

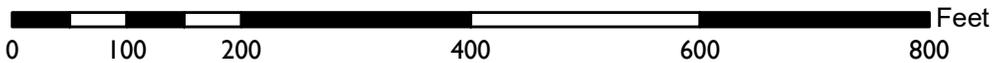
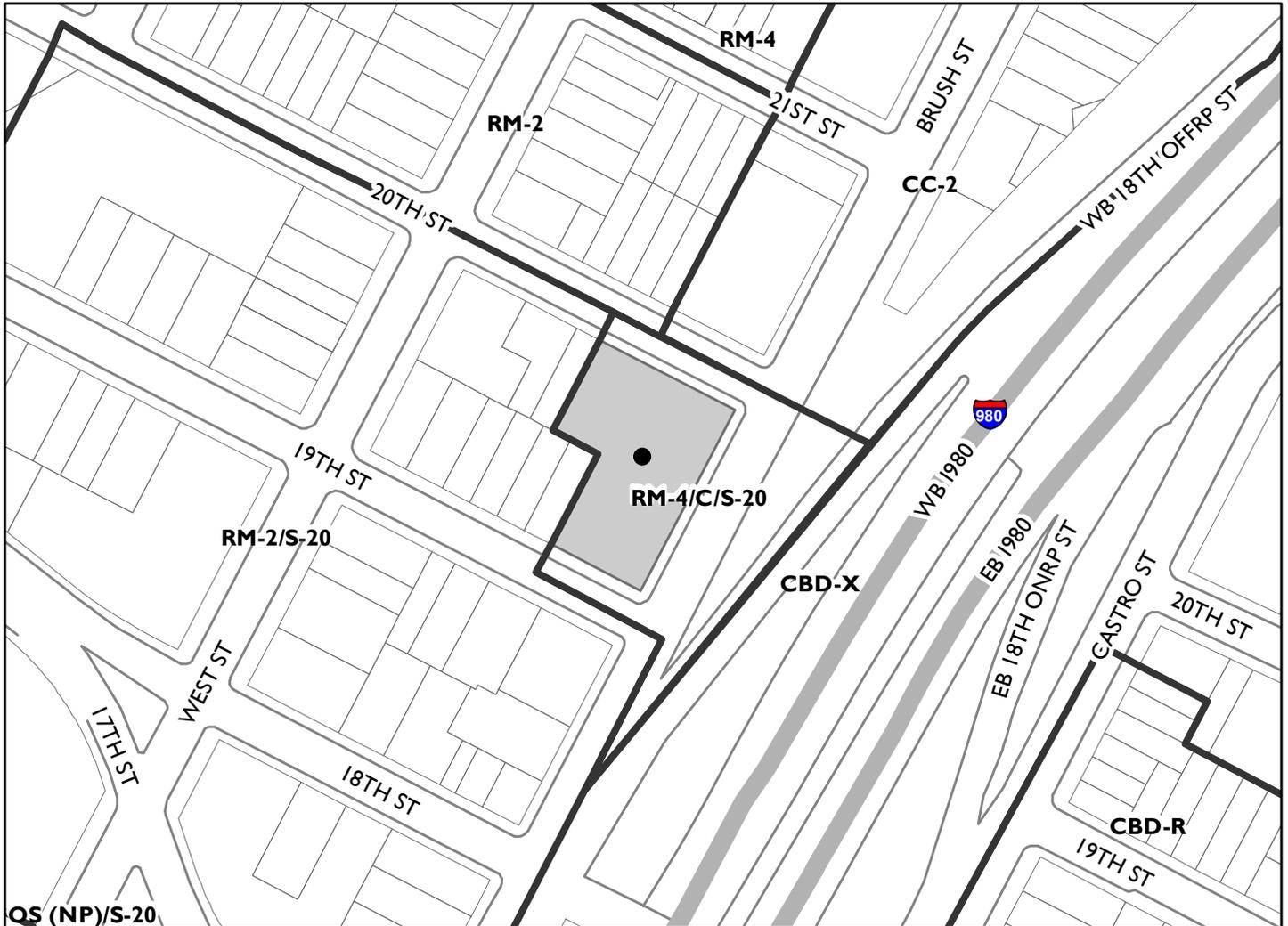
Location:	1925 Brush Street (See map on reverse)
Proposal:	Demolish an existing industrial building and construct a three-story building with 18 townhomes. The project includes nine parking spaces and a car share parking space.
Applicant:	Alex Walter, Riaz Development
Owner:	Snowdrop Patrick LLC
Planning Permits Required:	Major Conditional Use Permit for the construction of more than six dwelling units in the RM-4 Zone; Variance for providing usable open further than 20 feet from the units served; and Regular Design Review approval for new construction and creating new dwelling units.
General Plan:	Mixed Housing Type Residential
Zoning:	Mixed Housing Type Residential (RM) – 4/C Residential Commercial Combining Zone/S-20 Historic Preservation District Combining Zone.
Historic Status:	Non-contributor to Local Register District; Office of Cultural Heritage Rating Ec3
City Council District:	3 - Fife
Status:	Project under review
Action to be Taken:	Recommendation to the Planning Commission
Staff Recommendation:	Provide input based on staff report
Finality of Decision:	Final decision to be made by the full Planning Commission.
For Further Information:	Contact case planner Neil Gray at 510-238-3878 or ngray@oaklandca.gov

SUMMARY

Staff requests input from the Design Review Committee regarding a proposal to construct 18 townhomes on a 5,247 square-foot, L-shaped lot fronting on Brush Street between 19th and 20th Streets. Staff believes that the façade and site plan designs would meet the Design Review findings required for approval. The building would successfully relate to the design contexts on Brush, 19th, and 20th Streets, open space is integrated as a crucial element of the site plan, and on-site parking is hidden behind the buildings. The development preserves on-street parking by only proposing one curb cut and driveway.

Neighbors have expressed concerns that providing the proposed nine spaces would exacerbate an on-street parking shortage in the neighborhood resulting from commuters walking from their cars to downtown employment.

CITY OF OAKLAND PLANNING COMMISSION



Case File:
Applicant: PLN20149
Address: Alex Walter, Riaz Development
Zone: 1925 Brush Street
Height Area: RM-4/C/S-20

BACKGROUND

The developer held a virtual community meeting regarding the project on February 4, 2021. The majority of the comments expressed concern that the project does not have sufficient parking spaces. According to the neighbors, a shortage of available spaces exists due to commuters who walk into downtown after parking their car in their neighborhood. The neighbors stated that on-street parking is particularly difficult on Thursdays and Fridays due to street sweeping and they are concerned that a 95-unit, 100 percent affordable housing development at 2116 Brush Street has been approved with 26 parking spaces. This issue is further discussed in the “Key Issues and Impacts” section of this report. One neighbor also stated that he preferred the car entrance to be from Brush Street instead of the proposed driveway on 20th Street.

PROPERTY DESCRIPTION AND NEIGHBORHOOD CONTEXT

The project is on a 25,247 square-foot, L-shaped lot fronting on Brush Street between 19th and 20th Streets. The 20th Street side of the lot is 150 feet deep, and the 19th Street side of the lot is 100 feet deep. A relatively nondescript industrial building with a rating of “Ec” from the Office of Cultural Heritage Survey (OCHS) exists on the north side of the site and a parking lot is on the south side of the lot. Both of these facilities are proposed for demolition. An off-ramp from I-580 is across the street from the site.

There are distinct contexts in the surrounding neighborhood. North of the site, Brush Street is a mix of industrial buildings, outdoor storage, and a Victorian style home. South of the site, Brush Street contains two-story Victorian style homes that have been divided into multiple units and multi-family buildings constructed in the 1950’s and 1960’s.

The north side of 19th Street contains a row of six similar one- and two-story Queen Ann style homes built circa 1890. These homes have gable ends over one or two stories of bay windows on their right side and stairs that lead to a porch entrance on the left side. The four two-story homes are adjacent to the site. These contain a porch and a vertical bay feature fronting a box shaped structure with a hipped roof. These ornately decorated homes are significant contributors to the Curtis and Williams Tract Area of Secondary Importance (ASI) historic district.

The south side of 20th Street contains a less intact group of historic homes. The home adjacent to the site contains a Queen Ann Style home that has been significantly altered and an intact Queen Ann home is two lots from the site.

PROJECT DESCRIPTION

The following describes the proposed development. Architectural Plans are contained in Attachment A.

Site Plan

The project’s 18 townhome units are contained in three separate buildings: Building A, which fronts onto Brush Street and Buildings B and C, which front onto 19th and 20th Streets, respectively. Building A is setback from five to eleven feet from the edge of the sidewalk to provide space for landscaping while relating to the limited setbacks on Brush Street. Buildings B and C are setback 15 feet from the edge of sidewalk to match the setbacks of the homes on 19th and 20th Streets.

Ground floor units have direct access to internal open space between the three buildings, which varies in width from 15 to 22 feet. This open space spans the width of the project behind Building A, providing a contextual break between the industrial style of Building A and the more traditional forms of Buildings B and C and the Queen Ann Cottages on 19th and 20th Streets.

15 of the 18 townhomes have an entrance with direct access from the street. The remaining three are accessed from the internal open space. Eight internal parking spaces are generally obscured from public view behind Buildings B and C and are accessed from a 15-foot-wide driveway adjacent to 20th Street. This is the only curb cut and driveway proposed to serve the project.

Building Design

The designs of the buildings reflect their adjacent neighborhood contexts. With a shed roof facing Brush Street, a sawtooth roof facing 19th and 20th Streets, and blocky volumes, Building A has an industrial style that relates to buildings on Brush Street north of the site. The exterior of the building is cement plaster of various colors and textures that define different planes and volumes of the building. Horizontal hardie board siding provides additional definition to volumes on the facade. Each townhouse facing the street has an at-grade entrance defined by a porch recessed into the bottom floor.

Buildings B and C both have a contemporary style with forms that also relate to the historic architectural contexts on 19th and 20th Streets. The 19th Street elevation of Building B has gable end roofs, bay window forms, a vertical orientation, prominent entrances, and a height that would relate to the Queen Ann style buildings on 19th Street. Building C also relates to the architectural features on 20th Street with gable end roofs, bay windows and a prominent entrance. Two of the four gable ends of this elevation are asymmetrical and the forms are stouter than those on 19th Street, which somewhat reference the industrial styles on Brush Street. Staff feels this is appropriate because the north side of Brush Street has a commercial and industrial context, the 15-foot-wide driveway serves as a visual break to the historic context on 20th Street, and 20th Street has a spottier historic context than 19th Street.

Floor Plans

Each unit is between 1,520 and 1,670 square feet and contains four bedrooms and five bathrooms, and each bedroom has its own bathroom and counter. The layout of the units is conducive to independent living in the bedrooms within a larger household.

Landscaping Plan and Street Improvements

The conceptual landscape plan includes seven additional street trees and significant planting in the front yards and group open space areas. The types of plantings have yet to be determined. The developer will also be required to replace the adjacent sidewalk, curb, and gutters.

ZONING ANALYSIS

The project is within the Mixed Housing Type Residential (RM) - 4 base zone, a C commercial combining zone, and the S-20 Historic Preservation District Combining Zone. The C commercial combining zone does not apply to the proposal because no commercial activities or facilities are proposed for the site. Therefore, this section discusses the requirements of the RM-4 Zone and the S-20 Combining Zone.

RM-4 Zone

Intent. According to Section 17.17.010(B)(4) of the Planning Code, the intent of the RM-4 Zone is to “create, maintain, and enhance residential areas typically located on or near the City's major arterials and characterized by a mix of single-family homes, townhouses, small multi-unit buildings at somewhat higher densities than RM-3, and neighborhood businesses where appropriate.” The proposed small-scale townhome project is consistent with this intent.

Regulations. The following table summarizes the relevant RM-4 development regulations for the site.

Regulation	Requirement	Proposed	Compliance	Notes
Maximum density	22 units	18 units; CUP required	Complies	CUP required for more than 4 units in the RM-4 Zone. The CUP is major and requires a decision by the Planning Commission per Section 17.134.020 of the Planning Code.
Minimum usable open space	3,150 sf	3,500	Complies in amount but not location. See note.	The requirement is 175 sf per unit. Variance is required because some of the units are greater than 20 feet from the open space. Per 17.126.030(B), The open space must be within 20 feet of the living units served. This issue is addressed in the “Key Issues and Impacts” section, below.
Height limit	35 feet	35 feet	Complies	
Front yard setback (19 th Street)	15 feet	15 feet	Complies	
Rear yard setback (20 th Street)	15 feet	15 feet	Complies	
Interior side setback	4 feet	4 feet	Complies	
Street side setback	4 feet	5 feet	Complies	
Minimum number of auto parking spaces	9	9	Complies	The base requirement is one per unit. This is reduced to 0.7 per regular unit due to transit proximity and further reduced to .5 per unit with the provision of on-site car share parking spaces. See Section 17.116.110(C).
Minimum bike spaces	• 4 long term • 0 short term	• 5 long term • 1 short term	Complies	1 per 4 units required for long term and 1 per 20 units required for short term.
Minimum loading berths	0 berths	0 berths	Complies	1 loading berth is required if a development is over 50,000 sf of floor area. Total proposed floor area is 35,835 sf.

S-20 Historic Preservation District Combining Zone

According to Section 17.100B.010 of the Planning Code, the S-20 Zone “is intended to preserve and enhance the cultural, educational, aesthetic, environmental, and economic value of structures, other physical facilities, sites, and areas of special importance due to historical association, basic architectural merit, the embodiment of a style or special type of construction, or other special character, interest, or value, and is typically appropriate to selected older locations in the city. The S-20 Zone is similar to the S-7 Preservation Combining Zone, but is designed for larger areas, often with a large number of residential properties that may not be individually eligible for landmark designation but which as a whole constitute a historic district. The S-20 Zone provides generally more expeditious review procedures than those provided in the S-7 Zone.”

The S-20 Combining Zone implements this intent by being mapped in all areas of West Oakland that are within an historic residential district and requiring the following Design Review Criteria for new construction:

1. That the proposal will not substantially impair the visual, architectural, or historic value of the affected site or facility. Consideration shall be given to design, form, scale, color, materials, texture, lighting, detailing and ornamentation, landscaping, signs, and any other relevant design element or effect, and, where applicable, the relation of the above to the original design of the affected facility.
2. That the proposed development will not substantially impair the visual, architectural, or historic value of the total setting or character of the S-20 Historic Preservation Zone or of neighboring facilities. Consideration shall be given to the desired overall character of any such area or grouping of facilities, including all design elements or effects specified in Subsection (A) above; and
3. That the proposal conforms with the Design Guidelines for Landmarks and Preservation Districts as adopted by the City Planning Commission and, as applicable for certain federally-related projects, with the Secretary of the Interior's Standards for the Treatment of Historic Properties.

Compliance with these findings is discussed in the “Key Issues and Impacts” Section of this report.

Required Findings

Each of the findings required for approval of the project are listed in Attachment B of this report.

GENERAL PLAN ANALYSIS

The property is in the Mixed Housing Type Residential designation of the Land Use and Transportation Element of the General Plan (LUTE). The intent of the Mixed Housing Type land use classification is: “to create, maintain, and enhance residential areas typically located near the City’s major arterials and characterized by a mix of single-family homes, townhouses, small multi-unit buildings, and neighborhood businesses where appropriate.” Desired Character and Uses is: “Future development within this classification should be primarily residential in character.” The proposed design for a small, multi-family facility and is, therefore, consistent with the intent and desired character and uses of the General Plan as well as the following policies:

Policy N3.2 Encouraging Infill Development.

In order to facilitate the construction of needed housing units, infill development that is consistent with the General Plan should take place throughout the City of Oakland.

Policy N3.8 Required High-Quality Design.

High-quality design standards should be required of all new residential construction. Design requirements and permitting procedures should be developed and implemented in a manner that is sensitive to the added costs of those requirements and procedures.

Policy N6.1 Mixing Housing Types.

The City will generally be supportive of a mix of projects that provide a variety of housing types, unit sizes, and lot sizes which are available to households with a range of incomes.

Policy N7.1 Ensuring Compatible Development.

New residential development in Mixed Housing Type areas should be compatible with the density, scale, design, and existing or desired character of surrounding development.

The proposal is a residential infill development project that enhances the mix of housing types in a residential area.

In addition, this project is located within opportunity area 4A in the West Oakland Specific Plan area. The Plan designates this area as requiring a rezoning from Community Commercial – 2 to RM – 4 with a commercial overlay. This rezoning was completed during the West Oakland rezoning process.

KEY ISSUES AND IMPACTS

Staff requests input from the DRC regarding the overall design of the project and the amount of parking provided.

Project Design

Staff believes that the overall design of the project meets the required findings for the S-20 Zone and other required findings in the Planning Code. The Brush Street façade has an industrial style that relates to the industrial context north of the site and has a pleasing composition through the placement of windows, bay projections, and material changes. As discussed in the “Project Description” section, above, the 19th and 20th Street facades have prominent entrances, a height, and roof forms that relate and provide a successful transition to the traditional residential contexts on those streets. The separation of buildings also provides a break between the industrial context on Brush Street and the traditional contexts on 19th and 20th Streets. Implementation of these techniques to transition to the solid row of Queen Ann Bungalows on 19th Street is particularly successful.

Staff also believes the project has a convenient and functional site plan that contributes to the overall visual quality of the project. Most of the units have direct access to the group open space, which has been integrated between buildings as a crucial element of the plan. The remaining units can easily access the space through internal pathways. The open space spans the block, creating a visual break between the industrial context established by Building A and residential contexts on 19th and 20th Streets. The plan preserves on-street parking by only requiring one driveway and curb cut for the entire development. Placing this curb cut on a side street (20th Street) will allow for an uninterrupted façade on Brush Street, which will be the most visible side of the development.

Parking

Attendees to a neighborhood meeting were very concerned about the proposal only providing nine parking spaces for the 18 units. As described in the Zoning Analysis Section of this report, this reduction from one parking space per unit is permitted per 17.116.110(C) of the Planning Code, which states that parking is reduced to 0.7 per unit if a project is within one-half a mile from a major transit stop (the project is approximately 0.4 miles from the 19th Street BART Station) and parking can be further reduced to 0.5 parking spaces per unit if a project provides an on-site car share space.

The neighbors have stated that there is a parking shortage in the neighborhood, particularly on the two street sweeping days per week, due to commuters parking in their neighborhood and walking to Downtown offices. According to the participants, this issue was of particular concern before COVID-19 restrictions changed the commuting patterns of Downtown workers. The community also expressed concerns that the problem will be exacerbated due to a recently approved 100 percent affordable housing project at 2116 Brush Street, which will have 95 units and 26 parking spaces, and because each unit has four bedrooms and is designed for roommates who live independently. Due to this, neighbors have expressed concern that the project cannot meet findings relating to a project’s impact on the surrounding neighborhood (see Attachment B for a list of the required findings).

On the other hand, the proposed parking meets the City’s parking requirements and increasing the number of parking spaces would require removing dwelling units and/or reducing the amount of open space to less than required. More parking may reduce on-street parking by requiring an additional curb cut. Further, a reduction in parking is consistent with the City’s Equitable Climate Action Plan, adopted July of 2020. That document states that decreasing parking related to a development is an important part of reducing car trips and, thus, vehicle emissions. Two policies in this document are particularly germane to parking requirements in new developments:

1. *TLU-1 Align All Planning Policies & Regulations with ECAP Goals and Priorities* states: In the course of scheduled revisions, amend or update the General Plan, Specific Plans, Zoning Ordinance, Subdivision Regulations, Parks Master Plan, and appropriate planning policies or regulations to be consistent with the GHG reduction, adaptation, resilience, and equity goals in this ECAP. Specifically, appropriate planning policies should study the following strategies and should incorporate such policies that are found not to have adverse environmental or equity impacts:
 - Remove parking minimums and establish parking maximums where feasible, ensuring public safety and accessibility.
 - Remove barriers to and incentivize development of affordable housing near transit.
 - Provide density bonuses and other incentives for developments near transit that provide less than half of the maximum allowable parking.

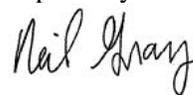
Note that these are three relevant items in a larger list of actions.

2. *TLU-10 Expand the Neighborhood Car Sharing Program* states: Expand the Neighborhood Car Sharing program, ensuring that all shared vehicles are electric vehicles by 2030 and that shared vehicle services address the needs of families, people with disabilities, and frontline communities. Coordinate program expansion with New Mobility programs, EV infrastructure planning, and with revised parking policies. Where feasible, work with partners including developers and property managers to provide dedicated EV car sharing services in multifamily affordable housing buildings to increase access and reduce the car cost burden to lower-income families.

RECOMMENDATION

Staff recommends that the DRC provide staff and the full Planning Commission input regarding the proposal, particularly in terms of the required findings contained in Attachment B.

Prepared by:



NEIL GRAY
Planner IV

Reviewed by:

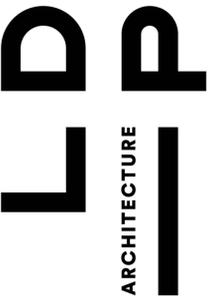


Robert Merkamp, Zoning Manager
Bureau of Planning

- ATTACHMENTS:**
- A. Project Plans
 - B. Required Findings

1925 BRUSH STREET

OAKLAND, CA



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1925 BRUSH STREET
OAKLAND, CA



1925 BRUSH STREET
OAKLAND, CA
PROJECT NO.

REV	DATE	DESCRIPTION
07-31-2020		PLANNING SUBMISSION
12-18-2020		PLANNING RE-SUBMISSION

CONTACT: TOBY LEVY

(415) 777-0561 P
(415) 777-5117 F

SCALE: AS NOTED

COVER SHEET

A0.0

ABBREVIATIONS	LEGEND	GENERAL NOTES	ELEVATION	CONTACT LIST																																																																																																																																																																																																																																																																																																																																																																																										
<p>A/C AIR CONDITIONING</p> <p>ADJ. ADJUSTABLE</p> <p>A.F.F. ABOVE FINISH FLOOR</p> <p>ALUM. ALUMINUM</p> <p>ALT. ALTERNATE</p> <p>ARCH. ARCHITECT(URAL)</p> <p>BLDG. BUILDING</p> <p>BLKG. BLOCKING</p> <p>BOT. BOTTOM</p> <p>C./CL. CENTER LINE</p> <p>CLB. CABINET</p> <p>CLG. CEILING</p> <p>CLR. CLEAR</p> <p>C.M.U. CONCRETE MASONRY UNIT</p> <p>COL. COLUMN</p> <p>CONC. CONCRETE</p> <p>CONSTR. CONSTRUCTION</p> <p>CTR. CENTER</p> <p>DET. DETAIL</p> <p>DIAM. DIAMETER</p> <p>DIM. DIMENSION</p> <p>DOWN DOWN</p> <p>DWG. DRAWING</p> <p>DS. DOWN SPOUT</p> <p>(E) EXISTING</p> <p>EA. EACH</p> <p>ELEC. ELECTRICAL</p> <p>EQ. EQUAL</p> <p>EQUIP. EQUIPMENT</p> <p>EXT. EXTERIOR</p> <p>F.D. FLOOR DRAIN</p> <p>FIN. FINISH</p> <p>FL. FLOOR</p> <p>F.O.F. FACE OF FINISH</p> <p>F.O.S. FACE OF STUD</p> <p>GAGE GAGE</p> <p>GALV. GALVANIZED</p> <p>GEN. GENERAL CONTRACTOR</p> <p>GL. GLASS</p> <p>GR. GRADE</p> <p>GYP. BO. GYPSUM BOARD</p> <p>H.B. HOSE BIB</p> <p>HWR. HARDWARE</p> <p>HGT. HEIGHT</p> <p>H.W. HOT WATER</p> <p>INSUL. INSULATION/INSULATED</p> <p>INT. INTERIOR</p> <p>JT. JOINT</p> <p>MAX. MAXIMUM</p> <p>M.D. MOTION DETECTOR</p> <p>MECH. MECHANICAL</p> <p>MIN. MINIMUM</p> <p>MTD. MOUNTED</p> <p>MTL. METAL</p> <p>(N) NEW</p> <p>N.I.C. NOT IN CONTRACT</p> <p>NO. NUMBER</p> <p>N.T.S. NOT TO SCALE</p> <p>O.C. ON CENTER</p> <p>OPNG. OPENING</p> <p>OPP. OPPOSITE</p> <p>PR. PAIR</p> <p>PTD. PAINTED</p> <p>PLYWD. PLYWOOD</p> <p>R.D. ROOF DRAIN</p> <p>REQ. REQUIRED</p> <p>RM. ROOM</p> <p>R.O. ROUGH OPENING</p> <p>S.C. SOLID CORE</p> <p>STOR. STORAGE</p> <p>SHT. SHEET</p> <p>SIM. SIMILAR</p> <p>STR. STRUCTURAL</p> <p>T&G TONGUE AND GROOVE</p> <p>TEL. TELEPHONE</p> <p>T.O. TOP OF</p> <p>TYP. TYPICAL</p> <p>U.O.N. UNLESS OTHERWISE NOTED</p> <p>V.I.F. VERIFY IN FIELD</p> <p>WD. WOOD</p> <p>W.P. WATERPROOF</p>	<p># DETAIL REFERENCE NUMBER</p> <p>X.XX SHEET WHERE DETAIL IS LOCATED</p> <p>DIRECTION OF SECTION VIEW</p> <p>INTERIOR SECTION IDENTIFICATION/ SHEET WHERE SECTION IS LOCATED</p> <p>#/X.XX</p> <p>SHEET WHERE ELEVATION IS LOCATED</p> <p>#/X.XX</p> <p>ELEVATION REFERENCE NUMBER</p> <p>(D) DOOR SYMBOL</p> <p>(W-01) WINDOW SYMBOL</p> <p>X.XX WALL / FLOOR TYPE SYMBOL</p> <p>ELEVATION DATUM</p> <p>X'-X" CEILING HEIGHT</p> <p>REVISION SYMBOL</p> <p>WALL-MOUNTED INCANDESCENT SCNCE LIGHT FIXTURE</p> <p>WALL-MOUNTED COMPACT FLUORESCENT SCNCE LIGHT FIXTURE</p> <p>RECESSED INCANDESCENT LIGHT FIXTURE AT CEILING</p> <p>RECESSED COMPACT FLUORESCENT LIGHT FIXTURE AT CEILING</p> <p>SURFACE-MOUNTED COMPACT FLUORESCENT LIGHT FIXTURE AT CEILING</p> <p>COMBINATION EXHAUST FAN AND COMPACT FLUORESCENT LIGHT FIXTURE, RECESSED AT CEILING</p> <p>SURFACE-MOUNTED FLUORESCENT STRIP LIGHT FIXTURE, CEILING-MOUNTED (WITH LENGTH AS INDICATED)</p> <p>SURFACE-MOUNTED TRACK LIGHT FIXTURE, CEILING-MOUNTED (WITH LENGTH AS INDICATED)</p> <p>EMERGENCY LIGHT FIXTURE WITH BATTERY PACK, CEILING OR WALL-MOUNTED</p> <p>EXIT LIGHT FIXTURE WITH BATTERY BACK-UP, CEILING OR WALL-MOUNTED (WITH DIRECTIONAL ARROWS AS REQUIRED)</p>	<p>GENERAL CONDITIONS: AIA DOCUMENT A201, GENERAL CONDITIONS FOR THE PERFORMANCE OF THE CONTRACT IS HEREBY INCORPORATED INTO THESE DRAWINGS AND SHALL BE CONSIDERED AS PART OF THE REQUIREMENTS FOR THE COMPLETION OF THE WORK.</p> <p>EXISTING CONDITIONS: CONDITIONS SHOWN ON THE DRAWINGS ARE AS SHOWN ON THE ORIGINAL DRAWINGS AND AS OBSERVED ON THE SITE, BUT THEIR ACCURACY IS NOT GUARANTEED. THE CONTRACTOR SHALL VERIFY ALL DIMENSIONS AND CONDITIONS AT THE SITE. ANY DISCREPANCIES SHALL BE REPORTED TO ARCHITECT PRIOR TO PROCEEDING WITH THE WORK. NOTE: DIMENSIONS SHALL TAKE PRECEDENCE OVER SCALE OF THE DRAWINGS.</p> <p>PERMITS: THE CONTRACTOR SHALL OBTAIN AND PAY ALL CITY AND/OR COUNTY FEES RELATING TO PROJECT, EXCEPTING THE GENERAL PERMIT, WHICH IS THE RESPONSIBILITY OF THE OWNERS' AND IS REIMBURSABLE TO THE G.C.</p> <p>CODES: ALL WORK SHALL BE DONE IN COMPLIANCE WITH ALL APPLICABLE CODES, INCLUDING BUT NOT LIMITED TO: UNIFORM BUILDING CODES, NATIONAL ELECTRICAL, MECHANICAL, AND PLUMBING CODES, HEALTH DEPARTMENT REGULATIONS, FIRE AND SAFETY CODES, CITY AND/OR COUNTY ORDINANCES AND REGULATIONS AND OTHER CODES GOVERNING CONSTRUCTION.</p> <p>SITE RESPONSIBILITY: IN ACCORDANCE WITH GENERALLY ACCEPTED CONSTRUCTION PRACTICES, THE CONTRACTOR WILL BE SOLELY AND COMPLETELY RESPONSIBLE FOR CONDITIONS ON THE JOB SITE, INCLUDING HEALTH AND SAFETY OF ALL PERSONS AND PROPERTY DURING PERFORMANCE OF THE WORK. CONTRACTOR TO LIMIT TRAFFIC AND ACCESS TO THOSE AREAS WHERE WORK IS PERFORMED.</p> <p>CLEAN UP AND REPAIRS: THE CONSTRUCTION SITE SHALL BE MAINTAINED IN AN ORDERLY MANNER AT ALL TIMES WITH ALL DEBRIS REMOVED AT THE END OF EACH DAY. AT THE COMPLETION OF THE CONSTRUCTION REMOVE ALL EXCESS MATERIALS AND REFUSE FROM SITE. LEAVE ALL SURFACES WITHIN CONSTRUCTION SITE FREE FROM DUST, DIRT AND STAINS. THE CONTRACTOR SHALL REPAIR OR REPLACE ANY SURFACES OR ITEMS DAMAGED BY CONSTRUCTION TO THE SATISFACTION OF THE ARCHITECT AND OWNER.</p> <p>PATCHING: PROPERLY PREPARE SURFACES FOR RECEIVING THE SPECIFIED FINISHES INCLUDING PATCHING OF SURFACES ALTERED BY CONSTRUCTION. ON PATCHED AREAS OR AREAS WHERE A FINISH IS NOT SPECIFIED, THE FINISH SHALL MATCH ADJACENT MATERIAL IN CONSTRUCTION, COLOR AND TEXTURE.</p> <p>ALL WORK NOTED "N.I.C." OR NOT IN CONTRACT IS TO BE PROVIDED BY A CONTRACTOR OTHER THAN THE GENERAL CONTRACTOR.</p> <p>"ALIGN" AS USED IN THESE DOCUMENTS SHALL MEAN TO ACCURATELY LOCATE FINISH FACES ON THE SAME PLANE.</p> <p>"TYPICAL" AS USED IN THESE DOCUMENTS SHALL MEAN THAT THE CONDITION IS THE SAME OR REPRESENTATIVE FOR SIMILAR CONDITIONS THROUGHOUT, U.O.N.</p> <p>DETAILS ARE USUALLY KEYS AND NOTED "TYPICAL" ONLY ONCE, WHEN THEY FIRST OCCUR, AND ARE REPRESENTATIVE FOR SIMILAR CONDITIONS THROUGHOUT, U.O.N.</p> <p>SCHEDULE: UPON SUBMITTAL OF THE FINAL CONSTRUCTION COSTS, THE CONTRACTOR SHALL ALSO SUBMIT A CONSTRUCTION SCHEDULE INDICATING THE REQUIRED CONSTRUCTION TIME FOR ALL SUBCONTRACTOR'S AND CONTRACTOR'S WORK AND A COST-BY-TRADE BREAKDOWN FOR USE IN SCHEDULING AND EVALUATING PAY REQUESTS.</p> <p>SUBSTITUTIONS: SUBSTITUTIONS, REVISIONS, OR CHANGES MUST HAVE APPROVAL BY THE ARCHITECT PRIOR TO PROCEEDING WITH THE WORK.</p> <p>DAMAGE: THE CONTRACTOR SHALL REPAIR OR REPLACE ANY SURFACES OR ITEMS DAMAGED BY CONSTRUCTION TO THE SATISFACTION OF THE ARCHITECT OR OWNER.</p> <p>GUARANTEES: THE CONTRACTOR SHALL GUARANTEE THAT THE PROJECT WILL BE FREE OF DEFECTS OF WORKMANSHIP AND MATERIALS FOR A PERIOD OF ONE YEAR FROM THE DATE OF ACCEPTANCE BY THE OWNER. NO WORK DEFECTIVE IN CONSTRUCTION OR QUALITY DEFICIENT IN ANY REQUIREMENT OF THE DRAWINGS OR NOTES WILL BE ACCEPTABLE IN CONSEQUENCE OF THE OWNER'S OR ARCHITECT'S FAILURE TO POINT OUT DEFECTS OR DEFICIENCIES DURING CONSTRUCTION. DEFECTS OF WORKMANSHIP OR MATERIALS REVEALED WITHIN A PERIOD OF ONE YEAR FROM THE ACCEPTANCE SHALL BE REPLACED BY WORK CONFORMING WITH THE INTENT OF THE CONTRACT AT NO COST TO THE OWNER. NO PAYMENT, EITHER PARTIAL OR FINAL, SHALL BE CONSTRUED AS AN ACCEPTANCE OF DEFECTIVE WORK.</p> <p>COLUMN CENTERLINES (ALSO REFERRED TO AS GRIDLINES) ARE SHOWN FOR DIMENSIONAL PURPOSES. (REFER TO BASE BUILDING DRAWINGS FOR EXACT LOCATIONS).</p> <p>CONSTRUCTION HOURS: VERIFY WITH CITY OF OAKLAND FOR CONSTRUCTION HOURS</p> <p>ANY HIDDEN CONDITIONS THAT REQUIRE WORK TO BE PERFORMED BEYOND THE SCOPE OF THE BUILDING PERMIT ISSUED FOR THESE PLANS MAY REQUIRE FURTHER CITY APPROVALS INCLUDING REVIEW BY THE PLANNING COMMISSION. THE BUILDING OWNER, PROJECT DESIGNER, AND/OR CONTRACTOR MUST SUBMIT A REVISION TO THE CITY FOR ANY WORK NOT GRAPHICALLY ILLUSTRATED ON THE JOB COPY OF THE PLANS PRIOR TO PERFORMING THE WORK.</p> <p>AN OSHA PERMIT TO BE OBTAINED FOR THE SHORING* AT THE EXCAVATION IN THE BASEMENT PER CAL/OSHA REQUIREMENTS. SEE CAL/OSHA HANDBOOK. *CONSTRUCTION SAFETY ORDERS: CHAPTER 4, SUBCHAPTER 4, ARTICLE 6, SECTION 1541.1.</p> <p>GRADING PERMIT, IF REQUIRED, TO BE OBTAINED FROM THE DEPARTMENT OF PUBLIC WORKS.</p>		<p>OWNER RIA2 CAPITAL 2744 EAST 11TH STREET OAKLAND, CA 94601 TEL: 510/982-0634 CONTACT: ALEX WALTER, PROJECT MANAGER</p> <p>ARCHITECT LEVY DESIGN PARTNERS 90 SOUTH PARK SAN FRANCISCO, CA 94107 TEL: 415/777-0561 CONTACT: TOBY LEVY</p> <p>LANDSCAPE ARCHITECT CFLA 74 DUDLEY AVE PIEDMONT, CA 94611 TEL: 510/601-8022 CONTACT: CHRIS FORD</p> <p>CIVIL ENGINEER LUK & ASSOCIATES 738 ALFRED NOBEL DRIVE HERCULES, CA 94547 TEL: 510/724-3388 CONTACT: JACKIE LUK</p>																																																																																																																																																																																																																																																																																																																																																																																										
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APPLY ONLY THE PORTION OF THE STANDARD THAT IS APPLICABLE TO THE CODE SECTION WHERE STANDARD IS REFERENCED, NOT THE ENTIRE SECTION.</p> <p>APPLICABLE CODES</p> <ul style="list-style-type: none"> 2019 CALIFORNIA BUILDING STANDARDS ADMINISTRATIVE CODE, PART 1, TITLE 24, CCR 2019 CALIFORNIA BUILDING CODE, PART 2, TITLE 24, CCR 2019 CALIFORNIA ELECTRICAL CODE, PART 3, TITLE 24, CCR 2019 CALIFORNIA MECHANICAL CODE, PART 4, TITLE 24, CCR 2019 CALIFORNIA PLUMBING CODE, PART 5, TITLE 24, CCR 2019 CALIFORNIA ENERGY CODE, PART 6, TITLE 24, CCR 2019 SAFETY CODE FOR ELEVATORS AND ESCALATORS (ASME A17.1-2010) 2019 CALIFORNIA HISTORICAL BUILDING CODE, PART 8, TITLE 24, CCR 2019 CALIFORNIA FIRE CODE, PART 9, TITLE 24, CCR 2019 CALIFORNIA EXISTING BUILDING CODE, PART 10, TITLES 24 CCR 2019 CALIFORNIA "GREEN" BUILDING REQUIREMENTS, PART 11, TITLE 24 CCR 2019 CALIFORNIA REFERENCED STANDARDS, PART 12, TITLE 24 CCR TITLE 8 CCR CH. 4 SUB-CH. 6 - ELEVATOR SAFETY ORDERS TITLE 19 CCR, PUBLIC SAFETY, STATE FIRE MARSHAL REGULATIONS THIS DESIGN IS IN COMPLIANCE WITH THE FAIR HOUSING ACT DESIGN REFERENCE MANUAL 2017 CALIFORNIA CODE OF REGULATIONS TITLE 8 ELEVATOR SAFETY ORDERS UNIFORM FEDERAL ACCESSIBILITY STANDARDS INCLUDING ANY AMENDMENTS AS ADOPTED IN ORDINANCE 1856-2010 AS WELL AS ANY OTHER APPLICABLE LOCAL AND STATE LAWS AND REGULATIONS <p>APPLICABLE STANDARDS</p> <ul style="list-style-type: none"> NFPA 10 STANDARD FOR PORTABLE FIRE EXTINGUISHERS, 2013 EDITION NFPA 13 AUTOMATIC SPRINKLER SYSTEMS, 2019 EDITION NFPA 14 STANDPIPE SYSTEMS, 2019 EDITION NFPA 17 DRY CHEMICAL EXTINGUISHING SYSTEMS, 2017 EDITION NFPA 17a WET CHEMICAL SYSTEMS, 2017 EDITION NFPA 20 STATIONARY PUMPS, 2019 EDITION NFPA 24 PRIVATE FIRE MAINS, 2019 EDITION NFPA 72 NATIONAL FIRE ALARM CODE, 2019 EDITION NFPA 253 CRITICAL RADIANT FLUX OF FLOOR COVERING SYSTEMS, 2015 EDITION NFPA 2001 CLEAN AGENT FIRE EXTINGUISHING SYSTEMS, 2015 EDITION ASME 17.1 ELEVATOR STANDARD, 2013 EDITION ASME/ANSI A18.1 SAFETY STANDARD FOR PLATFORM LIFTS AND STAIRWAY CHAIR LIFTS ADA STANDARDS FOR ACCESSIBLE DESIGN: ACCESSIBILITY GUIDELINES FOR BUILDINGS AND FACILITIES (ADAAG), (28 CFR PART 36, APPENDIX A) 		
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M2. Efficient Clothes Washing and Drying	1																																																																																																																																																																																																																																																																																																																																																																																													
M3. Energy Star Dryer	1																																																																																																																																																																																																																																																																																																																																																																																													
M4. Lighting Efficiency	2																																																																																																																																																																																																																																																																																																																																																																																													
M5. High-Efficiency Lighting	2																																																																																																																																																																																																																																																																																																																																																																																													
N. COMMUNITY																																																																																																																																																																																																																																																																																																																																																																																														
N1. Smart Development	2																																																																																																																																																																																																																																																																																																																																																																																													
N2. N1.1 Infill Site	1																																																																																																																																																																																																																																																																																																																																																																																													
N3. N1.2 Conserve Resources by Increasing Density	1																																																																																																																																																																																																																																																																																																																																																																																													
N4. Home(s)/Development Located Near Transit	1																																																																																																																																																																																																																																																																																																																																																																																													
N5. N1.3 Within 1/4 Mile of a Major Transit Stop	1																																																																																																																																																																																																																																																																																																																																																																																													
N6. Pedestrian and Bicycle Access	1																																																																																																																																																																																																																																																																																																																																																																																													
N7. N3.7 Reduced Parking Capacity	1																																																																																																																																																																																																																																																																																																																																																																																													
O. OTHER																																																																																																																																																																																																																																																																																																																																																																																														
O1. GreenPoint Rated Checklist in Blueprints	1																																																																																																																																																																																																																																																																																																																																																																																													
O2. Pre-Construction Kick-off Meeting with Rater and Subcontractors	1																																																																																																																																																																																																																																																																																																																																																																																													
O3. Green Appraisal Addendum	1																																																																																																																																																																																																																																																																																																																																																																																													
Summary	37.5	46	110.5	70	66	54																																																																																																																																																																																																																																																																																																																																																																																								
Total Available Points in Specific Categories	37.5	46	110.5	70	66	54																																																																																																																																																																																																																																																																																																																																																																																								
Minimum Points Required in Specific Categories	33	4	7.5	6	7	8.5																																																																																																																																																																																																																																																																																																																																																																																								
<h3>DRAWING INDEX</h3> <p>GENERAL</p> <p>A0.0 COVER SHEET</p> <p>A0.1 SITE CONTEXT</p> <p>A0.2A EGRESS & ACCESS PLANS</p> <p>A0.2B EGRESS & ACCESS PLANS</p> <p>A0.3 OPEN SPACE CALCULATIONS</p> <p>CIVIL</p> <p>SHEET 1 LAND TITLE SURVEY PLAN</p> <p>SHEET 2 LAND TITLE SURVEY PLAN</p> <p>C-4 PRELIMINARY GRADING PLAN</p> <p>C-5 PRELIMINARY UTILITY PLAN</p> <p>C-6 PRELIMINARY STORMWATER CONTROL PLAN</p> <p>LANDSCAPE</p> <p>L1.1 SCHEMATIC LANDSCAPE PLAN</p> <p>L2.1 PLANTING PLAN</p> <p>ARCHITECTURAL</p> <p>A1.0 SITE PLAN: DEMO</p> <p>A1.1 SITE PLAN: NEW</p> <p>A2.1 FLOOR PLAN: 1ST FLOOR</p> <p>A2.2 FLOOR PLAN: 2ND FLOOR</p> <p>A2.3 FLOOR PLAN: 3RD FLOOR</p> <p>A2.4 ROOF PLAN</p> <p>A3.1 ELEVATIONS</p> <p>A3.2 ELEVATIONS</p> <p>A3.3 ELEVATIONS</p> <p>A4.1 SECTIONS</p> <p>A5.1 UNIT PLANS</p>		<h3>SUMMARY / SCOPE OF WORK</h3> <p>1925 BRUSH STREET IS A PROPOSED PRIVATELY FUNDED 18 UNIT, 3-STORY RESIDENTIAL UNIT DEVELOPMENT</p> <p>PLANNING & BUILDING DATA</p> <p>ADDRESS: 1925 BRUSH STREET, OAKLAND, CA 94612</p> <p>PARCEL NUMBER: 003 004700901</p> <p>LOT SIZE: ±25,000 SQ. FT.</p> <p>ZONING DISTRICT: RM-4</p> <p>DENSITY:</p> <p>ALLOWED: 1 UNIT/1,100 SF 25,000 SF / 1,100 SF = 22</p> <p>PROPOSED: 18 UNITS TOTAL: 18 UNITS</p> <p>SETBACKS:</p> <p>REQUIRED: FRONT SETBACKS: 15' SIDE SETBACKS: 4' REAR SETBACKS: 15'</p> <p>PROPOSED: SEE ARCHITECTURAL PLANS (ALL MEET MIN. REQUIREMENTS)</p> <p>HEIGHT LIMIT:</p> <p>REQUIRED: 35' PITCHED ROOF HEIGHT</p> <p>PROPOSED: 35'-0" (WITH CU FOR RM-2)</p> <p>MAXIMUM LOT COVERAGE:</p> <p>ALLOWED: N/A</p> <p>PROPOSED: N/A</p> <p>OPEN SPACE:</p> <p>REQUIRED: 175 SF/UNIT GROUP OPEN SPACE (OR 100 SF/UNIT PRIVATE OPEN SPACE + 75 SF/UNIT GROUP OPEN SPACE)</p> <p>PROPOSED: SEE A0.3 FOR OPEN SPACE CALCULATIONS</p> <p>BIKE PARKING:</p> <p>REQUIRED: 5 (1 LONG TERM SPACE/4 UNITS) 1 (1 SHORT TERM SPACE/20 UNITS)</p> <p>PROPOSED: 5 LONG TERM SPACES & 1 SHORT TERM SPACE</p> <p>VEHICLE PARKING:</p> <p>REQUIRED: 18 SPACES (1 SPACE/UNIT)</p> <p>PROPOSED: 9 SPACES (50% REDUCTION)</p>																																																																																																																																																																																																																																																																																																																																																																																												

1925 Brush Street GPR Rating: Paul Correa, ID 13117	Points Targeted	Community	Energy	Health	Resources	Water
CALGreen	4	1	1	1	1	1
A. SITE						
A1. Job Site Construction Waste Diversion	2					
A2. 10% C&D Waste Diversion (Excluding Alternative Daily Cover)	2					
A3. Stormwater Control: Prescriptive Path	1					
A4. Stormwater Control: Prescriptive Path	1					
A5. Non-Leaching Roofing Materials	1					
C. LANDSCAPE						
C1. Three Inches of Mulch in Planting Beds	1					
C2. Resource Efficient Landscapes	1					
C3. Plants Chosen and Located to Grow to Natural Size	1					
C4. High-Efficiency Irrigation System	1					
E. EXTERIOR						
E1. Durable Roofing Materials	1					
E2. Roofing Membrane for Single Roofing	1					
G. PLUMBING						
G1. Efficient Distribution of Domestic Hot Water	1					
G2. Insulated Hot Water Pipes	1					
G3. Install Water-Efficient Fixtures	2					
G4. WaterSense Showerheads 1.5 gpm with Matching Compensation Valve	1					
G5. WaterSense Bathroom Faucets with 1.0gpm or less	1					
H. HEATING, VENTILATION, AND AIR CONDITIONING						
H1. Sealed Combustion Units	1					
H2. Sealed Combustion Water Heater	1					
H3. Effective Ductwork	1					
H4. H13 Duct Mastic on Duct Joints and Seams	1					
H5. Whole House Mechanical Ventilation Practices to Improve Indoor Air Quality	1					
H6. H1 Meet ASHRAE Standard 62.2-2010 Ventilation Residential Standards	1					
H7. Effective Range Design and Installation	1					
H8. H1 Effective Range Hood Ducting and Design	1					
K. FINISHES						
K1. Low-VOC Coatings and Adhesives	1					
M. APPLIANCES AND LIGHTING						
M1. ENERGY STAR® Dishwasher	1					
M2. Efficient Clothes Washing and Drying	1					
M3. Energy Star Dryer	1					
M4. Lighting Efficiency	2					
M5. High-Efficiency Lighting	2					
N. COMMUNITY						
N1. Smart Development	2					
N2. N1.1 Infill Site	1					
N3. N1.2 Conserve Resources by Increasing Density	1					
N4. Home(s)/Development Located Near Transit	1					
N5. N1.3 Within 1/4 Mile of a Major Transit Stop	1					
N6. Pedestrian and Bicycle Access	1					
N7. N3.7 Reduced Parking Capacity	1					
O. OTHER						
O1. GreenPoint Rated Checklist in Blueprints	1					
O2. Pre-Construction Kick-off Meeting with Rater and Subcontractors	1					
O3. Green Appraisal Addendum	1					
Summary	37.5	46	110.5	70	66	54
Total Available Points in Specific Categories	37.5	46	110.5	70	66	54
Minimum Points Required in Specific Categories	33	4	7.5	6	7	8.5

AREA MAP

PROJECT SITE

TRUE NORTH

PROJECT NORTH

BUILDING DATA

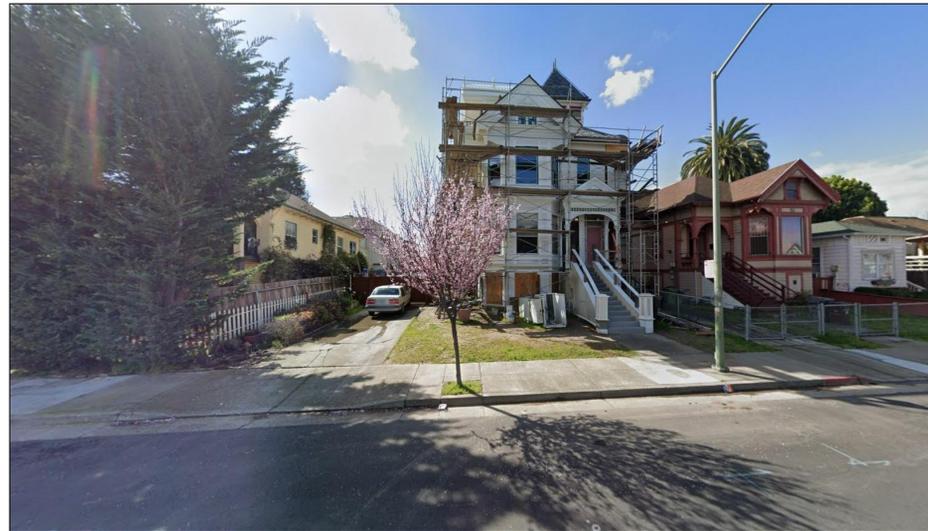
LOCATION	BUILDING 01	BUILDING 02	BUILDING 03
PROPOSED OCCUPANCY	R-2 (RESIDENTIAL)	R-2 (RESIDENTIAL)	R-2 (RESIDENTIAL)



1 1813 BRUSH ST



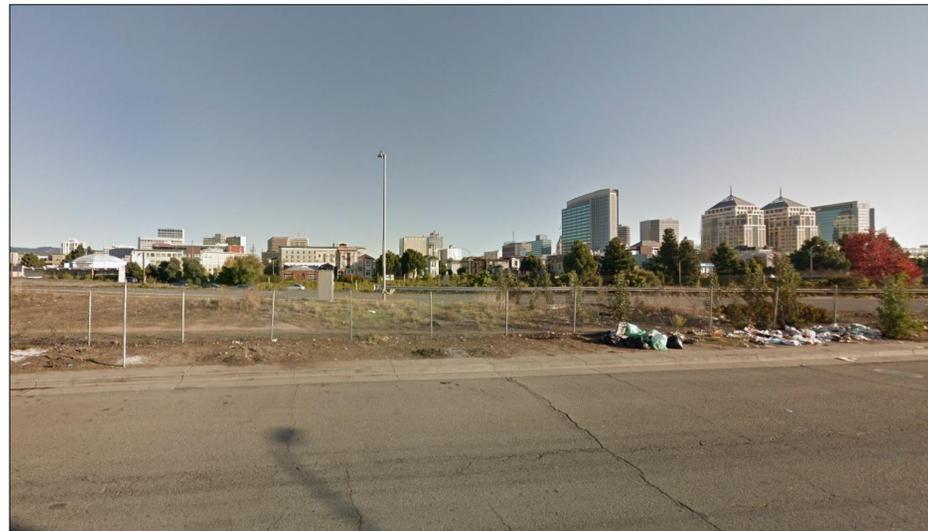
2 2011 BRUSH ST



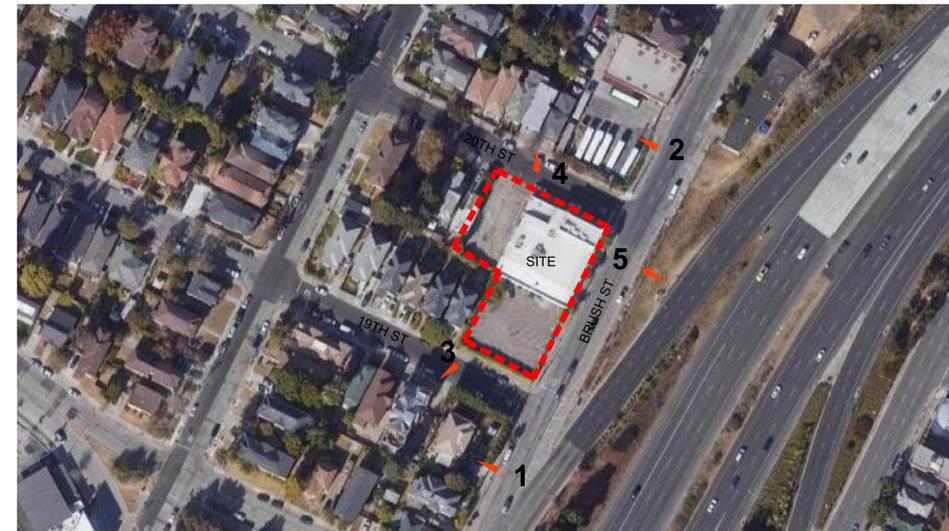
3 769 19TH ST



4 770 20TH ST



5 BRUSH ST



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1925 BRUSH STREET
OAKLAND, CA



1925 BRUSH STREET
OAKLAND, CA

PROJECT NO.

REV	DATE	DESCRIPTION
1	07-31-2020	PLANNING SUBMISSION
2	12-18-2020	PLANNING RE-SUBMISSION

CONTACT: TOBY LEVY

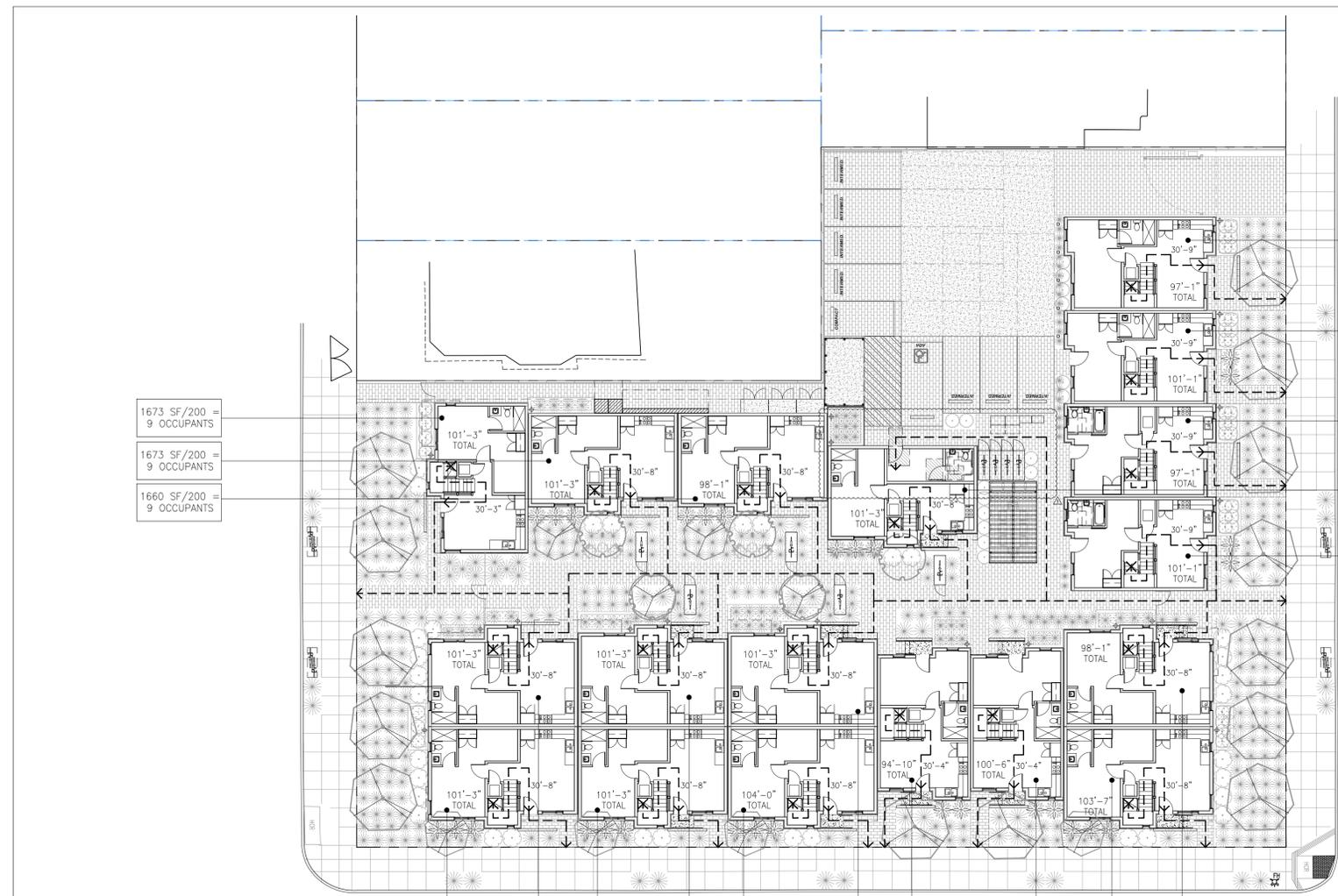
(415) 777-0561 P
(415) 777-5117 F

SCALE: AS NOTED

SITE
CONTEXT

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**1925 BRUSH STREET
OAKLAND, CA**



1673 SF/200 = 9 OCCUPANTS
1673 SF/200 = 9 OCCUPANTS
1660 SF/200 = 9 OCCUPANTS

1566 SF/200 = 8 OCCUPANTS
1630 SF/200 = 9 OCCUPANTS
1566 SF/200 = 8 OCCUPANTS
1673 SF/200 = 9 OCCUPANTS
1630 SF/200 = 9 OCCUPANTS

1673 SF/200 = 9 OCCUPANTS | 1658 SF/200 = 9 OCCUPANTS | 1673 SF/200 = 9 OCCUPANTS | 1673 SF/200 = 9 OCCUPANTS | 1690 SF/200 = 9 OCCUPANTS | 1673 SF/200 = 9 OCCUPANTS | 1566 SF/200 = 8 OCCUPANTS | 1630 SF/200 = 9 OCCUPANTS | 1694 SF/200 = 9 OCCUPANTS | 1656 SF/200 = 9 OCCUPANTS

A GROUND FLOOR

RESIDENTIAL		
OCCUPANCY TYPE/USE	RESIDENTIAL	
OCCUPANT LOAD FACTOR	200	
UNIT #	AREA	OCCUPANT LOAD*
1	1566 SF	8
2	1630 SF	9
3	1566 SF	8
4	1630 SF	9
5	1656 SF	9
6	1694 SF	9
7	1630 SF	9
8	1566 SF	9
9	1690 SF	9
10	1673 SF	9
11	1673 SF	9
12	1673 SF	9
13	1673 SF	9
14	1658 SF	9
15	1673 SF	9
16	1673 SF	9
17	1660 SF	9
18	1673 SF	9
TOTAL		98
EXITS REQUIRED***	2	
EXITS PROVIDED	2	
MIN. EGRESS WIDTH**	19.4"	
MIN. STAIR WIDTH**	29.1"	
EGRESS WIDTH PROVIDED	36"	
STAIR WIDTH PROVIDED	36"	
TRAVEL DISTANCE		
MAX. ALLOWED DISTANCE**	125'	
TRAVEL DISTANCE	SEE PLAN	
*PER CBC TABLE 1004.1.2		
**PER CBC 1005.3		
***PER CBC TABLE 1006.3.1		
- 36" STAIRWAY PER 1009.4		



1925 BRUSH STREET
OAKLAND, CA
PROJECT NO.

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CONTACT: TOBY LEVY

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SCALE: AS NOTED

EGRESS & ACCESS PLANS

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1925 BRUSH STREET
OAKLAND, CA



1925 BRUSH STREET
OAKLAND, CA

PROJECT NO.

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07-31-2020		PLANNING SUBMISSION
12-18-2020		PLANNING RE-SUBMISSION

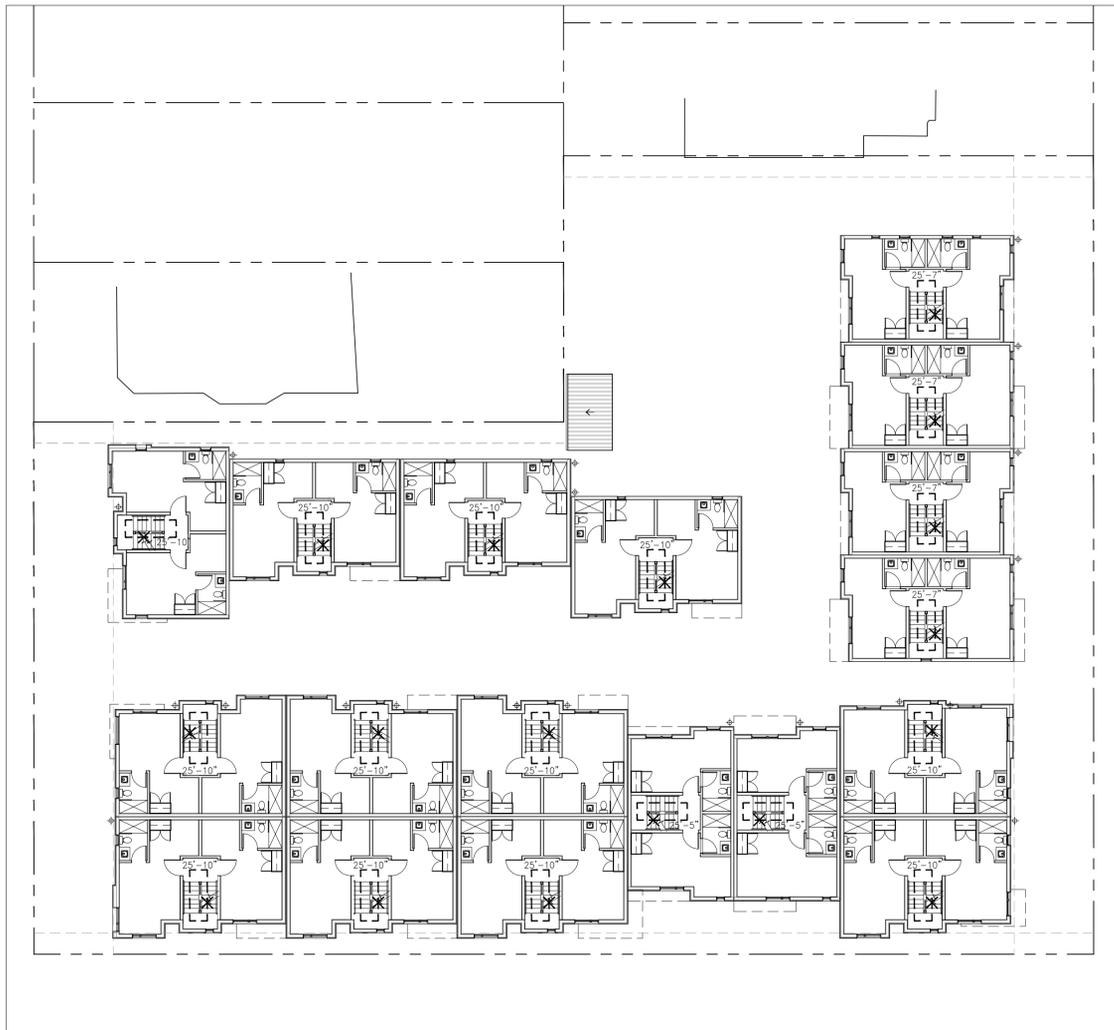
CONTACT: TOBY LEVY

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(415) 777-5117 F

SCALE: AS NOTED

EGRESS &
ACCESS PLANS

A0.2B



1 EGRESS PLANS: 2ND FLOOR
1/16"=1'-0"



2 EGRESS PLANS: 3RD FLOOR
1/16"=1'-0"

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**1925 BRUSH STREET
OAKLAND, CA**



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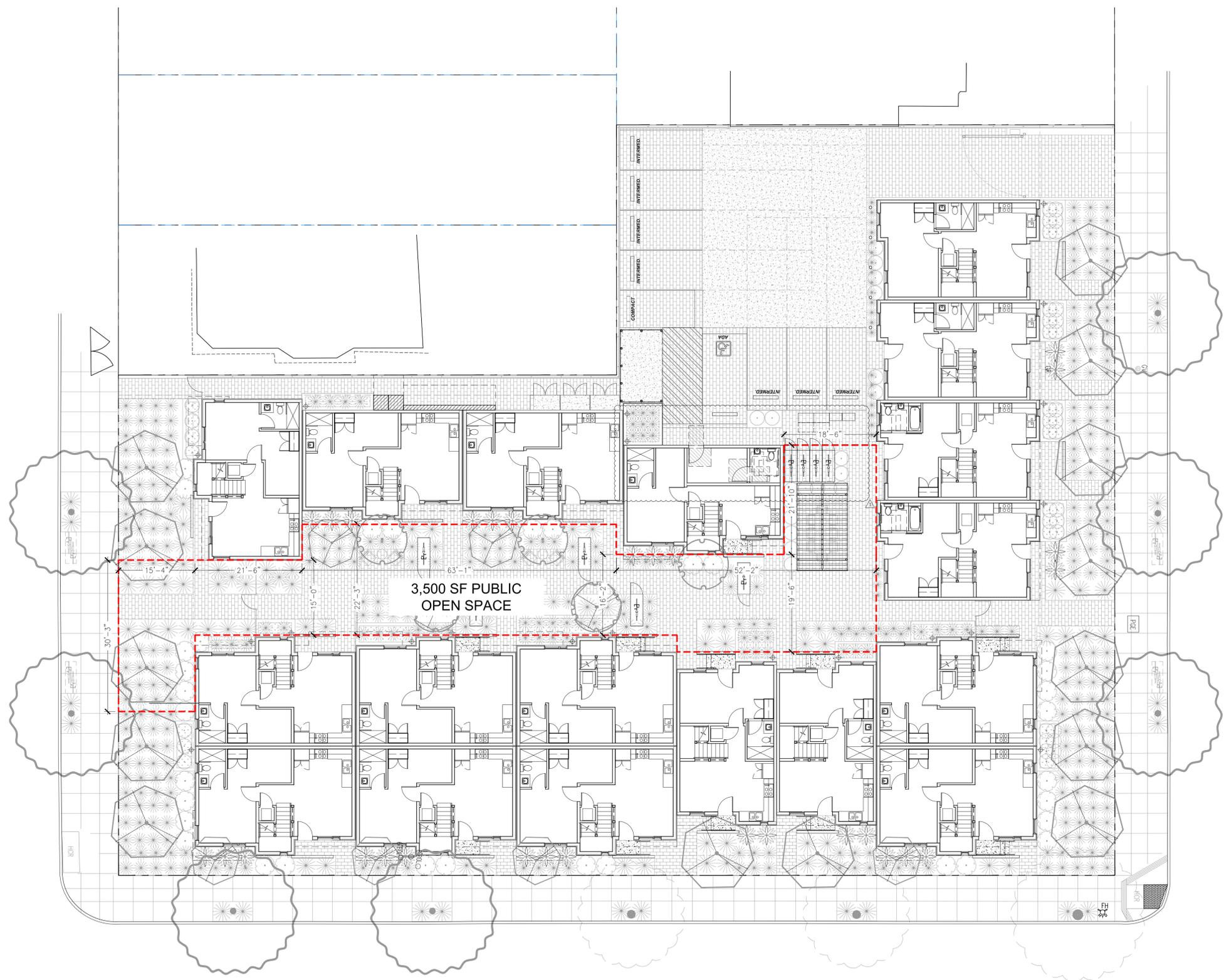
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SCALE: AS NOTED

OPEN SPACE CALCULATIONS

A0.3



OPEN SPACE REQUIREMENTS	
175 SF/UNIT IF PUBLIC OPEN SPACE	EACH PUBLIC OPEN SPACE SHALL HAVE NO DIMENSION LESS THAN 15 FEET
OR	
100 SF/UNIT IF PRIVATE OPEN SPACE + 75 SF/UNIT PUBLIC OPEN SPACE	EACH PRIVATE OPEN SPACE SHALL HAVE NO DIMENSION LESS THAN 10 FEET
OPEN SPACE CALCULATIONS	
UNIT #	OPEN SPACE SQ. FT.
1-18	PUBLIC OPEN SPACE 175 SF PER UNIT 175 SF x 18 UNITS = 3,150 SF REQUIRED OPEN SPACE

1 PLAN: GROUND FLOOR
3/32"=1'-0"



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OAKLAND, CA
PROJECT NO.

REV	DATE	DESCRIPTION
07-31-2020		PLANNING SUBMISSION
12-18-2020		PLANNING RE-SUBMISSION

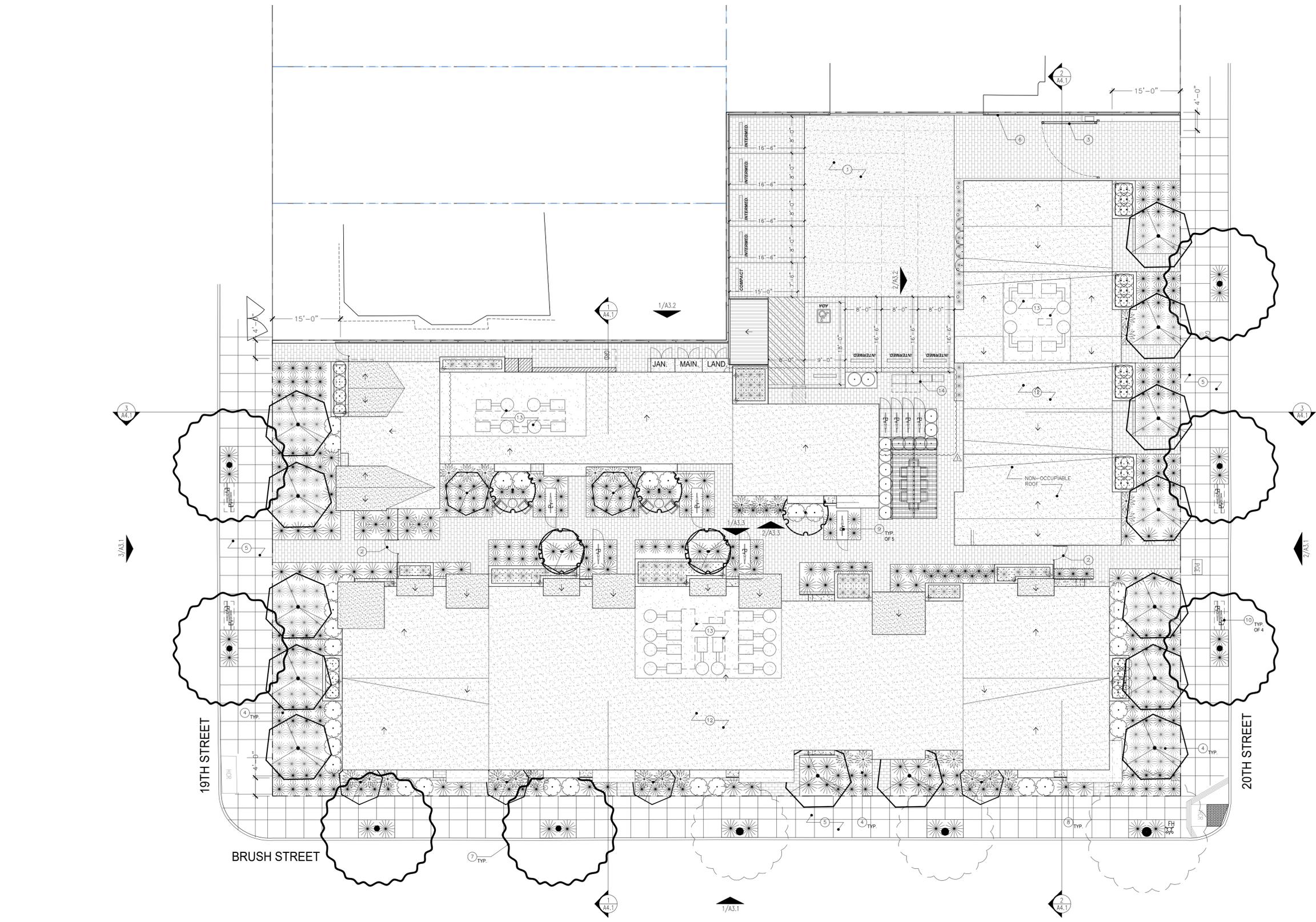
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SCALE: AS NOTED

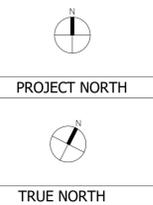
**SITE PLAN:
NEW**

A1.1



1 SITE PLAN: NEW
3/32"=1'-0"

GENERAL NOTES		SHEET NOTES		UNIT BREAK DOWN								
1. UNITS #3 & 4 ARE ACCESSIBLE UNITS. 1.1. GROUND FLOOR KITCHEN TO COMPLY WITH 2019 CBC 1133A 1.2. GROUND FLOOR BATHROOM TO COMPLY WITH 2019 CBC 1134A 1.2.1. MIN. 30"x48" CLR. SPACE IN FRONT OF SINK 1.2.2. MIN. 30"x48" CLR. SPACE AT SIDE OF TUB 1.2.3. MIN. 36"x48" CLR. SPACE IN FRONT OF TOILET	2. GROUND FLOOR DOORS IN ACCESSIBLE UNITS TO COMPLY WITH THE FOLLOWING: INTERIOR UNIT DOORS: 42" PUBLIC DOORS: 60" INTERIOR UNIT DOORS: 18" EXTERIOR PUBLIC DOORS: 24"	1 (N) UNCOVERED SURFACE PARKING 2 (N) PEDESTRIAN GATE 3 (N) VEHICULAR GATE 4 (N) LANDSCAPING, S.L.D. 5 REPLACE (E) SIDEWALK, CURBS & GUTTER 6 (N) PERIMETER FENCING 7 STREET TREE; S.L.D. 8 (N) STOOPS	9 (N) LONG-TERM BIKE PARKING 10 (N) SHORT-TERM BIKE PARKING 11 TRASH ENCLOSURE AREA, AT LEAST TWO (2) CUBIC FEET OF STORAGE AND COLLECTION SPACE PER RESIDENTIAL UNIT, WITH A MINIMUM OF TEN (10) CUBIC FEET 12 NON-OCCUPIABLE ROOF 13 (N) MECHANICAL UNITS 14 (N) MAILBOXES	UNIT #	TYPE	SQ. FT.	UNIT #	TYPE	SQ. FT.	UNIT #	TYPE	SQ. FT.
1	4BD/5BA	1560	7	4BD/5BA	1625	13	4BD/5BA	1670				
2	4BD/5BA	1625	8	4BD/5BA	1660	14	4BD/5BA	1670				
3	4BD/5BA	1560	9	4BD/5BA	1685	15	4BD/5BA	1670				
4	4BD/5BA	1625	10	4BD/5BA	1670	16	4BD/5BA	1670				
5	4BD/5BA	1665	11	4BD/5BA	1670	17	4BD/5BA	1655				
6	4BD/5BA	1670	12	4BD/5BA	1670	18	4BD/5BA	1520				



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1925 BRUSH STREET
OAKLAND, CA



1925 BRUSH STREET
OAKLAND, CA
PROJECT NO.

REV	DATE	DESCRIPTION
07-31-2020		PLANNING SUBMISSION
12-18-2020		PLANNING RE-SUBMISSION

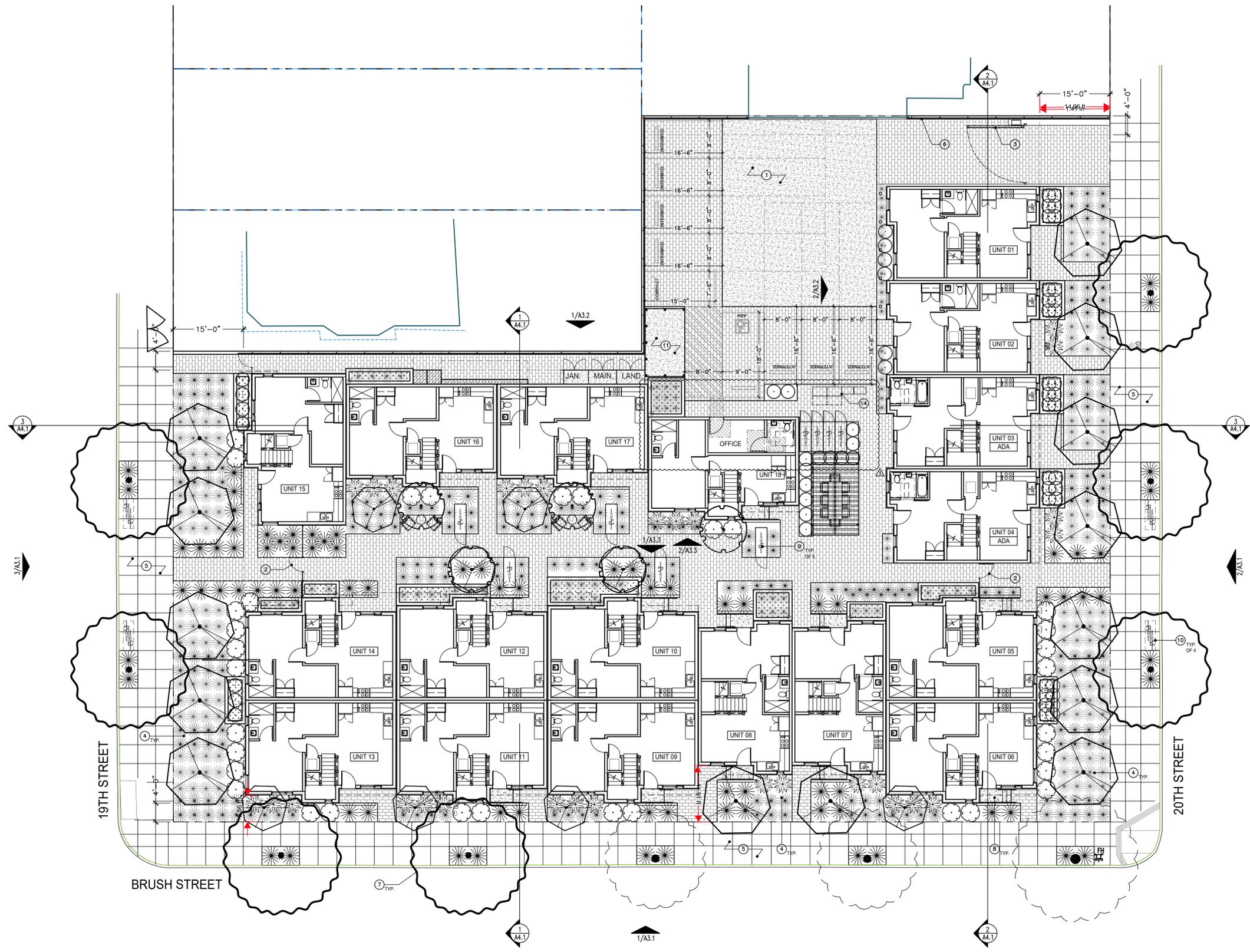
CONTACT: TOBY LEVY

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(415) 777-5117 F

SCALE: AS NOTED

FLOOR PLAN:
GROUND FLOOR

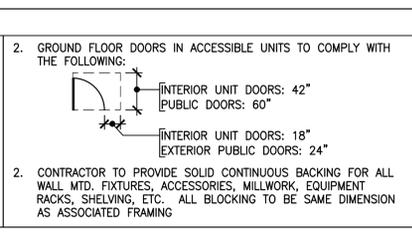
A2.1



1 PLAN: GROUND FLOOR
3/32"=1'-0"

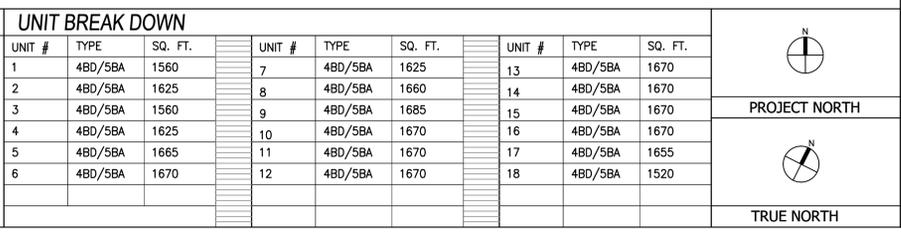
GENERAL NOTES

- UNITS #3 & 4 ARE ACCESSIBLE UNITS.
 - GROUND FLOOR KITCHEN TO COMPLY WITH 2019 CBC 1133A
 - GROUND FLOOR BATHROOM TO COMPLY WITH 2019 CBC 1134A
 - MIN. 30"x48" CLR. SPACE IN FRONT OF SINK
 - MIN. 30"x48" CLR. SPACE AT SIDE OF TUB
 - MIN. 36"x48" CLR. SPACE IN FRONT OF TOILET
- GROUND FLOOR DOORS IN ACCESSIBLE UNITS TO COMPLY WITH THE FOLLOWING:
 - INTERIOR UNIT DOORS: 42"
 - PUBLIC DOORS: 60"
 - INTERIOR UNIT DOORS: 18"
 - EXTERIOR PUBLIC DOORS: 24"
- CONTRACTOR TO PROVIDE SOLID CONTINUOUS BACKING FOR ALL WALL MTD. FIXTURES, ACCESSORIES, MILLWORK, EQUIPMENT RACKS, SHELVING, ETC. ALL BLOCKING TO BE SAME DIMENSION AS ASSOCIATED FRAMING



UNIT BREAK DOWN

UNIT #	TYPE	SQ. FT.	UNIT #	TYPE	SQ. FT.	UNIT #	TYPE	SQ. FT.
1	4BD/5BA	1560	7	4BD/5BA	1625	13	4BD/5BA	1670
2	4BD/5BA	1625	8	4BD/5BA	1660	14	4BD/5BA	1670
3	4BD/5BA	1560	9	4BD/5BA	1685	15	4BD/5BA	1670
4	4BD/5BA	1625	10	4BD/5BA	1670	16	4BD/5BA	1670
5	4BD/5BA	1665	11	4BD/5BA	1670	17	4BD/5BA	1655
6	4BD/5BA	1670	12	4BD/5BA	1670	18	4BD/5BA	1520



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1925 BRUSH STREET
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PROJECT NO.

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12-18-2020		PLANNING RE-SUBMISSION

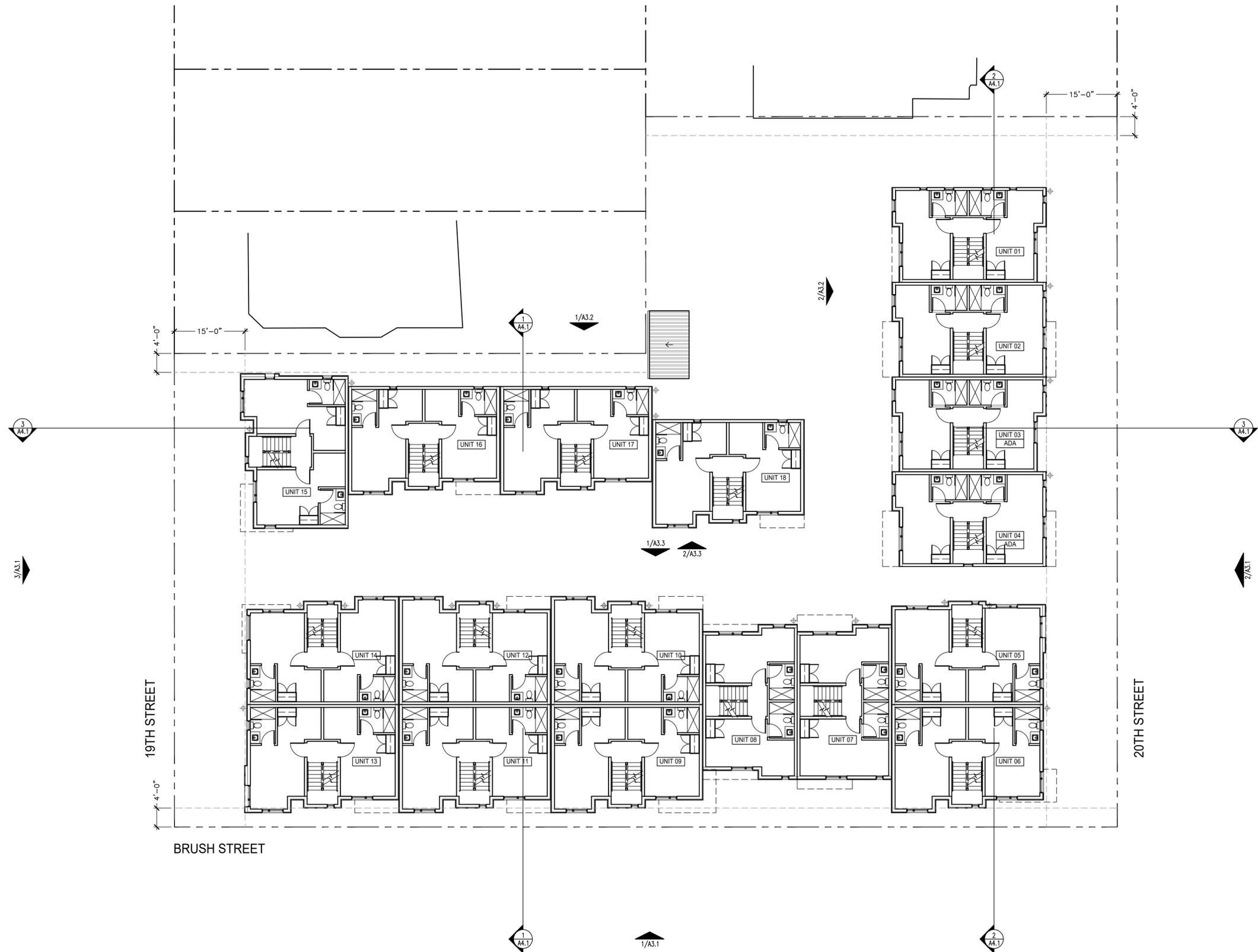
CONTACT: TOBY LEVY

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SCALE: AS NOTED

FLOOR PLAN:
SECOND FLOOR

A2.2



1 PLAN: SECOND FLOOR
3/32"=1'-0"

GENERAL NOTES

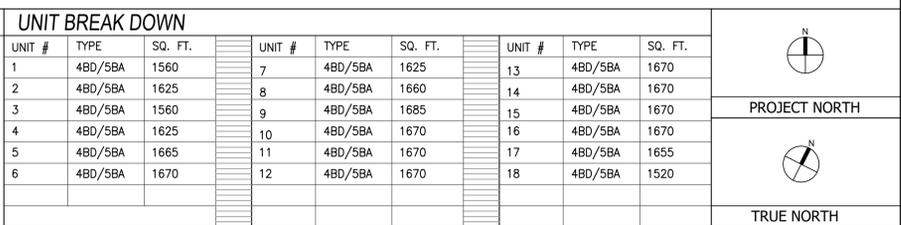
- UNITS #3 & 4 ARE ACCESSIBLE UNITS.
 - GROUND FLOOR KITCHEN TO COMPLY WITH 2019 CBC 1133A
- GROUND FLOOR BATHROOM TO COMPLY WITH 2019 CBC 1134A
 - MIN. 30"x48" CLR. SPACE IN FRONT OF SINK
 - MIN. 30"x48" CLR. SPACE AT SIDE OF TUB
 - MIN. 36"x48" CLR. SPACE IN FRONT OF TOILET

SHEET NOTES

- (N) UNCOVERED SURFACE PARKING
- (N) PEDESTRIAN GATE
- (N) VEHICULAR GATE
- (N) LANDSCAPING, S.L.D.
- REPLACE (E) SIDEWALK, CURBS & GUTTER
- (N) PERIMETER FENCING
- STREET TREE; S.L.D.
- (N) STOOPS
- (N) LONG-TERM BIKE PARKING
- (N) SHORT-TERM BIKE PARKING
- TRASH ENCLOSURE AREA, AT LEAST TWO (2) CUBIC FEET OF STORAGE AND COLLECTION SPACE PER RESIDENTIAL UNIT, WITH A MINIMUM OF TEN (10) CUBIC FEET
- NON-OCCUPIABLE ROOF
- (N) MECHANICAL UNITS
- (N) MAILBOXES

UNIT BREAK DOWN

UNIT #	TYPE	SQ. FT.	UNIT #	TYPE	SQ. FT.	UNIT #	TYPE	SQ. FT.
1	4BD/5BA	1560	7	4BD/5BA	1625	13	4BD/5BA	1670
2	4BD/5BA	1625	8	4BD/5BA	1660	14	4BD/5BA	1670
3	4BD/5BA	1560	9	4BD/5BA	1685	15	4BD/5BA	1670
4	4BD/5BA	1625	10	4BD/5BA	1670	16	4BD/5BA	1670
5	4BD/5BA	1665	11	4BD/5BA	1670	17	4BD/5BA	1655
6	4BD/5BA	1670	12	4BD/5BA	1670	18	4BD/5BA	1520



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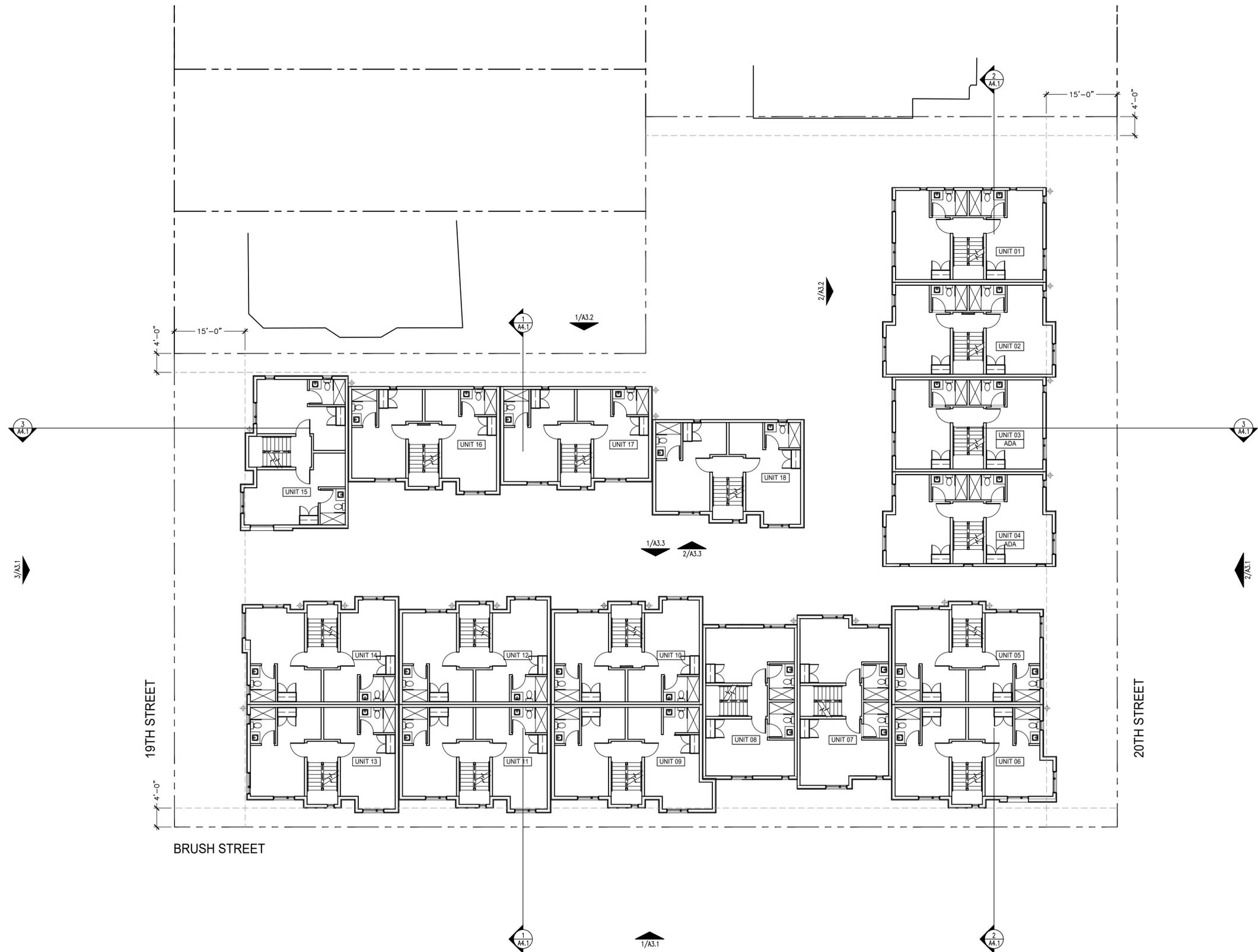
CONTACT: TOBY LEVY

(415) 777-0561 P
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SCALE: AS NOTED

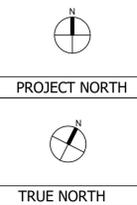
FLOOR PLAN:
THIRD FLOOR

A2.3



1 PLAN: THIRD FLOOR
3/32"=1'-0"

GENERAL NOTES		SHEET NOTES		UNIT BREAK DOWN																																																																		
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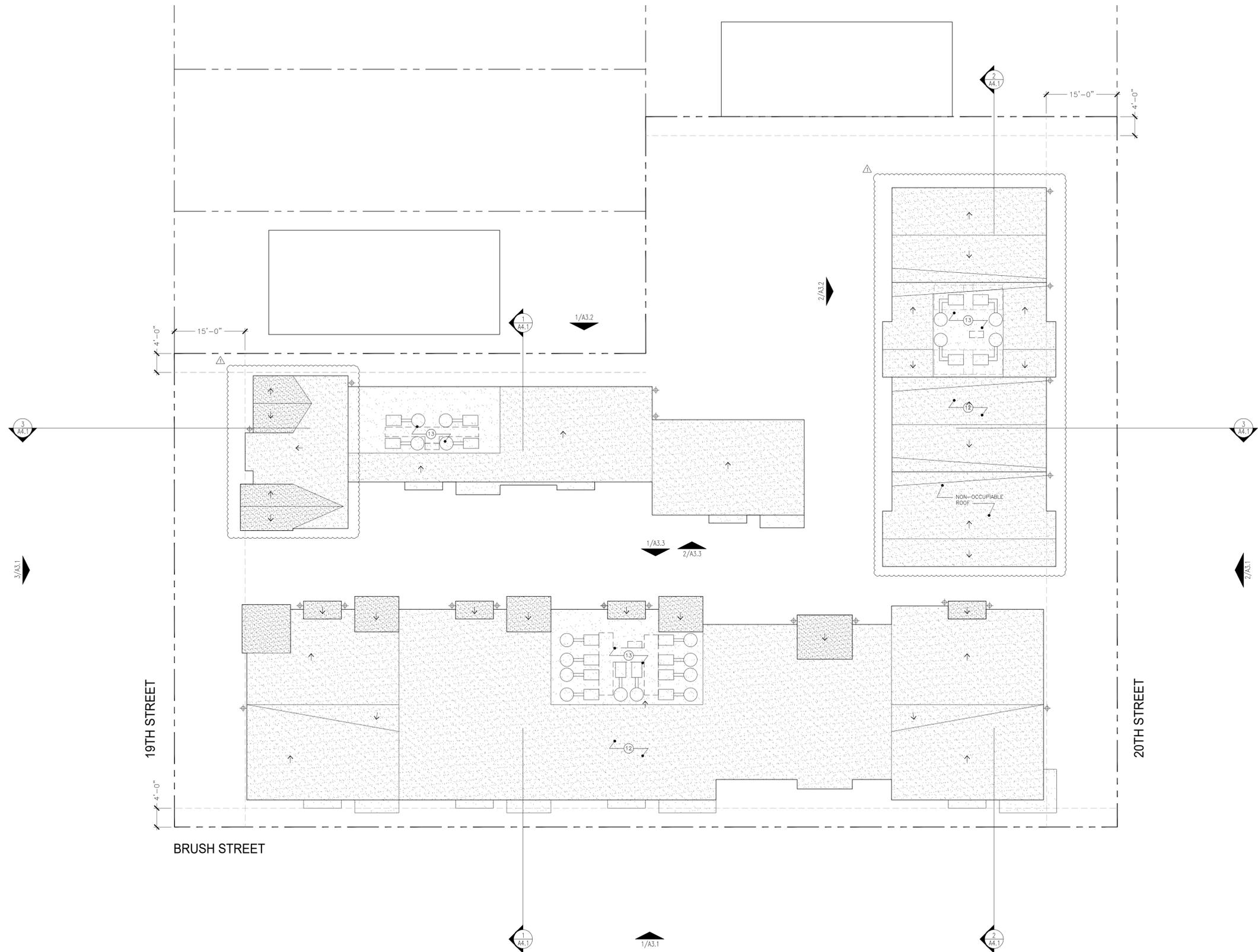
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SCALE: AS NOTED

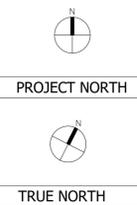
ROOF PLAN

A2.4



1 PLAN: ROOF PLAN
3/32"=1'-0"

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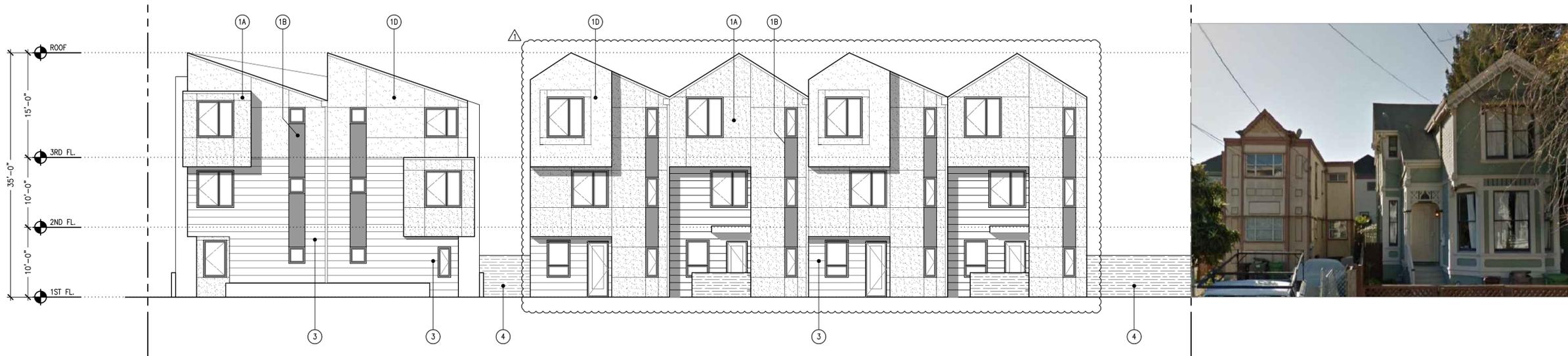
SCALE:
AS NOTED

ELEVATIONS

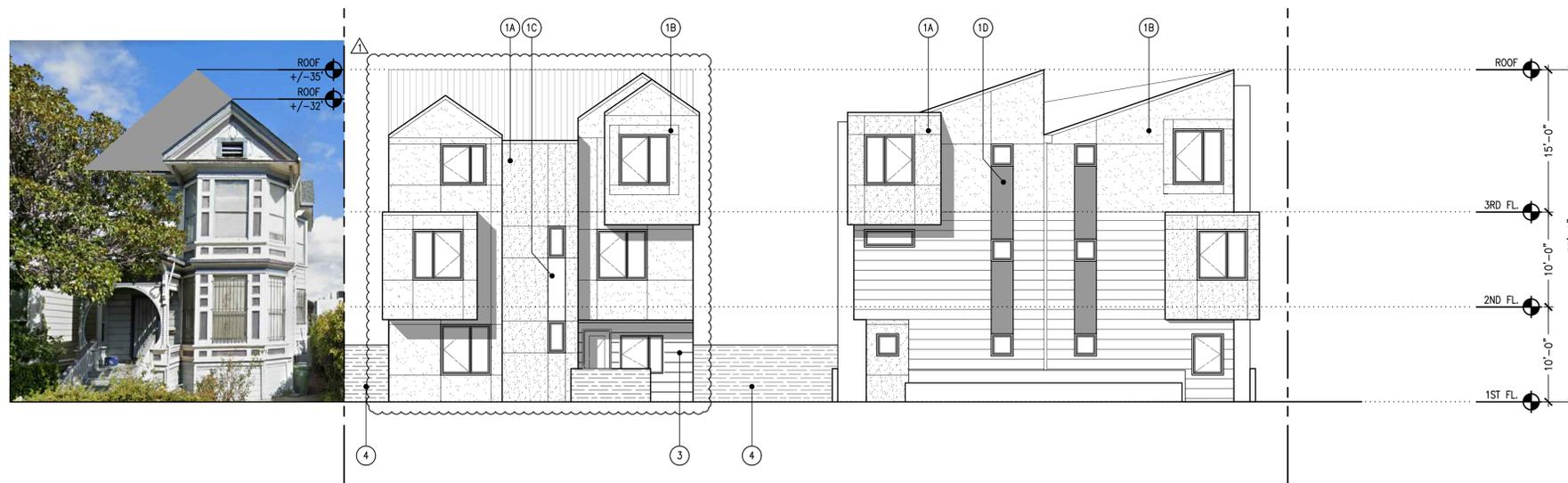
A3.1



1 BRUSH STREET ELEVATION (SOUTH ELEVATION)
1/8"=1'-0"



2 20TH ST ELEVATION (EAST ELEVATION)
1/8"=1'-0"



3 19TH ST ELEVATION (WEST ELEVATION)
1/8"=1'-0"

LEGEND

- ①A CEMENT PLASTER, COLOR #1
- ①B CEMENT PLASTER, COLOR #2
- ①C CEMENT PLASTER, COLOR #3
- ①D CEMENT PLASTER, COLOR #4
- ② NOT USED
- ③ CEMENT PLASTER
- ④ FENCING; S.L.D.
- ⑤ LANDSCAPING; S.L.D.
- ⑥ WOOD SHED
- ⑦ COMPOSITE WOOD SLAT TRASH ENCLOSURE

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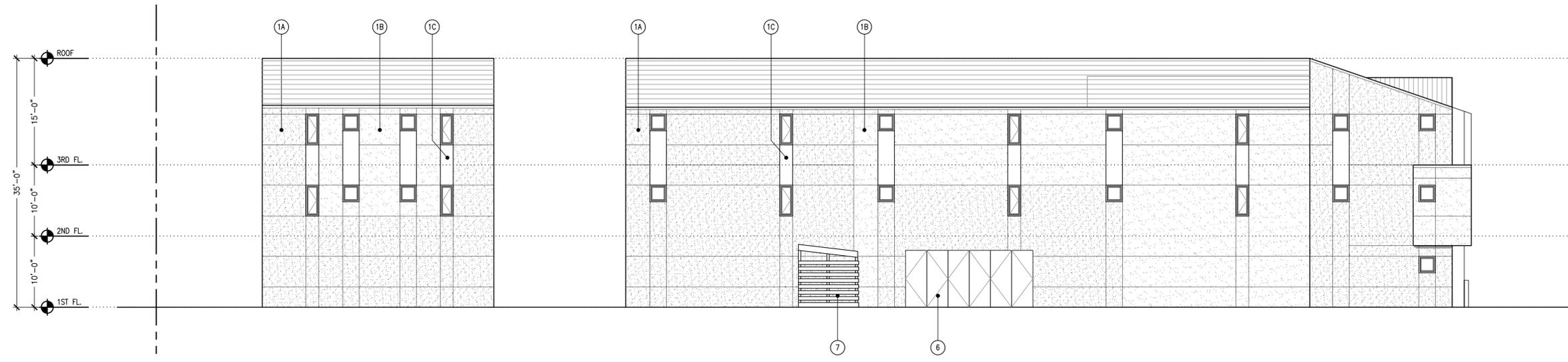
CONTACT: TOBY LEVY

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SCALE: AS NOTED

ELEVATIONS

A3.2



1 NORTH ELEVATION
1/8"=1'-0"



2 COURTYARD ELEVATION
1/8"=1'-0"

LEGEND

- ①A CEMENT PLASTER, COLOR #1
- ①B CEMENT PLASTER, COLOR #2
- ①C CEMENT PLASTER, COLOR #3
- ①D CEMENT PLASTER, COLOR #4
- ② NOT USED
- ③ CEMENT PLASTER
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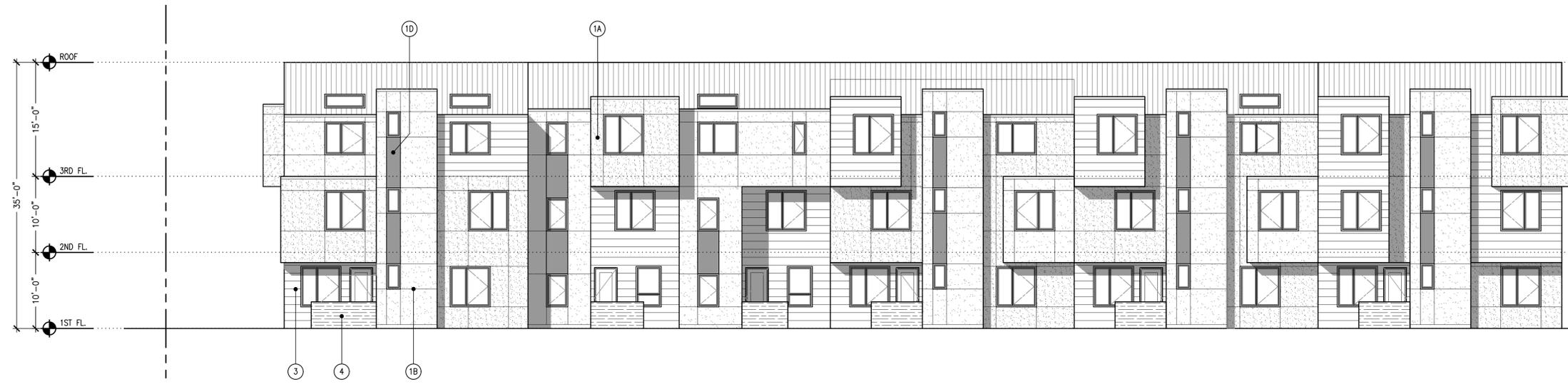
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SCALE: AS NOTED

ELEVATIONS

A3.3



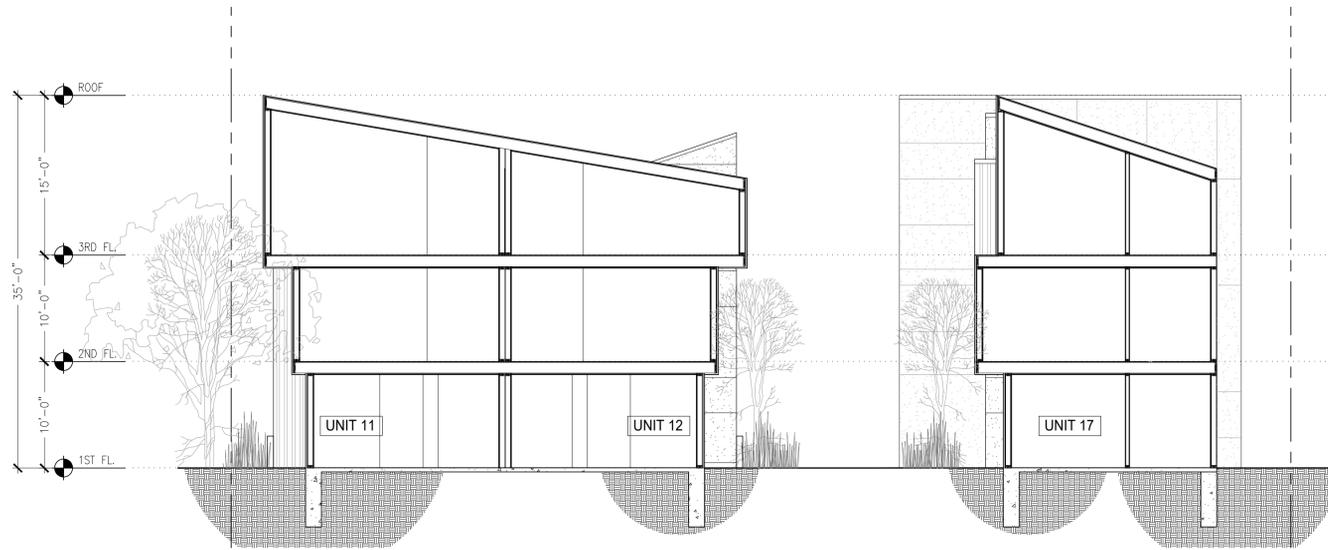
1 COURTYARD ELEVATION
1/8"=1'-0"



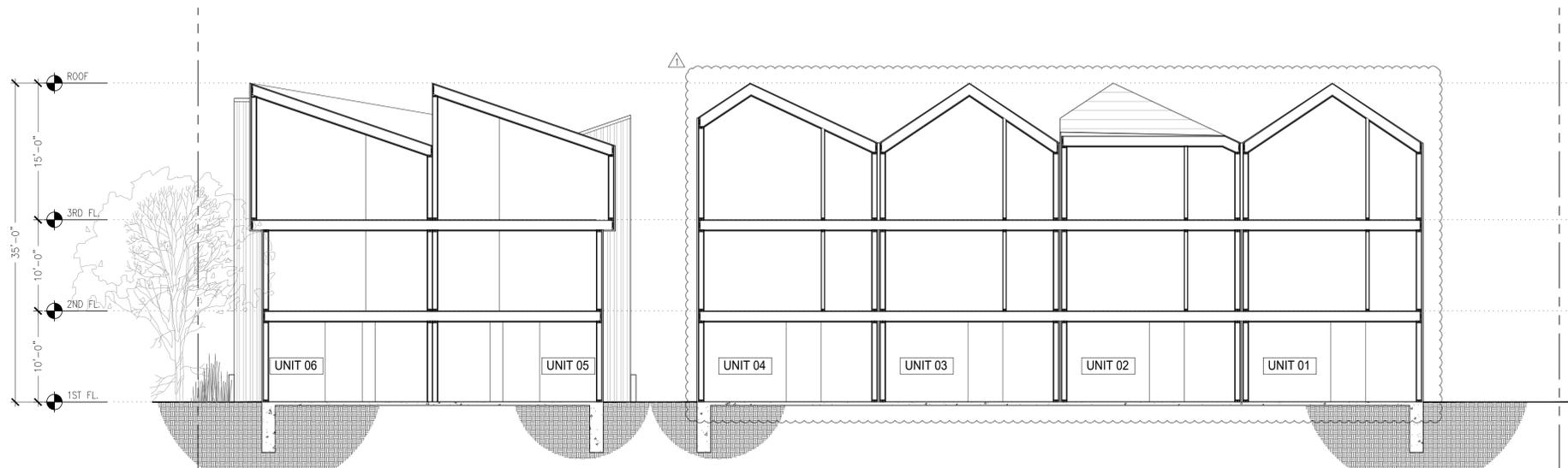
2 COURTYARD ELEVATION
1/8"=1'-0"

LEGEND

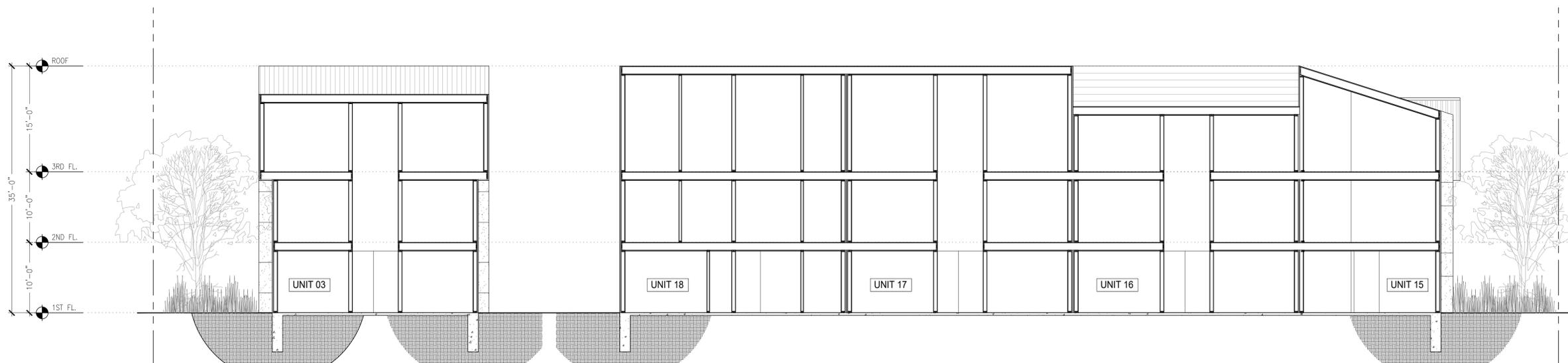
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- ⑥ WOOD SHED
- ⑦ COMPOSITE WOOD SLAT TRASH ENCLOSURE



1 BUILDING SECTION 01
1/8"=1'-0"



2 BUILDING SECTION 02
1/8"=1'-0"



3 BUILDING SECTION 03
1/8"=1'-0"

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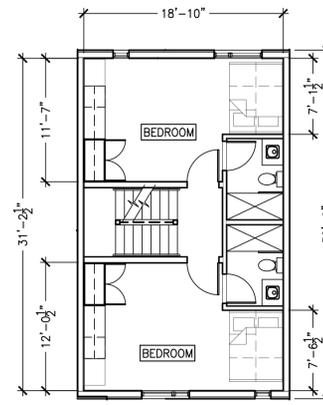
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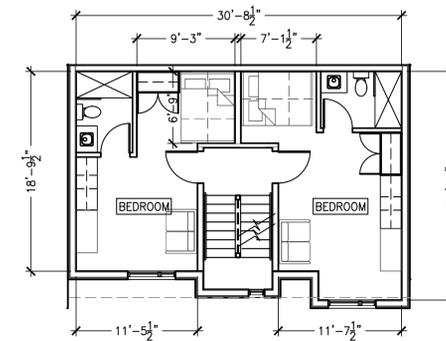
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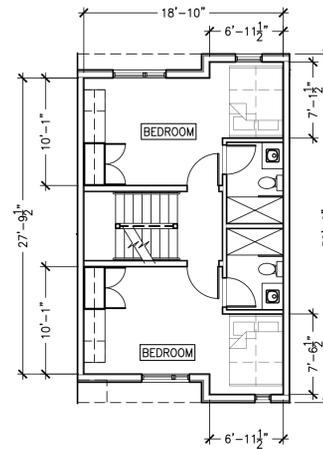
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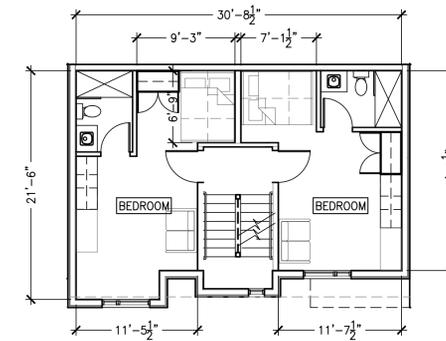
Ⓒ 3RD FLOOR



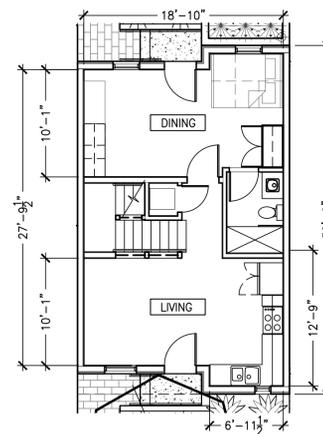
Ⓒ 3RD FLOOR



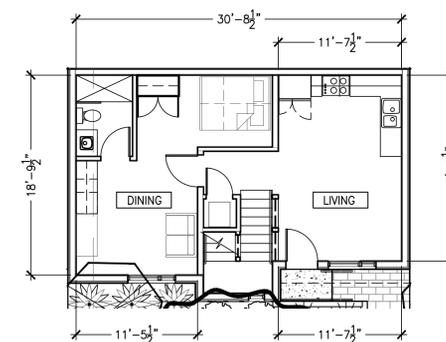
Ⓑ 2ND FLOOR



Ⓑ 2ND FLOOR



Ⓐ 1ST FLOOR



Ⓐ 1ST FLOOR

1 PLAN: UNIT A
1/8"=1'-0"

2 PLAN: UNIT B
1/8"=1'-0"



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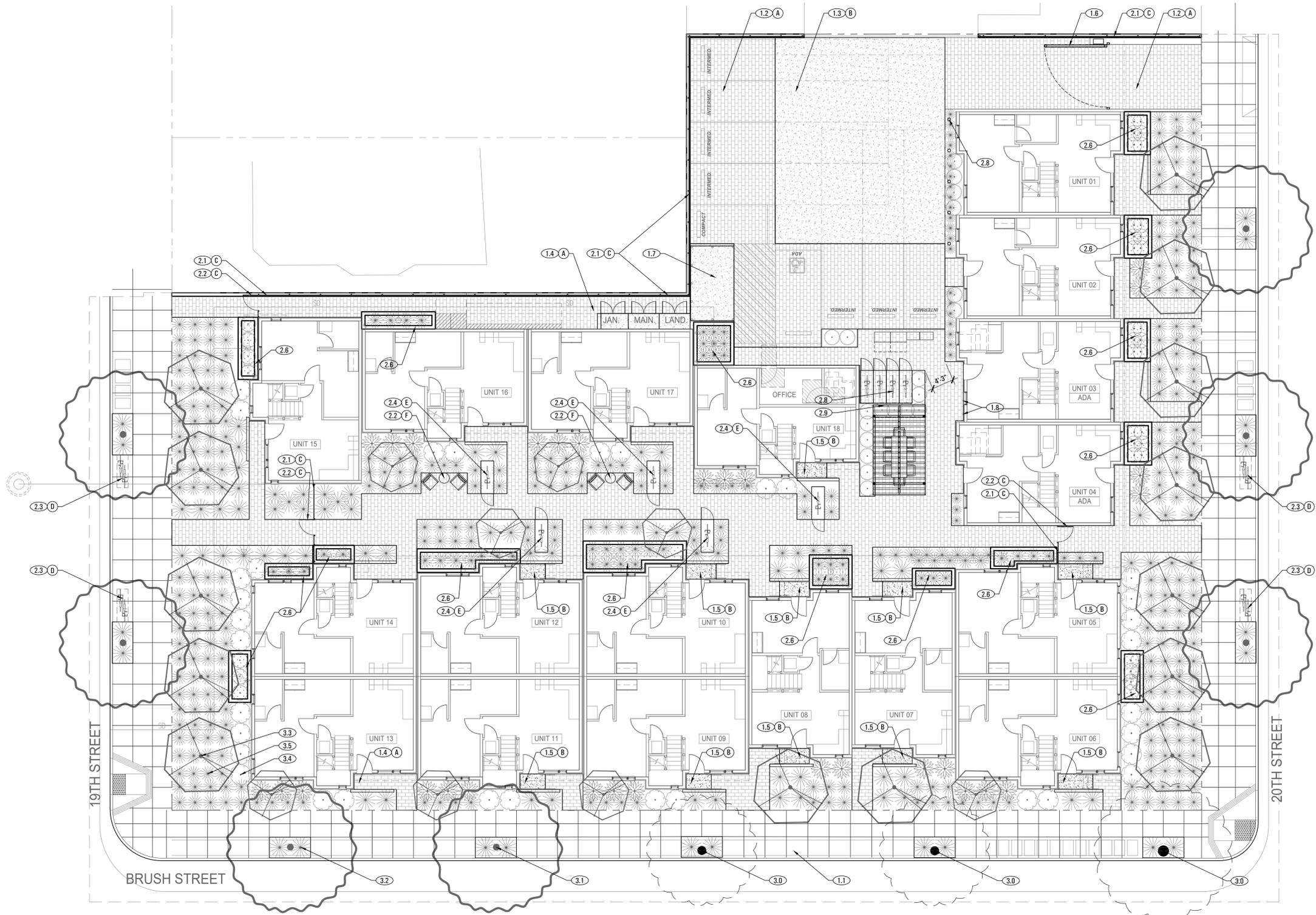
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SCALE:
AS NOTED

UNIT PLANS

A5.1

1925 BRUSH STREET
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LAYOUT LEGEND

- 1.1 CONCRETE PAVING IN RIGHT-OF-WAY. SEE CIVIL ENGINEER'S DRAWINGS.
- 1.2 PERMEABLE PAVERS IN PARKING SPACES. SEE CIVIL ENGINEER'S DRAWINGS.
- 1.3 CONCRETE PAVING IN DRIVE AISLE. SEE CIVIL ENGINEER'S DRAWINGS.
- 1.4 PERMEABLE PAVERS IN WALKWAYS & FRONT DOOR LANDINGS. SEE CIVIL ENGINEER'S DRAWINGS.
- 1.5 CONCRETE PAVING AT FRONT DOOR LANDINGS. SEE CIVIL ENGINEER'S DRAWINGS.
- 1.6 DRIVEWAY GATES. SEE ARCHITECT'S DRAWINGS.
- 1.7 TRASH ENCLOSURE. SEE ARCHITECT'S DRAWINGS.
- 1.8 MAILBOXES. SEE ARCHITECT'S DRAWINGS.

LAYOUT LEGEND

- 2.1 6' HIGH WOOD FENCE.
- 2.2 6' HIGH WOOD GATE.
- 2.3 SHORT-TERM BICYCLE PARKING BIKE RACK.
- 2.4 LONG-TERM BICYCLE PARKING BIKE LOCKER (PLACE ON CONCRETE PAD).
- 2.5 SITE FURNITURE IN GROUP OPEN SPACE.
- 2.6 C.3 STORMWATER TREATMENT PLANTER.
- 2.7 4' HIGH WOOD TRASH SCREEN, SIMILAR TO WOOD FENCE.
- 2.8 BOLLARD LIGHT TYPICAL.

PLANTING LEGEND

- 3.0 EXISTING STREET TREE (JACARANDA SP.) TO BE SAVED & PROTECTED FROM DAMAGE.
- 3.1 STREET TREE TYPICAL IN CITY OF OAKLAND RIGHT-OF-WAY. SEE PLANTING PLAN, SHEET L2.1.
- 3.2 GRASS OR PERENNIAL TYPICAL IN CITY OF OAKLAND RIGHT-OF-WAY. SEE PLANTING PLAN, SHEET L2.1.
- 3.3 SMALL SCALE DECIDUOUS TREE TYPICAL. SEE PLANTING PLAN, SHEET L2.1.
- 3.4 EVERGREEN SHRUB TYPICAL. SEE PLANTING PLAN, SHEET L2.1.
- 3.5 EVERGREEN ORNAMENTAL GRASS OR PERENNIAL TYPICAL. SEE PLANTING PLAN, SHEET L2.1.

MATERIALS LEGEND

- A PERMEABLE PAVERS IN PARKING SPACES & WALKWAYS, AS MFRD BY ACKERSTONE (www.ackerstone.com)
 MODEL: AQUA-VIA1. NOMINAL SIZE: 8cm x 4-3/8 x 8-3/4. COLOR: ANTIQUE PEWTER. FINISH: F.M. PATTERN: RUNNING BOND.
- B INTEGRAL CONCRETE COLOR FOR CONCRETE DRIVE AISLES & FRONT DOOR LANDINGS, AS MFRD BY DAVIS COLORS (800-356-4848). COLOR: PEWTER #860. FINISH: MEDIUM BROOM.
- C 6' HIGH FENCE & GATE
 - FENCE POSTS: 4x4 PTDF, STAIN BLACK
 - FENCE SLATS: 1x6 COMPOSITE, STAIN WITH MESSMERS UV PLUS CLEAR STAIN
 - GATE SLAT: 1x6 COMPOSITE, STAIN WITH MESSMERS UV PLUS CLEAR STAIN
 - GATE HINGES: HEAVY DUTY, BLACK COLOR.
 - GATE HARDWARE, AS SPECIFIED BY ARCHITECT
- D SHORT-TERM BICYCLE PARKING BIKE RACK (2 REQUIRED, 4 OFF-SITE PROVIDED)
 - AS MANUFACTURED BY THE PALMER GROUP, MOD NO WCRO21G P, BLACK.
- E LONG-TERM BICYCLE PARKING BIKE LOCKER (5 REQUIRED, 15 PROVIDED)
 - AS AVAILABLE AT THE PALMER GROUP, MOD NO BTWLO10M, BLACK, (76"W x 30"L x 47"H)
- F SITE FURNITURE (TO BE DETERMINED).
- G BOLLARD LIGHTS (TO BE DETERMINED).



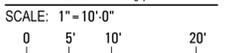
1925 BRUSH STREET
 OAKLAND, CA

DATE	SET ISSUE
07-31-2020	PLANNING SUBMISSION
12-18-2020	PLANNING RE-SUBMISSION

CONTACT: CHRIS FORD



If this print is not 24" x 36" it is a reduced print.
 Scale accordingly.



SCHEMATIC
 LANDSCAPE PLAN
 GROUND FLOOR

ATTACHMENT B: REQUIRED FINDINGS

The following is a list of the findings in the Planning Code required to be met by the Planning Commission to approve the project.

Section 17.136.050.A – Regular Design Review Criteria:

1. That the proposed design will create a building or set of buildings that are well related to the surrounding area in their setting, scale, bulk, height, materials, and textures:
2. That the proposed design will protect, preserve, or enhance desirable neighborhood characteristics;
3. That the proposed design will be sensitive to the topography and landscape.
4. That, if situated on a hill, the design and massing of the proposed building relates to the grade of the hill.

17.100B.050 – Regular Design Review Criteria in the S-20 Zone:

1. That the proposed design conforms in all significant respects with the Oakland General Plan and with any applicable design review guidelines or criteria, district plan, or development control map which have been adopted by the Planning Commission or City Council.
2. That the proposal will not substantially impair the visual, architectural, or historic value of the affected site or facility. Consideration shall be given to design, form, scale, color, materials, texture, lighting, detailing and ornamentation, landscaping, signs, and any other relevant design element or effect, and, where applicable, the relation of the above to the original design of the affected facility.
3. That the proposed development will not substantially impair the visual, architectural, or historic value of the total setting or character of the S-20 Historic Preservation Zone or of neighboring facilities. Consideration shall be given to the desired overall character of any such area or grouping of facilities, including all design elements or effects specified in Subsection (A) above; and
4. That the proposal conforms with the Design Guidelines for Landmarks and Preservation Districts as adopted by the City Planning Commission and, as applicable for certain federally-related projects, with the Secretary of the Interior's Standards for the Treatment of Historic Properties.

17.136.075(C) – Design Review Findings for the Demolition of a Noncontributing Structure in the S-20 Zone

1. For the demolition of noncontributors to an S-7 Zone, S-20 Zone, or API: The existing structure is either: i) seriously deteriorated or a hazard; or ii) the existing design is undistinguished and does not warrant retention. For this finding, a hazard constitutes a threat to health and safety that is not immediate;
2. The design quality of the replacement structure is equal/superior to that of the existing structure; and
3. The design of the replacement project is compatible with the character of the district, and there is no erosion of design quality at the replacement project site and in the surrounding area. This includes, but is not necessarily limited to, the following additional findings:

- a. The replacement project is compatible with the district in terms of massing, siting, rhythm, composition, patterns of openings, quality of material, and intensity of detailing;
- b. New street frontage includes forms that reflect the widths and rhythm of the facades on the street and entrances that reflect the patterns on the street;
- c. The replacement project provides high visual interest that either reflects the level and quality of visual interest of the district contributors or otherwise enhances the visual interest of the district;
- d. If the design contrasts the new to the historic character, the replacement project enriches the historic character of the district;
- e. The replacement project is consistent with the visual cohesiveness of the district. For the purpose of this item, visual cohesiveness is the architectural character, the sum of all visual aspects, features, and materials that defines the district. A new structure contributes to the visual cohesiveness of a district if it relates to the design characteristics of a historic district. New construction may do so by drawing upon some basic building features, such as the way in which a building is located on its site, the manner in which it relates to the street, its basic mass, form, direction or orientation (horizontal vs. vertical), recesses and projections, quality of materials, patterns of openings and level of detailing. When a combination of some of these design variables are arranged in a new building to relate to those seen traditionally in the area, but integral to the design and character of the proposed new construction, visual cohesiveness results; and
- f. The replacement project will not cause the district to lose its current historic status.

Section 17.134.050 – General Use Permit Criteria:

1. That the location, size, design, and operating characteristics of the proposed development will be compatible with and will not adversely affect the livability or appropriate development of abutting properties and the surrounding neighborhood, with consideration to be given to harmony in scale, bulk, coverage, and density; to the availability of civic facilities and utilities; to harmful effect, if any, upon desirable neighborhood character; to the generation of traffic and the capacity of surrounding streets; and to any other relevant impact of the development.
2. That the location, design, and site planning of the proposed development will provide a convenient and functional living, working, shopping, or civic environment, and will be as attractive as the nature of the use and its location and setting warrant.
3. That the proposed development will enhance the successful operation of the surrounding area in its basic community functions, or will provide an essential service to the community or region.
4. That the proposal conforms to all applicable design review criteria set forth in the design review procedure at Section 17.136.070.
5. That the proposal conforms in all significant respects with the Oakland Comprehensive Plan and with any other applicable plan or development control map which has been adopted by the City Council.

Section 17.17.050(A), Table 17.17.03, Note 3 – Conditional Use Permit Findings for Density in the RM Zones.

1. That the proposed development will not adversely affect adjoining property, nor the surrounding neighborhood, with consideration to be given to density; to the availability of neighborhood facilities and play space to the generation of traffic and the capacity of surrounding streets; and to all other similar, relevant factors;

2. That the site design and landscaping and the scale, height, length and width, bulk, coverage, and exterior treatment of structures are in harmony with neighborhood character and with facilities on nearby lots;
3. That the shape and siting of the facilities are such as to minimize blocking of views and direct sunlight from nearby lots and from other Residential Facilities in the surrounding neighborhood;
4. That the design and site planning of the buildings, open areas, parking and service areas, and other facilities provide a convenient, attractive, and functional living environment; and that paths, stairways, accessways, and corridors are designed to minimize privacy impacts;
5. That lot shape, size, and dimensions allow a development which will provide satisfactory internal living conditions without adversely affecting the privacy, safety, or residential amenity of adjacent residences.

17.148.050 – Variance Findings

1. That strict compliance with the specified regulation would result in practical difficulty or unnecessary hardship inconsistent with the purposes of the zoning regulations, due to unique physical or topographic circumstances or conditions of design; or, as an alternative in the case of a minor variance, that such strict compliance would preclude an effective design solution improving livability, operational efficiency, or appearance.
2. That strict compliance with the regulations would deprive the applicant of privileges enjoyed by owners of similarly zoned property; or, as an alternative in the case of a minor variance, that such strict compliance would preclude an effective design solution fulfilling the basic intent of the applicable regulation.
3. That the variance, if granted, will not adversely affect the character, livability, or appropriate development of abutting properties or the surrounding area, and will not be detrimental to the public welfare or contrary to adopted plans or development policy.
4. That the variance will not constitute a grant of special privilege inconsistent with limitations imposed on similarly zoned properties or inconsistent with the purposes of the zoning regulations.
5. That the elements of the proposal requiring the variance (e.g., elements such as buildings, walls, fences, driveways, garages and carports, etc.) conform with the regular design review criteria set forth in the design review procedure at [Section 17.136.050](#).
6. That the proposal conforms in all significant respects with the Oakland General Plan and with any other applicable guidelines or criteria, district plan, or development control map which have been adopted by the Planning Commission or City Council.