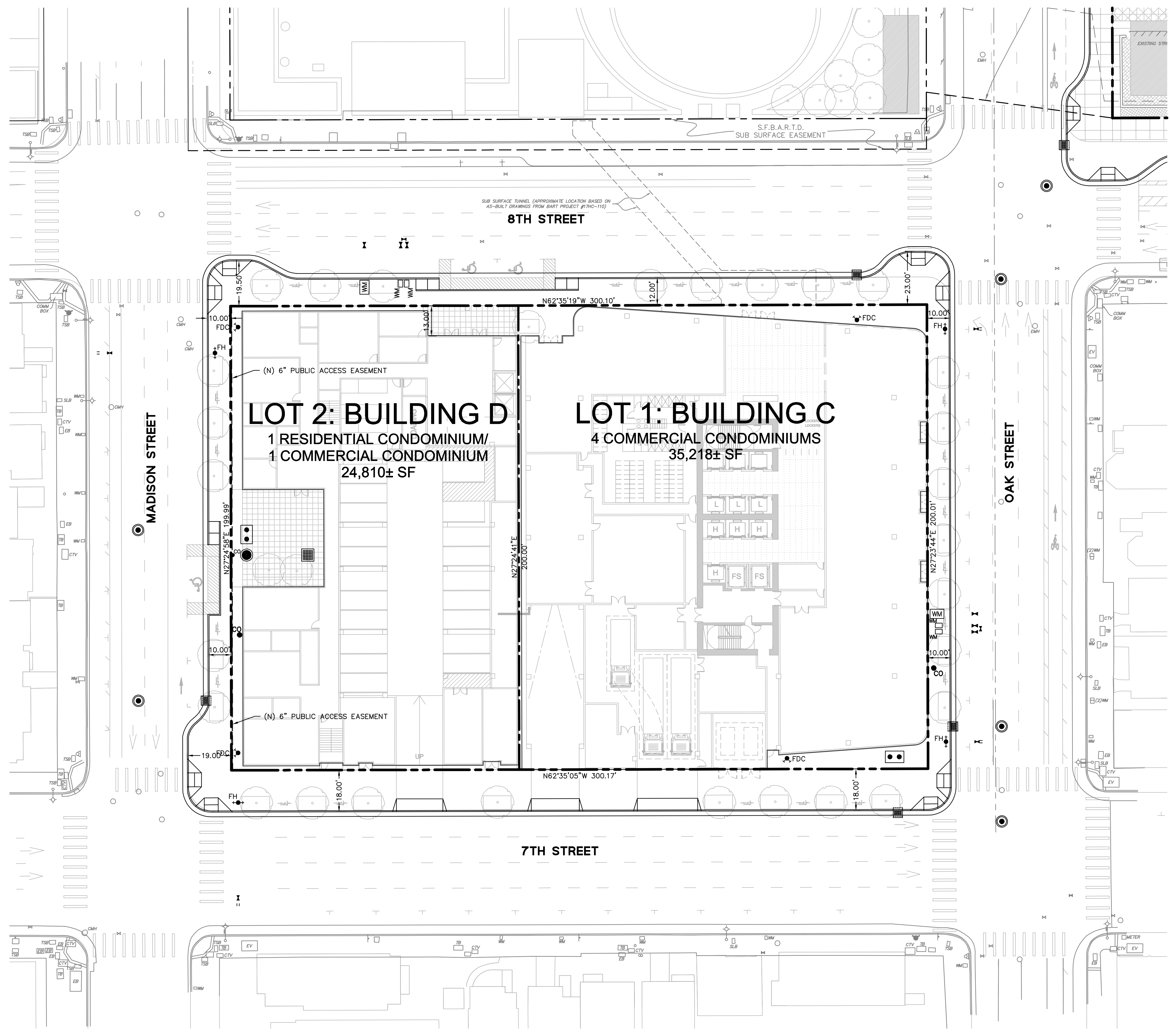
ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT. SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.



Revisions	Date	By	No.
	02/22/2021	DFS	

Scale: 1" = 20'  
 Design: DFS  
 Drawn: DFS  
 Approved: SRN  
 Job No: 20190110

DRAWING NAME: \\BKF-r-c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C3.1-C3.2\_ProposedParcelization.dwg  
PLOT DATE: 04-26-21 PLOTTED BY: suid



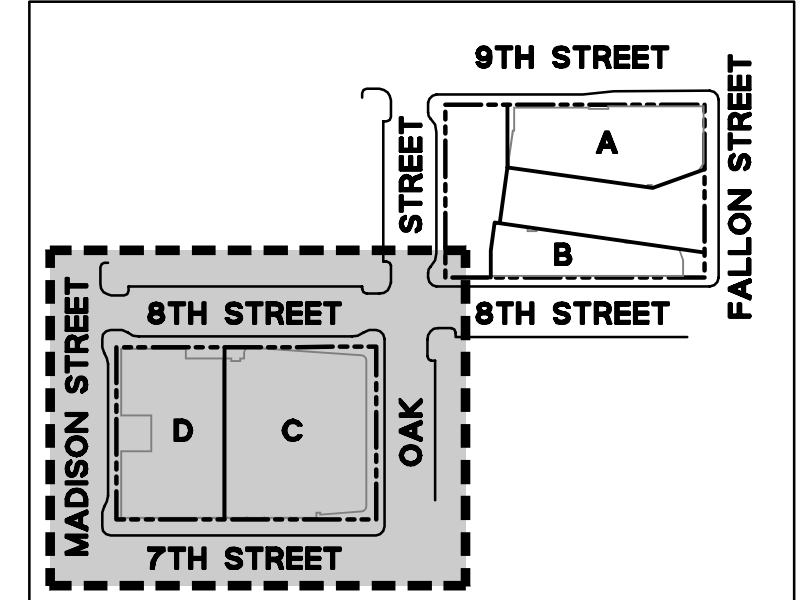
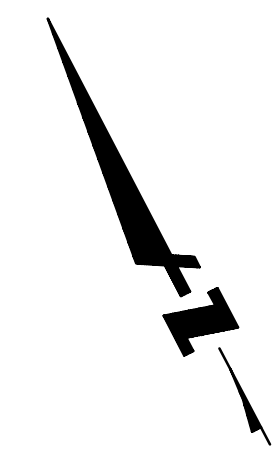
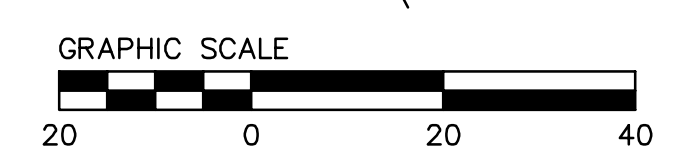
**SITE PLAN NOTES:**

1. ALL DIMENSIONS ON THE PLANS ARE IN FEET OR DECIMALS THEREOF UNLESS SPECIFICALLY CALLED OUT AS FEET AND INCHES
2. SEE GEOTECHNICAL REPORT FOR ALL FLATWORK AND VEHICULAR PAVEMENT SECTIONS AND BASE REQUIREMENTS.
3. THE FINAL OR SURFACE LAYER OF ASPHALT CONCRETE SHALL NOT BE PLACED UNTIL ALL ON-SITE IMPROVEMENTS HAVE BEEN COMPLETED, INCLUDING ALL GRADING, AND ALL UNACCEPTABLE CONCRETE WORK HAS BEEN REMOVED AND REPLACED BY PROJECT CIVIL ENGINEER.
4. ONSITE PAVING SHALL BE IN CONFORMANCE WITH SECTION 26 "AGGREGATE BASE" AND SECTION 39 "ASPHALT CONCRETE" PER LATEST EDITION OF CALTRANS STANDARD SPECIFICATIONS. PAVING WITHIN THE PUBLISH RIGHT OF WAY SHALL BE PER THE LATEST STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION ("GREENBOOK") AND CITY OF OAKLAND STANDARDS.
5. COLOR AND FINISH OF CONCRETE TO BE SPECIFIED BY LANDSCAPE ARCHITECT.
6. SEE LANDSCAPE PLANS FOR ALL SIDEWALK FINISHES AND MATERIALS.
7. FUTURE STRIPING AND MEDIAN ISLANDS SHOWN FOR REFERENCE ONLY.

**LEGEND:**

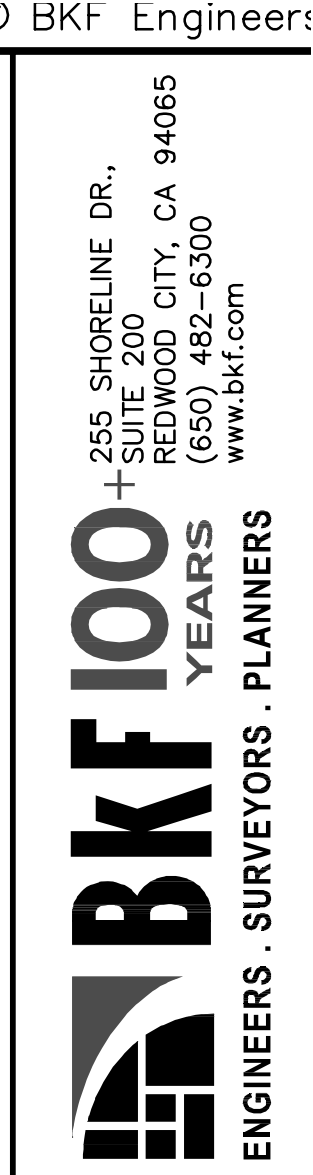
- BOUNDARY LINE
- - - LOT LINE
- - - EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COOVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COOVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COOVD ELEVATIONS.

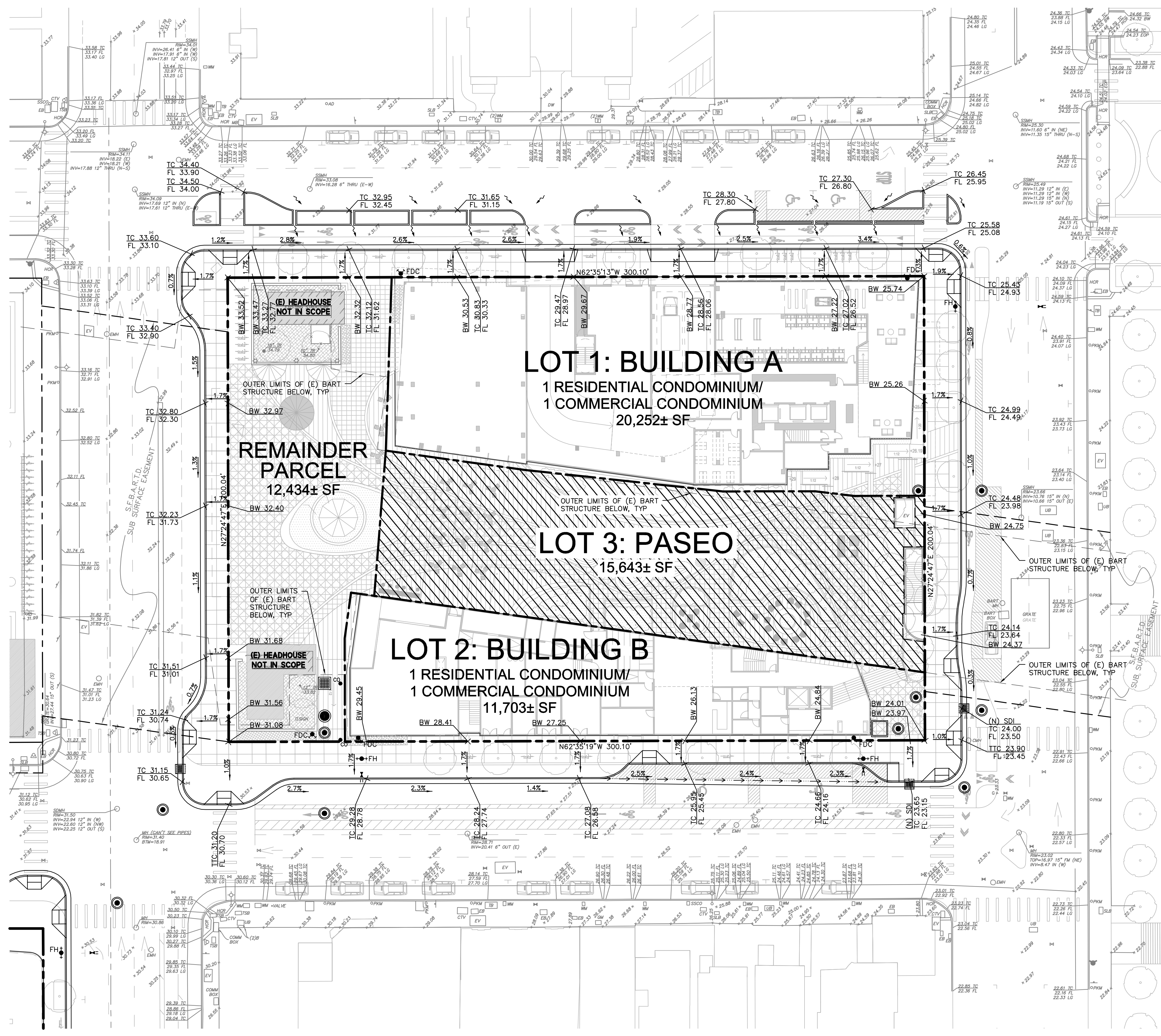


LAKE MERRITT BART DEVELOPMENT (BLOCK 2)  
 VESTING TENTATIVE TRACT MAP NO. 8577  
 PROPOSED PARCELIZATION PLAN (BLOCK 2)  
 CITY OF OAKLAND  
 ALAMEDA COUNTY  
 CALIFORNIA

Date	02/22/2021	No.	
Scale	1" = 20'	Design	DFS
Drawn	DFS	Approved SRN	
Drawing Number:	C3.2		
Job No	201910110		



DRAWING NAME: \\BKF-rc\c\d\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C4\_1-C4\_2\_GradingPlan.dwg PLOT DATE: 04-26-21 PLOTTED BY: suid



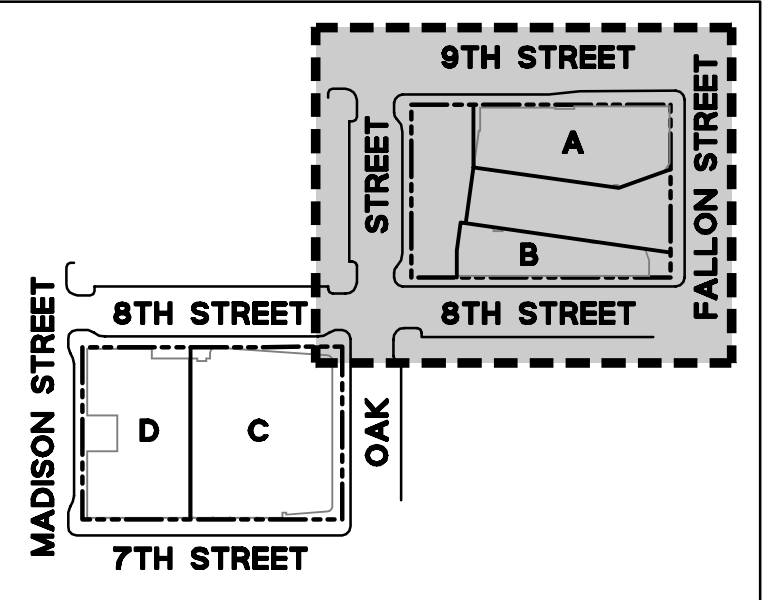
GRADING NOTES:

- 1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05', HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

LEGEND:

- BOUNDARY LINE
--- LOT LINE
--- EASEMENT LINE
[Diagonal Hatching] PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.

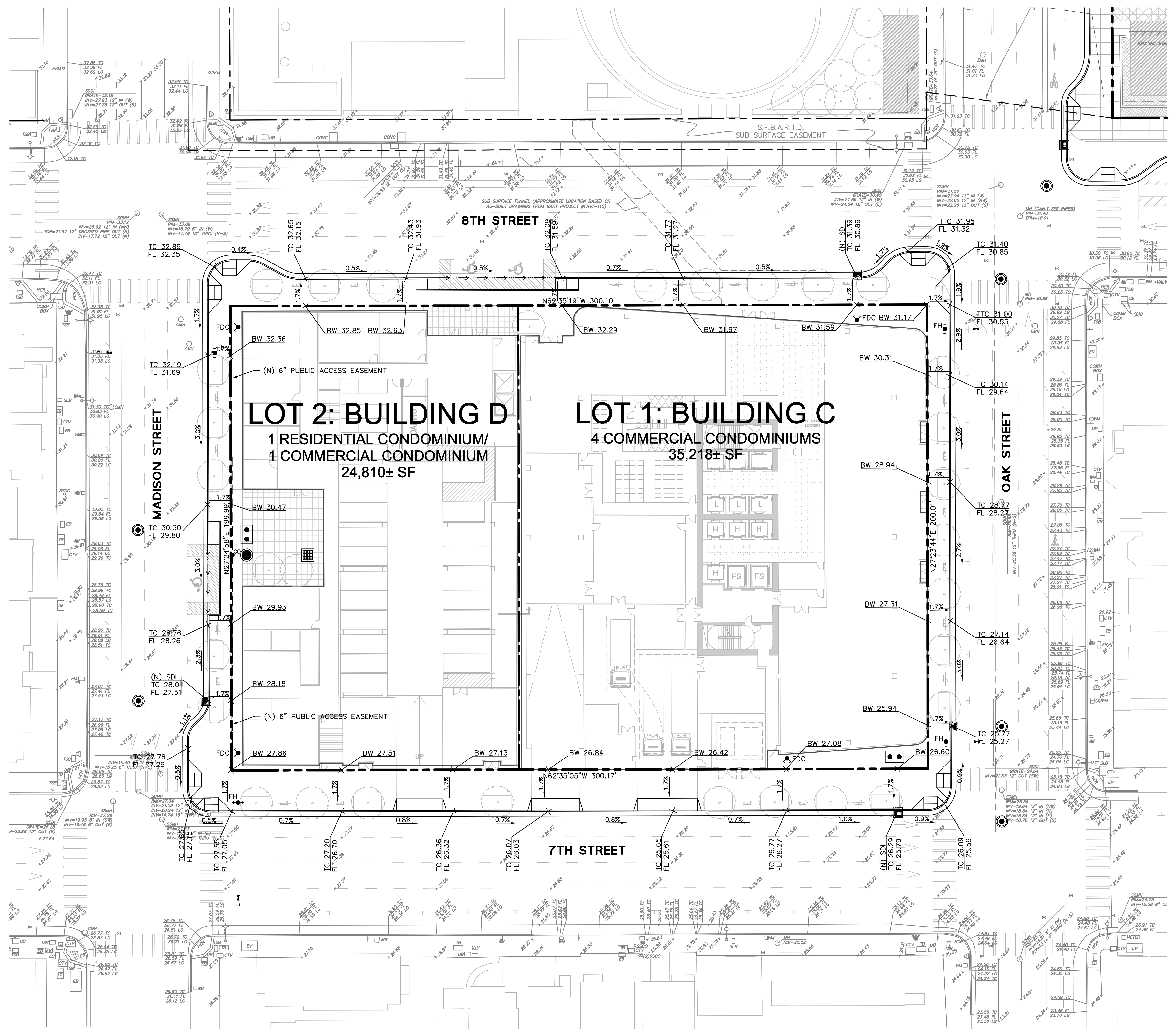


LAKE MERRITT BART DEVELOPMENT (BLOCK 1) VESTING TENTATIVE TRACT MAP NO. 8560 PROPOSED GRADING PLAN (BLOCK 1) CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA

BKF100 YEARS ENGINEERS SURVEYORS PLANNERS 255 SHORELINE DR., SUITE 200 REDWOOD CITY, CA 94065 (650) 482-6300 www.bkf.com

Table with columns: Date, Scale, Design, Drawn, Approved, Drawing Number. Includes revision table and drawing number C4.1.

DRAWING NAME: \\BKF-rc\o\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C4\_1-C4\_2\_GradingPlan.dwg PLOT DATE: 04-26-21 PLOTTED BY: suid



GRADING NOTES:

- 1. PROVIDE POSITIVE SURFACE DRAINAGE AWAY FROM ALL STRUCTURES BY SLOPING THE FINISHED GROUND SURFACE AT LEAST 2%, UNLESS OTHERWISE NOTED ON THE PLANS. SLOPE PORCHES, LANDINGS AND TERRACES 2% (1/4" PER FOOT) AWAY FROM STRUCTURES UNLESS OTHERWISE NOTED ON PLANS.
2. CONTRACTOR TO VERIFY ALL CONTROLLING DIMENSIONS WITH ARCHITECTURAL PLANS.
3. CONTRACTOR SHALL DETERMINE EARTHWORK QUANTITIES BASED ON THE TOPOGRAPHIC SURVEY, THE GEOTECHNICAL INVESTIGATION AND THE PROPOSED SURFACE THICKNESS AND BASE THE BID ACCORDINGLY. IT IS THE CONTRACTORS RESPONSIBILITY TO CONFIRM IF A SEPARATE DEMOLITION CONTRACT HAS BEEN ISSUED TO TAKE THE SITE FROM THE WAY IT IS AT THE TIME OF THE BID TO THE CONDITIONS DESCRIBED IN THESE DOCUMENTS. ANY DIFFERENCES BETWEEN THE STATE IN WHICH THE SITE IS DELIVERED TO THE CONTRACTOR AND THESE DOCUMENTS SHOULD BE NOTED TO THE ENGINEER/ARCHITECT.
4. ALL FILL SHALL BE COMPACTED PER THE GEOTECHNICAL REPORT, OR CITY OF OAKLAND STANDARDS, WHICHEVER IS MORE STRINGENT, AND THE CONTRACTOR SHALL COORDINATE AND COMPLY WITH THE CLIENT'S GEOTECHNICAL ENGINEER TO TAKE THE APPROPRIATE TESTS TO VERIFY COMPACTION VALUES.
5. IMPORT SOILS SHOULD MEET THE REQUIREMENTS OF THE SOILS REPORT AND SPECIFICATIONS.
6. DO NOT ADJUST GRADES ON THIS PLAN WITHOUT PRIOR WRITTEN APPROVAL OF THE ENGINEER/ARCHITECT.
7. ROUGH GRADING TO BE WITHIN 0.1' AND FINISH GRADES ARE TO BE WITHIN 0.05'. HOWEVER CONTRACTOR SHALL NOT CONSTRUCT ANY IMPROVEMENTS THAT WILL CAUSE WATER TO POND OR NOT MEET REQUIREMENTS IN GRADING NOTE #1.
8. THE CONTRACTOR SHALL EXERCISE EXTREME CARE TO CONFORM TO THE LINES, GRADES, SECTIONS, AND DIMENSIONS AS SET FORTH ON THESE PLANS. ALL GRADED AREAS SHALL CONFORM TO THE VERTICAL ELEVATIONS SHOWN WITH A TOLERANCE OF ONE-TENTH OF A FOOT. WHERE GRADED AREAS DO NOT CONFORM TO THESE TOLERANCES, THE CONTRACTORS SHALL BE REQUIRED TO DO CORRECTIVE GRADING, AT NO EXTRA COST TO THE CLIENT.
9. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO CONFIRM THE GROUND ELEVATIONS AND OVERALL TOPOGRAPHY OF THE SITE PRIOR TO THE START OF CONSTRUCTION AS TO THE ACCURACY BETWEEN THE WORK SET FORTH ON THESE PLANS AND THE WORK IN THE FIELD. ANY DISCREPANCIES SHALL BE IMMEDIATELY BROUGHT TO THE ATTENTION OF THE CONSTRUCTION MANAGER AND CIVIL ENGINEER IN WRITING PRIOR TO START OF CONSTRUCTION WHICH MAY REQUIRE CHANGES IN DESIGN AND/OR AFFECT THE EARTHWORK QUANTITIES.
10. TRENCHES SHALL NOT BE LEFT OPEN OVERNIGHT IN EXISTING PUBLIC STREET AREAS. CONTRACTOR SHALL BACKFILL TRENCHES, OR PLACE STEEL PLATING WITH ADEQUATE CUTBACK TO PREVENT SHIFTING OF STEEL PLATE AND/OR HOT-MIX ASPHALT REQUIRED TO PROTECT OPEN TRENCHES AT THE END OF THE WORKING DAY.

LEGEND:

- BOUNDARY LINE (solid line)
LOT LINE (dashed line)
EASEMENT LINE (dash-dot line)
PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE (hatched area)

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COOVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COOVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COOVD ELEVATIONS.

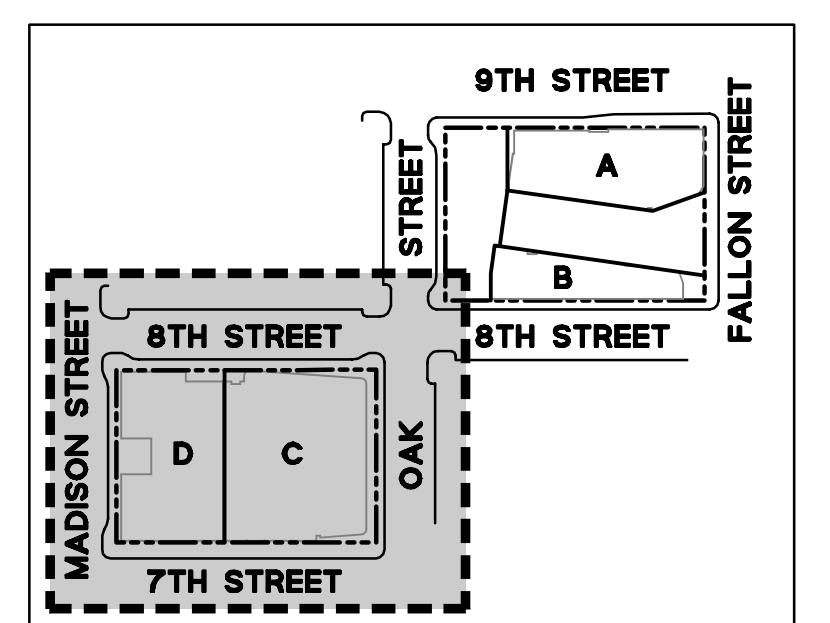
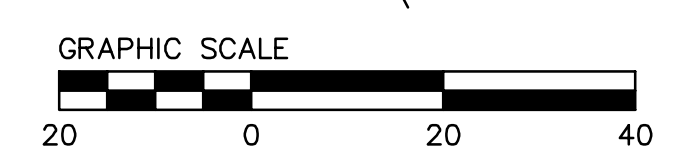
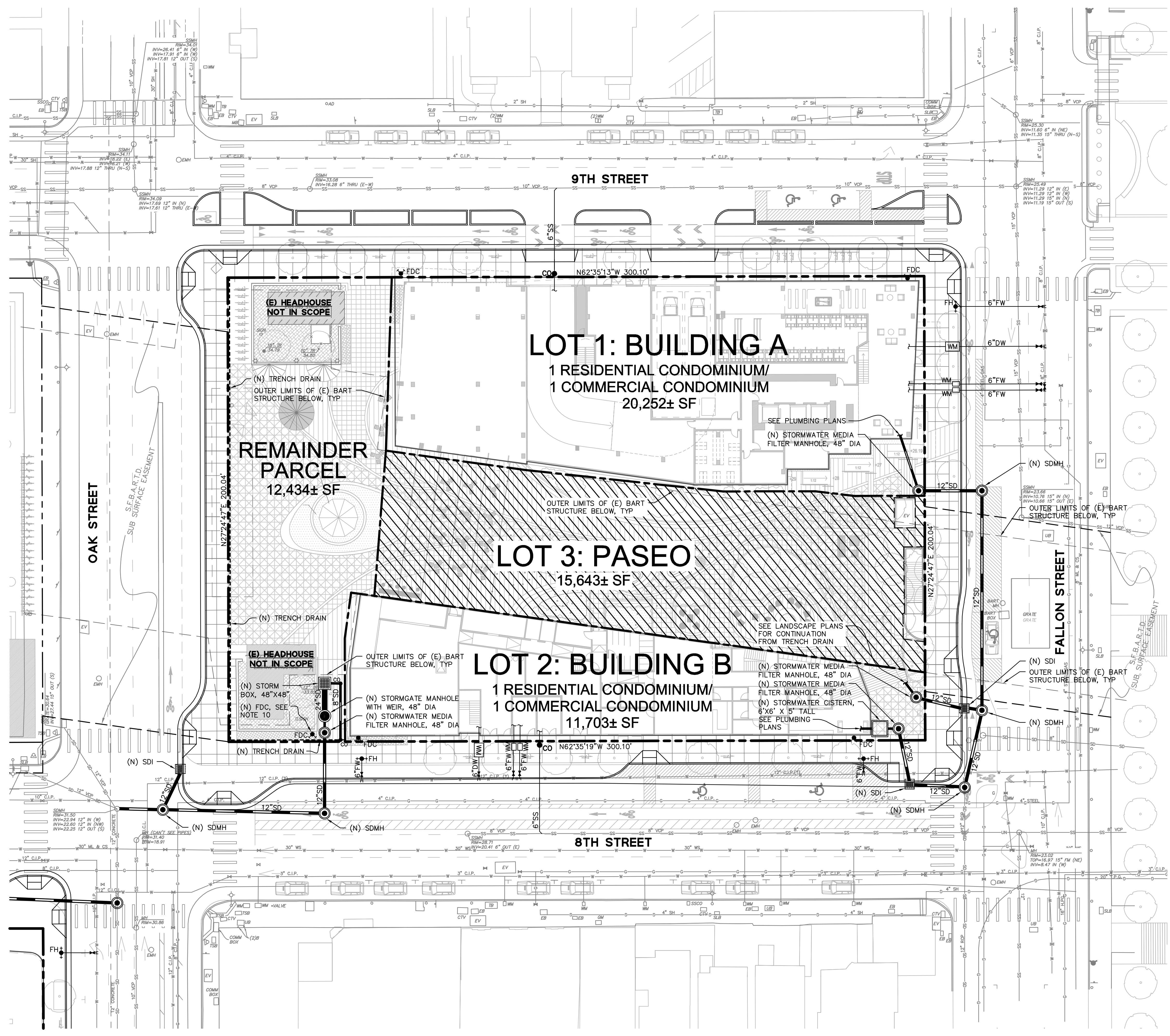


Table with columns: Date (02/22/2021), No., Scale (1" = 20'), Design (DFS), Drawn (DFS), Approved (SRN), Job No (20190110)

DRAWING NAME: \\BKF-r-c\da\2019\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\C5.1-CS.2\_UtilityPlan.dwg  
PLOT DATE: 04-26-21  
PLOTTED BY: suid



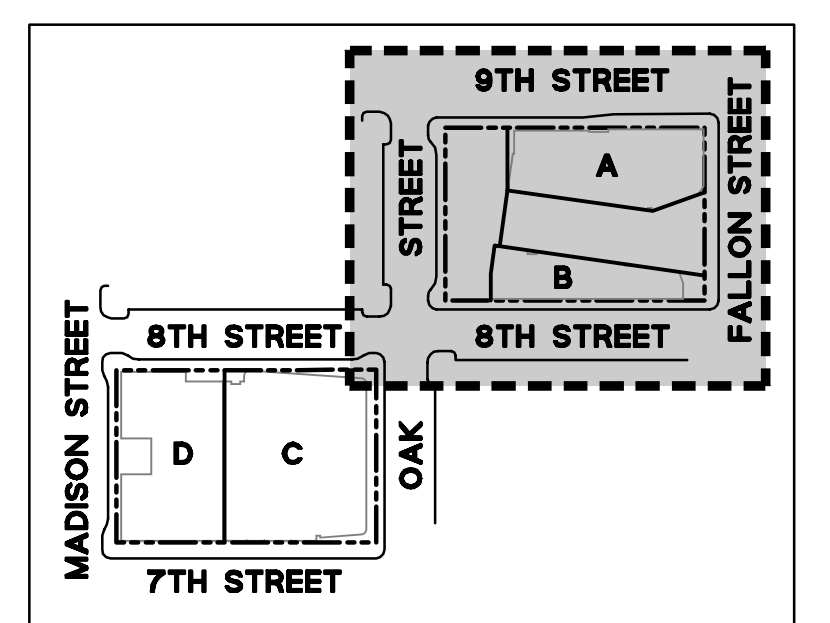
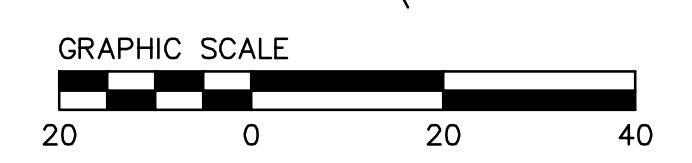
**UTILITY NOTES:**

1. INFORMATION REGARDING EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM RECORD DATA AND IS NOT MEANT TO BE A FULL CATALOG OF EXISTING CONDITIONS. CONTRACTOR SHALL CONDUCT FIELD INVESTIGATIONS, SUCH AS POTHOLES, AS REQUIRED TO VERIFY THE LOCATIONS, ELEVATIONS, AND CONNECTION POINTS OF ALL EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES, WHETHER SHOWN ON THESE PLANS OR NOT, PRIOR TO THE COMMENCEMENT OF WORK AND UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
2. DOMESTIC WATER, FIRE WATER, AND SANITARY SEWER LATERAL SIZES TO BE CONFIRMED DURING THE DETAILED DESIGN PHASE.
3. CONTRACTOR SHALL COORDINATE GRAVITY UTILITY WORK WITH ALL JOINT TRENCH/RULE 20 UNDERGROUND WORK. IF JOINT TRENCH SCOPE OF WORK IS TO BE PERFORMED FIRST, CONTRACTOR SHALL STAKE LOCATIONS AND ELEVATIONS OF ALL PROPOSED GRAVITY UTILITY CROSSINGS. JOINT TRENCH TO BE INSTALLED WITH MINIMUM 12" VERTICAL CLEARANCE TO PROPOSED GRAVITY UTILITY AT ALL CROSSINGS.
4. ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
5. ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
6. ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
7. FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POT HOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR GROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
8. DRAINS SHOWN ON CIVIL PLANS ARE NOT INTENDED TO BE THE FINAL NUMBER AND LOCATION OF ALL DRAINS. PLACEMENT AND NUMBER OF LANDSCAPING DRAINS ARE HIGHLY DEPENDENT ON GROUND COVER TYPE AND PLANT MATERIAL. CONTRACTOR SHALL ADD ADDITIONAL AREA DRAINS AS NEEDED AND AS DIRECTED BY THE LANDSCAPE ARCHITECT OR CIVIL ENGINEER.
9. THE LOCATION OF THE 12" WATER LINE IN 8TH STREET HAS BEEN SHOWN BASED ON RECORD INFORMATION PROVIDED BY EAST BAY MUD. EXACT LOCATION TO BE VERIFIED AND CONFIRMED PRIOR TO INSTALLATION OF THE PROPOSED DOMESTIC AND FIRE WATER SERVICES TO BUILDING B.
10. THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.

**LEGEND:**

- BOUNDARY LINE
- - - LOT LINE
- - - EASEMENT LINE
- ▨ PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.



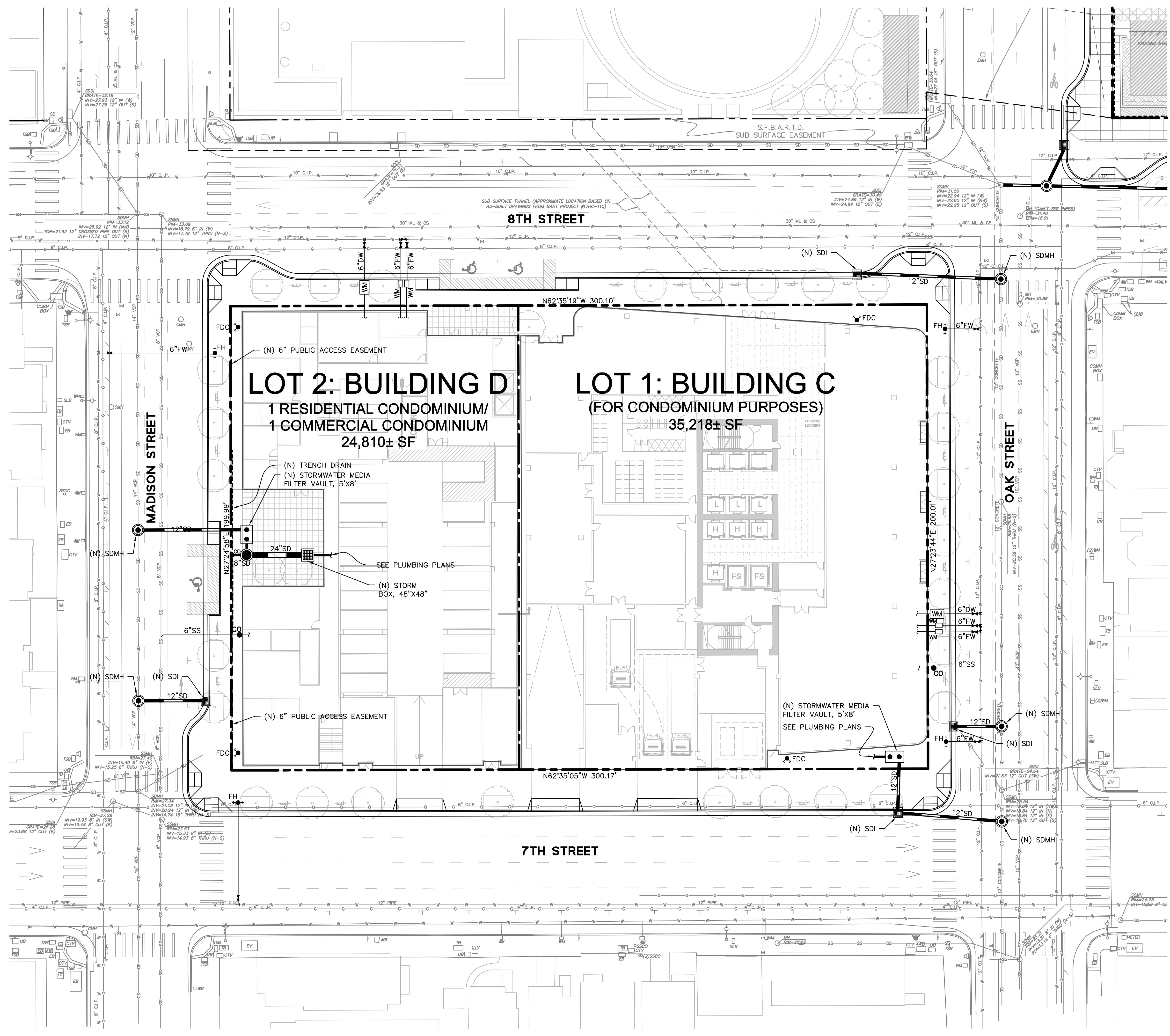
**LAKE MERRITT BART DEVELOPMENT (BLOCK 1)  
VESTING TENTATIVE TRACT MAP NO. 8560  
PROPOSED UTILITY PLAN (BLOCK 1)**  
ALAMEDA COUNTY  
CITY OF OAKLAND

**BKF100**  
YEARS  
ENGINEERS . SURVEYORS . PLANNERS  
255 SHORELINE DR.,  
REDWOOD CITY, CA 94065  
(650) 482-6300  
www.bkf.com

Revisions	Date	By	Check
	02/22/2021		

No. 64657  
Scale 1" = 20'  
Design DFS  
Drawn DFS  
Approved SRN  
Job No 20190110  
Drawing Number: **C5.1**  
8 OF 9

DRAWING NAME: \\BKF-rcc\o\190110\_BART\_Lake\_Merritt\_TOD\ENG\01-TPM\PLOTTED\_SHEETS\CS\_1-C5\_2\_UtilityPlan.dwg PLOT DATE: 04-26-21



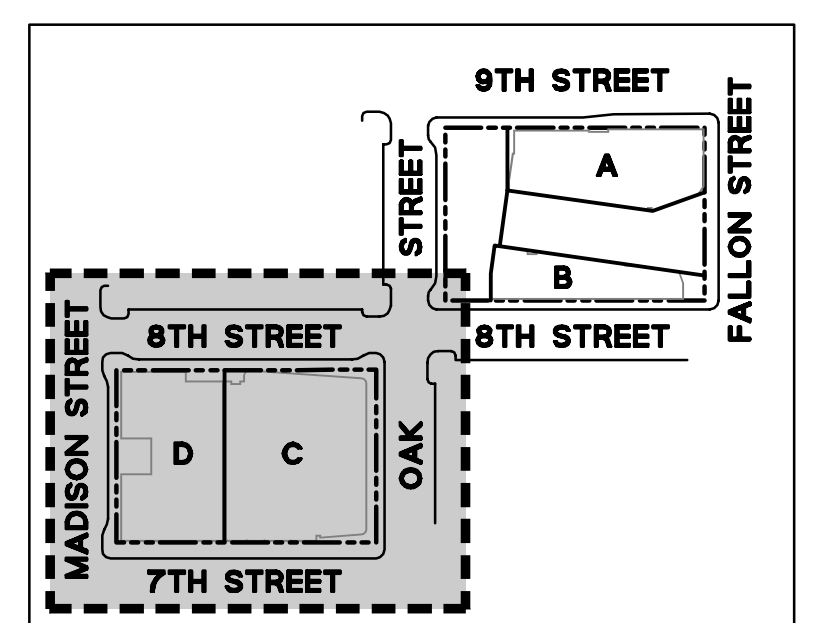
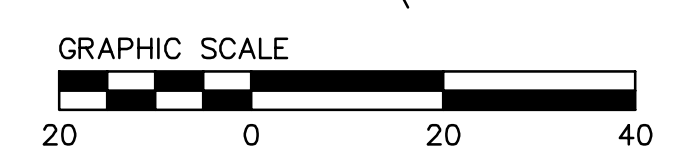
UTILITY NOTES:

- 1. INFORMATION REGARDING EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES SHOWN ON THESE PLANS WAS TAKEN FROM RECORD DATA AND IS NOT MEANT TO BE A FULL CATALOG OF EXISTING CONDITIONS. CONTRACTOR SHALL CONDUCT FIELD INVESTIGATIONS, SUCH AS POTHOLES, AS REQUIRED TO VERIFY THE LOCATIONS, ELEVATIONS, AND CONNECTION POINTS OF ALL EXISTING SUBSURFACE IMPROVEMENTS AND UTILITIES, WHETHER SHOWN ON THESE PLANS OR NOT, PRIOR TO THE COMMENCEMENT OF WORK AND UPON DISCOVERY OF ANY DISCREPANCIES BETWEEN EXISTING CONDITIONS IN THE FIELD AND INFORMATION SHOWN ON THESE PLANS.
2. DOMESTIC WATER, FIRE WATER, AND SANITARY SEWER LATERAL SIZES TO BE CONFIRMED DURING THE DETAILED DESIGN PHASE.
3. CONTRACTOR SHALL COORDINATE GRAVITY UTILITY WORK WITH ALL JOINT TRENCH/RULE 20 UNDERGROUNDING WORK. IF JOINT TRENCH SCOPE OF WORK IS TO BE PERFORMED FIRST, CONTRACTOR SHALL STAKE LOCATIONS AND ELEVATIONS OF ALL PROPOSED GRAVITY UTILITY CROSSINGS. JOINT TRENCH TO BE INSTALLED WITH MINIMUM 12" VERTICAL CLEARANCE TO PROPOSED GRAVITY UTILITY AT ALL CROSSINGS.
4. ALL GRAVITY UTILITY INSTALLATION SHALL BEGIN AT THE FURTHEST DOWNSTREAM POINT OF THE SYSTEM AND PROCEED UPSTREAM.
5. ALL AREA DRAIN AND LANDSCAPE DRAIN GRATES WITHIN PEDESTRIAN ACCESSIBLE AREAS SHALL MEET ADA REQUIREMENTS.
6. ALL TRENCHES SHALL BE BACK FILLED PER THE SPECIFICATIONS WITH APPROPRIATE TESTS BY THE GEOTECHNICAL ENGINEER TO VERIFY COMPACTION VALUES.
7. FOR GRAVITY FLOW SYSTEMS CONTRACTOR SHALL VERIFY (POT HOLE IF NECESSARY) SIZE, MATERIAL, LOCATION AND DEPTH OF ALL SYSTEMS THAT ARE TO BE CONNECTED TO OR GROSSED PRIOR TO THE TRENCHING OR INSTALLATION OF ANY GRAVITY FLOW SYSTEM.
8. DRAINS SHOWN ON CIVIL PLANS ARE NOT INTENDED TO BE THE FINAL NUMBER AND LOCATION OF ALL DRAINS. PLACEMENT AND NUMBER OF LANDSCAPING DRAINS ARE HIGHLY DEPENDENT ON GROUND COVER TYPE AND PLANT MATERIAL. CONTRACTOR SHALL ADD ADDITIONAL AREA DRAINS AS NEEDED AND AS DIRECTED BY THE LANDSCAPE ARCHITECT OR CIVIL ENGINEER.
9. THE LOCATION OF THE 12" WATER LINE IN 8TH STREET HAS BEEN SHOWN BASED ON RECORD INFORMATION PROVIDED BY EAST BAY MUD. EXACT LOCATION TO BE VERIFIED AND CONFIRMED PRIOR TO INSTALLATION OF THE PROPOSED DOMESTIC AND FIRE WATER SERVICES TO BUILDING B.
10. THE RELOCATION OF THE EXISTING FDC THAT CURRENTLY SERVES THE BART PROPERTY IS SHOWN SCHEMATICALLY. FINAL LOCATION AND PIPING SHALL BE DESIGNED AND VERIFIED BY A SEPARATE PLUMBING ENGINEER AND/OR FIRE ENGINEER.

LEGEND:

- BOUNDARY LINE
LOT LINE
EASEMENT LINE
PUBLIC ACCESS EASEMENT, 23 FEET ABOVE FINISH SURFACE

ELEVATIONS SHOWN ARE ON NAVD88, NOT ON CITY OF OAKLAND VERTICAL DATUM (COVD). A COMPARISON BETWEEN COLLECTED INFORMATION ON NAVD88 AND COVD BENCHMARKS RESULTED IN AN OBSERVED DIFFERENCE OF 5.68 FT; SUBTRACTING SHOWN NAVD88 ELEVATIONS BY 5.68 WILL RESULT IN COVD ELEVATIONS.



LAKE MERRITT BART DEVELOPMENT (BLOCK 2)
VESTING TENTATIVE TRACT MAP NO. 8577
PROPOSED UTILITY PLAN (BLOCK 2)
CITY OF OAKLAND
ALAMEDA COUNTY
CALIFORNIA

BKF100 YEARS
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REDWOOD CITY, CA 94065
(650) 482-6500
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Table with columns: Date (02/22/2021), Scale (1" = 20'), Design (DFS), Drawn (DFS), Approved (SRN), Job No (20190110), Drawing Number (C5.2), and Page (9 OF 9).



Category	Sub-header	Guide line	Description	Which FDP? (Horizontal, Vertical, Both, or N/A)	Com plies ?	Staff Response
Building Design	General Guidelines for Blocks 1 & 2	Coordinated Design	The design of the high-rise Buildings (A and C) and the mid-rise Buildings (B and D) should be coordinated to create a cohesive frontage and to reinforce the overall sense of identity for each Block. This coordinated design should take the form of a similar vocabulary of forms, openings, materials and colors. This should include consideration of how buildings work together to achieve the Design Principles for Lake Merritt BART, contribute to the public realm and the overall quality of life. (LMSAP/DG-1: Public Perception)	Both	Yes	Project is for Building B and Landscaping of the Paseo. Design of massing and materials could be further refined to create pragmatic groundwork of later phased projects. Later projects include: High-rise and mid-rise buildings that must be coordinated to create a cohesive frontage. Building A, C, and D design still pending, refer to Building B elevations on A3.00-A3.05 of the Vertical FDP.
Building Design	General Guidelines for Blocks 1 & 2	Transition-in-Scale	Mid-rise Buildings (B and D) on each block should be designed to provide a visual transition between the scale and rhythm of the neighboring buildings and high-rise Buildings (A and C). (LMSAP/DG-17: Reinforce the Existing Rhythm)	Both	Yes	Project is for Building B and Landscaping of the Paseo. Design of massing and materials could be further refined to create pragmatic groundwork of later phased projects. Later projects include: High-rise and mid-rise buildings will be coordinated to create a cohesive frontage. Building A, C, and D design still pending, refer to Building B elevations on A3.00-A3.05 of the Vertical FDP.  The height of Bldg-B (midrise) is designed to relate to the later phased podium base height of Bldg-A tower to assist in

						transition of massing scale.
Building Design	General Guidelines for Blocks 1 & 2	Service Areas at Ground Level	Service areas at the first level should be articulated with pedestrian-scaled facade articulation such as panels, contrasting textures, high-quality and interesting building materials, blind windows, doors, planting treatments, murals or other public art, and/or exterior detailing should create visual interest and diversity and to reinforce the pedestrian scale.	Vertical	Yes	<p>The Building B façade fronting the Paseo is articulated with various materials including textured brick, hardwood screens, and transparent storefront to allow for a visual connection between passersby and the residential building. Refer to elevations on A3.00, A3.03, and materials on A3.05 of the Vertical FDP.</p> <p>Bldg-B service area facing 8th Street is recessed from main façade to reduce visual impact and allow for potential public art to create visual interest at pedestrian ground level.</p>
Building Design	General Guidelines for Blocks 1 & 2	Family Friendly Housing	At residential units intended to accommodate families, especially in Building D as an affordable family building, the building design should include a variety of unit sizes to accommodate households of various sizes. Building design on both Block 1 and Block 2 should also incorporate other family friendly elements including outdoor play space designed to allow supervision and easy access. (LMSAP/DG-52: Family Friendly Housing)	Both	Yes	<p>Building D design pending. Block 1 incorporates family friendly elements along the public paseo. Refer to landscape plan on L4.01 of Horizontal FDP set.</p>
Building Design	General Guidelines for Blocks 1 & 2	Shared Outdoor Spaces	Shared outdoor spaces should include plantings, benches, lighting and other appropriate elements to create an inviting and useful space for the residents. Shared outdoor spaces should be designed to accommodate children where feasible by incorporating play	Horizontal	Yes	<p>Paseo design meets this design guidelines to include planting, lighting, and places for children. Outdoor spaces at buildings are optimized for sun exposure.</p>

			structures and play areas. Shared outdoor spaces such as the publicly accessible open space Paseo, the courtyard, the open space at the upper floor step backs and/or the rooftops, should be designed to have ample daylight and to be sheltered from the wind.			
Building Design	General Guidelines for Blocks 1 & 2	Residential Community Rooms	Where community rooms are provided, they should be located to promote active use by residents. Community rooms are encouraged to be located adjacent to shared outdoor spaces or public open areas to create strong connections between indoor and outdoor activities. (LMSAP/DG-57: Shared Spaces)	Vertical	Yes	Building B includes community room at ground level adjacent to public paseo to physically and visually connect residents to outside activity. Refer to A2.00 for L1 plan of the Vertical FDP.
Building Design	General Guidelines for Blocks 1 & 2	Exterior Color	The exterior color palette should be coordinated to provide a cohesive overall appearance and to reinforce the design intent. The color palette at adjacent buildings should be coordinated to achieve the desired balance of cohesion and variety. (LMSAP/DG-73: Color)	Vertical	Yes	Projects will coordinate overall exterior color pallet/appearance. Building A, C, and D pending. Refer to Building B elevations on A3.00-A3.05 of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Standards	The Building A tower shall be setback from the base building a minimum of 20 feet.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Standards	Parking uses located in the podium at Building A shall include 25% openings for natural ventilation.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Standards	The corner of the podium at the Paseo and Oak Street shall be notched a minimum of 10 feet by 10 feet.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Standards	A canopy shall be provided over entrances to the ground-floor commercial spaces. See Figure 13.	Vertical	Yes	Project provides canopy/awning at entrances to ground floor commercial. Refer to elevations on A3.00 and A3.03 of the Vertical FDP.

Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	Architectural articulations and landmark features should be used to highlight and emphasize the building corners facing Oak Street, to visually define and animate the mid-block pedestrian thoroughfare entry and to facilitate pedestrian flow.	Vertical	Yes	Project provides canopy/awnings, storefront, various materials, and signing for articulation. Refer to elevations on A3.00 and A3.03 and preliminary signing on G0.18 of the Vertical FDP. <b>SIGN PERMITS will be pursued under a separate application.</b>
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	Active commercial uses should be provided at the ground floor of all four building corners along Oak Street with visual transparency to create an active and inviting public realm and to connect the street life to the Paseo. (LMSAP/DG-39: Storefronts, DG-32: Views of Indoor Spaces)	Vertical	Yes	Building B includes commercial kitchen at ground floor corners along Oak Street with storefront for transparency. Public Restroom is accessible via the Paseo. Refer to L1 plan on Sheets A2.00 and elevations on A3.00 and A3.03 of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	Open spaces and/or articulated roof shapes should be provided at step backs and the rooftop facing Oak Street to emphasize the gateway elements and to provide shared outdoor spaces overlooking the BART Plaza and Downtown.	Vertical	Yes	Building B includes an upper-level lounge and deck overlooking the BART plaza and downtown facing Oak Street. The upper-level lounge is articulated by a pitched gable roof. Refer to L7 plan on Sheets A2.06 and elevations on A3.00 and A3.03 of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	The design of the high-rise Building A and the mid-rise Building B should be coordinated to create a cohesive frontage and to reinforce the overall identity for the block. This coordinated design should take the form of a similar vocabulary of forms, openings and materials.	Vertical	Yes	High-rise and mid-rise buildings will be coordinated to create a cohesive frontage. Building A design still pending, refer to Building B elevations on Sheets A3.00-A3.0 on the Vertical FDP. Further refinement of materials would make future coordination more realistic.

Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	Use of canopies, awnings or other projecting elements should provide shelter and shade at west facing active uses. At Building B, horizontal awnings above the ground floor should wrap around both corners of Oak Street and the Paseo and at Oak and 8th Street. (LMSAP/DG-35: Awnings)	Vertical	Yes	Building B provides canopies/awnings at both corners facing Oak Street. Refer to elevations on Sheets A3.00, A3.01 and A3.03 on the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	The visual prominence of the west facade of the Building A tower suggests incorporating three-dimensional elements such as balconies, recesses and similar architectural features that create deep shadows and provide shading for interior uses. (LMSAP/DG-24: Distinguished Tower Design)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Frontage: Design Guidelines	Varying materials, texture and scale should be used to differentiate the building base from the tower above. (LMSAP/DG-18: Transitions in Building Height)	Vertical	Yes	The base at Building B includes textured brick, hardwood screens, and storefront windows to differentiate from the upper levels above which are comprised of composite wood panels, fiber cement panels and painted cement plaster. Refer to elevations and material keynotes on Sheets A3.00 and A3.01 and materials on A3.05 on the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Business Frontage Zone	Located in part above the BART tunnel, the design in this area must comply with the BART Facilities Standards.	Horizontal	Yes	BART Facilities Standards are complied with in areas above the BART-tunnel.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Business Frontage Zone	Furniture should include benches which orient in multiple directions and provide a variety of ways to sit including tiered, backed, non-backed, communal, etc.	Horizontal	Yes	Various seating arrangements around planters, moveable furniture, and near eating establishments provide a variety of ways to sit.

Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Business Frontage Zone	Furniture should also accommodate all needs of transit users and include bike lockers, bike racks, lighting, waste stations, and information kiosks/maps.	Horizontal	Yes	Bike lockers, bike racks, lighting, and wayfinding signage included to accommodate transit users.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Business Frontage Zone	The Paseo pavements shall extend to surround the skylight.	Horizontal	Yes	Paseo paving pattern extends around the skylight
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Business Frontage Zone	The skylight should be visible and iconic and at night be lit for interest.	Horizontal	Yes	Skylight is a focal point of the paseo design.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	Sidewalk concrete should be colored with the maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street.	Horizontal	Yes	Corners as designed can accommodate stamped street names.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	Located in part above the BART tunnel, the furnishings in this area must comply with BART Facility Standards and should not block sightlines into the Paseo.	Horizontal	Yes	BART Facilities Standards are complied with in areas above the BART-tunnel. Sightlines are preserved across the paseo.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	Furniture should include benches which orient towards both the Paseo and Oak Street.	Horizontal	Yes	Benches underneath the bus canopy orient towards Oak and benches around the skylight orient towards the paseo.
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	The expression of Oak Street as a "green street" encourages emphasizing public transit and where possible planting that does not conflict with the transit	Horizontal	Yes	Planting is limited on Oak Street given subterranean structures.

			program. (LMSAP/DG-128: Streets with Special Functions)			
Building Design	Street Frontage Guidelines for Block 1	Oak Street Block 1: Streetscape / Pedestrian Pathway Zone	Street furnishings should be provided in transit waiting areas and as needed to facilitate multi-modal transfers from bike or scooter or bus to BART. (LMSAP/DG-131: Furnishings)	Horizontal	Yes	Benches underneath the bus canopy orient towards Oak and are directly adjacent to scooter corals and bike lockers and bike parking.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Standards	Parking uses shall include a minimum of 25% openings for natural ventilation.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Standards	A minimum six-foot deep canopy shall be provided on Building A at the corner of Fallon and the Paseo above the pedestrian level.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Standards	Horizontal fins with a minimum depth of one foot shall be provided on alternating upper floors. See Figure 21.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Guidelines	Emphasize the corner of 9th and Fallon Street as a prominent corner with architectural features and main residential entry. (LMSAP/DG-7: Corner Building Design, LMSAP/DG-30: Ground Floor Entries)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Guidelines	Emphasize the corners of Fallon Street and 8th and 9th Streets as gateways to the Paseo and main residential entrances as well as providing architectural details to define the corner and a vibrant pedestrian entry. (LMSAP/DG-7: Corner Building Design, DG-30: Ground Floor Entries)	Vertical	Yes	Building B emphasizes the corner of Fallon and 9th by providing a civic scale three-story height covered entrance space with hanging art and lighting and a public stairway leading into the Paseo. Refer to Sheets 1/A3.00, 1/A3.03 and rendered view on G0.06, all on the Vertical FDP.
Building Design	Street Frontage	Fallon Street Frontage:	Visual transparency should be used to create an active and inviting public realm and	Vertical	Yes	Building B includes transparent storefront on the ground floor

	Guidelines for Block 1	Design Guidelines	to connect the street life to the Paseo. (LMSAP/DG-32: Views of Indoor Spaces)			along 8th street and the paseo. Refer to elevations on A3.00 and A3.03.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Guidelines	Residential entries should be designed as prominent elements that reinforce and activate street frontage. (LMSAP/DG-30: Ground Floor Entries)	Vertical	Yes	Building B provides a three-story height covered space with hanging art and lighting at the main residential entry at 9th and Fallon. Refer to Sheets 1/A3.00 and 1/A3.03 on the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street Frontage: Design Guidelines	The narrow corner at Building B creates a dramatic frontage that should be enhanced with a recessed entry or other significant and welcoming element that provides a visual landmark for Block 1 and relates to the scale of the Laney Campus.	Vertical	Yes	Building B provides a recessed three-story height entry space along Fallon. The space includes hanging art and lighting and a public stairway leading into the Paseo resembling civic spaces surrounding, including Laney Campus. Refer to Sheets 1/A3.00 and 1/A3.03 on the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and extension of the lobby to the outside should be encouraged.	Horizontal	Yes	Urban furniture elements are planned to be provided around the lobby entrances along Fallon St. The Building B ground floor community room will open up onto the Paseo - which will also provide this lobby "extension". Please see Sheet L4.01 on the Horizontal FDP for more detail.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Streetscape / Business Frontage Zone	Elegant vertical circulation with clear sightlines to the Paseo should be maintained at entry points mid-block in the business frontage zone.	Horizontal	Yes	Design of massing and materials could be further refined to create "elegant" vertical circulation and pragmatic groundwork of later phased projects. The design of the visual connection and circulation from



						Fallon Street into the Paseo is a priority.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Streetscape / Business Frontage Zone	Plantings that screen BART structures and utilities are desirable within the constraints of the BART Facilities Standards.	Horizontal	Yes	Ample planting is proposed around the existing BART vent and egress staircase at the east end of the paseo to obscure these structures from view within the limitations of BFS standards. Planting also proposed around headhouses.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street or text connecting to cultural identity of site such as poetry or history.	Horizontal	Yes	Corners as designed can accommodate stamped street names.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Pedestrian Pathway Zone	Public seating that can be located at back of sidewalk or near lobbies is encouraged.	Horizontal	Yes	Promenade bench seating proposed at the entrance of Building B at the corner of Fallon and 8th as well as for Building A at the corner of Fallon & 9th.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Pedestrian Pathway Zone	New street trees and lush plantings should be provided along Fallon to connect the Paseo with the campuses of Laney College and the OMCA. (LMSAP/DG-110: Tree Planting and Preservation)	Horizontal	Yes	Four new street trees proposed along Fallon Street which create a green connection towards OMCA and Laney College.

Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.	Horizontal	Yes	Tree wells designed for healthy tree growth range from 2 to 3 ft in depth.
Building Design	Street Frontage Guidelines for Block 1	Fallon Street: Landscape Furniture Zone	Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, and concrete.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 of the Horizontal FDP for furniture specs.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Standards	The Building A tower shall be setback from the base building a minimum of 10 feet along the 9th Street frontage. See Figure 29.	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Standards	Parking uses located in the Building A podium shall be wrapped in a porous screen with minimum 25% openings to allow for natural ventilation. Screening element to be integrated with overall building design, or consist of high-quality, contrasting material to create an architectural feature. (LMSAP/DG-98: Integral Design)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	Set back the ground floor two to four feet from the public right-of-way to create a wider sidewalk with a minimum of eight-foot clearance. Upper levels of the building may extend over the ground floor set back to the public right-of-way, maintaining a 15-foot minimum height from the sidewalk. (LMSAP/DG-125: Sidewalk Elements)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.

Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	Active ground floor uses should be provided at the building corners at Fallon Street and Oak Street. The minimum length of the active frontage from each corner should be not less than 30 feet. (LMSAP/DG-8: Primary Lot Frontage)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	Transparent openings should be provided at active uses such that windows allow views of indoor space between two and nine feet above the sidewalk. (LMSAP/DG-32: Views of Indoor Space)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	The access for parking and loading areas should be as narrow as functionally possible. (LMSAP/DG-92: Vehicular Access)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	Set back garage and other service access doors from the public right-of-way two to four feet to reduce prominence. Access doors should be fabricated from high-quality and easily maintained materials, with glazing incorporated where possible. (LMSAP/DG-93: Site Design)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	Non-active ground floor uses including service areas and parking facilities should be articulated with architectural elements such as a change in material and/or texture, screening elements, translucent windows and plantings (LMSAP/DG-93: Site Design)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Design Guidelines	At the 9th Street frontage, the tower should be set back 10 feet from the Building Base to reduce apparent scale and modulation of the podium facade. (LMSAP/DG-19: Step Back Above the Podium Height)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage	9th Street Frontage (Building A): Design Guidelines	Step backs above the Building Base should be utilized as roof gardens and	N/A	N/A	This is for Building A. Details to be confirmed during

	Guidelines for Block 1	A): Design Guidelines	active outdoor space. (LMSAP/DG-27: Active Upper Stories)			Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.	Horizontal	Yes	Street trees provide a visual 3-block connection towards Chinatown. Bike parking provided along 9th street. Historical signage proposed at the corner of 9th and Oak provides historical context (See Sheet L9.1 of the Horizontal FDP). Banner poles along 8th and 9th provide a clear vocabulary along this pedestrian thoroughfare connecting the station to Chinatown.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Pedestrian Pathway Zone	An overhead steel cable for lighting elements should be provided at each light pole and at equal distances not to exceed 18'-0" between poles. Secured power should be provided at the corner-most light poles for the lighting elements.	Horizontal	Yes	See Sheet L9.1 for banner poles along 8th and 9th Street. See Sheet L5.1 for location of street pole lights. There are only two pole lights on 9th Street. See lantern cable infrastructure on Sheet L10.1 (Item #18) of the Horizontal FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Control joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street and or text connecting to cultural identity of site such as poetry or history.	Horizontal	Yes	Corners as designed can accommodate stamped street names.

Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Pedestrian Pathway Zone	New street trees that could be carried west to Madison Park should be provided along 9th to connect to the campuses of Laney College and OMCA, strengthen its identity as a green street, and increase livability. (LMSAP/DG-110: Tree Planting and Preservation)	Horizontal	Yes	Eight new street trees (Quercus Suber 36") proposed along 9th Street create a green connection towards Madison Park and OMCA and Laney. See Sheet L7.3 & L7.4 for Planting Plan of the Horizontal FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.	Horizontal	Yes	Tree wells designed for healthy tree growth range from 2 to 3 ft in depth.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Landscape Furniture Zone	Between tree wells concrete unit pavers, stamped concrete, or cobblestones should be used to unify the three blocks of 8th and 9th at the curb edge. Stamp design could be created with an artist to be culturally relevant to Chinatown.	Horizontal	Yes	Decorative edge at 8th and 9th Street shown on Sheet L4.03 of the Horizontal FDP.
Building Design	Street Frontage Guidelines for Block 1	9th Street Frontage (Building A): Streetscape / Landscape Furniture Zone	Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials. For 8th and 9th these selections should extend to three block open space and should be colorful or otherwise stand out to reinforce the frame of the open space on 8th and 9th Streets.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 of the Horizontal FDP for furniture specs.

<p>Building Design</p>	<p>Street Frontage Guidelines for Block 1</p>	<p>8th Street Frontage (Building B): Design Standards</p>	<p>8th Street is considered a Principal Street and serves as an important connector for auto, bicycle and pedestrian traffic. It is designated as a pedestrian-oriented spine, a priority lighting corridor, a commercial corridor and transit preferential street. The Oak Street corner is the priority location for active retail uses on Block 1 and provides an important gateway to the public open spaces at the BART Plaza and the Paseo. The Fallon Street corner is also highly prominent due to the narrow building footprint and also provides an important gateway to the Paseo. Necessary ground floor services are located on 8th Street frontage to avoid impacting the Fallon Street frontage and public spaces at the Paseo and BART entrance areas.</p>	<p>Both</p>	<p>Yes</p>	<p>8th Street: Bike land and transit loading emphasized on 8th Street to fulfill the City's objective of 8th being a priority lighting, transit, and commercial corridor. The commercial kitchen on 8th Street faces out towards 8th with large picture windows to emphasize retail activity.</p> <p>Oak St Corner: Retail is currently programmed at Building A and Building B facing Oak Street and the BART plaza.</p> <p>Fallon St Corner: Ground floor services are located far from the Fallon Street corner to promote traffic into the paseo and activation at both residential entrances to Building A and B which are accessed on opposite corners of Fallon.</p>
<p>Building Design</p>	<p>Street Frontage Guidelines for Block 1</p>	<p>8th Street Frontage (Building B): Design Guidelines</p>	<p>To allow for an eight-foot minimum clear width at sidewalk, the ground floor should be setback an average depth of two to four feet from the public right-of-way for the entire frontage length. Upper levels of the building may extend over the ground floor setback to the public right-of-way, maintaining a 15-foot-minimum height from the sidewalk. The ground floor setback may also provide room for planting treatments to reduce blank wall impacts where blank walls are unavoidable. See Figure 36. (LMSAP/DG-</p>	<p>Vertical</p>	<p>Yes</p>	<p>Yes, the ground floor of Building B is setback at least at an average depth of two to four feet from the public right-of-way for the entire frontage length. Potential locations for public art and planting along this frontage are starting to be identified - particularly to reduce the amount of blank wall near service rooms. See Sheet L10.3 on the Horizontal FDP (8th St. cross section at</p>

			29: Distinct Ground Floor, DG-38: Blank Wall Limitations)			Block 1), as well as Sheet A3.01 on the Vertical FDP (Building B 8th St. elevation) for more detail.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Use massing breaks and architectural elements to reduce the apparent building bulk along 8th Street. The massing breaks could correspond to the internal function of the building. (LMSAP/DG-16: Three-dimensional Articulation)	Vertical	Yes	The Building B elevation along 8th Street is articulated with recessed and projecting elements and various materials to break up the building mass. Refer to rendering on G0.06, elevations on A3.01 and A3.04, and materials on A3.05.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Building design should take advantage of the south facing frontage on 8th Street to create visually interesting patterns of lights and shadows. Provide regular rhythms of elements such as awnings, metal canopies, sunshades and bays, and/or recessed windows to create playful light and shadow patterns. Consider use of awnings, canopies, and other details over 'back of house' activities to create a more interesting facade. (LMSAP/DG-16: Three-dimensional Articulation)	Vertical	Yes	The Building B south elevation along 8th Street is articulated with recessed and projecting elements and perforated sunshades to provide rough, staccato rhythm and visually interesting lights and shadows - however rhythm would be improved by simplifying the pattern and materials. Refer to elevations on A3.01 and A3.04, and materials on A3.05, of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Active ground floor commercial spaces should be provided at the Oak Street corner with a recommended minimum length on 8th Street of not less than 30 feet, utilizing large glazing and horizontal awnings that fully wrap the corner of this façade. The minimum height of the ground floor commercial space from the 8th Street sidewalk should be no less than 15 feet and no colonnades are allowed in front of the commercial	Vertical	Yes	Building B is designed to provide an active ground floor commercial space along the entire Oak Street elevation. The commercial kitchen is lined with storefront to allow for visibility into the active space. Refer to L1 Plan on A2.00 and elevations on A3.00 and A3.01 for heights, on the Vertical FDP.

			space. (LMSAP/DG-32: Views of Indoor Spaces, DG-7: Corner Building Design, DG-6: Avoid Colonnades)			
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Emphasize and highlight the building corner at Oak Street and 8th Street by architectural forms and features such as change in the height, roof form, different material, and recessed or projected architectural elements, to visually define and animate the intersection. (LMSAP/DG-7: Corner Building Design)	Vertical	Yes	The building corner at 8th and Oak Street are articulated with large storefront windows and awnings to allow pedestrians a look into the active space. Refer to elevations on A3.01, A3.04 and materials on A3.05, on the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Provide active ground floor uses at Fallon Street corner with a minimum length on 8th Street of not less than 30 feet. Active uses at this location should include residential lobby entrance or residential amenity spaces. (LMSAP/DG-32: Views of Indoor Spaces)	Vertical	Yes	The ground floor at Building B along Fallon and 8th Street consists of active common spaces including the residential lobby, lounge, and community room which total more than 30-ft of the 8th Street frontage. The spaces are lined with storefront to allow for visibility. Refer to L1 Plan on A2.00 and elevations on A3.00 and A3.01, on the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Provide windows on both sides of the community room and lobby to allow views from 8th Street frontage to the Paseo. This would activate the street frontage and also create visual connections between 8th Street and the Paseo. (LMSAP/DG-1: Public Perception, DG-32: Views of Indoor Spaces)	Vertical	Yes	Building B community room and lobby are lined with storefront on both 8th street and Paseo elevations to create a visual connection between the two. Refer to L1 Plan on A2.00 and elevations on A3.00 and A3.01, on the Vertical FDP.



Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Provide a minimum of 50% active ground floor frontage.	Vertical	Yes	<p>The majority of Building B ground floor is active ground floor space along all 4 frontages. Refer to L1 plan on A2.00.</p> <p>Also refer to sheet G0.17, #9: L1 - Active Ground Floor Diagram, on the Vertical FDP.</p>
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	Provide a ground floor setback at the Fallon Street corner to enhance public access and views to the Paseo from 8th Street. Upper levels of the building may extend over the corner setback to the public right-of-way. The corner ground floor setback should be a minimum of two stories. (LMSAP/DG-29: Distinct Ground Floor, DG-26: Pedestrian Scale)	Vertical	Yes	<p>The first three floors of the Building B corner at 8th Street and Fallon St are set back at an angle to provide public access and views to the paseo. Refer to L1 plan on A2.00, on the Vertical FDP</p>
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	<p>Limit blank walls, service doors and other non-active elements to a maximum of 30% of the linear ground floor frontage on 8th Street. Consider use of windows, architectural details, landscaping, or art details at these non-active elements. (LMSAP/DG-38: Blank Walls)</p>	Vertical	Yes	<p>The majority of Building B ground floor frontage at 8th street is active and articulated by storefront and textured materials. Service spaces are limited to a small portion and will be masked by architectural features and potentially public art. Refer to elevation on A3.01 and A3.04, on the Vertical FDP.</p> <p>Per Design Guidelines for Lake Merritt BART Project, Section B4 - 8th St. Frontage (Bldg. B): A minimum of 50% active ground floor frontage is required."</p> <p>Please refer to Vertical FDP sheet G0.17, #9 Level 1 - Active Frontage Diagram. We</p>

						are providing 52% active frontage at ground floor and the service frontage is 27% of the linear ground floor frontage on 8th Street.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Design Guidelines	New street trees should be provided along 8th to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)	Horizontal	Yes	Trees/planting will be placed along the 8th Street sidewalk. Refer to landscape plan L4.01 and L7.1 in Horizontal FDP set.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Business Frontage Zone	The building wall of 8th Street is experienced head on by those arriving from the south and east and as such should be memorable.	Vertical	Yes	The Building B elevation along 8th street is articulated with recessed and projecting elements and various materials to create visual interest for those approaching from south and east. Refer to rendering on G0.06, elevations on A3.01 and A3.04, and materials on A3.05, of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.	Horizontal	Yes	Street trees provide a visual 3-block connection towards Chinatown. Bike parking provided along 9th street. Historical signage proposed at the corner of 8th and Oak provides historical context (See Sheet L9.1 of the Horizontal FDP). Banner poles along 8th and 9th provide a clear vocabulary along this pedestrian thoroughfare connecting the station to Chinatown.

Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Pedestrian Pathway Zone	An overhead steel cable for lantern elements should be provided at each light pole and at equal distances not to exceed 18'-0" between poles. Secured power should be provided at the corner-most light poles for the lantern elements	Horizontal	Yes	On the Horizontal FDP, see Sheet L11.1 for banner poles along 8th and 9th Street. See Sheet L11.2 for location of street pole lights. There are only two pole lights on 9th Street. See lantern cable infrastructure on Sheet L11.2 (Item #18)
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street.	Horizontal	Yes	Corners as designed can accommodate stamped street names.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Pedestrian Pathway Zone	Extended space for pedestrians and transition from shuttles and cars to BART is provided outside the landscape furnishing zone	Horizontal	Yes	Additional loading for shuttles, passengers, paratransit, provided on 8th Street with additional sidewalk width to accommodate pedestrians.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.	Horizontal	Yes	Tree wells designed for healthy tree growth range from 2 to 3 ft in depth.

Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Landscape Furniture Zone	Between tree wells concrete unit pavers, stamped concrete, or cobblestones should be used to unify the three blocks of 8th and 9th at the curb edge. Stamp design could be created with an artist to be culturally relevant to Chinatown.	Horizontal	Yes	Decorative edge at 8th and 9th Street shown on Sheet L4.03. Seven new street trees (Quercus Suber 36") proposed along 8th Street create a green connection towards Madison Park and OMCA and Laney. See Sheet L7.1 for Planting Plan on the Horizontal FDP.
Building Design	Street Frontage Guidelines for Block 1	8th Street Frontage (Building B): Streetscape / Landscape Furniture Zone	Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials. For 8th and 9th these selections should extend to a three-block open space and should be colorful or otherwise stand out to reinforce the frame of the open space on 8th and 9th.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 for preliminary furniture ideas, further details are still needed.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Standards	Building A tower shall be setback from the building base	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Standards	Parking uses located in the Building A podium shall be wrapped in a porous screen with minimum 25% openings to allow for natural ventilation. Screening element to be integrated with overall building design, or consist of high-quality, contrasting material to create an architectural feature. (LMSAP/DG-98: Integral Design)	N/A	N/A	This is for Building A. Details to be confirmed during Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B):	A canopy at least four feet in width shall be located above pedestrian level at the corner	N/A	N/A	This is for Building A. Details to be confirmed during

		Design Standards	of the Paseo and Oak Street. See Figure 13.			Building A Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Guidelines	At the Paseo-fronting facades, visually interesting architectural features should be employed to encourage interactions with passersby and highlight focal points. (LMSAP/DG-26: Pedestrian Scale)	Vertical	Yes	The Building B façade fronting the Paseo is articulated with various materials including textured brick, hardwood screens, Juliet-balconies at upper-level residential units and transparent storefront to allow for a visual connection between passersby and the residential building. Refer to elevations on A3.00, A3.03, and materials on A3.05 of the Vertical FDP. Visual interest would be improved by refinement of materials and generation of material language that could be carried over to the later phased market rate units/ Building A -- See A3.05
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Guidelines	Active uses such as storefronts, dining and building entrances should be located at the ground floor frontage on both sides of the Paseo to create a destination and active public space. (LMSAP/DG-39: Storefronts)	Vertical	Yes	The Building B ground floor includes entries into the residential community room as well as entrance and signage for the commercial kitchen space. Refer to L1 plan on A2.00 and elevations on A3.00-A3.05 of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Guidelines	Use pedestrian-scale articulation such as overhangs, recesses, enhanced lighting, and increased transparency to create an active and inviting public scale. (LMSAP/DG-26: Pedestrian Scale, DG-78: Building Lighting)	Vertical	Yes	The Building B façade fronting the Paseo is articulated with various materials including textured brick, hardwood screens, transparent storefront, and awnings to create a pedestrian scale

						experience. Refer to elevations on A3.00, A3.03, and materials on A3.05 of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Guidelines	Ensure that awnings and canopies are in scale with the building and complement the overall design while providing protection from weather and sun. (LMSAP/DG-35: Awnings)	Vertical	Yes	The Building B awnings and canopies along the paseo are designed to provide protection from the elements and are sized to create an inviting pedestrian scale experience. Refer to elevation on A3.00 and A3.01 of the Vertical FDP.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Guidelines	Where blank walls are unavoidable, their facades should be articulated with architectural elements such as a change in material and/or texture, screening elements, translucent windows, public art, and/or plantings. (LMSAP/DG-38: Blank Walls)	Vertical	Yes	Blank wall areas on the Building B ground floor have been reduced as much as possible. At these areas use of textured brick, hardwood screens, signage and public art will articulate these areas. Refer to elevations on A3.00, A3.03, and materials on A3.05.
Building Design	Street Frontage Guidelines for Block 1	Paseo Frontage (Building A & B): Design Guidelines	Activate the roofs of the adjacent podium building(s) on the Paseo with shared amenities and other tenant uses to increase vibrancy of the pedestrian experience.	Vertical	Yes	Building B will include a roof deck on the 7th Floor that overlooks the BART transit plaza on the Oak St. and 8th St. frontage, and beyond towards core commercial Chinatown. This will include a community room and laundry room with generous windows and doors that open up to a deck with ample seating and planting, for residents to enjoy. More details can be found on the Vertical FDP set sheet L1.0 - L2.0 and A2.03.

Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	To allow for a five-foot six-inch minimum clear width at sidewalk, set back the ground floor an average depth of two to four feet from the public right-of-way for the entire frontage length. The depth of this ground floor step back may vary. Upper levels of the building may extend over the ground floor set back to the public right-of-way. (LMSAP/DG-29: Distinct Ground Floor)	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	Architectural articulations and landmark features should be used to highlight and emphasize the building corners at Madison and Oak Street to define and animate the intersections. (LMSAP/DG-7: Corner Building Design)	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	Active ground floor uses with high transparency should be provided for the entire 8th Street frontage. Active uses may include building entries, residential amenities, childcare space or other community serving uses and retail. Office and administrative uses at the ground floor should not exceed 25% of the length of the frontage. (LMSAP/DG-32: Views of Indoor Spaces)	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	At the Oak Street corner, Building C should provide an active use that wraps the corner on to Oak Street with large windows and other architectural features that create a strong relationship to the BART entrances and to Block 1. A flexible retail use with possible connection to the office lobby should be explored. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.

Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	A small plaza, building recess or other street level open space should be provided at or near the juncture of Building C and Building D. This mid-block break should be designed as a welcoming “front porch” to accommodate a primary entry or entries for one or both buildings. (LMSAP/DG-32: Views of Indoor Spaces, DG-48: Entry)	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	The Tower at Building C should be stepped back from the interior lot line at Building D to provide a mid-block massing break between Building C and Building D. This setback should be a minimum 10 feet for a minimum of 50% of tower perimeter, measured from the public right-of-way. It is recommended that this massing break be coordinated with the street level open space.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	The design of the high-rise Building C and the mid-rise Building D should be coordinated to create a cohesive frontage and to reinforce the overall sense of identity for the block. This coordinated design may take the form of a similar vocabulary of forms, openings and materials.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	At the 8th Street frontage, the tower should be stepped back up to 10 feet minimum from the building base to reduce the apparent scale of the tower.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	The tower massing should employ articulation strategies such as massing breaks, tapering and sculpted corners and roofline to reduce apparent mass and create a distinctive profile on the skyline. (LMSAP/DG-22:	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.



			Slender Towers, DG-24: Distinguished Tower Design)			
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	Open spaces should be provided at step backs and roof tops facing 8th Street to take advantage of views to BART Plaza, Lake Merritt and to downtown Oakland.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	Blank walls, service doors and other non-active elements should be limited to a maximum of 15%of the linear ground floor frontage on 8th Street. (LMSAP/DG-38: Blank Walls)	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street Frontage (Building C & D): Design Guidelines	Where blank walls or service doors occur at the ground level, consider incorporating artwork to enhance the 8th Street as the primary frontage for Block 2.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	8th Street: Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.	Horizontal	Yes	The Building B 8th street façade is articulated with building signage, refer to signage on G0.18. The sidewalk along 8th street is also lined with sidewalk planting, and a green space with a bench, refer to landscape plans on L4.1 and L7.1 in Horizontal FDP set. Sign Permit will be pursued under a separate permit.
Building Design	Street Frontage Guidelines for Block 2	8th Street: Streetscape / Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.
Building Design	Street Frontage Guidelines for Block 2	8th Street: Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street.	Horizontal	Yes	Corners as designed can accommodate stamped street names.

Building Design	Street Frontage Guidelines for Block 2	8th Street: Streetscape / Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.	Horizontal	Yes	Tree well and soil depth will be noted in detailed drawings and specifications at a later phase.
Building Design	Street Frontage Guidelines for Block 2	8th Street: Streetscape / Landscape Furniture Zone	Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.	Horizontal	Yes	Furnishings noted in drawings on L4.03 and L5.1 in Horizontal FDP set as well as preliminary lighting on L11.1 of Horizontal FDP set.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	Set back parts of the ground floor to allow for additional width at the sidewalk. The depth of the ground floor setback may vary but cannot exceed five feet. Where upper levels of the building extend over the ground floor setback, a generous 15-foot minimum clear height should be provided, and the setback be free of columns to increase visibility between the interior and the public way. (LMSAP/DG-6: Avoid Colonnades, DG-32: Views of Indoor Spaces, DG-128: Streets with Special Functions)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	Active ground floor uses should be provided along the entire Oak Street frontage. Active uses may include building entries, a maker space or retail use. Multiple entries are encouraged where feasible. Office and administrative uses at the ground floor should not	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.

			exceed 25% of the length of the frontage. (LMSAP/DG-30: Ground Floor Entrances)			
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	Architectural articulations and landmark features should be used to highlight and emphasize the building corners at 8th Street and 7th Street, including sculpted corners, changes in material and other features. (LMSAP/DG-7: Corner Building Design)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	At the 8th Street corner, Building C should provide an active ground floor use that wraps the corner onto 8th Street with large windows and other architectural features to create a strong relationship to the BART Plaza and to Block 1. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	At the 7th Street corner, Building C should provide an active ground floor use that wraps the corner onto 7th Street with architectural features to create a welcoming gateway element and a transition in scale to adjacent residential uses. (LMSAP/DG-32: Views of Indoor Spaces, DG-42: Flexible Commercial Space)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	The tower at Building C should be stepped back from the building base a minimum of 10 feet to reduce the apparent scale of the tower. (LMSAP/DG-19: Step Back Above the Podium Height)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	Limit blank walls, service doors and other non-active elements to a maximum of 15% of the linear ground floor frontage on Oak Street. (LMSAP/DG-38: Blank Wall Limitation)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.

Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	Where blank walls or service doors are unavoidable at the ground level, consider incorporating artwork to reinforce Oak Street as a gateway to the Lake Merritt BART Project. (LMSAP/DG-38: Blank Wall Limitation)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	New street trees and lush planting should be provided along Oak to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage (Building C): Design Guidelines	The Oak St. facade should be designed in response to existing buildings that form part of the 7th St/Harrison API by considering the use of step-backs, texture and/or scale at the base of the building.	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage: Streetscape / Business Frontage Zone	Setbacks at portions of the frontage of the ground floor provide space for the extension of the lobby program outside	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage: Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.	Horizontal	Yes	Promenade bench seating is provided on 8th Street at the corner of Oak Street in front of Building C. Uniform street trees provide a visual 3-block connection, rhythm towards Chinatown. Bike parking provided along 9th street.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage: Streetscape / Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.

Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage: Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street.	Horizontal	Yes	Corners as designed can accommodate stamped street names.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage: Streetscape / Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.	Horizontal	Yes	Tree wells designed for healthy tree growth range from 2 to 3 ft in depth.
Building Design	Street Frontage Guidelines for Block 2	Oak Street Frontage: Streetscape / Landscape Furniture Zone	Furniture types include loop bike racks, and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 and L5.1 of the Horizontal FDP for furniture specs.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	To allow for additional width at sidewalk, the ground floor should be setback an average depth of two feet from the public right-of-way for the entire frontage length. The depth of the ground floor setback may vary but cannot exceed five feet. Upper levels of the building may extend over the ground level setback. See Figure 36.	Horizontal	Yes	Building B is set back an average of 2ft from the public right of way for the entire length.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	Architectural articulation should be used to highlight the building corners and active ground floor uses at Oak Street and Madison Street.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.

Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	Active ground floor uses should be provided at the Oak and Madison Street corners with a minimum length on 7th Street of 30 feet. Active uses may include building entries, childcare classrooms, maker space or retail use. Entries to active space from 7th Street are encouraged where feasible.	N/A	N/A	This is for Building C & D. Details to be confirmed during Building C & D Vertical FDPs.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	Parking access and service areas should be located mid-block. Curb cuts for parking access, loading and waste rooms should be consolidated where feasible. (LMSAP/DG-92: Vehicular Access)	Horizontal	Yes	Curb cut is on mid-block 8th street.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	The design of parking entries and service areas at Building C and Building D should be coordinated to provide an integrated design and an attractive pedestrian frontage.	Horizontal	Yes	Uniform street frontage and widened sidewalks provides an attractive pedestrian realm.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	Where the Building C tower is stepped back from the building base, occupied roof terraces should be provided to activate and improve the pedestrian experience. (LMSAP/DG-27: Active Upper-Stories)	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Design Guidelines	The tower at Building C should be stepped back from the interior lot line at Building D to provide a mid-block massing break between Building C and Building D. The setback should be at least 10 feet in width and at least 30 feet in depth, measured from the public right-of-way.	N/A	N/A	This is for Building C. Details to be confirmed during Building C Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 and L5.1 of the Horizontal FDP for furniture specs.

Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Streetscape / Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Concrete Color and Pattern to be complex, rich tones, and special install. See page L6.1 of Horizontal FDP Set with Civil for details.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street.	Horizontal	Yes	Corners as designed can accommodate stamped street names.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Streetscape / Pedestrian Pathway Zone	New street trees and lush planting along 7th are encouraged to reinforce its role as a green street and provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)	Horizontal	Yes	Eight (8) new trees proposed along 7th street to create a green street and provide additional livability.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Streetscape / Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet.	Horizontal	Yes	Tree wells designed for healthy tree growth range from 2 to 3 ft in depth.
Building Design	Street Frontage Guidelines for Block 2	7th Street Frontage (Building C & D): Streetscape / Landscape Furniture Zone	Furniture types include loop bike racks, backed and non-backed benches, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 for furniture specs.
Building Design	Street Frontage	Madison Street Frontage	A six-foot deep and a 40-foot-long awning along 7th Street and a 10-foot long	N/A	N/A	This is for Building D. Details to be confirmed during

	Guidelines for Block 2	(Building D): Design Standards	along Madison Street shall be provided at the southwest corner of Building D.			Building D Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Standards	Upper-level horizontal awnings shall be included at the second and third floors of the 7th and Madison Street corners. These upper floor awnings shall be four-foot deep and 30-foot long along 7th Street from the 7th and Madison Street. Additional horizontal awnings above the third floor are not required but encouraged. See Figure 63.	N/A	N/A	This is for Building D. Details to be confirmed during Building D Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Guidelines	To allow for an eight-foot minimum clear width at sidewalk, the ground floor should be setback a maximum depth of five feet from the public right-of-way for the entire frontage length. The depth of the ground floor setback may vary. Upper levels of the building may extend over the ground level set back to the public right-of-way. Architectural and landscape features such as awnings and planting should be used to enhance the pedestrian experience and to reduce the impact of upper floor overhang. See Figure 30. (LMSAP/DG-29: Distinct Ground Floor)	Horizontal	Yes	Building D setback complies along Madison Street.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Guidelines	Entries, transparent windows and glazing should be provided at the 8th Street and 7th Street corners with a minimum length on Madison Street of 30 feet. (LMSAP/DG-32: Views of Indoor Spaces)	N/A	N/A	This is for Building D. Details to be confirmed during Building D Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Guidelines	Active uses are encouraged at the remainder of the frontage. These uses may include offices, community amenity and childcare care facilities. (LMSAP/DG-42: Flexible Commercial Space)	N/A	N/A	This is for Building D. Details to be confirmed during Building D Vertical FDP.



Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Guidelines	Architectural articulations should be used to highlight the building corners and to active ground floor uses at 7th Street and 8th Street. (LMSAP DG-26: Pedestrian Scale)	N/A	N/A	This is for Building D. Details to be confirmed during Building D Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Guidelines	A street level plaza or courtyard is encouraged to create a mid-block break at the street level. This space should be at least 30 feet in width and at least 20 feet in depth. This space may serve as a building entry, outdoor space for residents or outdoor space for a potential childcare space or other community serving use. A decorative fence may be provided to secure street level open space provided the fence is at least 75% transparent with a maximum eight-foot height. (LMSAP/DG-46: Street Wall Openings)	Horizontal	Yes	Building D courtyard is at least 30ft wide and 20ft deep.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Design Guidelines	If a mid-block residential courtyard is provided at Building D at the street level or at an upper level, an opening to Madison Street is encouraged to provide additional sunlight into the courtyard and to provide a massing break at the upper floors. This opening should be at least 20 feet in width. Bridging elements are acceptable within this opening provided, they are open-air and allow significant visual transparency into the courtyard. (LMSAP/DG-46: Street Wall Openings)	N/A	N/A	This is for Building D. Details to be confirmed during Building D Vertical FDP.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Streetscape / Business Frontage Zone	Small urban furniture elements including benches, container plants, and signs provide interest and rhythm for pedestrians and should be encouraged.	Horizontal	Yes	Promenade bench seating is provided along Madison Street at the mid-block courtyard.

Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Streetscape / Pedestrian Pathway Zone	Sidewalk should be colored with maximum amount of lamp black allowable by the City of Oakland standards. Concrete jointing should avoid a joint line at the center of the walk width if at all possible. Joints should be tooled at 1/4 inch wide using a radius of 1/8 inch each side.	Horizontal	Yes	Not typically specified at this level of detail in an FDP, but we can add this note to our materials plans.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Streetscape / Pedestrian Pathway Zone	Corners of sidewalks should be stamped with letters to show name of street.	Horizontal	Yes	Corners as designed can accommodate stamped street names. Details to be confirmed at later stage of design.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Streetscape / Pedestrian Pathway Zone	New street trees are encouraged along Madison to provide additional livability. (LMSAP/DG-110: Tree Planting and Preservation)	Horizontal	Yes	Five (5) new street trees are proposed along Madison St.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Streetscape / Landscape Furniture Zone	Tree wells should provide continuous depth of soil volume for tree health within structural constraints of curb and sidewalk. Tree wells should be designed to achieve healthy plant growth including the provision of adequate drainage and quality planting soil. Durable, non-woody, evergreen plants should be selected for areas likely to be impacted by human feet	Horizontal	Yes	Tree wells designed for healthy tree growth range from 2 to 3 ft in depth.
Building Design	Street Frontage Guidelines for Block 2	Madison Street Frontage (Building D): Streetscape / Landscape Furniture Zone	Furniture types include loop bike racks, backed and non-backed benches that face two directions, and lights. All products should be selected to be durable for the heavy impacts of an urban site such as using thermally modified woods, powder coated or galvanized metals, concrete, and other suitable materials.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 for furniture specs.

Open Space	Block 1	Open Space	The following recommendations are guidelines for the larger open spaces, not the sidewalk open space that is described above in the street frontages:	Horizontal	See Below	See Below
Open Space	Block 1	Open Space	In order to create a public realm that not only meets the needs of the community, but does so with the highest attention to health, safety, and environmental considerations the design should co-locate public or active ground floor programs and adjacent open space as an important partnership in the success of both. (LMSAP/DG-11: Crime Prevention through Environmental Design)	Horizontal	Yes	Paseo is flanked by active retail commercial uses as well as the residential lobbies for both Building A and Building B to promote active eyes on the public space.
Open Space	Block 1	Open Space	Maximize planting opportunities are encouraged to create identity, buffer, to create strong edges and to generally increase greening that can be experienced by users of the open space. It is recommended that planting selections meet WELO criteria and emphasize native and drought tolerant tough urban plants. Plants should also be selected for cultural resonance with Chinatown. (LMSAP/DG-84 through DG-87: Landscaping)	Horizontal	Yes	Planting palette creates a clear connection and distinct vocabulary along 8th and 9th street which create a direct three block connection to Chinatown.  In the paseo, planting is proposed along the edges of the entire paseo to soften the connection between the built environment and the public gathering spaces. Minor revision of allowing openings in the planting on the north of the paseo, would allow additional access to later phased market-rate Building A.
Open Space	Block 1	Open Space	It is encouraged to maximize a variety of conditions that include, sunny, shady, partially sunny, partially shady in well-scaled and hospitable locations for	Horizontal	Yes	Awnings flanking the paseo will provide shade cover and changing lighting conditions throughout the day will provide

			seating. (LMSAP/DG-137: Sun Exposure)			varied sun exposure in the paseo.
Open Space	Block 1	Paseo	Concentrate outdoor dining at the west end of the Paseo to create a destination with multiple co-located options for dining.	Horizontal	Yes	Dining is concentrated at the west end of the paseo to capture maximum visibility for BART riders and transit riders in the Oak Street corridor.
Open Space	Block 1	Paseo	Create a layered program of use within the Paseo that is attractive at different times of day and attracts multigenerational users. (LMSAP/DG-142: Amenities and DG-146 through 148)	Horizontal	Yes	Varied gathering spaces for dining, passive gathering, and more quiet contemplative space is all programmed throughout the paseo. The west end is the more active commercial facing and transit interface, the middle is an area for passive recreation and play, and the eastern end is quieter and interfaces directly with the residential lobbies and amenity spaces in Building A and Building B.
Open Space	Block 1	Paseo	Utilize materials that are appropriate to the civic and public nature of this location and meet BART's Facilities Standards. (LMSAP/DG-143: Surfaces)	Horizontal	Yes	High quality materials are proposed in the paseo that speak to the civic nature of this destination and transit thoroughfare. See Sheet L4.03 and L6.1 for materials.
Open Space	Block 1	Paseo	Paseo design shall accommodate BART maintenance truck accessibility for regular maintenance of BART infrastructure."	Horizontal	Yes	A BART light weight maintenance vehicle can be accommodated through the planned paseo improvements and between moveable furniture.
Open Space	Block 1	Paseo: Planting	Planting in the Paseo must conform with BART Facilities Standards for work over existing BART tunnels. The planting must be hand watered and drainage must be directed off-site. Structural load criteria for the tunnel may limit soil volume and	Horizontal	Yes	Planting has been reviewed by BART and meets their standards for maintenance and soil volume.

			therefore the size or type of plants that could be planted here. The community process has resulted in a strong desire to maximize greening, so the design should try to achieve as much planting as possible within these above stated constraints.			
Open Space	Block 1	Paseo: Planting	Plant selected should be low water, durable in a heavily used urban realm, and provide evergreen lush planting year-round. Where possible with sun exposure, native plants, pollinator species, seasonal change, and culturally relevant plants should take priority in selections.	Horizontal	Yes	A variety of plant species provide greening throughout the year and various seasons. See Sheet L7.3, L7.4 for Planting Palette.
Open Space	Block 1	Paseo: Planting	Adequate soil volume and drainage should be provided for the long-term health of the plants.	Horizontal	Yes	Adequate soil volume proposed for plant species proposed.
Open Space	Block 1	Paseo: Furniture	Seating should be provided in a variety of types including movable, fixed, communal, tiered, face to face, lounging, and back-to-back.	Horizontal	Yes	Fixed bench seating around planters as well as moveable furniture proposed.
Open Space	Block 1	Paseo: Furniture	Locations of seating should consider microclimate including wind, sun, and shade.	Horizontal	Yes	Furniture located in a variety of locations throughout the paseo to maximize utility in a variety of microclimates and as daylight exposure changes throughout the day.
Open Space	Block 1	Paseo: Furniture	Furniture should be high quality, durable, and beautiful. Color palette and material for furniture elements should be composed with both Paseo materials and architectural facade materials.	Horizontal	Yes	Durable furniture, bike racks, benches selected. See Sheet L4.03 and L5.1 for furniture specs.
Open Space	Block 1	Paseo: Furniture	Waste stations should provide for trash, recycling, and compost.	Horizontal	Yes	Trash locations to be detailed at later stage of design.
Open Space	Block 1	Paseo: Furniture	No ash urns should be provided.	Horizontal	Yes	No ash urns provided.

Open Space	Block 1	Paseo: Lighting	Site lighting should be designed to provide a consistent level of lighting for faces across the Project site and at transitions off the Project area.	Horizontal	Yes	See lighting Plan and Specs on Sheet L11.1--L11.6 of the Horizontal FDP. Ample lighting in the paseo provides for diverse but consistent lighting zones.
Open Space	Block 1	Paseo: Lighting	The Paseo identity during early evening gathering and dining should be strengthened with lighting that creates and celebrates the cultural identity of this site. Lanterns, color, and other lighting should be used to create a ceiling and sense of place.	Horizontal	Yes	A lighting cluster at the western end of the paseo creates a visual anchor above the paseo which will draw in pedestrians.
Open Space	Block 1	Paseo: Pavement	Pavement in the Paseo must conform with BART Facilities Standards for work over existing BART tunnels.	Horizontal	Yes	Paving complies with BFS standards.
Open Space	Block 1	Paseo: Pavement	Pavement must be easily cleaned and durable for the heavy uses that are anticipated at most urban transit locations.	Horizontal	Yes	Paving complies with BFS standards. See Sheet L4.03 of the Horizontal FDP for materials spec.
Open Space	Block 1	Paseo: Pavement	Cast-in-place concrete with Lithocrete finish or other durable materials may be used for strong graphic pattern.	Horizontal	Yes	Paving complies with BFS standards. See Sheet L4.03 of the Horizontal FDP for materials spec.
Open Space	Block 1	Paseo: Pavement	Trench grate will integrate with ground floor pattern.	Horizontal	Yes	See Horizontal FDP Sheet L4.03 for materials spec and trench grate. See Horizontal FDP Sheet L8.0 Grading Plan for location of trench conforming to paving pattern.
Open Space	Block 1	Roof Deck	In consideration of local ecology of Lake Merritt and the open water, roof deck plantings should provide ecosystem services in the form of a pollinator garden. (LMSAP/DG-84 through DG-87)	Vertical	Yes	Please refer to Vertical FDP Sheet L2.0 for planting design. Additional detail clarifying how the planting is selected for pollinator species will be added.

Open Space	Block 1	Roof Deck	Program and utilization of roof decks should provide activation and diverse opportunities by multigenerational users.	Vertical	Yes	Building B will include a roof deck on the 7th Floor that overlooks the BART transit plaza on the Oak St. frontage, and beyond towards core commercial Chinatown. This will include a community room and laundry room with generous windows and doors that open up to a deck with ample seating and planting, for residents to enjoy. More details can be found on the Vertical FDP set sheet L1.0 - L2.0 and A2.03.
Open Space	Block 2	Entry Plaza	Entry plaza pavement should use high quality materials and be differentiated from adjacent sidewalks. (LMSAG/DG-145: High Quality Materials)	Horizontal	Yes	See Horizontal FDP Sheet L4.03 for materials spec which differentiate entry plaza from adjacent sidewalks.
Open Space	Block 2	Entry Plaza	Visual connection to interiors of the building should be maintained for people arriving or departing from the building. (LMSAP/DG-11: Crime Prevention through Environmental Design)	Vertical	Yes	Stairs in Building B speak to stairs directly in the Paseo (entry at 8th & Fallon) as part of a holistic entry experience into the Building and Paseo. Additionally, the glazing on both sides of the ground floor community room (mid-block at 8th St.) will provide an interrupted visual connection between 8th St. and the Paseo.
Open Space	Block 2	Residential Courtyard	Create a layered program of use within the courtyard that is attractive at different times of day and attracts multigenerational users. (LMSAP/DG-142: Amenities, DG-146 through DG-148)	Vertical	N/A	This is for Building D. Details to be confirmed during Building D Vertical FDP.
Open Space	Block 2	Residential Courtyard	Provide low-water and native planting that contributes to the local ecology.	Vertical	N/A	This is for Building D. Details to be confirmed during

			(LMSAP/DG-84 through DG-87)			Building D Vertical FDP.
Open Space	Block 2	Daycare Open Space	Children’s play environments must meet the required codes of play spaces, but should also connect children with the natural world through materials, planting selections, and art.	Horizontal	Yes	Building D daycare open space conforms.
Signage	Consistency	Consistency	Signage will be consistent with the guidelines set forth in the Lake Merritt Station Area Plan Design Guidelines (LMSAP/DG-79 through DG-83).	Both	Yes	<p>Sign Permit to be pursued under separate permit. For Vertical FDP, see sheet G0.18 for signage plan. For Horizontal FDP, see Sheet L9.1 for signage plan.</p> <p>DG-79 Illumination - Complies.                      DG-80 Visibility - Complies. Signs are placed for easy visibility for residents and transit riders.                      DG-81 Architectural Compatibility - Complies. Will fit context of the Neighborhood, Building, and applicable surrounding/background materials.                      DG-82 Consistency w/Area Character - Complies. Signage designed to promote a strong connection to Chinatown.                      DG-83 Legibility - Complies. Exact copy and typography to be confirmed at later stage of design.</p>



Signage	Integrated Design	Integrated Design	Signage should be designed to reinforce the overall design character of the Lake Merritt Project. Signs and mounting systems should be integrated into the exterior design and should be constructed of high-quality materials that complement the exterior material and color palette. (LMSAP/DG-79 through DG-82)	Both	<p>Sign Permit to be pursued under separate permit. For Vertical FDP, see sheet G0.18 for signage plan. For Horizontal FDP, see Sheet L9.1 for signage plan.</p> <p>DG-79 Illumination - Complies.                  DG-80 Visibility - Complies. Signs are placed for easy visibility for residents and transit riders.                  DG-81 Architectural Compatibility - Complies. Will fit context of the Neighborhood, Building, and applicable surrounding/background materials.                  DG-82 Consistency w/Area Character - Complies. Signage designed to promote a strong connection to Chinatown.                  DG-83 Legibility - Complies. Exact copy and typography to be confirmed at later stage of design.</p>
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Signage	Visibility and Illumination	Visibility and Illumination	Signage should be located and designed to be readily visible by pedestrians. Graphics should be designed to be highly legible and consistent with the exterior design intent. Illumination should be provided to ensure signage is visible in the evening hours. (LMSAP/DG-79: Illumination, DG-83: Legibility and Readability)	Both	Yes	<p>Sign Permit to be pursued under separate permit. For Vertical FDP, see sheet G0.18 for signage plan. For Horizontal FDP, see Sheet L9.1 for signage plan.</p> <p>DG-79 Illumination - Complies.                  DG-80 Visibility - Complies. Signs are placed for easy visibility for residents and transit riders.                  DG-81 Architectural Compatibility - Complies. Will fit context of the Neighborhood, Building, and applicable surrounding/background materials.                  DG-82 Consistency w/Area Character - Complies. Signage designed to promote a strong connection to Chinatown.                  DG-83 Legibility - Complies. Exact copy and typography to be confirmed at later stage of design.</p>
Lighting	Lighting	Lighting	Exterior site lighting will be consistent with most of the guidelines set forth in the Lake Merritt Station Area Plan Design Guidelines (LMSAP/DG-123: Lighting and Safety, DG-124: Pedestrian-Oriented Lighting, DG-150: Lighting) However, the additional of light as a goal without consideration of overall levels is not recommended. Lighting design proposed is consistent with best practices and generally focused on evenness of transitions rather than increased brightness.	Both	Yes	<p>For Vertical FDP, see Sheets LT2.00 - LT3.00. For Horizontal FDP, see Sheet L11.1 --- L11.6.</p> <p>DG-123 Lighting &amp; Safety - Complies. Public spaces are well lit.                  DG-124 Pedestrian-Oriented Lighting - Complies. Continuous pedestrian lighting on all street frontages provided to increase safety.                  DG-150 Lighting -</p>

						Complies. Lighting maximized at transit connections and in public plazas.
Lighting	Lighting	Lighting	Provide adequate lighting to provide a safe environment for pedestrian safety while conforming to current best practices to mitigate light pollution.	Both	Yes	<p>Ample lighting will be provided on Building B to illuminate the adjacent sidewalk, walkways and Paseo. This includes major lighting features along the length of building - especially along 8th St. In addition to large areas of transparency on 8th St., blank wall and back-of-house entryways will be illuminated to create a continuous, well-lit pedestrian experience. Please see sheets LT2.00 - LT4.02 for more detail.</p> <p>The lighting plan for the entire TOD will result in well-lit plazas and pedestrian pathways. Lighting fixtures at numerous, context-sensitive scales, are provided along the length of the Paseo. This will create a pedestrian-friendly experience that creates a safe public space environment at night. All non-lantern lights will be dark skies compliant. For the TOD Horizontal FDP, please see sheets L11.1-11.5 for more detail.</p>

Lighting	Lighting	Lighting	The lighting plan will be designed to create well-lit plazas and pedestrian pathways through the site including surrounding sidewalks.	Both	<p>Ample lighting will be provided on Building B to illuminate the adjacent sidewalk, walkways and Paseo. This includes major lighting features at the corners of the building. The 8th &amp; Oak corner will be illuminated around the ground floor commercial space of Building B, and will augment existing illumination at the existing BART headhouse. The double-height for Building B at the corner of 8th and Fallon will also have exceptional lighting features to create a well-lit entrance to the Paseo and residential lobby for Building B. Please see sheets LT2.00 - LT4.02 for more detail.</p> <p>The lighting plan for the entire TOD will result in well-lit plazas and pedestrian pathways. Lighting fixtures at numerous, context-sensitive scales, are provided along the length of the Paseo. This ranges from larger mounting poles and pedestrian poles to smaller fixtures such as light wands, up lights, etc. For the TOD Horizontal FDP, please see sheets L11.1-11.5 for more detail.</p>
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Sustainable Design	Site Design Measures	Site Design Measures	Orient units and/or provide architectural shading treatments to maximize winter solar exposure and minimize summer exposure.	Vertical	Yes	Building B is employing numerous design techniques to maximize winter solar exposure and minimize summer exposure. This includes having a large share of units that will be south-facing (along 8th St.), while also placing many units on the corners of the building, to be east and west-facing. For south-facing units, Building B will provide sunshades to minimize summer solar exposure. Large glazed openings will be provided on north-facing units to which provide more light and air to these units. See sheets A3.00 - A3.01 and A3.03 - A3.04 for more detail.
Sustainable Design	Site Design Measures	Site Design Measures	Provide on-site stormwater treatment as appropriate to the scale of the buildings and available open space. The stormwater design shall comply with the Municipal Regional Permit Order No. R2-0074 and the City of Oakland Storm Drainage Design Guidelines, which establish a 25% goal for peak flow reduction compared to existing conditions, to the extent possible.	Both	Yes	The stormwater design will comply with relevant local guidelines. There is limited space for detention measures onsite, as almost 90% of the site covered by permanent structure, including the BART tunnel. A 25% peak flow reduction will be accomplished in all areas except the Paseo. Please see sheet C7.1 for more detail for the Vertical FDP, and sheets C7.1 - C7.2 for the Horizontal FDP. Stormwater Supplemental Forms for both the Vertical and Horizontal FDPs

						were also submitted at Intake.
Sustainable Design	Site Design Measures	Site Design Measures	Provide on-site secure bicycle parking and secure bicycle parking for the most biked to station of the BART network, reducing the carbon footprint.	Both	Yes	<p>The TOD (for the Horizontal FDP) will provide ample secure bicycle parking. This includes 38 bike racks accommodating 76 bikes total (16 block 1, and in a later Phases, 22 Block 2) spots across the TOD on sidewalks and on the perimeter of the Paseo. On Block 1, there will also be 32 bike locker spaces (accommodating 32 bikes), directly adjacent to the BART Headhouse on 9th &amp; Oak. Please see sheets L4.01-L4.02 for more detail.</p> <p>Building B (for the Vertical FDP) will provide 49 secure bicycle parking spaces within the building. Please see sheet A2.00 for more detail.</p>
Sustainable Design	Site Design Measures	Site Design Measures	Reduce parking capacity to a reasonable minimum.	Both	Yes	<p>The TOD will provide an overall low amount of parking across the project. The entire TOD (for the Horizontal FDP) limits the usage of curb space for parking, in order to promote active/shared uses. Building B (for Vertical FDP) will provide 0 parking spaces.</p> <p>For the Horizontal FDP - please refer to sheets L4.1 - L4.2 to see the curb space uses across the TOD.</p>

Sustainable Design	Site Design Measures	Site Design Measures	Consider designated parking for carpool vans or car share vehicles.	Both	Yes	Buildings A & Building C within the TOD that include off-street parking have at least one (1) dedicated parking space for car share vehicles.
Sustainable Design	Site Design Measures	Site Design Measures	Use native, drought-tolerant and shade tolerant landscaping to minimize irrigation required.	Both	Yes	<p>The landscaping for the TOD (for Horizontal FDP) and around/on Building B (for Vertical FDP) intends to utilize drought-tolerant and shade tolerant landscaping. This includes Tree Species such as the Brisbane Box, Paper box Tree, and Cork Oak. Because of the location over the BART tunnel on Block 1 in particular, the Developers do not plan to provide intensive irrigation.</p> <p>For the Horizontal FDP - please refer to sheets L7.1 - L7.4 to see the overall Planting Plan for the whole TOD.</p> <p>For the Vertical FDP - please refer to sheets L1.00 - L1.01 for detail on planting on the Roof Deck on Building B.</p>
Sustainable Design	Site Design Measures	Site Design Measures	Provide more vegetated spaces and street trees to reduce the heat island effect.	Both	Yes	<p>Ample vegetated spaces and streets trees are provided throughout the TOD (for Horizontal FDP) and around/on Building B (for Vertical FDP).</p> <p>For the Horizontal FDP - please refer to sheets L7.1 - L7.4 to see the overall</p>

						<p>Planting Plan for the whole TOD.</p> <p>For the Vertical FDP - please refer to sheets L1.00 - L1.01 for detail on planting on the Roof Deck on Building B.</p>
Sustainable Design	Building Design Measures	Building Design Measures	Use reflective roofing to minimize heat island effect.	Vertical	Yes	<p>Building B will plan to provide reflective roofing. The Building will utilize a cool roof design for the flat roof area to reduce heat island effect. Please see sheet A2.04. Specs to be confirmed during later design stages/Building Permit stage.</p>
Sustainable Design	Building Design Measures	Building Design Measures	Use water-conserving fixtures and irrigation systems.	Vertical	Yes	<p>Building B will include a multitude of water-conserving fixtures and systems. EBALDC uses Energy-STAR appliances and fixtures as a requirement for all residential units. An irrigation system that is compliant with State requirements and the City's Water Efficient Landscape Ordinance (WELo) will be provided on the roof deck garden, as well as any Building-adjacent planting. More details can be found on sheets L2.0. Specs to be confirmed during later design stages/Building Permit stage.</p>
Sustainable Design	Building Design Measures	Building Design Measures	Design building envelope, HVAC systems, lighting, and other systems to maximize energy efficiency. Consider fundamental commissioning of development systems.	Vertical	Yes	<p>Building B will be highly energy efficient, in order to meet the requirements of being GPR Gold certified. The building will provide an energy efficient VRF HVAC</p>



						system, for example. The building also includes large windows and doors to maximize natural light throughout the day.
Sustainable Design	Building Design Measures	Building Design Measures	Consider on-site electrical generation or purchase of off-site renewable energy.	Vertical	Yes	Building B will provide solar/PV arrays to offset some of the electrical demands of the building. Please see sheet A2.04.
Sustainable Design	Building Design Measures	Building Design Measures	Consider all-electrical building design and reduction of gas.	Vertical	Yes	Building B will be an All-Electric building, per City of Oakland New Construction Ordinance.
Sustainable Design	Building Design Measures	Building Design Measures	Provide adequate facilities to allow for recycling by residents.	Vertical	Yes	Building B will be providing ample, user-friendly methods for recycling. There will be a three-chute system, with chutes provided on each floor. Please see sheet A1.01 for more detail on the ground floor plan - trash room is labeled, and in gray.
Sustainable Design	Building Design Measures	Building Design Measures	Where possible, use recycled, salvaged, sustainably harvested, or locally produced materials.	Vertical	Yes	Building B will attempt to use recycled, salvaged, sustainably harvested, or locally produced materials, when feasible and cost efficient.  Per GPR Checklist: Per our GPR checklist: (TBD) A3. Recycled Content Base Material F1. Insulation with 30% Post-Consumer or 60% Post-Industrial Recycled Content: (Yes) F1.1 Walls and Floors (Yes) F1.2 Ceilings  As examples we will consider recycled-content ceramic tiles

						and recycled-content carpet.
Sustainable Design	Building Design Measures	Building Design Measures	Use low- or no-VOC materials in interior spaces.	Vertical	Yes	Building B will use low- or no-VOC materials in interior spaces. Specs to be confirmed during later design stages/Building Permit stage. Per GPR checklist: (Yes) K2.1 Zero-VOC Interior Wall and Ceiling Paints (< 5 gpl)
Sustainable Design	Building Design Measures	Building Design Measures	The development should be designed and constructed in accordance with the recommendations of a recognized "Green" rating system such as GreenPoint, Enterprise Green Communities, and USGBC LEED rating.	Vertical	Yes	Building B will achieve GreenPoint Rated (GPR) "Gold". A copy of the current GPR scorecard for Building B was provided in the Basic Application.

Attachment E: 2021 Lake Merritt Bart PUD COA

## **ATTACHMENT D:**

### Conditions of Approval:

1. Standard Conditions of Approval
  - a. Standard Conditions of Approval and Mitigation Monitoring and Reporting Program
2. Oakland Department of Transportation, Engineering Services Conditions of Approval
3. Oakland Department of Transportation, City Surveyor Conditions of Approval
4. Oakland Fire Department Conditions of Approval

## Part 1: Standard Conditions of Approval – General Administrative Conditions

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### 1. Approved Use

The project shall be constructed and operated in accordance with the authorized use as described in the approved application materials, May 19, 2021 Planning Commission Staff Report and the approved plans dated May 12, 2021, as amended by the following conditions of approval and mitigation measures, if applicable (“Conditions of Approval” or “Conditions”).

### 2. Effective Date, Expiration, Extensions and Extinguishment

This Approval shall become effective immediately, unless the Approval is appealable, in which case the Approval shall become effective in ten (10) calendar days unless an appeal is filed. Unless a different termination date is prescribed, this Approval shall expire May 19, 2023, two years from the Approval date, or from the date of the final decision in the event of an appeal, unless within such period a complete Phase 1 Final Development Plan (FDP) has been filed with the Bureau of Planning and diligently pursued towards approval. Subsequent Phase 2 Final Development Plans shall be filed within 2 years of the approval of previous FDP to ensure the Preliminary Development Plan (PDP) does not expire. The FDP shall expire within two years from the approval date unless within such period a complete building permit application has been filed with the Bureau of Building and diligently pursued towards completion, or the authorized activities have commenced in the case of a permit not involving construction or alteration. Upon written request and payment of appropriate fees submitted no later than the expiration date of this Approval, the Director of City Planning or designee may grant a one-year extension of this date, with additional extensions subject to approval by the approving body. Expiration of any necessary building permit or other construction-related permit for this project may invalidate this Approval if said Approval has also expired. If litigation is filed challenging this Approval, or its implementation, then the time period stated above for obtaining necessary permits for construction or alteration and/or commencement of authorized activities is automatically extended for the duration of the litigation.

The approved Vesting Tentative Tract Map (VTTM) shall expire twenty-four (24) months after its approval or conditional approval, unless an extension is granted.

### 3. Compliance with Other Requirements

The project applicant shall comply with all other applicable federal, state, regional, and local laws/codes, requirements, regulations, and guidelines, including but not limited to those imposed by the City’s Bureau of Building, Fire Marshal, Department of Transportation, and Public Works Department. Compliance with other applicable requirements may require changes to the approved use and/or plans. These changes shall be processed in accordance with the procedures contained in Condition #4.

**4. Minor and Major Changes**

- a. Minor changes to the approved project, plans, Conditions, facilities, or use may be approved administratively by the Director of City Planning, such as a less than 10% change in the size of ground floor commercial, reduction in the amount parking that is not required by code, or reduction in the amount of off-street loading that is not required in the Planning Code.
- b. Major changes to the approved project, plans, Conditions, facilities, or use shall be reviewed by the Director of City Planning to determine whether such changes require submittal and approval of a revision to the Approval by the original approving body or a new independent permit/approval. Major revisions shall be reviewed in accordance with the procedures required for the original permit/approval. A new independent permit/approval shall be reviewed in accordance with the procedures required for the new permit/approval. Increase in the density or number of units, a reduction in the affordable residential units, inability to meet the design standards in the Design Guidelines for Lake Merritt BART, substantial changes to height, design, envelope, massing, or size of improvements, substantial changes to the public improvements including sidewalk, bicycle infrastructure, or plaza design, or changes that will result in any of the circumstances requiring further environmental review pursuant to CEQA Guidelines section 15162 or 15163 represent a non-exhaustive list of changes that would be considered Major Changes.

**5. Compliance with Conditions of Approval**

- a. The project applicant and property owner, including successors, (collectively referred to hereafter as the “project applicant” or “applicant”) shall be responsible for compliance with all the Conditions of Approval and any recommendations contained in any submitted and approved technical report at his/her sole cost and expense, subject to review and approval by the City of Oakland.
- b. The City of Oakland reserves the right at any time during construction to require certification by a licensed professional at the project applicant’s expense that the as-built project conforms to all applicable requirements, including but not limited to, approved maximum heights and minimum setbacks. Failure to construct the project in accordance with the Approval may result in remedial reconstruction, permit revocation, permit modification, stop work, permit suspension, or other corrective action.
- c. Violation of any term, Condition, or project description relating to the Approval is unlawful, prohibited, and a violation of the Oakland Municipal Code. The City of Oakland reserves the right to initiate civil and/or criminal enforcement and/or abatement proceedings, or after notice and public hearing, to revoke the Approval or alter these Conditions if it is found that there is violation of any of the Conditions or the provisions of the Planning Code or Municipal Code, or the project operates as or causes a public nuisance. This provision is not intended to, nor does it, limit in any manner whatsoever the ability of the City to take appropriate enforcement actions. The project applicant shall be responsible for paying fees in accordance with the City’s Master Fee Schedule for inspections conducted by the City or a City-designated third-party to investigate alleged violations of the Approval or Conditions.

**6. Signed Copy of the Approval/Conditions**

A copy of the Approval letter and Conditions shall be signed by the project applicant, attached to each set of permit plans submitted to the appropriate City agency for the project, and made available for review at the project job site at all times.

**7. Blight/Nuisances**

The project site shall be kept in a blight/nuisance-free condition. Any existing blight or nuisance shall be abated within sixty (60) days of approval, unless an earlier date is specified elsewhere.

**8. Indemnification**

- a. To the maximum extent permitted by law, the project applicant shall defend (with counsel acceptable to the City), indemnify, and hold harmless the City of Oakland, the Oakland City Council, the Oakland Redevelopment Successor Agency, the Oakland City Planning Commission, and their respective agents, officers, employees, and volunteers (hereafter collectively called "City") from any liability, damages, claim, judgment, loss (direct or indirect), action, causes of action, or proceeding (including legal costs, attorneys' fees, expert witness or consultant fees, City Attorney or staff time, expenses or costs) (collectively called "Action") against the City to attack, set aside, void or annul this Approval or implementation of this Approval. The City may elect, in its sole discretion, to participate in the defense of said Action and the project applicant shall reimburse the City for its reasonable legal costs and attorneys' fees.
- b. Within ten (10) calendar days of the filing of any Action as specified in subsection (a) above, the project applicant shall execute a Joint Defense Letter of Agreement with the City, acceptable to the Office of the City Attorney, which memorializes the above obligations. These obligations and the Joint Defense Letter of Agreement shall survive termination, extinguishment, or invalidation of the Approval. Failure to timely execute the Letter of Agreement does not relieve the project applicant of any of the obligations contained in this Condition or other requirements or Conditions of Approval that may be imposed by the City.

**9. Severability**

The Approval would not have been granted but for the applicability and validity of each and every one of the specified Conditions, and if one or more of such Conditions is found to be invalid by a court of competent jurisdiction this Approval would not have been granted without requiring other valid Conditions consistent with achieving the same purpose and intent of such Approval.

**10. Special Inspector/Inspections, Independent Technical Review, Project Coordination and Monitoring**

The project applicant may be required to cover the full costs of independent third-party technical review and City monitoring and inspection, including without limitation, special inspector(s)/inspection(s) during times of extensive or specialized plan-check review or construction, and inspections of potential violations of the Conditions of Approval. The project applicant shall establish a deposit with Engineering Services and/or the Bureau of Building, if directed by the Director of Public Works, Building Official, Director of City Planning, Director of

Transportation, or designee, prior to the issuance of a construction-related permit and on an ongoing as-needed basis.

**11. Public Improvements**

The project applicant shall obtain all necessary permits/approvals, such as encroachment permits, obstruction permits, curb/gutter/sidewalk permits, and public improvement (“p-job”) permits from the City for work in the public right-of-way, including but not limited to, streets, curbs, gutters, sidewalks, utilities, and fire hydrants. Prior to any work in the public right-of-way, the applicant shall submit plans for review and approval by the Bureau of Planning, the Bureau of Building, Engineering Services, Department of Transportation, and other City departments as required. Public improvements shall be designed and installed to the satisfaction of the City.

**12. Compliance Matrix**

The project applicant shall submit a Compliance Matrix, in both written and electronic form, for review and approval by the Bureau of Planning and the Bureau of Building that lists each Condition of Approval (including each mitigation measure if applicable) in a sortable spreadsheet. The Compliance Matrix shall contain, at a minimum, each required Condition of Approval, when compliance with the Condition is required, and the status of compliance with each Condition. For multi-phased projects, the Compliance Matrix shall indicate which Condition applies to each phase. The project applicant shall submit the initial Compliance Matrix prior to the issuance of the first construction-related permit and shall submit an updated matrix upon request by the City.

**13. Construction Management Plan**

Prior to the issuance of the first construction-related permit, the project applicant and his/her general contractor shall submit a Construction Management Plan (CMP) for review and approval by the Bureau of Planning, Bureau of Building, and other relevant City departments such as the Fire Department, Department of Transportation, and the Public Works Department as directed. The CMP shall contain measures to minimize potential construction impacts including measures to comply with all construction-related Conditions of Approval (and mitigation measures if applicable) such as dust control, construction emissions, hazardous materials, construction days/hours, construction traffic control, waste reduction and recycling, stormwater pollution prevention, noise control, complaint management, and cultural resource management (see applicable Conditions below). The CMP shall provide project-specific information including descriptive procedures, approval documentation, and drawings (such as a site logistics plan, fire safety plan, construction phasing plan, proposed truck routes, traffic control plan, complaint management plan, construction worker parking plan, and litter/debris clean-up plan) that specify how potential construction impacts will be minimized and how each construction-related requirement will be satisfied throughout construction of the project.

**14. Standard Conditions of Approval / Mitigation Monitoring and Reporting Program (SCAMMRP)**

- a. All mitigation measures identified in the Lake Merritt BART Station Redevelopment Project CEQA Checklist are included in the Standard Condition of Approval / Mitigation Monitoring



and Reporting Program (SCAMMRP) which is included in these Conditions of Approval and are incorporated herein by reference, as Attachment A and at the end of these Conditions of Approval, as Conditions of Approval of the project. The Standard Conditions of Approval identified in the Lake Merritt BART Station Redevelopment Project CEQA Checklist are also included in the SCAMMRP, and are, therefore, incorporated into these Conditions by reference but are not repeated in these Conditions. To the extent that there is any inconsistency between the SCAMMRP and these Conditions, the more restrictive Conditions shall govern. In the event a Standard Condition of Approval or mitigation measure recommended in the Lake Merritt BART Station Redevelopment Project CEQA Checklist has been inadvertently omitted from the SCAMMRP, that Standard Condition of Approval or mitigation measure is adopted and incorporated from the Lake Merritt BART Station Redevelopment Project CEQA Checklist into the SCAMMRP by reference, and adopted as a Condition of Approval. The project applicant and property owner shall be responsible for compliance with the requirements of any submitted and approved technical reports, all applicable mitigation measures adopted, and with all Conditions of Approval set forth herein at his/her sole cost and expense, unless otherwise expressly provided in a specific mitigation measure or Condition of Approval, and subject to the review and approval by the City of Oakland. The SCAMMRP identifies the timeframe and responsible party for implementation and monitoring for each Standard Condition of Approval and mitigation measure. Unless otherwise specified, monitoring of compliance with the Standard Conditions of Approval and mitigation measures will be the responsibility of the Bureau of Planning, with overall authority concerning compliance residing with the Environmental Review Officer. Adoption of the SCAMMRP will constitute fulfillment of the CEQA monitoring and/or reporting requirement set forth in section 21081.6 of CEQA.

- b. Prior to the issuance of the first construction-related permit, the project applicant shall pay the applicable mitigation and monitoring fee to the City in accordance with the City's Master Fee Schedule.

## Part 3: Standard Conditions of Approval – Other Standard Conditions

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### 15. **Employee Rights**

**Requirement:** The project applicant and business owners in the project shall comply with all state and federal laws regarding employees’ right to organize and bargain collectively with employers and shall comply with the City of Oakland Minimum Wage Ordinance (chapter 5.92 of the Oakland Municipal Code).

**When Required:** Ongoing

**Initial Approval:** N/A

**Monitoring/Inspection:** N/A

### 16. **Public Art for Private Development**

**Requirement:** The project is subject to the City’s Public Art Requirements for Private Development, adopted by Ordinance No. 13275 C.M.S. (“Ordinance”). The public art contribution requirements are equivalent to one-half percent (0.5%) for the “residential” building development costs, and one percent (1.0%) for the “non-residential” building development costs.

The contribution requirement can be met through: 1) the installation of freely accessible art at the site; 2) the installation of freely accessible art within one-quarter mile of the site; or 3) satisfaction of alternative compliance methods described in the Ordinance, including, but not limited to, payment of an in-lieu fee contribution. The applicant shall provide proof of full payment of the in-lieu contribution and/or provide plans, for review and approval by the Planning Director, showing the installation or improvements required by the Ordinance prior to issuance of a building permit.

Proof of installation of artwork, or other alternative requirement, is required prior to the City’s issuance of a final certificate of occupancy for each phase of a project unless a separate, legal binding instrument is executed ensuring compliance within a timely manner subject to City approval.

**When Required:** Payment of in-lieu fees and/or plans showing fulfillment of public art requirement – Prior to Issuance of Building permit

Installation of art/cultural space – Prior to Issuance of a Certificate of Occupancy.

**Initial Approval:** Bureau of Planning

**Monitoring/Inspection:** Bureau of Building

### 17. **Neighborhood Retail Survey**

**Requirement:** The project applicant shall conduct a survey of community members located within one-half mile of the project site to identify neighborhood needs and preferences for the

proposed commercial space. The City strongly encourages the project applicant to seek tenants for the proposed commercial space that meet the needs and preferences of local community members. Please refer to the City's Survey Guidelines for more information (contained in a separate document and available from the Oakland Planning Bureau).

When Required: Prior to commercial operations

Initial Approval: N/A

Monitoring/Inspection: N/A

## **18. Affordable Residential Rental Units - Agreement and Monitoring**

Requirement #1: In Compliance with Section 15.72.100 of the Oakland Code, the proposed project shall provide a minimum of 233 target dwelling units available at very low/ low/ moderate income: 9% (51 units) at 30% AMI; 13% (72 units) at 50% AMI; 13% (74 units) at 60% AMI; and 6% (36 units) at 120% AMI.

Requirement #2: The approved residential affordable units that are part of this approval shall remain and continue to be affordable at the specified level in accordance with California Health and Safety Code Section 50053 and its implementing regulations for a term of not less than 55 years or a longer period of time if required by the construction or mortgage finance assistance program, mortgage insurance program, or rental subsidy program.

Requirement #3: Prior to submittal of a construction-related permit, the applicant shall contact the Housing and Community Development Department (Housing Development Services Division) to enter into a Regulatory Agreement based on the City's model documents, as may be amended from time to time, governing the target dwelling units. The Agreement shall contain restrictive covenants to ensure the continued affordability of the target dwelling units at the specified rent levels for a period of not less than fifty-five (55) years and restrict the occupancy of those units only to residents who satisfy the affordability requirement as approved for this project. Only households meeting the eligibility standards for the target dwelling units shall be eligible to occupy the target dwelling units.

If the property has an approved condominium map and the developer chooses to rent the affordable units at initial occupancy, the units cannot convert to ownership during the term of the Agreement, even if the market rate units in the development convert to ownership.

The Regulatory Agreement shall be recorded with the Alameda County Recorder's Office as an encumbrance against the property, and a copy of the recorded agreement shall be provided to and retained by the City. The Regulatory Agreement may not be subordinated in priority to any other lien interest in the property.

Requirement #4: Rental target dwelling units shall be managed / operated by the developer or developer's agent or the developer's successor. The developer of rental target dwelling units shall submit for review and approval by the Housing and Community Development Department and any other relevant City departments, an annual report identifying which units are target dwelling units, the monthly rent, vacancy information, monthly income for tenants of each

target rental dwelling unit throughout the prior year, and other information required by the City. Said agreement shall maintain the tenants' privacy. The applicant shall pay to the Housing and Community Development Department an annual monitoring fee pursuant to the Master Fee Schedule (updated annually and available from the Budget Office of the City Oakland's Finance Department: <https://www.oaklandca.gov/departments/finance-department>) for City monitoring of target dwelling units.

Requirement #5: The floor area, number of bedrooms, and amenities (such as fixtures, appliances, location and utilities) of the affordable units shall be substantially equal in size and quality to those of the market rate units. Further, the proportion of unit types (i.e. three-bedroom and four-bedroom, etc.) of the affordable units shall be roughly the same as the project's market rate units.

Requirement #6: Tenant households in affordable units must have equal access to the project's services and facilities as tenant households in all other units within the project.

Requirement #7: Affordable units must be evenly distributed throughout the project.

Requirement #8: Affordable units shall be constructed before or concurrent with the construction of the market rate units in each phase of the project.

Requirement #9: The City will not issue final certificates of occupancy for more than fifty percent (50%) of the market rate units in any phase of development until final certificates of occupancy are issued for all of the affordable units in that phase.

When Required: First Construction-Related Permit Application and Ongoing

Initial Approval: Housing and Community Development Department – Housing Development Services Division

Ongoing Monitoring/Inspections: Housing Development Services Division

## 19. **Phasing**

Requirement: The project shall be implemented in the following phases:

- Phase 1: Horizontal FDP and Block 1 FDPs (Building A and Building B). May 2023. Within two (2) years after the approval the Preliminary Development Plan (PDP), the applicant shall file a complete application with the Planning Bureau for Final Development Plans (FDPs) for the horizontal improvements, Building A, and Building B to ensure the PDP does not expire. Within two years of approval of each Phase 1 FDP, a complete building permit application shall be submitted to the Building Bureau for Phase 1 development and shall be diligently pursued toward approval to ensure the FDP does not expire. In addition, within two years of approval of each Phase 1 FDP, a complete PX/PZ permit application for all horizontal improvements (including the paseo and BART plaza) associated with Phase 1 shall be submitted and diligently pursued toward approval, consistent with the FDP for Horizontal Improvements. The applicant shall bond for all horizontal improvements in Phase 1 at the time of the issuance of the Phase 1 PX/PZ permit in accordance with the City's established bonding

requirements. All horizontal improvements related to Phase 1 shall be completed prior to COO of the final building completed in Phase 1.

- Phase 2: Block 2 FDPs (Building C and Building D). May 2025. Within four (4) years of the approval of the PDP, the applicant shall file complete applications with the Planning Bureau for the FDPs for the parcels associated with Phase 2 (Building C and Building D) to ensure the PDP does not expire. Within two years of approval of each Phase 2 FDP, a complete building permit application shall be submitted for each Phase 2 development and shall be diligently pursued toward approval. In addition, within two years of approval of the Phase 2 FDP, a complete PX/PZ permit application for all horizontal improvements associated with Phase 2 shall be submitted and diligently pursued toward approval, consistent with the FDP for Horizontal Improvements. The applicant shall bond for all horizontal improvements in Phase 2 at the time of the issuance of the Phase 2 PX/PZ permit in accordance with the City's established bonding requirements. All horizontal improvements related to Phase 2 shall be completed prior to COO of the final building completed in Phase 2.

When Required: After approval of PDP/PUD

Initial Approval: Planning Bureau

Monitoring/Inspection: Planning Bureau and Building Bureau

## 20. **Submittal and Approval of FDP for Horizontal Improvements.**

Requirement: The project applicant shall apply for the Final Development Plan (FDP) for all horizontal improvements before any other FDPs shall be considered. The FDP for horizontal improvements shall be approved before, or in conjunction with, any other FDP approval. The horizontal improvements required in Phase I should be developed prior to any other improvements in any other Phase. The FDP for horizontal improvements shall include a phasing plan for implementation of all horizontal improvements, ensuring uninterrupted AC Transit service and provision of adequate emergency vehicle access, to the satisfaction of AC Transit, the City of Oakland, and BART. Horizontal improvements include, but are not limited to: publicly accessible paseo, public plazas, public walkways, sidewalk improvements, public lighting, bikeways, crosswalks, curb extensions, bus stops, intersection improvements, etc.

The FDP for Horizontal Improvements can be implemented in the two phases approved as part of the PDP. The FDP for Horizontal Improvements shall have the same expiration requirements as the PDP. The FDP will provide information about construction phasing within Phase 1 and Phase 2 and will be implemented through a series of PJob permits that will include streets, sidewalks, and the paseo. The proposed construction phasing shall be approved by OakDOT and the Building Bureau.

When Required: Prior to submittal or approval of any other FDP.

Initial Approval: Bureau of Planning, Oakland Department of Transportation

Monitoring/Inspection: N/A

## 21. **Submittal and Approval of PX/PZ permit for horizontal improvements.**

Requirement: The project applicant shall apply for the PX/PZ permit for all horizontal improvements and receive approval from all relevant departments and agencies before any

building permits will be approved. Horizontal improvements include, but are not limited to: the publicly accessible paseo, public plazas, public walkways, sidewalks improvements, bikeways, crosswalks, curb extensions, bus stops, intersection improvements, etc. If the applicant requests to implement the public improvements in phases, the PX/PZ permits shall be submitted consistent with the desired phases. In addition, the following phasing criteria shall be met:

- All improvements for Block 1 shall be operational and functional prior to the issuance of the Certificate of Occupancy for the final building constructed in Phase 1.
- All improvements for Block 2 shall be operational and functional prior to the issuance of the Certificate of Occupancy for the final building constructed in Phase 2.

When Required: Prior to approval of any building permit.

Initial Approval: Bureau of Building and OakDOT

Monitoring/Inspection: N/A

**22. 17.142.070 Performance bonds.**

Requirement: The City Planning Commission or, on appeal the City Council, may, as a condition of approval of any development for which a permit is required by Section 17.142.030, require a cash bond or surety bond for the completion of all or specified parts of the development deemed to be essential to the achievement of the purposes set forth in Section 17.142.010. The bond shall be in a form approved by the City Attorney, in a sum of one hundred percent (100%) of the estimated cost of the work, and conditioned upon the faithful performance of the work specified within the time specified.

**23. Project-Specific Public Improvements.**

Requirement: The project includes public improvements to the public right of way on all four sides of each block. The public improvements include:

- Dual-directional curb-ramps at the intersection corners adjacent to the Project and as mid-block ramps at the designated loading areas;
- High-visibility crosswalks on all the approaches of the intersections adjacent to the Project;
- Concrete bulb-outs at the intersection corners adjacent to the Project;
- Sidewalk improvements that generally provide a minimum pedestrian clear width of 8 feet along Block 1 frontages and 5.5 feet along Block 2 frontages;
- On-street passenger loading (including ADA-designated passenger loading) and associated sidewalk, curb improvements, and striping;
- ADA-designated on-street parking spaces;
- A two-way Class 4 separated bikeway, at the roadway level, on the south side of 9th Street between Oak and Fallon Streets;
- A one-way westbound Class 2B buffered bicycle lane on the north side of 8th Street between Fallon and Oak Streets

- A one-way southbound Class 4 separated bikeway, at the roadway level, on the west side of Fallon Street between 8th and 9th Streets; and
- Amenities such as street trees, short-term bicycle parking, and dockless scooter corrals along the Project frontage sidewalks that do not block the pedestrian through zones.

When Required: Prior to building permit final or as otherwise specified

Initial Approval: Bureau of Building; Department of Transportation

Monitoring/Inspection: Bureau of Building

## 24. **Transportation Improvements.**

**Requirement:** Consistent with SCA-TRANS-3: Transportation Improvements (#77), the project applicant shall implement the recommended on- and off-site transportation-related improvements contained within the Transportation Impact Review for the project (e.g., signal timing adjustments, restriping, signalization, traffic control devices, roadway reconfigurations, transportation demand management measures, and transit, pedestrian, and bicyclist amenities). The project applicant is responsible for funding and installing the improvements, and shall obtain all necessary permits and approvals from the City and/or other applicable regulatory agencies such as, but not limited to, Caltrans (for improvements related to Caltrans facilities) and the California Public Utilities Commission (for improvements related to railroad crossings), prior to installing the improvements. While not required to address a CEQA impact, the City of Oakland has determined that the following should be implemented as part of the final design for the project. These improvements shall be submitted as part of a FDP and/or a PJob application for review and approval by the Department of Transportation (DOT). The full non-CEQA Transportation Assessment can be found in Attachment B to these conditions. If approved they shall be implemented.

**Recommendation 1:** While not required to address a CEQA impact, the following shall be implemented by the Project at the discretion of City staff:

- Ensure adequate sight distance between exiting vehicles and pedestrians on the adjacent sidewalk at the Building A driveway on 9th Street and the Building C and Building D driveways on 7th Street. If adequate sight distance cannot be achieved, provide audio and visual warning devices at the driveway and/or provide three-foot low landscaping buffer along the building edge adjacent to the driveways.
- Consider extending the Buildings C and D curb-cut to the west to widen the curb-cut to 27-feet to allow incoming and outgoing vehicles to utilize the driveways simultaneously.
- Study the turning movements for larger trucks (such as WB-40) maneuvering into and out of the Building A loading docks on 9th Street and the Building C loading docks on 7th Street to ensure adequate truck access.
- Consider redesigning the Building C garage to provide adequate circulation for vehicles and to allow two vehicles to simultaneously enter and exit the internal garage ramps. If the Building C garage cannot be redesigned, install mirrors at the bottom and top of each internal ramp to improve visibility.

**Recommendation 2:** While not required to address a CEQA impact, the following shall be implemented by the Project at the discretion of City staff:

- 9th Street/Oak Street - Replace existing signals with new mast arms and signal heads to provide signal head for the westbound 9th Street bike approach.

**Recommendation 3:** While not required to address a CEQA impact, the following shall be implemented by the Project at the discretion of City staff:

- Ensure that at least two of the short-term bicycle parking spaces near the Building D day-care are cargo-bike accessible to facilitate day-care pick-ups and drop offs.
- Ensure the bike parking in the sidewalks, fronting all streets in the Project vicinity do not conflict with the minimum pedestrian clear width areas or do not conflict with the minimum of 48-inch clear distance at the curb to ensure access from the accessible passenger loading zones or parking spaces to the sidewalk.

**Recommendation 4:** While not required to address a CEQA impact, the following improvements shall be implemented by the Project at the discretion of City staff for at least the intersection corners along the Project frontages and the receiving corners, and preferably for the entire intersection, unless noted otherwise:

- 9th Street/Oak Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps, pedestrian countdown signal heads, high-visibility crosswalk markings, leading pedestrian intervals, and advance stop bars.
- 9th Street/Fallon Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps, high-visibility crosswalk markings, and advance stop bars on the west and south approaches of the intersection. In addition, consider installing a raised intersection or a raised crosswalk on the south side of the intersection.
- 8th Street/Madison Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps and leading pedestrian intervals.
- 8th Street/Oak Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps and leading pedestrian intervals.
- 8th Street/Fallon Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps, high-visibility crosswalk markings, and advance stop bars. Eliminate one of the two left-turn lanes on the northbound Fallon Street approach.
- 7th Street/Madison Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps, and leading pedestrian intervals.
- 9th Street/Oak Street intersection - Install concrete bulb-outs with ADA-accessible directional curb ramps, accessible pedestrian signals, pedestrian countdown signal heads, high-visibility crosswalk markings, leading pedestrian intervals, and advance stop bars.

**Recommendation 5:** While not required to address a CEQA impact, the following shall be implemented by the Project at the discretion of City staff:

- If feasible, subject to additional approval by BART, replace the existing concrete canopy with ADA-accessible bus shelters near the curb at each bus stop on the east side of Oak Street between 8th and 9th Streets.



**Recommendation 6:** While not required to address a CEQA impact, the following shall be implemented by the Project at the discretion of City staff:

- Consider eliminating one of the loading berths in Building A and relocating the remaining loading berth to have access through the main garage driveway to reduce the number of curb-cuts on 9th Street.

When Required: Varies depending on the specific recommendation. Prior to application for; issuance of; Building Permits; final inspections; issuance of Certificate of Occupancy; and Ongoing

Initial Approval: Bureau of Planning; Bureau of Building; Oakland Department of Transportation

Monitoring/Inspection: OakDOT

**25. Transportation Demand Management Measures**

Requirement: The applicant shall implement each mandatory Transportation Demand Management (TDM) Plan measure that is Memo that is required in the SCAMMRP (see Attachment A) and the Transportation and Parking Demand Management Memo (see Attachment C). The project sponsor shall submit an annual compliance report for review and approval by the City. This report will be submitted within one year of occupancy and every following year for a total of at least five years. If timely reports are not submitted, the reports indicate a failure to achieve the stated policy goals, or the required alternative mode split is still not achieved, staff will work with the project sponsor to find ways to meet their commitments and achieve Vehicle Trip Reduction (VTR) goals. If the issues cannot be resolved, the matter may be referred to the Planning Commission for resolution. Project sponsors shall be required, as a condition of approval to reimburse the City for costs incurred in maintaining and enforcing the VTR program for the approved project.

When Required: Prior to application for; issuance of; Building Permits; final inspections; issuance of Certificate of Occupancy; and Ongoing

Initial Approval: Bureau of Planning; Bureau of Building; Oakland Department of Transportation

Monitoring/Inspection: Bureau of Building

**26. Transit Passes.**

Requirement: The property owner or homeowners association shall make permanently available a monthly transit benefit to each dwelling unit in an amount equal to either one-half the price of an Adult 31-Day AC Transit Pass or an AC Transit EasyPass. This benefit shall be placed on a Regional Transit Connection Clipper Card. A notice describing this transit benefit shall be permanently posted in a common area of the building such as a lobby or mailroom that is clearly visible to residents.

When Required: The notice is required prior to Certificate of Occupancy, transit passes are ongoing

Initial Approval: OakDOT

Monitoring/Inspection: OakDOT

27. **High-Quality Design of Ground-Floor Public Paseo, Plazas, and Walkways.**

Requirement: In order to ensure a safe and lively pedestrian realm around the BART station and the proposed project, the ground floor public plazas and walkways shall be high-quality, well designed spaces, as determined by City Staff, that include excellent pedestrian-scaled lighting, extensive furnishings, and interactive art or other amenities for children.

When Required: FDP for horizontal improvements and PX/PZ Permit.

Initial Approval: Bureau of Planning

Monitoring/Inspection: Bureau of Building

28. **Building A and Building C Tower Design.**

Requirements: In order to meet the requirements for waiving the tower dimensional standards for maximum diagonal length and maximum tower elevation length, ensure the proposals for both towers will result in a signature building within the neighborhood, City, or region based on qualities, including but not limited to, exterior visual quality, craftsmanship, detailing, and high quality and durable materials.

When Required: FDP for Building A and FDP for Building C

Initial Approval: Bureau of Planning

Monitoring and Inspection: Bureau of Building

29. **Car Share Parking Spaces Requirements (17.116.105.A.)**

Requirement: The Planning Code requires the following care share parking spots:

- Building A: 2 spaces
- Building B: 1 space
- Building C: none
- Building D: 1 space

The project shall provide a parking space for car-share purposes, either for private car-share or public car-share. A private car-share, operated by the property owner or homeowners association, provided within the development will have a private car-share space assigned to a vehicle owned and maintained by the property owner or homeowners association for the use of residents within the development. For public car-share, a parking space shall be provided at no cost to a public car-share organization for purposes of providing car-share services for its car-share service subscribers. A notice describing the requirement for car-sharing spaces shall be permanently posted in a common area of the building such as a lobby or mailroom that is clearly visible to residents.

The car-share spaces may be provided on the building site or on another site within 600 feet of the building site. All car-share vehicles shall be accessible to both non-resident and resident subscribers.

The required car share spaces will be provided as follows:

- Building A: 3 total spaces (2 spaces for A, plus 1 additional space for Building B)
- Building C: 1 car share space provided for Building D.

The owner or owners of both lots shall prepare and execute to the satisfaction of the City Attorney, and file with the Alameda County Recorder, an agreement guaranteeing that such facilities will be maintained and reserved for the activity served, for the duration of said activity.

When Required: Certificate of Occupancy

Initial Approval: OakDOT

Monitoring/Inspection: OakDOT

**30. Compliance with the Lake Merritt Station Area Design Guidelines and Project Specific Design Guidelines “Design Guidelines for Lake Merritt BART”**

Requirement: The Final Development Plans for the Project need to be consistent with *both* the Lake Merritt Station Area Design Guidelines (LMSADG) *and* the project specific Design Guidelines for Lake Merritt BART. The intent of the project specific guidelines is to be additive and provide more specificity to the LMSADG, but not replace or contradict the LMSADG, which were developed through a community-based planning process. The project specific Design Guidelines include design standards and design guidelines. If the project cannot meet the design standards, a Major Revision is required.

When Required: Final Development Plans

Initial Approval: Planning Bureau

Monitoring/Inspection: Building Bureau

**31. Compliance with all relevant Conditions of Approval.**

Requirements: In addition to the conditions above, project applicant shall comply with the conditions in the attached exhibits, including:

- Exhibit A: City of Oakland Department of Transportation, Engineering Services Conditions of Approval
- Exhibit B: City of Oakland Department of Transportation, Office of the City Surveyor Conditions of Approval
- Exhibit C: City of Oakland Fire Department, Conditions of Approval

When Required: As specified in the specific conditions of approval

Initial Approval: As specified in the specific conditions of approval

Monitoring/Inspection: As specified in the specific conditions of approval

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**Applicant Statement**

I have read and accept responsibility for the Conditions of Approval. I agree to abide by and conform to the Conditions of Approval, as well as to all provisions of the Oakland Planning Code and Oakland Municipal Code pertaining to the project.

LMTOD PROPERTY, LLC (DBA STRADA INVESTMENT GROUP)

\_\_\_\_\_  
Name of Project Applicant

  
\_\_\_\_\_  
William Goodman (Jun 4, 2021 16:34 PDT)

\_\_\_\_\_  
Signature of Project Applicant

Jun 4, 2021

\_\_\_\_\_  
Date

# Lake Merritt COA

Final Audit Report

2021-06-04

Created:	2021-06-04
By:	Bryan Fat (bfat@stradasf.com)
Status:	Signed



## "Lake Merritt COA" History



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